



Carter Moore Scenic Drive
Corridor Analysis,
Recommendations and
Guidelines

COC-901

Final Report

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A. Introduction

The Carter More Drive-FM 3083 (the “Corridor”) Guidelines study was undertaken to define minimum criteria by which properties fronting the Corridor would be developed with regard to a number of specific site development elements. Specifically, the principal site development elements covered by the Corridor Guidelines include setbacks, forest preservation, utility locations, edge landscape, landscape screening, parking lot landscape, parking lot lighting, signage and a trail system. Additionally, the Guidelines identify a number of public sector opportunities within the Corridor including the creation of “Entry Portals”, enhanced signalization, street signage and an array of street furniture elements that can contribute to the aesthetic unification of the Corridor.

The guideline recommendations have been crafted in a manner consistent with those commonly used by Master Planned Communities and several cities in and around the Houston metropolitan area. Careful consideration has been given to both undeveloped and developed properties in order that an unreasonable burden is not imposed on properties within the Corridor.

Key recommendations in the Corridor Guidelines include:

1. To establish a 40’ preservation/landscape setback
2. Establish maximum understory removal for forested tracts.
3. Place all utilities underground within the highway right-of-way and out of the setback.
4. Require the planting of trees from a selected number of tree species to create a unified tree canopy throughout the corridor.
5. Require street edge and buffer landscape for fronting tracts.
6. Limit the number and sizes of facility identification signage.
7. Define materials that must be used for facility identification signage.
8. Require the use of a Corridor logo on all signage.
9. Require that all parking lot lighting use metal halide lamps and a similar fixture type and define maximum light levels and limit glare.
10. Facilitate the construction of a 7’ paved trail system within the Corridor.
11. Require landscaped islands with minimum plant sizes and numbers in parking lots.
12. Establish more specific parking ratios for commercial uses.
13. Permit the use of Urban Land Institute “Shared Parking Analysis” to reduce parking requirements in mixed-use developments.
14. Implement a Corridor identity program including portals, monumentation and street furniture.
15. Implement an enhanced traffic control device system.
16. Implement a unique street signage program.

The implementation of these Guidelines will support the economic development initiatives thus far undertaken and enhance the value of the properties along the Corridor while creating a unique urban character in the City of Conroe.

B. Part 1 Analysis of Corridor Physical and Cultural Elements

1. CORRIDOR ANALYSIS:

The CMD-FM 3083 Corridor has been analyzed in terms of its ownership, land use, topography, vegetation and road system. The analysis led to the segmentation of the district into six component areas within two zones forming subdivisions of the Western Zone and the Eastern Zone as shown on Exhibit 1, "Corridor Boundary Map". These sub-areas were demarcated through corridor landform analysis utilizing both photographic survey and topographic analysis. This identified a series of high and low points that were chosen to form the edges of six geographic Corridor segments.

2. CORRIDOR BOUNDARY DEFINITION & ANALYSIS:

The Carter Moore Drive/ FM 3083 Corridor extends in length from its western boundary at SH 105 easterly to its intersection with Loop 336 and includes the Public Right of Way ("ROW") plus the contiguous area of land extending from each edge of the ROW a distance of 600 feet or to the front face of a "primary structure" constructed within the corridor which ever distance is less. For the purposes of this definition, the front face of a pad site building sited between an anchor store, office building or other large structure, and the Corridor ROW would not constitute a "primary structure". The length of the Corridor is depicted in Exhibit 1.

Exhibit 1: Corridor Boundary Map



The aerial photo illustrates the current conditions along the Corridor. The western section is considerably less developed than the area east of SH 75. However, significant areas of flood plain, and perhaps undevelopable land, currently exist in the east. These flood plain/prone areas represent an opportunity to retain significant stands of forested areas thereby maintaining a highly vegetated character similar, but not as extensive, as the area west of Interstate 45.

It is recommended that reasonable efforts be made by the City and the Landowners to preserve this landscape resource.



The following exhibits summarize SLA's field and mapping observations of the study area's six geographic segments.

Segment 1 SH 105 to White Oak Creek

West Fork Entry looking west.



View looking "up" Carter Moore Drive @ SH 105.



View looking "up" Carter Moore Drive @ SH 105 and west on SH 105.



Walgreens from Carter Moore Drive north of SH 105



View from Longmire Road & Carter More Drive intersection. The White Oak Creek bridge is in the low spot.



Rural estate development south of Carter More Drive & east of Longmire

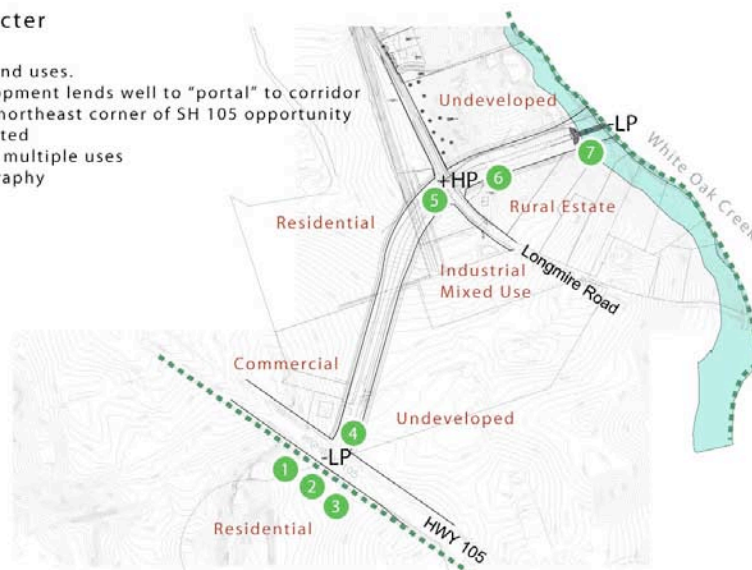


View of pond next to White Oak Creek looking south from the bridge



Visual Character

- Transitional land uses.
- Quality development lends well to "portal" to corridor
- Undeveloped northeast corner of SH 105 opportunity
- Partially Forested
- Developing in multiple uses
- Rolling topography



Segment 2 White Oak Creek to Minor Drainage Swale

West Fork Entry looking west.



View looking "up" Carter Moore Drive @ SH 105.



View looking "up" Carter Moore Drive @ SH 105 and west on SH 105.



Walgreens from Carter Moore Drive north of SH 105



View from Longmire Road & Carter More Drive intersection. The White Oak Creek bridge is in the low spot.



Rural estate development south of Carter More Drive & east of Longmire

View of pond next to White Oak Creek looking south from the bridge

Visual Character

- Quality Residential Development
- Preserved Carter Moore Drive edges well maintained
- Browntoned Stone Monuments
- White Fencing
- Rolling topography
- Heavily forested



Segment 3 Minor Drainage Swale to SH 75

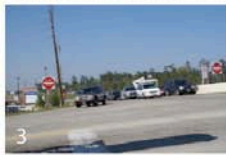
Approaching Center of westside commercial mixed use development.



Teas Crossing multi-tenant pylon sign



North view "up" I-45 frontage road



Looking west from east side of I-45 @ new retail and cemetery



Beginning of un-forested area east of I-45



Undeveloped land in foreground with distribution facility near SH 75



View of Carter Moore Drive and SH 75 looking east towards Gulf State Electric service facility and Exxon station on NW corner.



View of Exxon station at northwest corner of SH75& CMD/3083



Visual Character West

- I-45 intersection bifurcates district
- New Bridge is one city block in length
- Three corners developed as retail/commercial
- Large water element on northeast corner
- Southwest corner forested
- Major City portal approaching from the north
- "Center" of corridor



- Nexus of retail residential and industrial
- Connection to "Lake District" and Airport

Visual Character East

- Sign Blight
- Strong Corporate southeast edge
- Northeast and southwest corners remain potentially positive
- Large areas of non-vegetated undeveloped land need will require landscape standards

Segment 4 SH 75 To Transmission Corridor

Approaching Center of westside commercial mixed use development.



1 North view "up" I-45 frontage road

Looking west from east side of I-45 @ new retail and cemetery

Teas Crossing multi-tenant pylon sign



2



3

Beginning of un-forested area east of I-45



4

View of Bauer Manufacturing site construction with remaining frontage trees



5

View of ATEX sign looking up Pollok Drive



6

View of Stewarts Creek floodplain



7

Industrial use on north side of CMD/3083



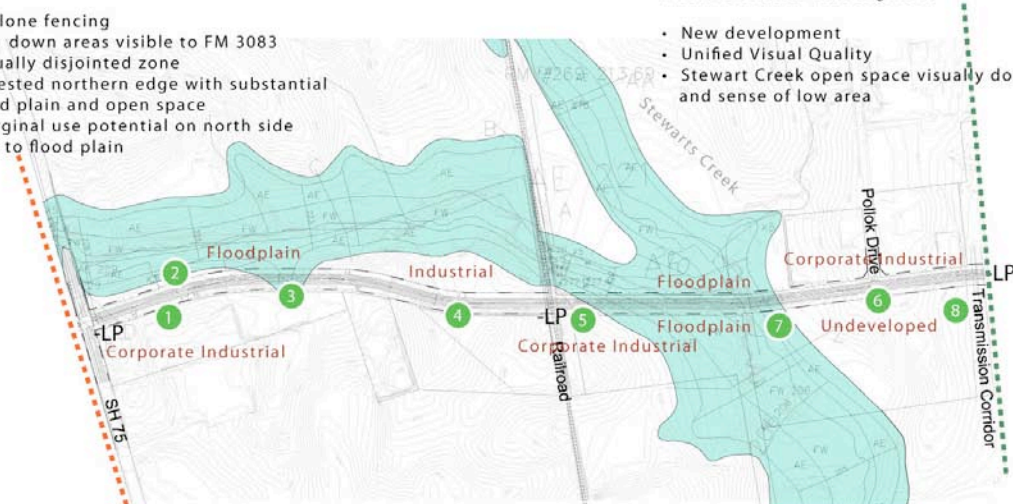
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Visual Character West Segment

- Cyclone fencing
- Lay down areas visible to FM 3083
- Visually disjointed zone
- Forested northern edge with substantial flood plain and open space
- Marginal use potential on north side due to flood plain

Visual Character East Segment

- New development
- Unified Visual Quality
- Stewart Creek open space visually dominant and sense of low area



Segment 5 Transmission Corridor to East of Caney Branch

New development @ TxDot Road



Industrial/distribution with some road buffering



View of Airport Parkway intersection and CMD/3083



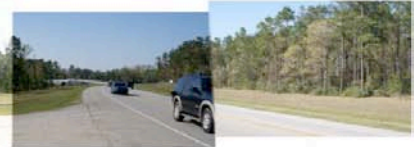
Large distribution facility with marginal dock buffering



Environmental Crossings facility and undeveloped lands on north side of CMD/3083

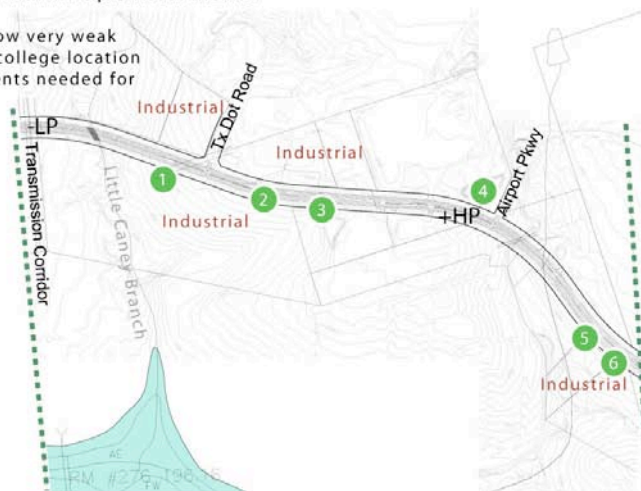


View Approaching North Loop 336 & Airport Road



Visual Character Segment

- Dominantly industrial with multiple visual themes
- Visually Dissonant
- Airport Parkway window very weak
- Potential community college location
- Landscape improvements needed for visual continuity



Segment 6 East of Caney Branch to North Loop 336 East

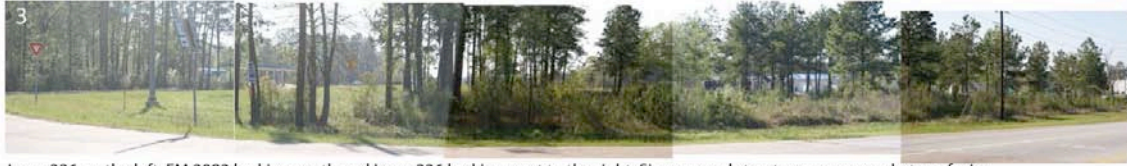
Looking north along Airport Road with the Lone Star Convention Center on the left.



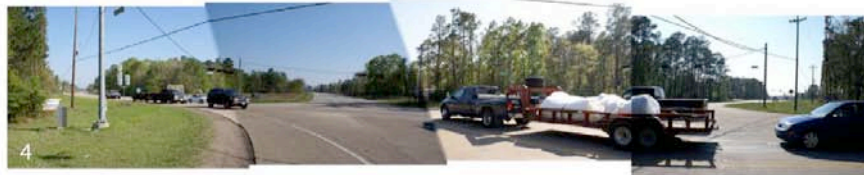
East along CMD/3083 with "triangle" to the right



View of the "triangle". Potentially a strong portal element to the adjoining civic facilities and the Corridor.

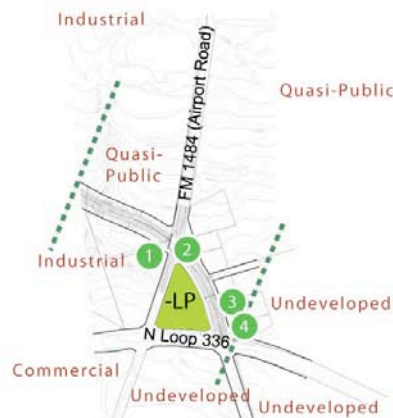


Loop 336 on the left, FM 3083 looking south and Loop 336 looking west to the right. Signage and street names somewhat confusing.



Visual Character West

- Somewhat chaotic partially due to geometry
- Strong civic elements lack visual clarity
- Triangle opportunity as "portal" to corridor and civic uses?
- Weak public and private sector signage adds to chaotic image
- Significant percentage of undeveloped tracts offer integrative future development.



3. CORRIDOR OWNERSHIP MAP:

The land ownership pattern conveys a sense of current visual character within the study area. The western area is largely controlled by eight landowners. Of these eight landowners, three control approximately two thirds of the western frontage much of which is undeveloped at this time. East of Interstate 45, there are over 25 landowners.

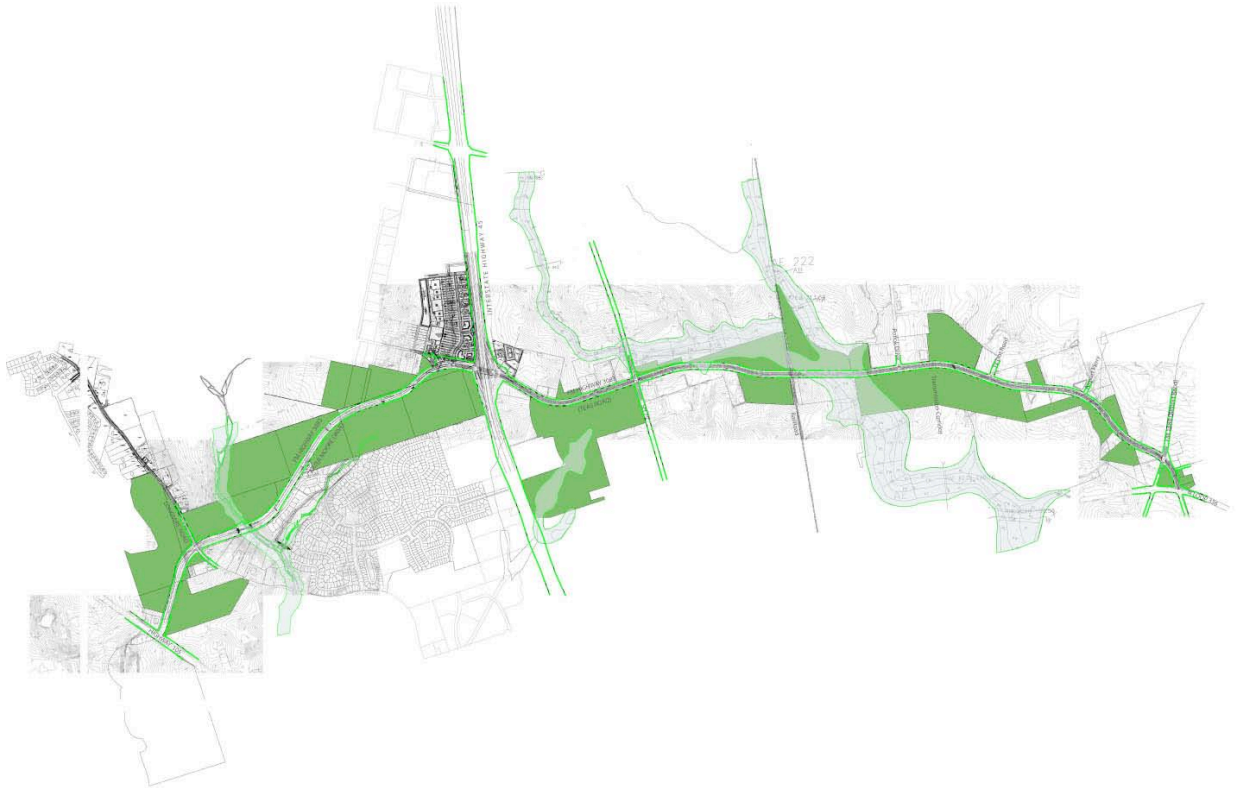


4. UNDEVELOPED LANDS:

The undeveloped lands along the Corridor contribute greatly to the attractiveness of establishing Corridor Landscape Guidelines. The undeveloped areas within the area west of I-45 equal about 84% of the frontage with $\pm 67\%$ of the developed area being Graystone Hills. Because Graystone Hills has reserved and preserved their frontage south of FM 3083, approximately 67% of the $\pm 16\%$ developed area remains forested. The net effect is that the western area appears to be continuously forested for virtually its entire length except for the frontages of the commercial developments at each end.

East of I-45, a combination of deforested open areas and developed lands produce a somewhat different landscape character. With the developed portion equaling approximately 40% of the Corridor frontage and non-forested developed and non-developed areas being located at the “entry” to the eastern area off of I-45, a challenge exists to “link” the east and west through this critical area/segment.

Undeveloped Land Area Map



5. LAND USE:

The Land Use Map below was assembled using a combination of “windshield survey”, photo inventory and owner plan maps. As such, errors no doubt have been made. However, the broad pattern of uses is evident. The west is/will be dominated by residential, retail and office and the east appear to be dominated by industrial, distribution and institutional/public uses. If these patterns continue as expected, the challenge presented to preserve and unify the entire Corridor frontage is significant but not overwhelming.



Part 2 Conceptual Corridor Opportunities

1. CORRIDOR OPPORTUNITIES WESTERN ZONE-FOREST PRESERVATION:

The CMD-FM 3083 Western Zone from SH 105 and extending to SH 75 is the immediate focus of the Corridor study recommendations. With the Western Zone being the least developed of the two study areas, forest preservation is paramount to maintaining the Corridor's visual beauty and integrity. This zone is extensively forested as can be seen from the following diagram.

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Forested Tracts



This combination of heavily forested and undeveloped tracts should be protected through reasonable guidelines sensitive to development needs while at the same time capable of protecting a meaningful visual buffer to the developments that will occur. Differing land uses will want different levels of highway visibility. As a general rule, the more retail a business the more visibility is desired. The guidelines should enable this objective while at the same time providing the forest preservation goals.

2. CORRIDOR OPPORTUNITIES WESTERN ZONE- **SIGNAGE:**

An inventory and review of existing signage within the Corridor and the Western Zone is partially documented below. While seemingly unintended, a significant number of signs within the Corridor are constructed with a brown sedimentary stone base. This stone color is complimentary to the local soils color and as such appears to be “natural” and therefore conveys a sense of appropriateness. In the Western Zone, two prominent projects have used this material with noteworthy effect. Teas Crossing and Graystone Hills have integrated this material palette in a prominent and thoughtful manner. Perhaps as an influence or simply a coincidence, the Conroe entry monument located off SH 105 near the downtown Historic District has used a similar material. This continuity of a regionally appropriate material represents another opportunity for visual and aesthetic Corridor unification.

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CORRIDOR SIGNAGE EXAMPLES

Many of the signs within the study area as well as the City's own entry sign, use a brown sedimentary stone. White accents in stone or metal are also used on numerous signs. Brick can be seen on many of the buildings and most are in a mid-tone color range. The use of the same and/or similar materials and colors can build on the equity thusfar invested and help visually unify the corridor.



3. CORRIDOR OPPORTUNITIES WESTERN ZONE- **OVERALL CONCEPT:**

The integration of the analysis and opportunities forms the basis for the development of the Guidelines. The three segments that comprise the Western Zone provide similar and somewhat overlapping opportunities for achieving Corridor unification and preservation. The plan below depicts the key or critical areas within the Zone.

Those key components by Segment are as follows:

Segment 1

- Develop a corridor portal @ SH105 using monuments and street furniture and brown stone elements
- Implement a signage wayfinding system along 105
- Retrofit planting along the Walgreens frontage
- Implement Guidelines for undeveloped tracts
- Add identifier signage for White Oak Creek crossing
- Enhance traffic control devices using Corridor design theme

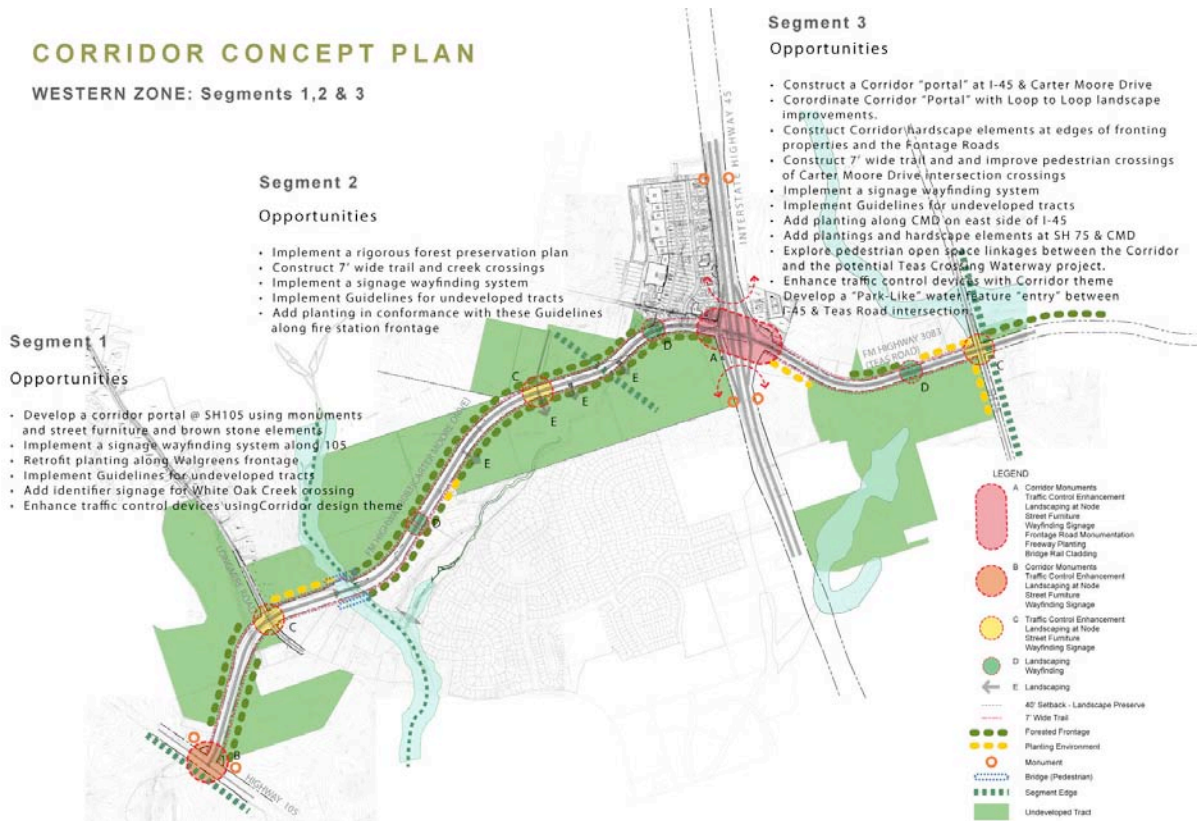
Segment 2

- Implement a rigorous forest preservation plan
- Construct 7' wide trail and creek crossings @ White Oak Creek
- Implement a signage wayfinding system
- Implement Guidelines for undeveloped tracts
- Add planting in conformance with these Guidelines along the fire station frontage.

Segment 3

- Construct a Corridor "Portal" at I-45 & Carter Moore Drive
- Coordinate Corridor "Portal" with Loop to Loop landscape improvements.
- Construct Corridor hardscape elements at edges of fronting Properties and the Frontage Roads
- Construct 7' wide trail and improve pedestrian crossings of Carter Moore Drive intersections
- Implement a signage wayfinding system
- Implement Guidelines for undeveloped tracts
- Add planting along CMD on east side of I-45
- Add plantings and Hardscape Elements at SH 75 & CMD
- Explore a pedestrian open space link between the Corridor and the potential Teas Crossing Waterway project.
- Enhance traffic control devices utilizing elements of the Corridor theme.
- Develop a Park-Like" water feature "Corridor entry" between I-45 & Teas Road intersections.

Through the implementation of these segmental goals and objectives, the Corridor character will be preserved and enhanced. It is believed that many of the principles identified for the Western Zone and be adapted and extended through the Eastern Zone. The study will seek to achieve this in working with the Eastern Zone landowners and stakeholders.

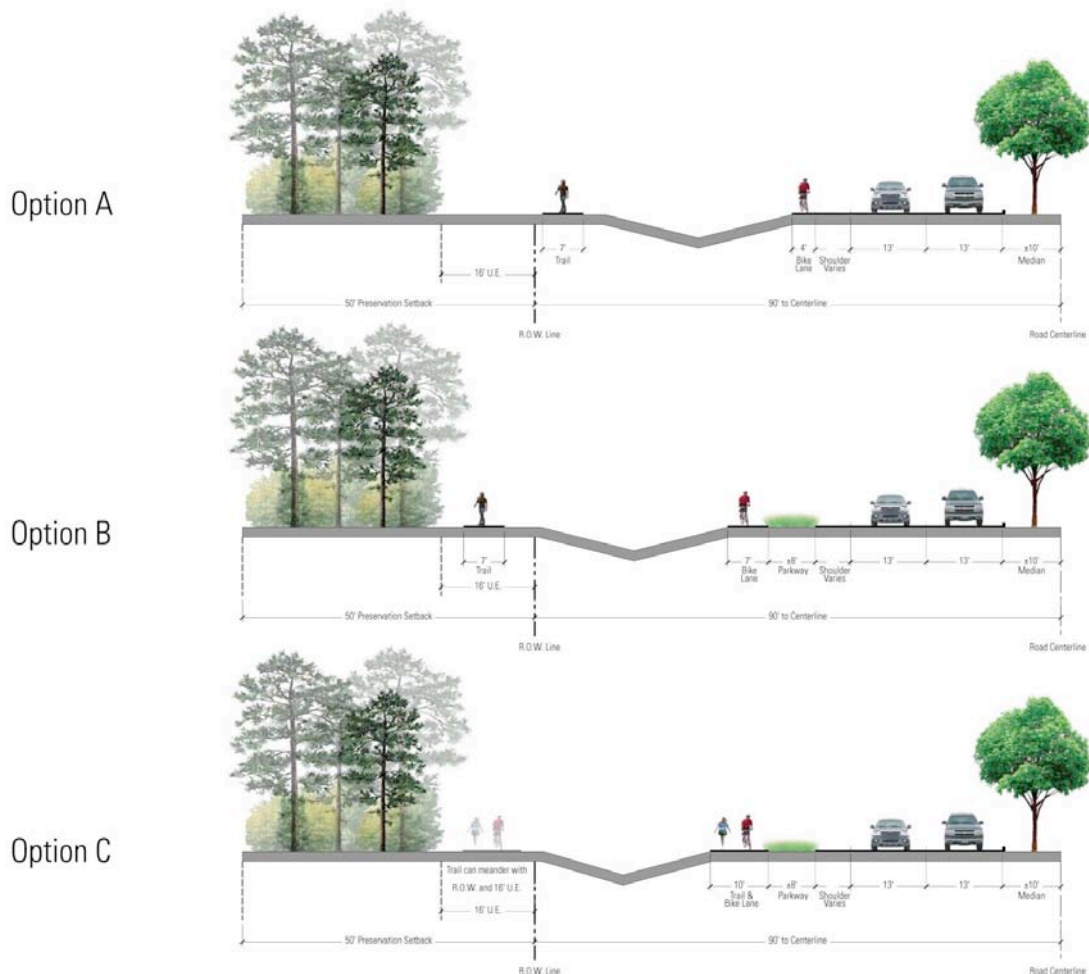


4. CORRIDOR OPPORTUNITIES WESTERN ZONE- TRAILS:

Concurrent with this Corridor Study, the Goodman Company has made recommendations for a “Paths and Parkways” grant for Carter Moore Drive/ FM 3083. In support of that effort, SLA has prepared three possible options for the location and dimensions of the trails and parkways along the corridor. To the extent that they may differ from the recommendations contained within the guidelines does not represent a conflict of opinion but rather the extent of the scope of this study/report. The options are illustrated below for informational purposes and will be refined during the detailed study that will likely follow if the grant is awarded the City or other funds are made available.

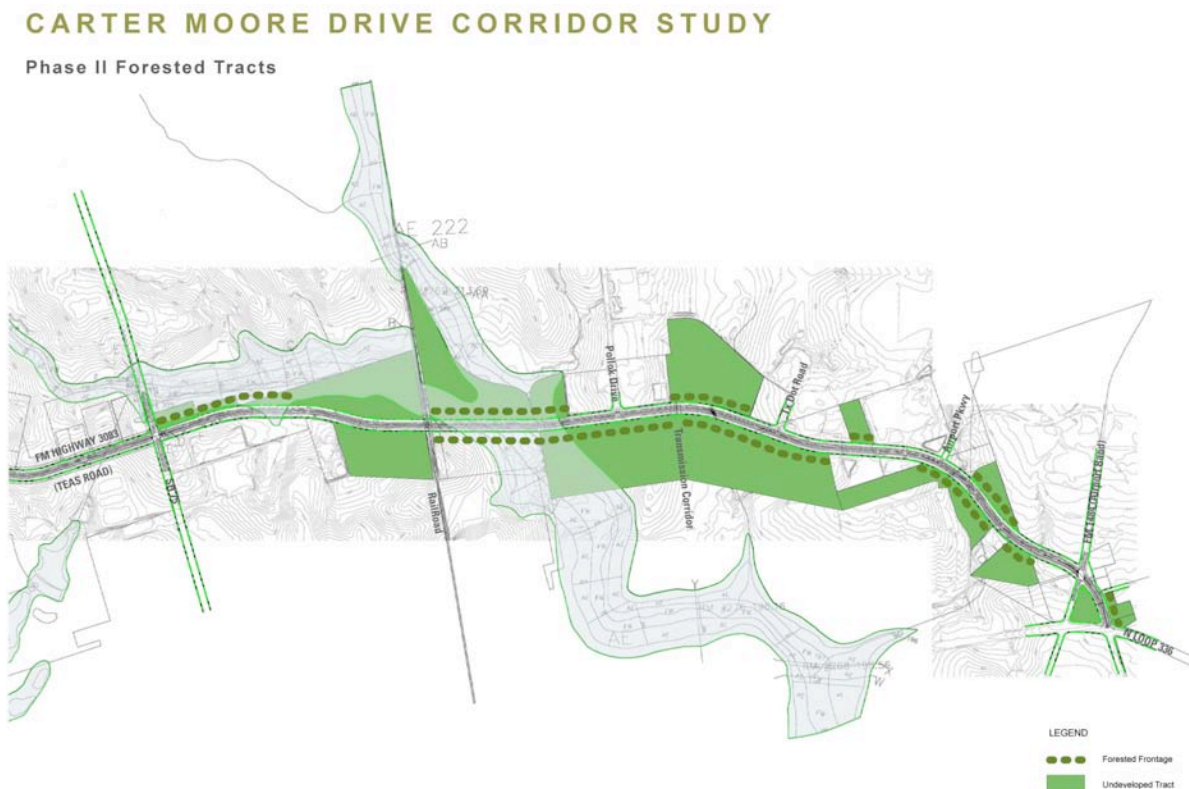
Notwithstanding the “Paths and Parkways” initiative, it is recommended that a trail system be implemented that would connect the Lone Star Convention Center in the east with the SH 105 corridor in the west. The proposed alignments are shown on the Conceptual Plans for both zones.

Preliminary Roadway-Trail-Parkway Section Options



5. CORRIDOR OPPORTUNITIES EASTERN ZONE-FOREST PRESERVATION:

The CMD-FM 3083 Eastern Zone from SH 75 and extending to North Loop 336 East offers challenges different from those of the Western Zone. Somewhat equal in distribution, the forested and non-forested areas call for different approaches. Complicated by a relatively high number and percentage of developed properties, an equal emphasis on enforcing these guidelines while in conjunction encouraging existing developed properties to retrofit their Corridor Landscape/preservation setbacks with landscape improvements consistent with these guidelines. The following diagram illustrates the extent of the forested and non-forested frontage.



The establishment of landscape continuity through the Corridor's Eastern Zone can best be accomplished through the City's participation and/or encouragement to improve the planting and buffering within the Landscape/Preservation setbacks. Through incentives or other means, in support of existing property owners retrofitting their Corridor setbacks, the continuity can be accomplished overtime. Publicly funded projects such as signage, portal monuments and intersection landscape, implemented in selected key areas can stimulate private sector investment within the corridor.

6. CORRIDOR OPPORTUNITIES EASTERN ZONE- **SIGNAGE:**

An inventory and review of existing signage within the Corridor and the Eastern Zone is partially documented below. Developed over a period of time and representing a number of land uses, the existing signage in this zone is understandably more varied in form, scale and materials than those in the Western Zone. As such, developing a unified appearance signage program in the east represents a greater challenge than in the west. Similar to the landscape recommendations, the City's encouragement and incentives for existing property owners to bring their signage into conformity with the proposed Corridor Guidelines will likely be necessary. Fortunately, there are a significant number of existing public or quasi-public entities within the Eastern Zone that if motivated, could spur signage redevelopment through their example making leadership. The implementation of wayfinding signage and monumentation could further advance these goals.

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CORRIDOR SIGNAGE EXAMPLES

Many of the signs within the study area as well as the City's own entry sign, use a brown sedimentary stone. White accents in stone or metal are also used on numerous signs. Brick can be seen on many of the buildings and most are in a mid-tone color range. The use of the same and/or similar materials and colors can build on the equity thusfar invested and help visually unify the corridor.



7. CORRIDOR OPPORTUNITIES EASTERN ZONE- OVERALL CONCEPT:

The integration of the analysis and opportunities forms the basis for the development of the Guidelines. The three segments that comprise the Western Zone provide similar and somewhat overlapping opportunities for achieving Corridor unification and preservation. The plan below depicts the key or critical areas within the Zone.

Those key components by Segment are as follows:

Segment 4

- Add shrub and tree plantings along the Gulf States “lay down” yard fronting FM 3083.
- Implement a signage wayfinding system
- Construct a 7’ wide trail along northern side of ROW.
- Implement Guidelines for undeveloped tracts
- Enhance traffic control devices with Corridor theme @ SH 75 and other signalized intersections.
- Apply Corridor Guidelines to all City & CIDC properties fronting FM 3083
- City to consider encouraging existing developed properties fronting on FM 3083 to bring the landscape improvements within the landscape setbacks into conformance with the Corridor Guidelines.

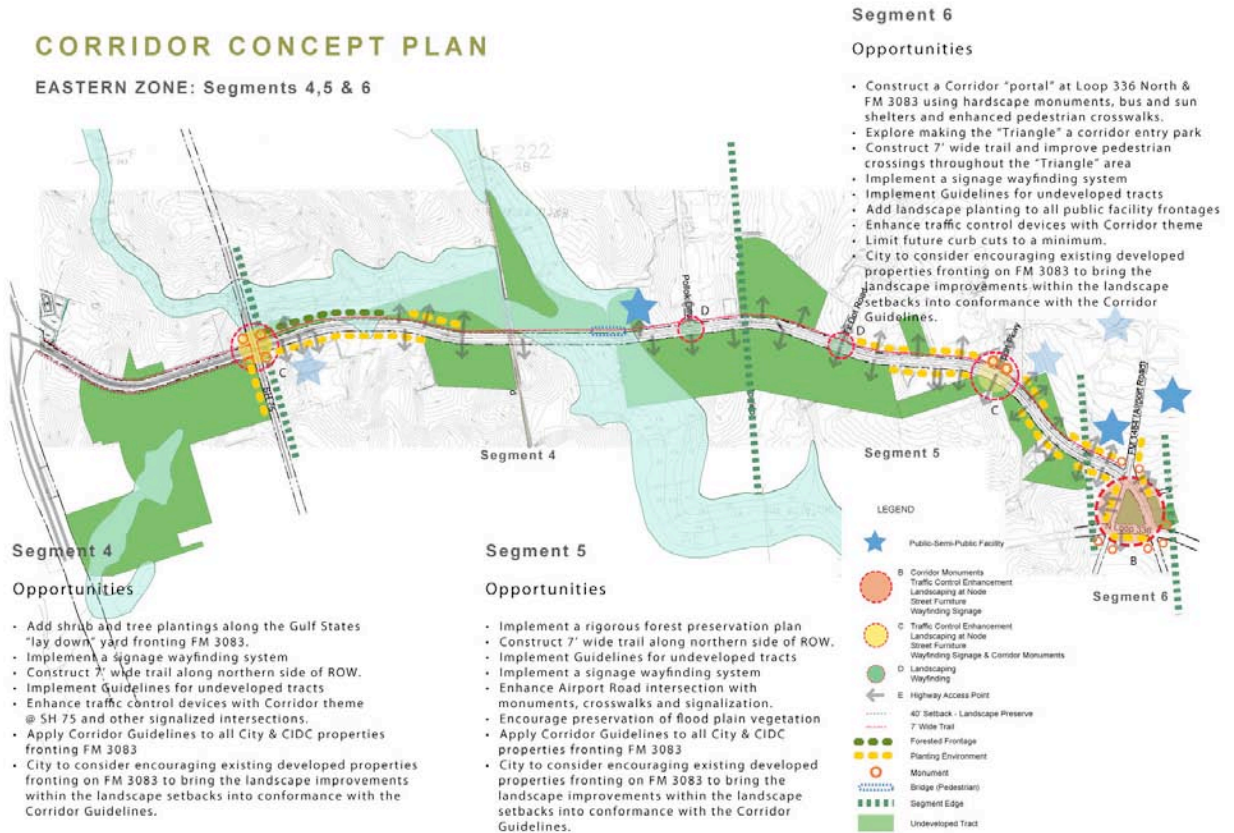
Segment 5

- Implement a rigorous forest preservation plan
- Construct a 7’ wide trail along northern side of ROW.
- Implement Guidelines for undeveloped tracts
- Implement a signage wayfinding system
- Enhance Airport Road intersection with Corridor monuments, crosswalks and signalization.
- Encourage preservation of flood plain vegetation
- Apply Corridor Guidelines to all City & CIDC properties fronting FM 3083
- City to consider encouraging existing developed properties fronting on FM 3083 to bring the landscape improvements within the landscape setbacks into conformance with the Corridor Guidelines.

Segment 6

- Construct a Corridor “portal” at Loop 336 North & FM 3083 using hardscape monuments, bus and sun shelters and enhanced pedestrian crosswalks.
- Explore making the “Triangle” a corridor entry park
- Construct 7’ wide trail and improve pedestrian crossings throughout the “Triangle” area
- Implement a signage wayfinding system
- Implement Guidelines for undeveloped tracts
- Add landscape planting to all public facility frontages
- Enhance traffic control devices with Corridor theme
- Limit future curb cuts to a minimum.
- City to consider encouraging existing developed properties fronting on FM 3083 to bring the landscape improvements within the landscape setbacks into conformance with the Corridor Guidelines.

The Conceptual Plan identifies the location and distribution of key recommendations by Zone Segment. With the concentration of public facilities and developed tracts in the far eastern portion of this Zone, improvements to Segment 5 and 6 should produce the greatest visual impact in the short term. Similarly, the length of untreated frontage along the Gulf States Utilities FM 3083 Corridor frontage presents a high value target for a landscape retrofit.



8. CORRIDOR OPPORTUNITIES EASTERN ZONE- TRAILS:

The proposed trail improvements in this zone are proposed to be located along the northern edge of FM 3083. With a high concentration of public facilities located on the north side of the Corridor and an extensive length of floodplain of Stewarts Creek, a north side trail alignment would best leverage existing and future public sector investments. Well marked and designed pedestrian crossings at key selected intersections will provide for pedestrian crossings of the highway and promote safe usage of the trail system.

Part 3 Corridor Guidelines

1. Introduction:

These Guidelines establish minimum development criteria with the intent of preserving and establishing the beauty of Carter Moore Drive from SH 105 to North Loop 336 East, the “Corridor”. The fundamental goal of the Guidelines is “To promote quality of place and economic growth” within the Corridor.

Creating a positive image for the Corridor is a goal of the Carter Moore Drive Corridor property owners and City. A positive image will add value to all property within the Corridor. A joint effort by all property owners within the Corridor will ensure success in creating a gateway to the City, the Lone Star Executive Airport and the Corridor, the results of which it is hoped that all residents and property owners will be proud.

These Standards are applicable to the following Corridor:

The tracts of land on both sides of FM 3083-Carter Moore Drive from SH 105 east to North Loop East 336.

The CMD Corridor Guidelines apply to all tracts with frontage and direct access off of and/or on to CMD as defined in Part 1. “Corridor Boundary Definition”. The proposed standards/Guidelines would be applicable to the site development on that portion of the tracts that front Carter Moore Drive-FM 3083 within the Corridor Boundary.

2. LANDSCAPING AND ARCHITECTURAL GUIDELINES

These Landscaping and Architectural Guidelines have been created to establish minimum standards for development within the Corridor. In addition to these standards, “Signature Items” such as a Corridor logo, special water features and Corridor monumentation will be used to create a unique identity for the Corridor. These Landscaping and Architectural Standards have been approved by the City of Conroe. Review and enforcement of these standards will be carried out by the City or its designee.

Due to the varied vegetative conditions along the Corridor, some tracts are heavily forested while others open pasture land, two landscape preservation standards have been created that reflect the varied vegetative conditions.

The setback dimension recommendation contained in these Guidelines is based upon an assumption that the City of Conroe will be successful in negotiating an agreement with the utility providers that will locate all future utilities within the Corridor ROW rather than in a 16' Utility Easement as has been the condition to date. While in the eastern zone, some utility easements have been created reflecting the 16' standard, it is recommended that the standard going forward require the placement of all utilities within the ROW. The western zone is predominantly forested and the placement of the utilities in the ROW is particularly important in this zone. The general distribution of the forested tracts in the Corridor is depicted on Exhibits 2A & 2B. Exhibit 3 depicts the conditions if the utilities are located within the ROW as recommended while Exhibit 4 shows the recommendation for varying setback dimensions if the utilities are to be placed in a 16' easement within the boundary of the fronting tracts of land.

CARTER MOORE DRIVE CORRIDOR STUDY

Forested Tracts

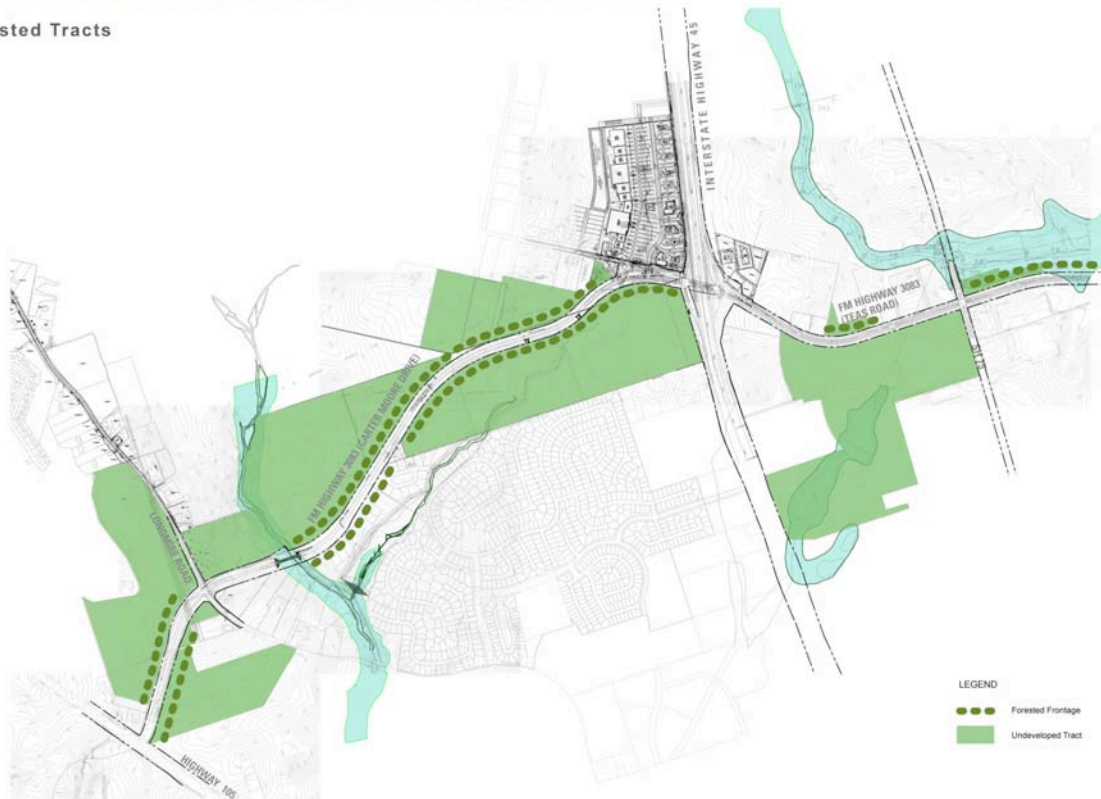


Exhibit 2 A: Forested Tracts Western Zone

CARTER MOORE DRIVE CORRIDOR STUDY

Phase II Forested Tracts

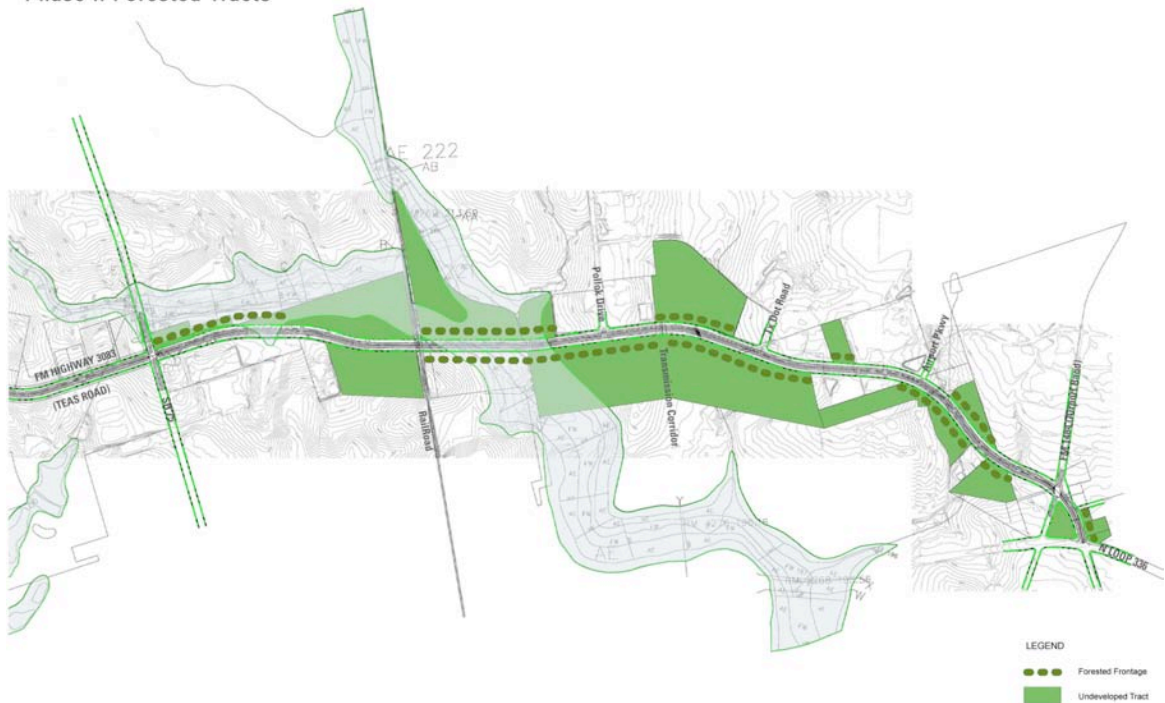


Exhibit 2 B: Forested Tracts Western Zone

The Landscaping and Architectural Guidelines cover the following development issues:

- A. Setbacks
- B. Minimum Parking Ratios
- C. Minimum Landscape Standards
- D. Sign Criteria
- E. Site Lighting
- F. Screening
- G. Corridor Thematic Identity Elements

A. Minimum Building and Parking Setbacks

The setback and preservation/landscape recommendations reflect a condition where the existing 16' UE policy remains in force and alternatively, a condition where all future utilities are placed within the public ROW. The preferred recommendation is to place all future utilities that are needed to be located along the fronting edge of the properties along Carter Moore Drive/FM 3083 within the existing road ROW.

Commercial and Retail Buildings in the Carter Moore Drive-FM 3083 Corridor (CMD-3083 Corridor)

1. *Setbacks from CMD-3083* *Current Policy*

- a. All buildings and parking lots shall be setback a minimum of fifty feet (50') from the right-of-ways of Carter Moore Drive-FM 3083 for forested tracts and 40' for non-forested tracts. These setbacks are generally described on Exhibit 3.

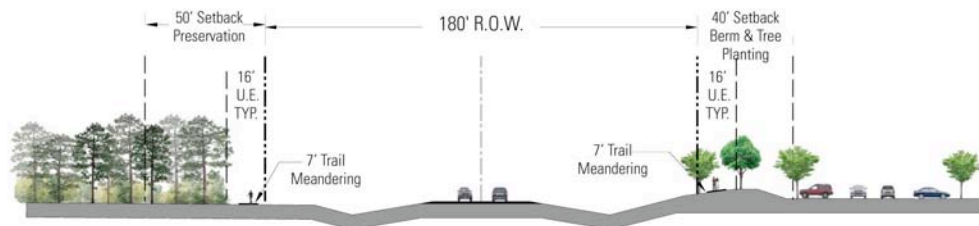


Exhibit: 3 Forested and Non-Forested Tract Setbacks Current Policy

The road section illustrates the utilities within a 40' setback in the non-forested tracts versus within a 50' setback on the forested tracts. This difference is explained by the area needed to preserve existing forested area versus the area needed to establish a landscape buffer. In the case of the non-forested tracts, the planting of trees could occur within the utility easement in combination with the remaining 24' of a 40' setback and thereby would provide an adequate landscape buffer.

2. *Setbacks from CMD-3083* *Recommended Policy*

- a. All buildings and parking lots shall be setback a minimum of fifty feet (40') from the right-of-ways of Carter Moore Drive-FM 3083 for forested tracts and 40' for non-forested tracts. These setbacks are generally described on Exhibit 4.

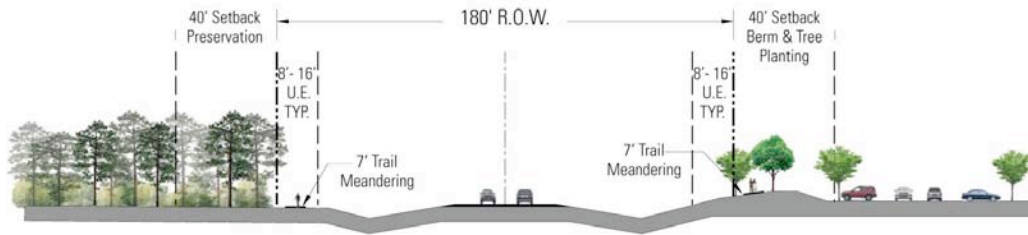


Exhibit: 4 Non-Forested Tract Setback

The road section illustrates the utilities within the ROW for both forested and non-forested tracts with both conditions having a 40' setback. The forested buffer would be a full 40' versus a 34' where the utility easement would occupy the front 16' as is the current policy illustrated in Exhibit 3..

3. *Setbacks from Secondary Roads (Side streets)*

- a. **Forested Tracts** All buildings and parking lots shall be setback a minimum of twenty-five feet (25') from the right-of-ways of a Side Street.
- b. **Non-Forested Tracts:** All buildings and parking lots shall be setback a minimum of fifteen feet (15') from the right-of-ways of a Side Street.

4. *Setbacks from Side and Rear Property Lines*

Side and Rear Property Lines setbacks shall be in conformance with the City of Conroe Ordinance "Chapter 94 SUBDIVISIONS".

B. Minimum Parking Ratios:

In order to assure that the CMD-3083 Corridor aesthetic character is of the highest reasonable possible quality, off street parking is to be provided in ratios that reflect the current prevailing standards in the development industry as well as those required by the City of Conroe. Notwithstanding the following ratios, for mixed use developments, the City will consider reductions in the approved ratios where a shared parking analysis, utilizing the methodology documented in the “Urban Land Institute’s Shared Parking, Second Edition, 2005” or approved alternate analysis method, results in a lower combined parking total for the uses considered. To that end, the following parking ratios have been adopted by the City:

1. General Retail Per City Code
2. Hotel Per City Code plus food and conference
3. Commercial

Financial Institution	4.5
Veterinary Clinic	4.5
Restaurant	10.0
Food & Beverage	18.0*
Clothing Store	4.5
Furniture Store	3.0
Free Standing Retail	4.5
Home Improvement Center	4.5
Discount Store	4.5
Office	3.0
“Fast Food” Restaurant	5.0
Health and Fitness Center	5.5

*Note: National Food & Beverage chains shall submit to the City examples of similar establishments in similar developments and markets evidence of the parking ratios used for these examples. The user shall provide telephone numbers and contact information for the Developer, Property Owner’s Association and the local authority’s Planning Director as references to confirm the necessary parking spaces required to accommodate peak usage times.

4. All other uses per City Code

C. Minimum Landscape Standards

1. Forested Tracts

a. Forest Preserve On forested tracts of land within the Corridor, a 50' Setback Forest Preserve is established adjacent and parallel to the Carter Moore Drive R.O.W. The front 16' of land is reserved for City utility easement(s) as stipulated by the City of Conroe. ***(In cases where the City does not require a 16' utility easement in front of the Forest Preserve a 40' tree screen is acceptable subject to review of final plat approval designating utility easements).*** Tree clearing shall be in accordance with the City of Conroe's Trees Ordinance. No clearing, excavation, paving, parking, construction or storage of materials or other improvements shall be allowed within the Forest Preserve except for "Permitted Construction". The term Permitted Construction shall include driveways, "Permitted Signs" and utilities constructed across but not along the length of Forest Preserve excepting City mandated easements.

Access across the Forest Preserve for construction purposes shall be restricted to the areas designated for permanent driveway access. Existing trees and landscaping located within the Forest Preserve shall be fenced during construction activity in order to preserve and protect such existing landscaping and vegetation. Nothing contained herein shall prohibit the construction and operation of a pedestrian pathway within the Forest Preserve.

b. Under Brushing

On forested tracts, "under brushing" is permitted but is restricted to the removal of ***shrubs and vines but not trees*** unless a tree is either a.) In a sight triangle or b.) Diseased or dead/dieing. Landowners and Developers are encouraged to preserve native stands of shrubs where practical. The "under brushing" of the total frontage along CMD-FM 3083 is discouraged. Selective clearing to create site visibility of buildings and/or site features such as plazas and site structures is permitted. In general, a ratio of 50% understory clearing and 50% existing conditions is the goal of these Guidelines.

c. Tree Preservation Inside Development Tracts

Within the Boundary of the Tract and outside the setback area, the City of Conroe Trees Ordinance shall govern all tree preservation activity.

d. Overhead Utilities

No overhead utilities shall be permitted within the setback along the frontage of CMD within the Corridor.

2. Non-Forested Tracts

Tracts that are not forested, wooded or contain significant tree stands within a 50' distance behind the CMD-FM 3083 ROW, shall provide a landscape plan consistent with the following standards within a 40' Setback Area.

Landscape Requirements for Front Setback Area

- a. Trees and grass shall be the primary landscape materials that provide a landscape buffer in the Setback Area. The grass ground cover shall be hydro-mulched on a graded surface that provides adequate drainage. Weeds and underbrush shall be removed and where necessary to establish turf, topsoil shall be provided to enable a healthy stand of grass to grow. Fertilization shall be scheduled on a regular basis to maintain the turf in a healthy condition.
- b. Single trunk, three and one-half inch (3.0") caliper, forty-five (45) to sixty-five gallon (65 gal) soft and hardwood trees shall be planted in an "informal" manner. Machine dug trees are acceptable but must be a minimum of 1" larger in caliper. The trees planted shall be in a ratio of 30% Loblolly pine and 70% hardwood trees. One half of the hardwood trees shall be made up of either or both of the following species; Burr Oak and/or Water Oak. The remaining 35% (1/2 of the hardwoods) may be made up of any of the "CLASS I" trees on the City of Conroe's tree list in the Trees Ordinance Sec. 102-72.
- c. The total number of trees required shall be determined by a formula that takes the linear front footage along CMD-FM 3083 and divides it by 30. If, for example, a property owner had six hundred linear feet of frontage they would be required to install a total of 20 trees ($600/30 = 20$).
- d. If desired, a re-forestation landscape approach may be used, When chosen, the understory planting shall be of native species generally found in the Conroe area. The density and distribution of the understory plantings shall be in addition to the tree planting requirements described above and shall be arranged in a manner that will achieve the

same density of understory (50% of frontage) that is described in the “Forested Tract” section above. Understory planting areas shall be mulched and invasive noxious weeds and plants shall be removed as a part of the normal landscape maintenance schedule.

3. Landscape Requirements in Side Setback Areas for Commercial and Retail Buildings

- a. The side street building and parking setback for Corridor Tracts is Twenty-five feet (25’).
- b. A double row of Yaupon Holly or Wax Myrtles shall be installed in triangular spacing of eighteen inches (18") along all side street parking lot edges. Sixty-five (65) gallon oak trees of the same species as the frontage, three and one-half inches (3.5") in caliper, twelve feet (12') to fourteen feet (14') in height, shall also be installed within all side property setbacks, spaced at an average of thirty feet (30') on center. Machine dug trees are acceptable but must be a minimum of 1” larger in caliper.



Exhibit: 5 Side Setback Planting

4. Landscape Requirements for Parking Lots

Parking lots must be properly screened from public streets and adjacent properties with the use of hedges and trees. The intent of this screening requirement is to break up the view of the lower portion of cars with the use of

high density shrubs. Trees shall be used to provide shade and soften large expanses of concrete. Planting islands shall be required at the end of all parking bays.

Two Live Oak trees shall be planted within each end of the parking bays, ten feet (10') from the edge of the bay planter as shown on Exhibit 5. The trees shall be three and one-half inch (3.5") caliper, single trunk, twelve feet (12') to fourteen feet (14') in height. Machine dug trees are acceptable but must be a minimum of 1" larger in caliper. Every fifth space shall contain a six foot (6') curbed planter diamond. Live Oak trees shall be planted in each diamond planter (see Exhibit 6). The trees shall be under-drained and drip irrigated and regulated with an automatic irrigation controller.

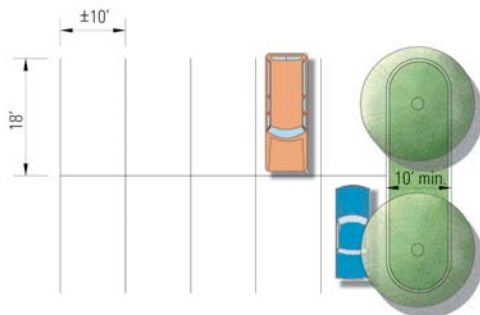


Exhibit: 6 End Bay Planter

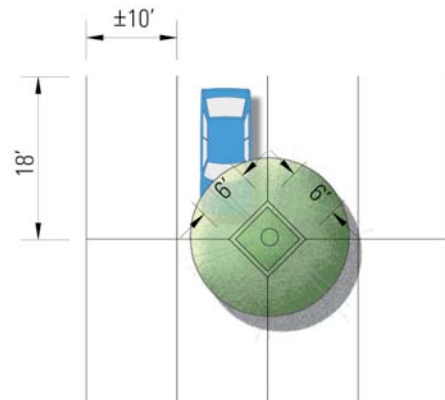


Exhibit: 7 Parking Lot Diamond Planter

A Yaupon Holly or Wax Myrtle hedge shall be required along the outside border of all parking lots. The plants shall be planted in a double row, triangularly spaced on eighteen inch (18") centers. The hedge shall be maintained at a height of \pm four feet (4').

5. Landscape Requirements for Cart Storage Areas Within Parking Lots

A minimum planting bed two feet (2') in width (inside dimensions) shall be provided on both sides of a cart storage area. A single row of five (5) gallon Yaupon Holly or Wax Myrtles shall be planted twenty-four inches (24") on center in beds triangularly spaced as generally shown on Exhibit 7.

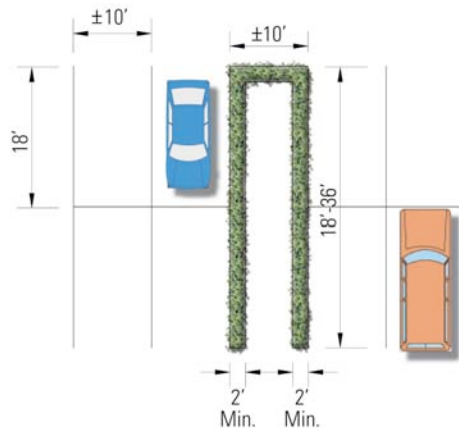


Exhibit: 8 Cart Storage Buffer Planting

6. Driveways for Drive-Thru Lanes

Drive-Thru lanes for fast food restaurants, financial institutions, pharmacies, or any other use requiring a drive-thru lane shall require landscaping to screen vehicles from public view. The landscaping shall consist of a double row of five (5) gallon Wax Myrtles or Yaupon Holly planted eighteen inches (18") on center triangularly spaced to create a hedge that will be maintained at a three foot (3') height. Additional shrub or tree standards may be added to these areas if desired.

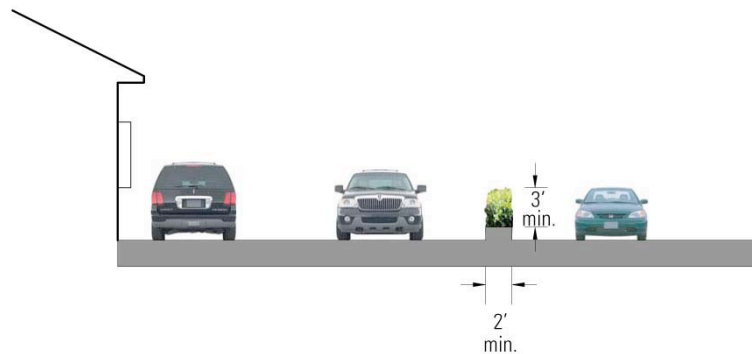


Exhibit: 9 Drive Through Planting

7. Thematic Entry Planting

All entries to properties intersection CMD-FM 3083 shall include the use of elements of the Corridor thematic landscape palette. It is not the intent of this guideline to create “look a like” entries but rather to produce a strong sense of continuity within the Corridor area. The plant material and planting concept is shown in Exhibit 9.

8. Landscape Maintenance

- a. All dead or dying plants or trees shall be removed or replaced within 90 days of notice.
- b. Trees shall be fertilized on a regular schedule (a minimum of 1 time per year with 1 lb of slow release tree fertilizer per tree) or as required to maintain their health and growth vigor.
- c. All trees, shrubs and turf areas shall be adequately irrigated with an automatic irrigation system providing drip irrigation for trees and shrubs and spray irrigation for turf areas.
- d. Parking lot edge shrubs will provide a hedge that will be maintained at a ±four foot (4') height.

D. Sign Criteria

Signs within the Corridor are intended to provide appropriate project and tenant identity without producing “sign clutter” that reduces sign effectiveness through an overly signed streetscape. The most attractive and economically attractive business areas maintain sign controls that result in an ordered and legible “signscape”.

1. Prohibited Signs Within The Corridor

All signage within the Corridor must be approved by the City of Conroe, or its designee. Signs, including but not limited to off premise or on premise billboards, temporary trailer signs, any vehicles used as signage through daily or overnight parking in prominent locations, animated, moving or flashing signs, iridescent painted signs, exposed neon, fluorescent or incandescent illumination, Day-Glo colored signs, bandit signs and signs that create noise are strictly prohibited within the Corridor.

No pole mounted pylon signs shall be permitted. All signs shall be “skirted” or “boxed” from the top to bottom of sign where it meets the masonry sign base-foundation.

No product, service or event (other than approved Grand Opening signs described below), “advertising” signs of any kind, nature, or description, whether on or off premises shall be permitted. No rooftop or paper signs shall be permitted. All banners and streamers are prohibited other than for one “Grand Opening” event and for no more than 7 days in duration per business. No off-premise signs will be permitted. All signs shall be placed on the owner or tenant’s property. Monument signs for anchors and/or individual businesses within a commercial center, located along a major street frontage shall not be deemed to be “off-premise” for the purposes of these Guidelines.

2. Standard Colors & Materials

Each development shall create a sign package that conforms to these Guidelines as well as establishes a coordinated color and letterstyle theme within each development. The sign “can” shall be a dark color that expresses the architectural character of the buildings and shall have white internally lit letters. The can color should be selected for maximum contrast with the white letters and to promote continuity within the overall Corridor.

Where a development lacks a strong architectural theme or character, the sign cans shall be a dark bronze color. Sign bases shall be constructed of concrete and clad with the Corridor stone.



Exhibit: 10 Facility Identification Sign

3. Standard Type Face

Goudy Bold has been selected as the preferred standard type style for all temporary and permanent signs. Where a development's sign program is driven by either a prominent anchor tenant/user or unique architectural theme from which a sign program letterstyle is preferred, the City will consider alternate sign letterstyle for the project signage.



Exhibit: 11 Standard Typeface

Refer to the elevation drawings for copy size and layout dimensions. All lettering on signs is to be sharp and precise and of professional quality.

4. Application of Logo

The Carter Moore Drive logo will appear on all permanent and temporary signs. Logo and type are always to appear white on the standard dark background. Camera-ready artwork for the Corridor logo is available from the City. Refer to sign elevations for proportions and dimensions.

5. Commercial Signage Types, Numbers and Distribution

The intent is that signage not be cluttered or busy while at the same time provides a reasonable amount of visibility and market communication by businesses to their

customers traveling along and through the Corridor. Therefore, the City focus is on simplicity in signage to maximize the quality appearance of the Corridor. The number and sizes of permitted signs in the Corridor varies depending upon type and size of business, number and spacing of signs and the configuration of the tract of land(s) fronting the Corridor.

a. Overview of Sign Sizes and Numbers

Business	Sign Type	Max. Width	Max. Height	Spacing	Number of Signs	Max. Sign Area
Tenant 10-30KSF	Monument	12'	6'	125'	1*	
Tenant >30KSF	Monument	14'	22'	125'	1*	
Multi-Tenant Commercial	Monument	18'	35'	1400'	2*	
Tenant Building Signage 0-10KSF	On Building				1	50
Tenant Building Signage 10-30KSF	On Building				2	150
Tenant Building Signage >30KSF	On Building				2	300
Freestanding Commercial	On Building				1*	10% Elevation area

* Corner tracts may be permitted 1 sign per fronting street.

b. Explanation of Sign Size, Numbers and Area

Anchor tenants, (those with 10,000 square feet or more of lease space), national chains, freestanding single tenant buildings, or other significant tenants may be incorporated in a monument sign.

The City will review and approve the size, proportion, and color, color mix and copy of each sign. The City may reject copy or design that they feel is too wordy or busy.

Nationally or regionally recognized business logos may be incorporated into the sign can's main panels.

There are three (3) types of monument signs permitted:

Type	WxH	Application
Type A	12'x6'	Pad users and Anchor Tenants from 10,000-30,000 SF
Type B	14'x22'	Anchor Tenants with 30,000 SF or larger

Unless otherwise approved by the City, monument signs must be single tenant signs with the exact same copy on both sign faces. Only under special

circumstances will multi tenant signs be permitted such as a center that is of a size where the use of individual monument signs spaced as required by these standards cannot provide enough locations for the retailers within the center. In this case, the use of a multi-tenant sign (Type C) may be permitted.

Type	WxH	Application
Type C	14'x35'	Multi-tenant developments with insufficient frontages to accommodate the number Type A & B signs needed

6. Freestanding Retail Building Signage Single Tenant

Freestanding, single tenant buildings have the option of wall mounted identification signs. The purpose of this sign is business identification only. Promotional or advertising verbiage will not be permitted. The City reserves the right to disapprove any signs or graphics it deems to be too wordy, inappropriate or not in keeping with the Corridor standards.

The maximum square footage of signage shall equal no more than one-tenth (1/10) of the building facade.

Signs must be manufactured of cast metal (non-ferrous), painted acrylic, backlit letters or individually mounted channel letters with or without interior neon tube illumination.

The sign letters shall have translucent Plexiglas faces constructed of bronzed anodized aluminum sidewalls and trim, with a minimum of gage of .040. #7328 white Plexiglas facings, a minimum of three-sixteenths (3/16") inches thick.

Owners of freestanding buildings have the right to request signs on multiple building elevations. City must approve this request in writing. All signs must be exactly the same, copy and size.

Other than for national and/or regionally recognized retail business as provided for in section "D 4" above, business logos are not permitted on/in building mounted signage under any circumstances. It is the sole discretion of the City to determine what constitutes a "logo."

7. Sign Locations in Primary Road Setbacks

The maximum dimension of any sign placed in the fifty foot set back adjacent to a CMD-FM 3083 shall be as stipulated in Section "D-5" above. Two Type A or B signs shall be allowed in cases where a single tenant or user is located on a corner tract having 2 street frontages. The signs shall be located in the front 18'

of the landscape setback and the sign edge closest to the street shall be located so that the sign edge is immediately inside the ROW line.

The signs shall be constructed sheet metal with an internally lit Plexiglas insert. The sheet metal shall be bronze in color. A logo of the Corridor shall be incorporated in the lower right hand corner of the Plexiglas insert. The insert shall have a minimum four inch (4") border of sheet metal as generally shown on Exhibit 10. The minimum distance between signs along Primary Roads shall be one hundred twenty five feet (125'). Each separate property owner fronting CCMD-FM 3083 will be allowed at least one sign.

8. Multi Tenant Retail and Office Signs

For a multi tenant retail center or professional office building, or professionals in a retail environment, a multi tenant sign may be permitted.

This sign will be designed with a dark or bronze face, with routed out lines of copy. The sign will have a white Plexiglas face behind the lettering. All lines of copy must be the same font and size with the exception being that the top line may be copy that identifies the center or building name. A maximum of six lines of copy are permitted.

9. Temporary Construction, Sales & Marketing Signs

Temporary Construction signage may be displayed at the job site through the construction and initial leasing stages (Marketing) of the development. Banners, moving or flashing signs and portable signs of any kind are forbidden.

There is permitted one Temporary Construction sign per site entry. These signs may be a maximum of 8'x12' in size and smaller signs of either 6'x8' or 4'x8' may be used at the discretion of the owner/developer. The Temporary Construction Sign must include all information for the project. No additional independent sub-contractor or service provider signs are permitted.

There is permitted one Temporary Marketing sign and one For Sale sign per site entry. These signs may be a maximum of 8'x12' in size and smaller signs of either 6'x8' or 4'x8' may be used at the discretion of the owner/developer. The Marketing & For Sale Signs must include only marketing and sales information for the project and no additional brokerage or owner marketing information signs are permitted. Nothing contained herein shall prohibit a property owner, or designated agent, from placing and maintaining a "For Sale" or "For Lease" sign on its property including reference to a listing broker, if any.

All temporary signage within the Corridor will be fabricated of 1/2"-3/4" thick Medium Density Overlay plywood on both sides of a frame of 4" x 4" treated yellow pine. Galvanized nails or screws should be used and must be installed so that the heads are flush. All seams are to be tight. Holes must be filled and sanded smooth as required, prior to painting or application of graphics. All signs are to be properly capped at ends.

All temporary sign posts are to be set in sufficient diameter sand filled holes at a depth as required for structural stability. The Owner is responsible for the structural integrity of the sign and will be responsible for any and all damage that results from the sign(s) impairing visibility, being struck by a pedestrian or motorist or from falling over or collapsing from any force whether human caused or natural. Sign position is to be plumb and level. The sign contractor is responsible for obtaining all necessary sign permits. A detailed sign location plan will be submitted with the sign drawings and specifications to the City for their approval.

No temporary construction signs shall be illuminated. However, sales office and project-information signs intended to attract the public may be illuminated subject to the approval of the City. External ground mounted and shielded metal halide lights are required. Lights shall evenly illuminate the entire sign face.

E. Site Lighting Standards Commercial Development

Exterior lighting should enhance the atmosphere and safety of all public parking areas, walkways and entrances. Lighting shall be low key and placement of fixtures shall coincide with major walkway areas.

Where used, site structure illumination shall be lit with a concealed, stationary-shielded light source.

All areas within Retail Centers must be illuminated with area and decorative lighting continuously from dusk until at least 11:00 p.m. All site lighting must be illuminated during hours of operations. Each Owner of the Tract within a Retail Center shall maintain at least one (1) exterior night light.

1. Retail Centers

A lighting photometric drawing shall be submitted to the City depicting the design layout and illustrating a point-by-point foot-candle lighting achieve a minimum average level of ten foot candles with a maximum/minimum uniformity ratio of 8 to 1.

2. Office Developments

All office building lighting will achieve a minimum average lighting level of five foot candles. The types, size, and style of the lighting fixtures shall also appear in the form of a schedule to the lighting layout plan. Choice of light fixtures and poles must reflect the theme and architecture of the commercial development as approved by the City.

3. Lamp and Light Fixture Type

All parking lot lighting shall be metal halide (MH) and lighting fixtures shall be “shoebox” type fixtures with a minimum rating of 400 watts. Fixtures and poles shall have a bronze color powder-coated or anodized finish. Standards for parking lots shall be at least thirty feet (30') tall and shall have a glare light shield light source. Fixtures may be single, double, triple or four fixture pole standards.

Exhibit 12: Prototypical Parking
Lot Light



4. Supplemental Lighting

Buildings must be adequately lighted at night. Light fixtures shall be attractive, efficient, and shall not create glare or spillover into adjacent properties. No flood lights shall be mounted on buildings except for wall packs at the rear of the buildings as approved in advance by the City.

5. Glare and Directionality

Maximum spillover at perimeter property lines shall be a maximum of .25 footcandles measured along the property line. Lighting fixtures located across from Single Family Residential shall use “House Side Shields” to minimize glare.

F. Screening

1. Service and Utility Areas

All service and utility areas shall be screened from public view by screens, masonry fences, and other devices approved by the City. Trash areas and dumpster locations shall be permanently fenced or screened with enclosures which shall not be visible from any public street. The open side of trash area screens shall face the back of the associated building when located in close proximity to the building. In all cases, screening must be compatible with building materials and shall reflect the overall style of Retail Center or Office Development.

2. Mechanical Equipment

All mechanical equipment such as water towers, storage tanks, cooling towers, heating equipment, air conditioning or ventilating equipment, or electrical equipment shall be properly mounted and screened from public view by fencing or landscaping approved by the City. Rooftop mounted equipment must also be screened with materials that match appropriate building components. A site line study is recommended to verify that the rooftop mechanical and roof surfaces are not visible from parking areas, adjoining tracts and public right of ways. Unless as otherwise provided for in these Guidelines, the loading dock areas shall be buffered from public view.

3. Cart Storage Areas

Where used, shopping carts shall be screened either adjacent to a building, with a masonry wall or with landscape as defined above in Section C-4.

Within the parking lots, the cart storage areas shall be of size and number to handle the needs of each tenant. Typically, the length of a cart storage area should be the same as the length of two (2) head-to-head parking stalls. The buffering for the parking lot cart storage areas shall be a minimum planting bed two feet (2') in width (inside dimensions) shall be provided on both sides of the cart storage area.

G. Corridor Thematic Identity Elements

1. Corridor Thematic Monuments

The Corridor will erect Corridor Identification Monuments using a combination of signature materials and colors to define key entry points along the Corridor. These monuments are intended to define the Corridor boundaries, enhance a sense of entry into the Corridor and reinforce the material palette established for the Corridor. Additionally, other street furniture elements will be “customized”, thereby expressing the unity of material and color theme that has been defined in these Guidelines.

Some of the elements that may be customized for these purposes include traffic control devices, wayfinding signs, bus shelters, shade structures and street signs. Some examples of this concept are shown below.

Exhibit: 13 Identity Monument

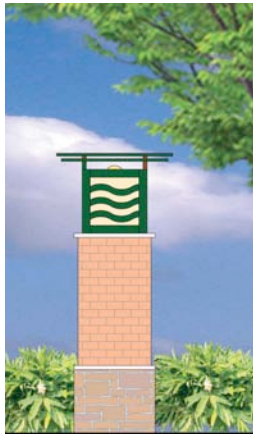


Exhibit: 14 Wayfinding Signage



Exhibit: 15 Prototypical Sun Shelter



Exhibit: 16 Prototypical Bus Stop Shelter



Exhibit: 17 Prototypical Traffic Control Device



2. Street Signs

Street signs can contribute to the unique character of a district or special geographic area. Through the coordination of colors, typefaces, logos and other design elements, each street corner reinforces the overall urban design composition. The illustration provides a conceptual idea of how this might be accomplished within the Corridor. To maximize the value to the Corridor character and unity, a more detailed design effort will need to be undertaken for all of the conceptual identity elements depicted in these guidelines.

Exhibit: 18 Prototypical Street Sign



3. Other Street Furniture

In addition to the elements shown in these guidelines, a “package” of street furniture elements complimentary to the Thematic elements depicted herein should be considered for public spaces such as parks, transit stops and transit terminals. Such street furniture components might include bicycle racks, trash containers, benches, planters, bollards and other elements customarily provided for in pedestrian environments. The coordination of colors and the use of the Corridor logo will also reinforce the aesthetic unity.