

AN ORDINANCE AMENDING THE ZONING MAP TO REZONE A 1700 ACRE TRACT, DALLAS COUNTY, GENERALLY DESCRIBED AS THE N.E. 8th/N. BELT LINE ROAD CORRIDOR BETWEEN TARRANT ROAD AND TRINITY BOULEVARD INCLUDING THE INTERSTATE 30 CORRIDOR BETWEEN NO. BELT LINE ROAD AND N.W. 7th STREET (see Exhibit "A"), FROM PD-39, PD-70, PD-78, PD-165, PD-170, PD-207, PD-208, MULTI FAMILY-ONE (MF-1), MULTI FAMILY-TWO (MF-2), GENERAL RETAIL (GR), COMMERCIAL (C), LIGHT INDUSTRIAL (LI), AND HEAVY INDUSTRIAL (HI) TO A PLANNED DEVELOPMENT FOR MIXED USES INCLUDING ENTERTAINMENT, RETAIL, OFFICE USES AND A SPECIFIC USE PERMIT FOR A CAR DEALERSHIP; SAID ZONING MAP AND ORDINANCE BEING NUMBERED ORDINANCE NUMBER 4779 AND PASSED ON NOVEMBER 20, 1990; REPEALING ALL ORDINANCES OR PARTS OF ORDINANCES IN CONFLICT HEREWITH; CONTAINING A SAVINGS CLAUSE; AND TO BECOME EFFECTIVE UPON ITS PASSAGE AND APPROVAL.

WHEREAS, the owners of the property described hereinbelow filed application with the City of Grand Prairie, Texas, petitioning an amendment of the Zoning Ordinance and map of said city so as to rezone and reclassify said property from its classification of PD-39, PD-70, PD-78, PD-165, PD-170, PD-207, PD-208, Multi Family-One (MF-1), Multi Family-Two (MF-2), Commercial (C), Light Industrial (LI), and Heavy Industrial (HI), to a Planned Development for Mixed Uses including Entertainment, Retail, and Office Uses; and

WHEREAS, the Planning and Zoning Commission of Grand Prairie, Texas, held a public hearing on said application on September 9, 1996 after written notice of such public hearing before the Planning and Zoning Commission on the proposed rezoning had been sent to owners of real property lying within 200 feet of the property on which the change of classification is proposed, said Notice having been given not less than ten (10) days before the date set for hearing to all such owners who rendered their said property for City taxes as the ownership appears on the last approved City Tax Roll, and such Notice being served by depositing the same, properly addressed and postage paid, in the City Post Office; and

WHEREAS, after consideration of said application, the Planning and Zoning Commission of the City of Grand Prairie, Texas voted 6-1 to recommend to the City Council of Grand Prairie, Texas, that the hereinafter described property be rezoned from PD-39, PD-70, PD-78, PD-165, PD-170, PD-207, PD-208, Multi Family-One (MF-1), Multi Family-Two (MF-2), Commercial (C), Light Industrial (LI), and Heavy Industrial (HI), to a Planned Development for Mixed Uses including Entertainment, Retail, Office Uses and a Specific Use Permit for a car dealership; and

WHEREAS, Notice was given of a further public hearing to be held by the City Council of the City of Grand Prairie, Texas, in the City Hall Plaza Building at 7:30 o'clock P.M. on September 17, 1997 to consider the advisability of amending the Zoning Ordinance and Map as recommended by the Planning and Zoning Commission, and all citizens and parties at interest were notified that they would have an opportunity to be heard, such Notice of the time and place of such hearing having been given at least fifteen (15) days prior to such hearing by publication in the Grand Prairie Daily News, Grand Prairie, Texas, a newspaper of general circulation in such municipality; and

WHEREAS, all citizens and parties at interest have been given an opportunity to be heard on all the matter of the proposed rezoning and the City Council of the City of Grand Prairie, Texas, being informed as to the location and nature of the use proposed on said property, as well as the nature and usability of surrounding property, have found and determined that the property in question, as well as other property within the city limits of the City of Grand Prairie, Texas, has changed in character since the enactment of the original Zoning Ordinance from the classification of PD-39, PD-70, PD-78, PD-165, PD-170, PD-207, PD-208, Multi Family-One (MF-1), Multi Family-Two (MF-2), Commercial (C), Light Industrial (LI), and Heavy Industrial (HI), and, by reason of changed conditions, does consider and find that this amendatory Ordinance should be enacted since its provisions are in the public interest and will promote the health, safety and welfare of the community.

NOW, THEREFORE, BE IT ORDAINED BY THE CITY COUNCIL OF THE CITY OF GRAND PRAIRIE, TEXAS:

I.

That Ordinance Number 4779, being the Zoning Ordinance and Map of the City of Grand Prairie, Texas, showing the locations and boundaries of certain districts, and said Zoning Ordinance and Map having been made a part of an Ordinance entitled:

"AN ORDINANCE AMENDING IN ITS ENTIRETY CHAPTER 28 OF THE CODE OF ORDINANCES KNOWN AS THE ZONING ORDINANCE OF THE CITY OF GRAND PRAIRIE, TEXAS, AS PASSED AND APPROVED BY THE CITY COUNCIL ON THE 20TH DAY OF NOVEMBER, 1990, TOGETHER WITH ALL AMENDMENTS THERETO AND ENACTING A REVISED ORDINANCE ESTABLISHING AND PROVIDING FOR ZONING REGULATIONS; CREATING USE DISTRICTS IN ACCORDANCE WITH A COMPREHENSIVE PLAN..."

and passed and approved November 20, 1990, as amended, is hereby further amended so as to rezone the following area as shown and described in Exhibit "A" from PD-39, PD-70, PD-78, PD-165, PD-170, PD-207, PD-208, Multi Family-One (MF-1), Multi Family-Two (MF-2), Commercial (C), Light Industrial (LI), and Heavy Industrial (HI), to a Planned Development for Mixed Uses including Entertainment, Retail, Office Uses and a Specific Use Permit for a car dealership:

See Exhibit "A"

II.

All development must comply with the development requirements as shown as shown in Exhibit "B".

III.

These regulations were developed as a result of a study and hereby incorporated in Exhibit "C".


IV.

All ordinances or parts of ordinances in conflict herewith are specifically repealed.

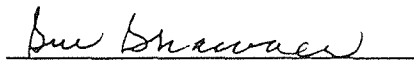
V.

That this Ordinance shall be in full force and effect from and after its passage and approval.


PASSED AND APPROVED BY THE CITY COUNCIL OF THE CITY OF GRAND PRAIRIE, TEXAS, this the 17th day of September, 1996.


MAYOR
City of Grand Prairie, Texas

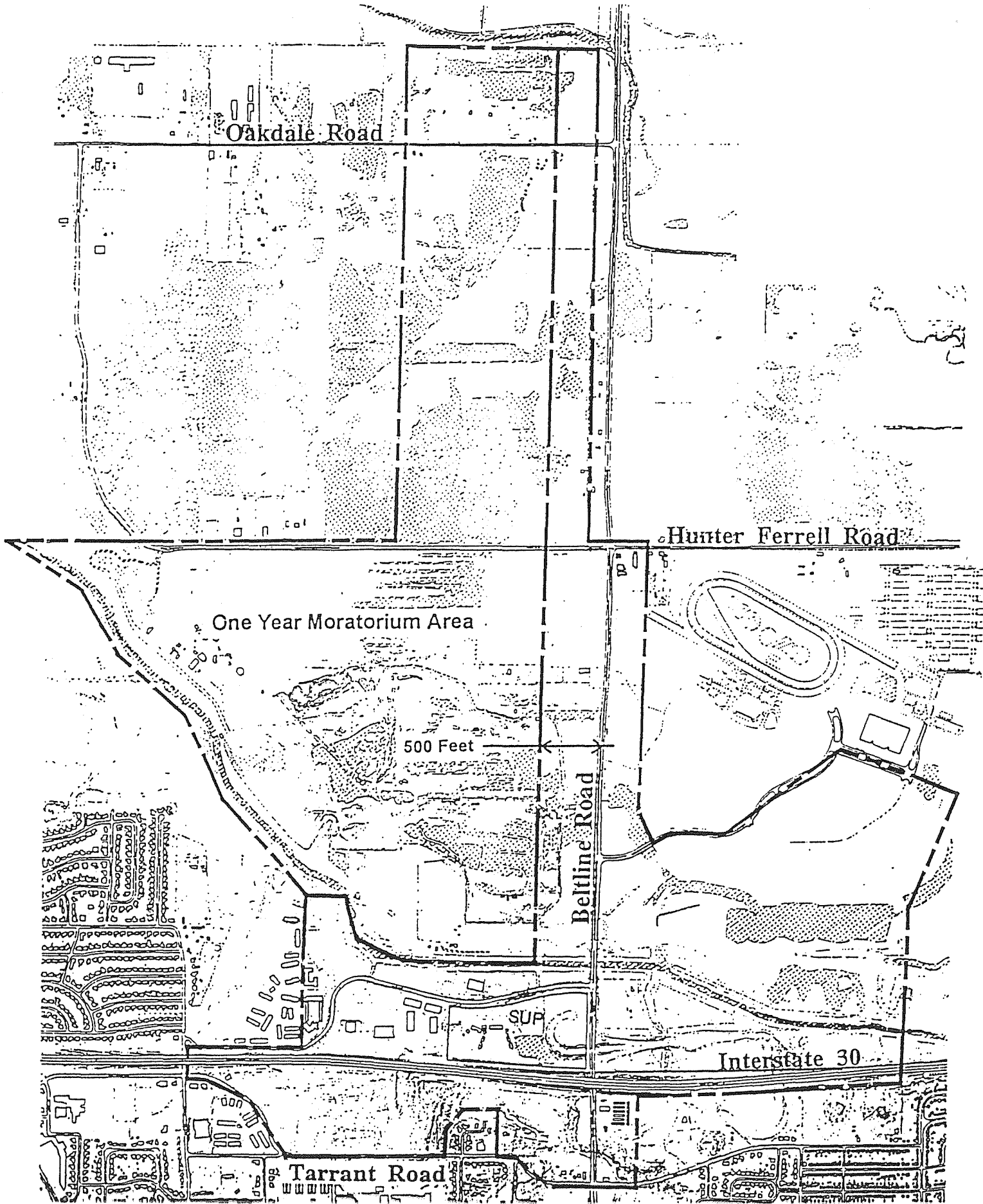
ATTEST:


City Secretary

APPROVED AS TO FORM AND LEGALITY:


City Attorney

Zoning Case No. CPA960901/Z960901



DISTRICT BOUNDARY

Exhibit "A"

Draft Zoning Ordinance for BELTLINE CORRIDOR

Section 1 Intent

The intent of the creation of the Beltline Corridor District is to provide support for the development of a unified area with a distinct identity as a family oriented recreational destination within the City of Grand Prairie and the Metroplex. It is intended to blend with the Lone Star Park at Grand Prairie race track and the flood plain, open space and water features resulting from the Trinity River to create a recreational and equestrian centered park-like environment.

Section 2 Land Use

2.1 Permitted Land Uses

Land Uses in the District shall conform to the permitted list of uses in the attached Land Use Chart.

[Land Use List]

2.2 Non-Conforming Land Uses

Non-conforming land uses buildings and structures shall comply with Article XIX of the UDC.

Section 3 Development Standards

3.1 General

- A. Utilities. All utilities shall be placed below ground, except for major high voltage transmission lines.
- B. Parking and Driveway Areas. All parking, driveway, loading and vehicular circulation areas shall be constructed of concrete.

3.2 Dimensional Requirements

Minimum development dimensional requirements shall be as follows:

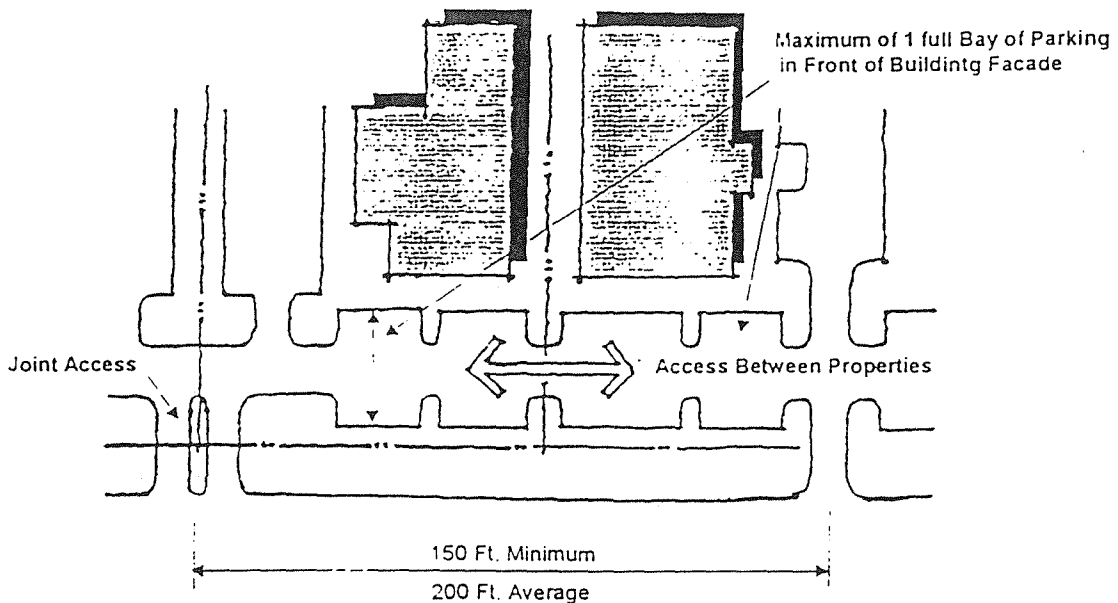
<i>Site Area</i>	15,000 s.f.
<i>Lot Width</i>	150 Feet
<i>Lot Depth</i>	100 Feet
<i>Front Yard</i>	25 Feet
<i>Rear Yard</i>	25 Feet, 0 Feet in Adjacent to a Preserved Flood Plain or Open Space

<i>Side Yard</i>	5 Feet; or 0 Feet if separately platted buildings are built as one building; or 20 Feet if Adjacent to a Residential Zone
<i>Between Buildings</i>	20 Feet
<i>Maximum Height</i>	Unlimited, except must be setback 2 feet for each 1 foot in height from a lot in a residential zoning district.
<i>Maximum Floor Area</i>	1:1 FAR
<i>Minimum Required Landscaping</i>	20%, except may be reduced to 15% with the provision of trees. see 3.4.D below.

3.3 Site Layout

- A. Location of Parking. A maximum of 1 bay of parking (approximately 64 feet plus landscaping) shall be allowed between the front building face and the front property lines. Any remaining parking shall be provided beside the building or to the rear of the building.
- B. Circulation Between Development Sites. Connections between development parcels shall be required in order to provide all development sites with street access, in order to meet the standards in this section and to smooth the flow of traffic both on site and on the public roadways. Joint access agreements between development sites shall be provided.

FIGURE 1: SITE LAYOUT



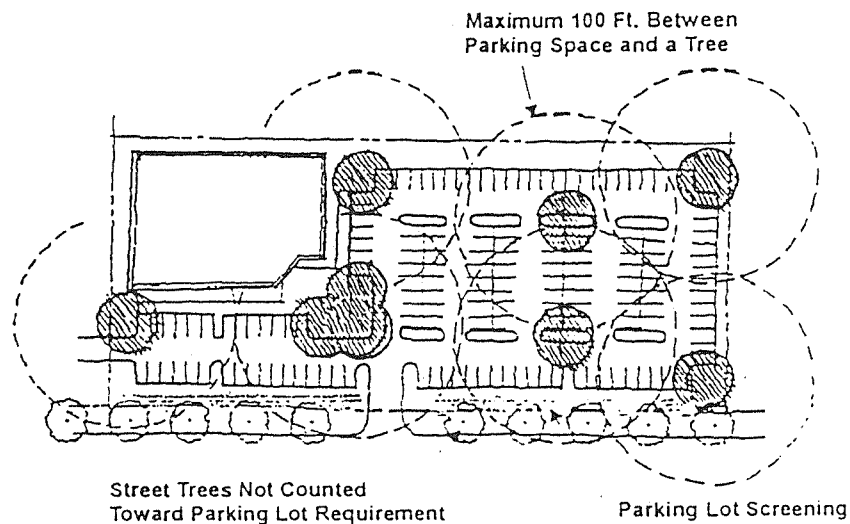
3.4 Landscaping

A. Intent. In order to ensure the attractiveness of the Beltline Corridor District as a place for visitors throughout the year, it is the intent of this sub-section to build a park-like environment by securing street tree planting, low level parking screening, a general parking lot canopy of trees, and a minimum amount of landscaped area on site.

B. Parking Lots

1. Tree Canopy. One tree shall be provided for each 20 parking spaces within the parking lot area, however, no car parking space shall be located greater than 100 feet from the center of a tree. Trees shall be a minimum of 3 inch caliper and planted within a planting island with a minimum dimension of 5 feet. The tree planting island must be further planted with ground cover, grass or shrubs.

FIGURE 2: PARKING LOT LANDSCAPING



2. Screening of Parking. Parking areas shall be screened from the adjacent roadway by a minimum 3 foot high solid shrub hedge, berm, fence or some combination of these. The height shall be measured from the surface of the subject parcel at the front property line. In no case shall the slope of a berm exceed 3:1 unless it is being retained on the private property side of the berm. Railroad ties may not be used for retaining.

However, a minimum 3 foot high berm shall be used along both sides of Beltline Road as the screening method along this roadway, unless prevented due to unique site conditions. The berm may be located partially in the street right-of-way and partially in the Beltline Landscape Buffer (See "D" below).

3. **Parking lot Materials.** No railroad tie type material shall be used in parking areas.

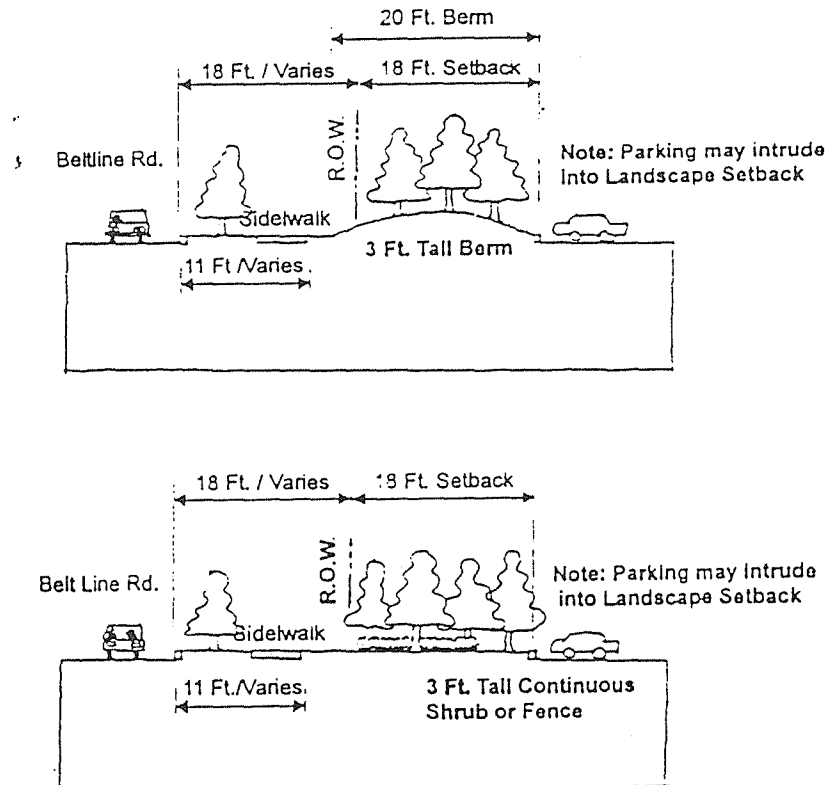
C. Streetscaping

1. **Street Trees.** A minimum of 1 tree shall be planted for each 40 linear feet of frontage.
 - a. The minimum caliper for street trees on each development parcel shall be 3 inches.
 - b. All trees intended to meet this requirement shall be a species approved by the City for the intended use, as specified in Section 29, Article VIII of the UDC.
2. **Placement of Trees.** All required street trees shall be placed between the future roadway curb line and the parking area or front building line, whichever is closest to the street right-of-way. However, trees shall not be planted closer than 3 feet to the curb line.
3. **Sidewalks.** Sidewalks shall be placed on both sides of the street and shall be a minimum of 5 feet in width.

D. Site Landscape Area

1. **Minimum Landscape Area.** Each developed site in the district shall maintain a minimum of 20 % of the site in landscaping. However, the minimum landscape area may be reduced to 15% of the site by the planting of additional trees as provided for in "2. Reduction of Landscape Area" below.
2. **Reduction of Landscape Area.** The amount of required landscaping may be reduced by 120 s.f. for each 3 inch (average) caliper shade tree which is planted. This does not include the trees which are required for parking areas or for streetscaping. However—
 - a. Up to 50% of the trees provided under this provision may be added to the parking lot or roadway landscaping areas.
 - b. Two ornamental trees may be substituted for one shade tree.
3. **Definition of Landscape Area.** Landscape areas shall include:
 - a. All natural and planted areas
 - b. Outdoor plazas and walkways which are not used for commercial purposes, and which are constructed of enhanced materials such as stamped concrete or unit pavers.
4. **Beltline Landscape Buffer.** A landscape buffer of 18 feet shall be provided along both sides of Beltline Road. However, parking may intrude into that buffer area by up to 8 feet, provided that the berm height is not reduced.

FIGURE 3: SECTION OF BELTLINE ROAD AT PROPERTY LINE

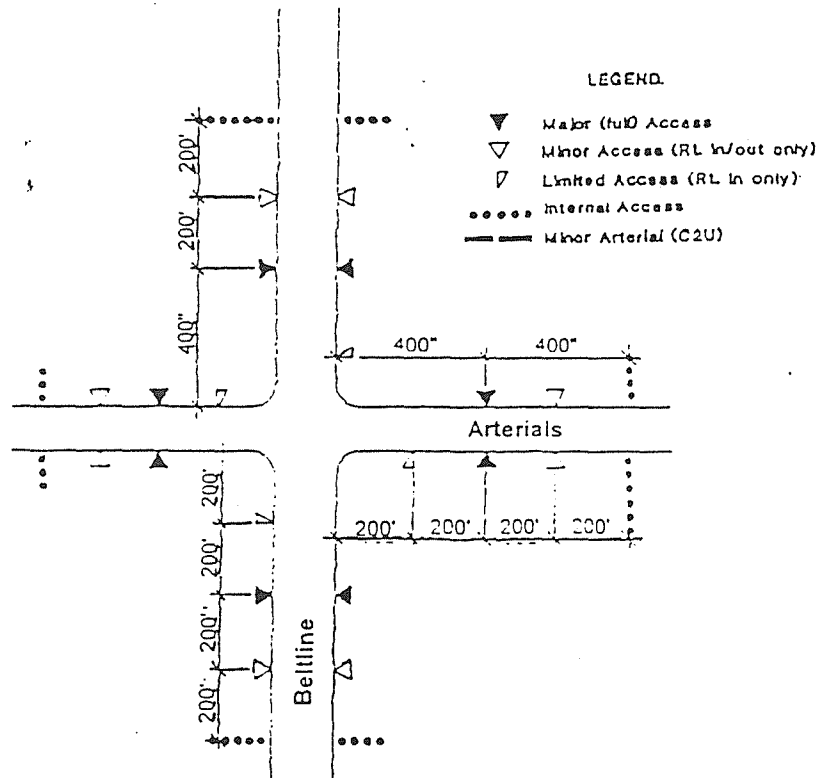


5. Landscape Buffer on Other Streets. A landscape buffer of 10 feet shall be provided along both sides of all other public streets in the District.
6. Boulevard Landscaping. The boulevard (the area between the outside curb edge and the private property line) shall be landscaped, sprinklered and maintained by the property owner.
7. Other Standards. Other landscape buffering, materials and standards shall conform to Article VIII of the Unified Development Code unless otherwise specifically established in this ordinance, or approved as a variance an approved Site Plan.

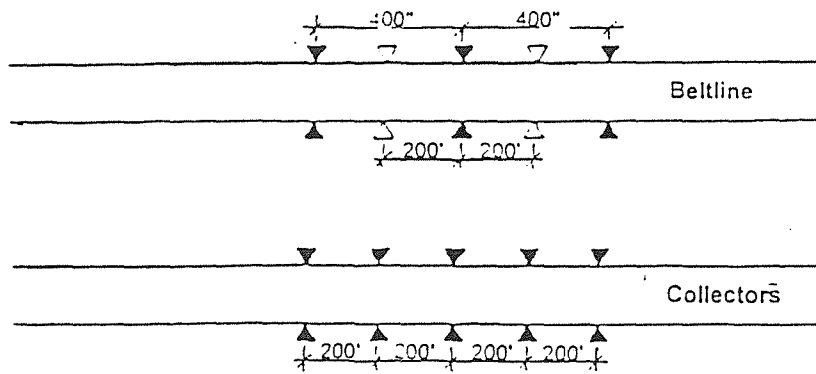
3.5 Site Access

- A. Site Access and Curb Cuts. Site access and curb cuts shall generally meet the standards for Beltline and other streets established in the attached diagram of Site Access Standards. One curb cut shall be allowed for each 200 feet of frontage, but curb cuts may not be closer than 150 feet (center line to center line).

FIGURE 4: SITE ACCESS STANDARDS



TYPICAL INTERSECTION ACCESS



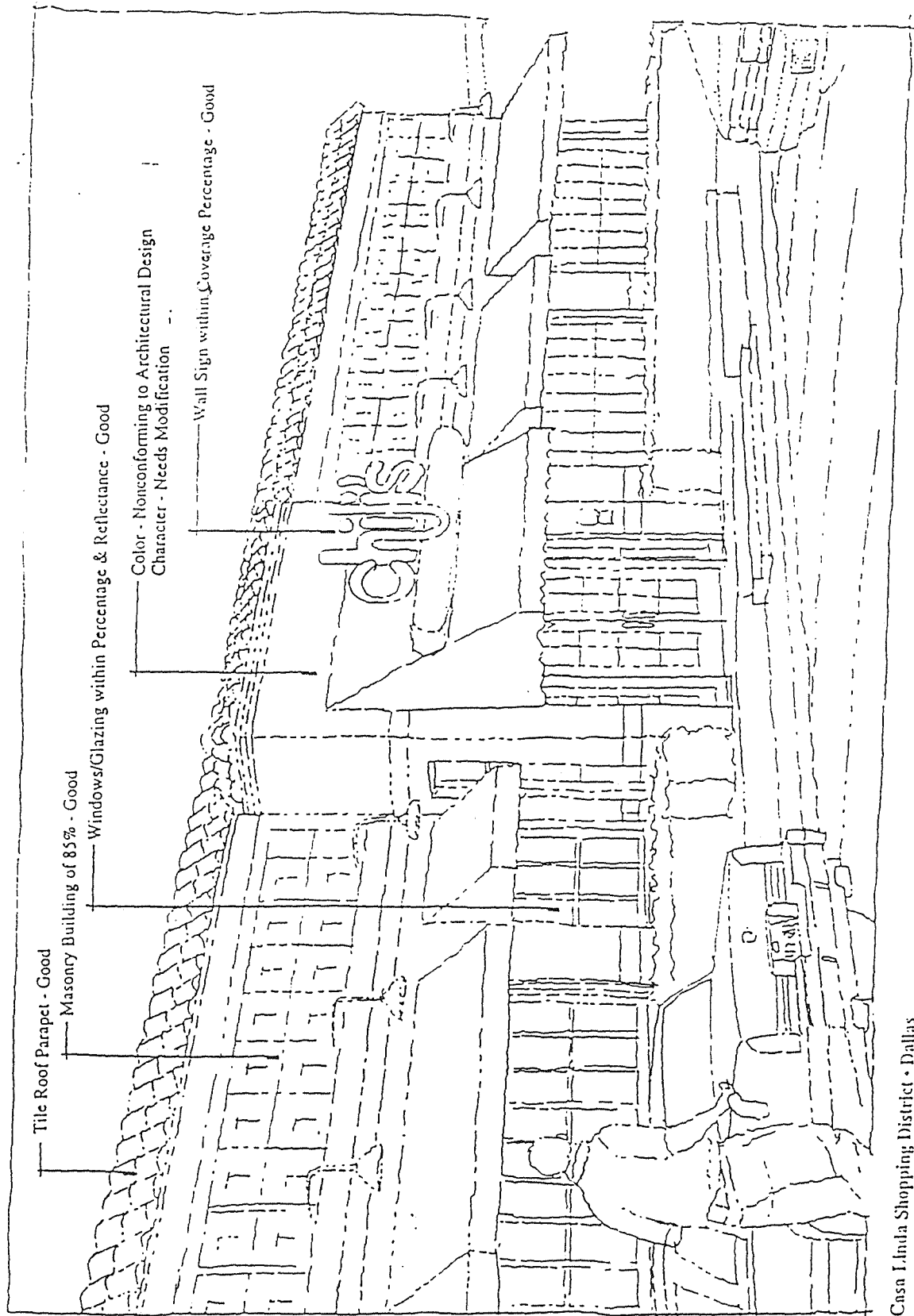
TYPICAL BETWEEN ARTERIAL INTERSECTIONS

- B. **Shared Driveway Access.** The use of shared driveway access shall be required where necessary to ensure adequate street access to development parcels, using cross-access agreements.
- C. **Site Plan Approval for Site Access.** Where unique circumstances exist, the City Council may approve additional driveways as part of the Site Plan approval process.

3.6 Architectural Standards

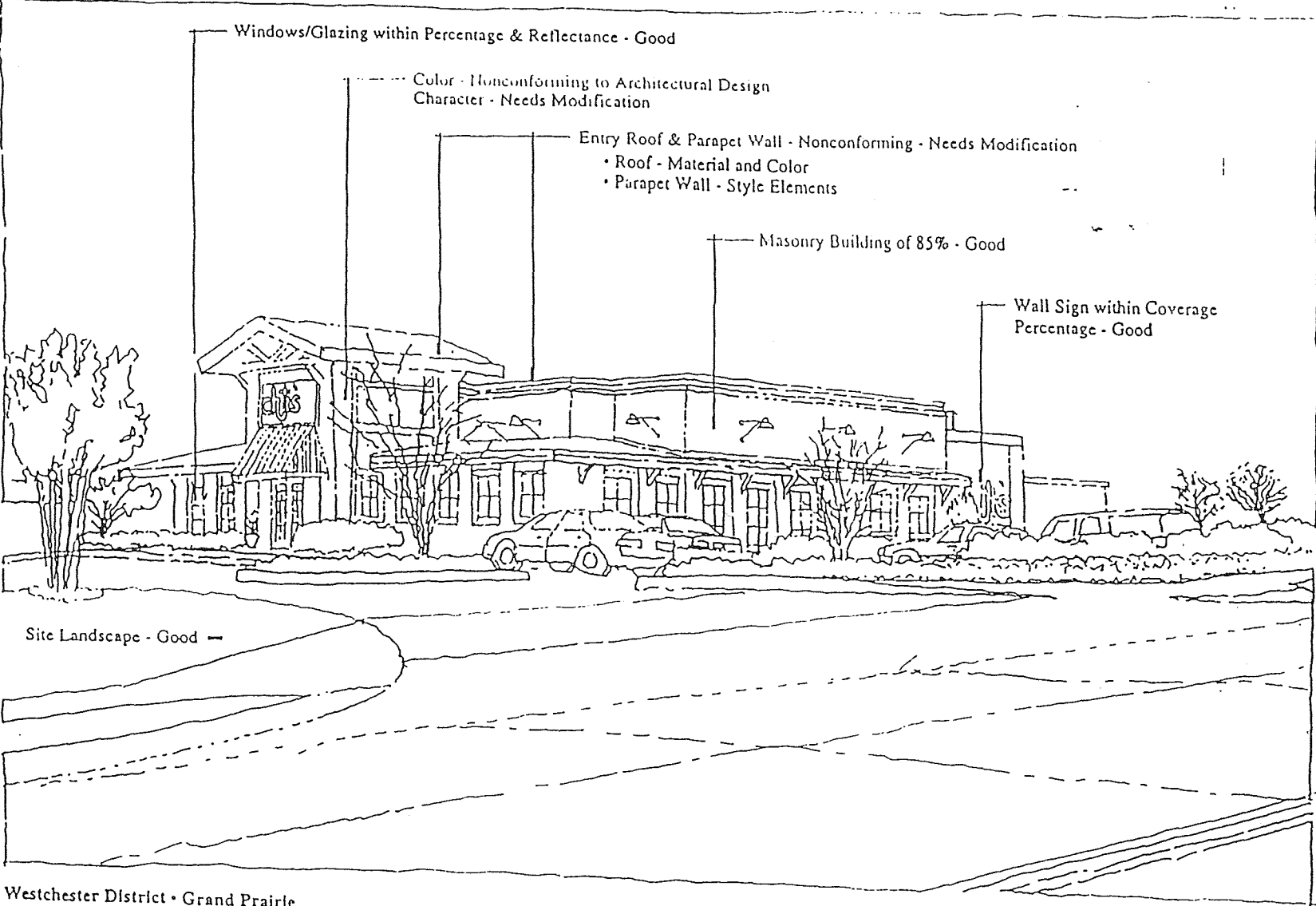
- A. **Intent.** The objective of this sub-section is to ensure that the district is unified through the use of very flexible architectural standards which are based on the theme or architectural style of Spanish Revival which has been established at the Lone Star Park at Grand Prairie Race Track. The standards are intended to provide for a wide variety of development designs within the framework established under this section.
- B. **Architectural Materials.** Standards for building materials are established in order to ensure that unity of the district is achieved and that buildings in the district shall not require an excessive amount of maintenance.
 - 1. **Materials.** At least 85% of exterior walls of buildings in the district shall be comprised of stone, cast stone, "split-face" concrete block, brick, stucco or glass (with a maximum reflectance of less than 15%—e.g. no noticeably reflective coatings). In no case shall glass exceed greater than 50% of the building facade. However, an alternative building surface material may be specifically approved as part of the Site Plan if it is consistent with the style objectives of the district.
 - 2. **Color.** The primary exterior color of buildings within the district shall be comprised of pastel earth tones and shall be used on at least 85% of the building facade (except glass area). (Samples of the allowed primary exterior color range shall be kept on file with the Department of Planning.) Secondary accent color may be used on up to 15% of the exterior facade. Colors must be specifically approved on building elevations submitted as part of the Site Plan.
- C. **Architectural Style Elements.** The architectural style elements in the attached Diagram of Style Elements reflect the Spanish Revival and Mission Styles. These style elements provide a pallet of architectural elements that would be considered conforming to the intent of this District.
 - 1. **Conforming Architectural Design.** A Site Plan where buildings on the site contain elements which are similar to the elements in the attached Diagram of Style Elements for the roofs, front door

FIGURE 6: EXAMPLE A—CHILLI'S CASA I INDA



Casa I, India Shopping District • Dallas

FIGURE 7: EXAMPLE B—CHILI'S WESTCHESTER



and windows visible from the street shall be considered conforming to the objectives of the district. Site Plans which are deemed conforming, as interpreted by the Director of Planning and the Director of Public Works, may be approved administratively.

2. **Non-Conforming Architectural Design.** Site Plans where the primary elements of the site and buildings are not consistent with the required landscaping, building materials or the attached Diagram of Style Elements, as interpreted by the Director of Planning and the Director of Public Works, shall require approval of the City Council with a recommendation by the Planning and Zoning Commission.

Section 4 Signage

Signs within the Beltline Corridor District shall adhere to the sign standards in Article IX of the UDC except that no signage except for the following, shall be allowed:

4.1 General

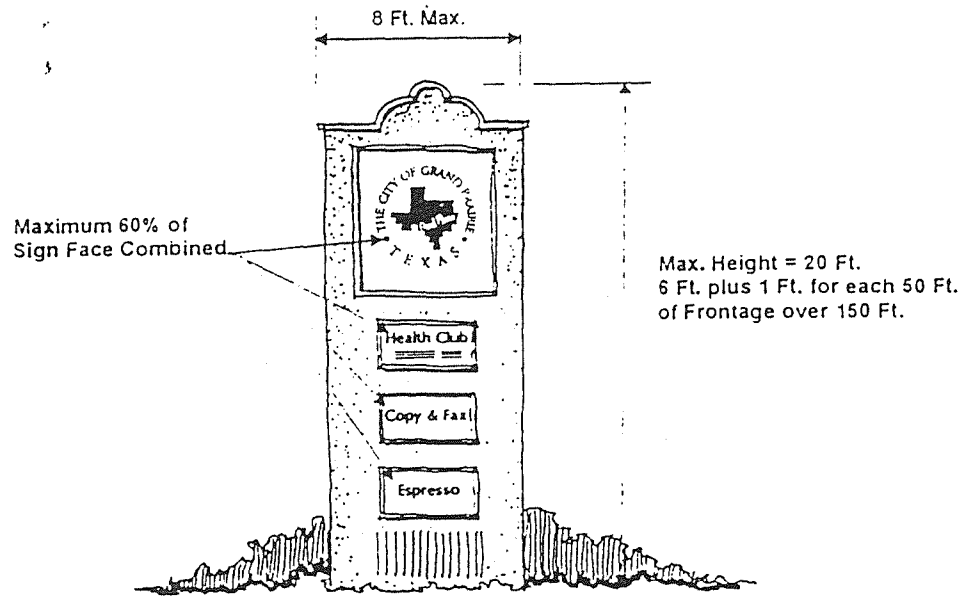
- A. **Permit Required.** A permit shall be required for all signs.
- B. **On-Site Signage.** Only on-site signage shall be allowed. Signs located anywhere in the District which identify the District or major facilities (greater than 100,000 s.f. in floor area and 100 acres in site area) which are located within the District shall be considered on-site signs.
- C. **Circulation Visibility.** Signs shall not obstruct the visibility of traffic entering or leaving the public roadway.
- D. **Conformity with Design Standards.** All sign designs shall be submitted as part of the Site Plan and shall be reviewed for conformity with the design and theme objectives of the District.

4.2 Monument Signs

- A. **Size.** A monument sign may be 6 feet in height and 8 feet in width, with the height increasing by 1 foot for each 50 feet beyond 150 feet of site frontage up to a maximum of 20 feet in height.
- B. **Sign Area.** A maximum of 60% of the monument sign structure face may be utilized to identify the businesses or activities on that site.
- C. **Identification.** Monument Signs may only list the project name and tenants or activities. Where a site contains multiple tenants, the names of the main tenants and or activities should be listed on the monument sign.

- D. Sign Location. Monument signs must be located in a landscape setting.

FIGURE 8: MONUMENT SIGN



- E. Combined Signs. A series of contiguous properties may combine signage into a single monument sign where the properties have been planned as a unified development. However, each individual property would not be allowed their own individual monument sign in addition to the combined sign.
- F. Number of Signs. Each lot may have one Monument Sign. However, a multi-tenant complex comprised of more than one building, shall be permitted one sign per building, provided that buildings separated by a distance of less than 60 feet shall be considered a single building, and provided that all signs meet the spacing requirements below.
- G. Spacing of Signs. Monument Signs must be spaced a minimum of 150 ft. apart, and 60 feet from another lot.
- H. Changeable Messages. Signs may include electronically changeable messages which identify special events on the site or in the district, and which identify tenants or activities on site.

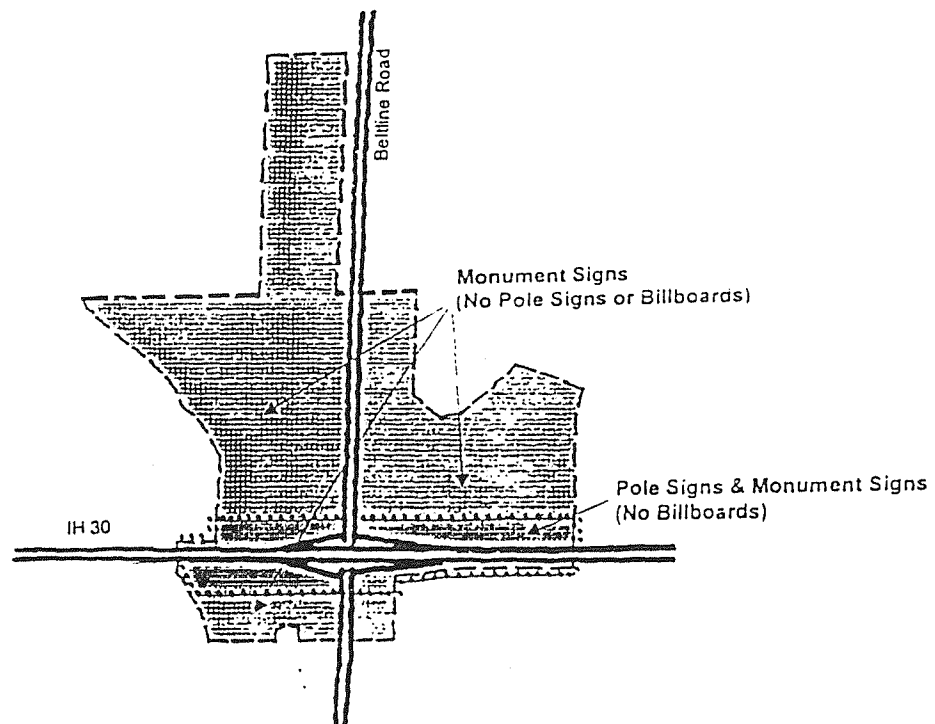
4.3 Wall Signs

- A. On-Site Signage. Only on-site signage shall be allowed.
- B. Sign Area. The maximum area of all wall signs combined on one building elevation shall not exceed 3% of the total area of that elevation.
- C. Wall Signs. Wall signs may include Canopy Signs, and Flat Wall/Facia Signs.

4.4 Pole Signs

- A. Location. Pole signs are prohibited in the District except that they shall be permitted within 50 feet of the IH 30 right-of-way.
- B. Size. No Pole Sign shall exceed 300 s.f. of sign area on each of up to 2 faces.
- C. Height. No Pole Sign shall exceed 50 feet in height from the grade of the adjacent public road.

FIGURE 9: SIGN TYPE LOCATION



4.5 Other Permitted Signs

- A. Officially sponsored flags, banners and insignias

- B. Banner Signs advertising only District special events or the District, provided that they do not exceed 30 days once per year. This includes "Opening" signs.
- C. Construction Signs

4.6 Prohibited Signs

The following signs are specifically prohibited:

- a) Portable Signs
- b) Window Signs
- c) Roof signs
- d) Pole Signs, Except as permitted by this Section
- e) Billboards

Section 5 Review Process

- A. **Site Plan Required.** Prior to issuance of a Building Permit, a Site Plan shall be approved, either as a separate application or as part of the Building Permit Application.
- B. **Content of Site Plan.** The Site Plan shall include such things as structures, streets and sidewalks, Off-street parking and loading, landscaping, and drainage, as required in Section VII.1.7 of the UDC. In addition, Site Plan documentation shall include building elevations specifying surface materials and color, and signage.
- C. **Review Process.** Site Plan review shall generally follow the procedure established for Site Plan approval for planned development districts.
 - 1. Chief Building Official and the Planning Director shall approve the Site Plan, or it may be forwarded to City Council, if there substantial variances with the intent of the Beltline Corridor District, or at the discretion of either Official.
 - 2. Any Site Plan may be approved, or if it does not meet the standards, it may be denied or approved with conditions. A Site Plan may not be denied on the basis of land use, if the proposed use is permitted in the District.
 - 3. The imposition of a condition or a denial by the Building Official and Planning Director may be appealed to City Council.
- D. **Content of Review.** Site Plans shall be reviewed to ensure the following
 - 1. Compliance with the intent of this zoning ordinance, including landscaping, facade materials, architectural style elements, screening regulations, setbacks, land use regulations, signage, parking, and other standards.

AGRICULTURAL USES

Orchard
 Greenhouse (Non-Retail/Hobby)
 Greenhouse (Retail) --
 Plant Nursery (Growing)
 Plant Nursery (Retail Sales)
 Farms, General (Crops)
 Farms, General (Livestock/Ranch)
 Hay, Grain, and/or Feed Sales
 Veterinarian (Indoor Kennels)
 Veterinarian (Outdoor Kennels)
 Stables (Private, Principle Use)
 Stables (Private, Accessory Use)
 Animal Specialty Services,
 Except Veterinary
 Livestock Sales
 Kennels

RESIDENTIAL USES

Single Family Townhouse	SUP
Multi-Family (Apartments)	SUP
Accessory Building/Structure	
Home Occupation	
College Dormitory (On Campus)	

INSTITUTIONAL / GOVERNMENTAL

Emergency Ambulance Service	
Post Office (Governmental)	
Mailing Service (Private)	
Airport	
Heliport	
Helistop	
Phone Exchange/Switching Station	
Radio/Television Tower (Commercial)	SUP
Cellular Communications Tower	SUP
Cellular Communications Equipment - No Tower	
Electrical Substation	SUP
Electrical Transmission Line	
Gas Transmission Line	
Utility Distribution Line	
Utility Shop and Storage	

INSTITUTIONAL / GOVERNMENTAL (Cont.)

Water Treatment Plant
Water Supply Facility (Public)
Water Supply Facility (Private)
Sewage Pumping Station
Retirement Home/Home for the Aged SUP
Hospice
Hospital
Psychiatric Hospital
Clinic
Emergency Care Clinic
School, K thru 12 (Public)
School, K thru 12 (Private)
School, Vocational
College and/or University
Registered Family Home
Child Day Care (7 or more)
Orphanage
Community Center
Fraternal Organization
Civic Club
Philanthropic Organization
Church/Place of Worship
Use Associated to a Religious Institution
Rectory/Parsonage
Monastery/Convent
Governmental Building
Police Station
Fire Station
Library

OFFICE USES

Credit Agency
Office (Brokerage Service)
Insurance Agency Offices
Real Estate Offices
Offices (Health Services)
Offices (Legal Services)
Offices (Counseling)
Offices (Miscellaneous)

PERSONAL AND BUSINESS SERVICES

Bank
 Financial Services (Advice/Invest)
 Savings and Loans --
 Credit Unions
 Automatic Teller Machines (ATM's)
 Apartell Inns
 Hotel/Motel
 Bed & Breakfast Hotel ;
 Laundry/Dry Cleaning (Drop Off/Pick Up)
 Tailor Shop
 Shoe Repair
 Travel Agency
 Beauty Shop (Non-College)
 Barber Shop (Non-College)
 Photo Studio
 Kiosk (Providing A Service)
 Photocopying/Duplicating
 Security Quarters as Associated with
 A Business (Live-In)
 Auction (Indoors)
 Appliance Repair
 Communication Equipment
 (Installation and/or Repair)
 Locksmith
 Karate School (Martial Arts)
 Automobile Driving School
 Dance/Drama/Music Schools
 (Performing Arts)
 Artist Studio

RETAIL USES

Building Material Sales	SUP
Hardware Store	
Garden Shop (Inside Storage)	
Plant Nursery (Outside Storage)	
Department Store	
Variety Store	
Convenience Store	SUP
Retail Store with Gasoline as an Associated Use	
Fruit and/or Vegetable Store	
Confectionery Store (Retail)	

RETAIL USES (Cont.)

Bakery (Retail)	
Auto Supply Store for New and Primarily Rebuilt Parts	
Gasoline Service Station	
Auto Dealer (Primarily New)	SUP
Boat Dealer/Sales Only	SUP
Recreational Vehicle Dealer/Sales Only	SUP
Motorcycle Dealer	SUP
All Terrain Vehicle Dealer/Sales Only	SUP
Furniture Sales (Indoor)	
Drapery Shop	
Major Appliance Sales (Indoor)	
Used Merchandise (Antiques)	
Restaurant (Serving Alcohol) (No Entertainment)	
Restaurant (Serving Alcohol) (With Entertainment)	SUP
Restaurant	
Restaurant (Drive-In)	
Restaurant (Drive-Thru)	
Restaurant (Kiosk)	
Private Club (Alcohol Served)	
Alcohol Sales/Off-Premise Consumption	
Bike Sales and/or Repair	
Handicraft Shop	
Needlework Shop	
Florist	
Art Dealer	
Pet Shop	
Retail Store (Miscellaneous)	

TRANSPORTATION AND AUTO SERVICES

Railroad Right-of-Way
Passenger Terminal
Limousine Service
Taxi Stand
Accessory Parking
Commercial Parking Lot (Cars)
Recreational Vehicle Parking Lot
Auto Repair as an Associated Use to Retail Sales

WHOLESALE TRADE

Warehouse/Storage (Inside) SUP
 Paper and/or Paper Products (Wholesale) SUP

MANUFACTURING MINING AND CONSTRUCTION

Contractor's On-Site Construction Office (Approval by Chief Building Official)
 Batching Plant (Temporary) (By Resolution of City Council)

AMUSEMENT AND RECREATIONAL SERVICES

Boat Launching Ramp
 Marina
 Fishing Pier
 Radio Station (without Tower)
 Television Station (without Tower)
 Dinner Theatre
 Motion Picture Theater (Indoors)
 Motion Picture Theater (Outdoors)
 Theater (Non-Motion Picture)
 Amusement Services (Indoors)
 Amusement Services (Outdoors) SUP
 Bowling Center
 Health Club (Physical Fitness)
 Amusement Devices/Arcade
 (Four or more devices)
 Billiard Parlor (Three or more tables)
 Skating Rink
 Exhibition Hall
 Museum / Wax Museum
 Art Gallery
 Stadium
 Membership Sports
 Golf Course (Public/Private)
 Recreational Club (Members Only)
 Swimming Pool (Public)
 Day Camp
 Park and/or Playground
 Fairground
 Zoo
 Earth Satellite Dish (Private)
 Earth Satellite Dish (Public)
 Non-Commercial Radio Tower
 Less Than 65 Feet High

AMUSEMENT AND RECREATIONAL SERVICES (Cont.)

Special Events (Temporary)

Bingo Parlor

SUP

2. Compliance with other codes and ordinances established by the City of Grand Prairie.

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BELTLINE ORD

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BELTLINE CORRIDOR CONCEPT PLAN

Background

This Concept Plan was prepared in conjunction with a zoning ordinance for the area identified in a 90 day building moratorium imposed on June 18, 1996. The intent of the moratorium was to facilitate a change in land uses, and to ensure that when development takes place, it will contribute to the long term success of the area.

This plan provides the context for preparation of the zoning ordinance. It does not reflect a thorough study of the market, or a detailed urban design plan; but it endeavors to enunciate a clear and reasonable concept for how the area should develop.

Creation of the District

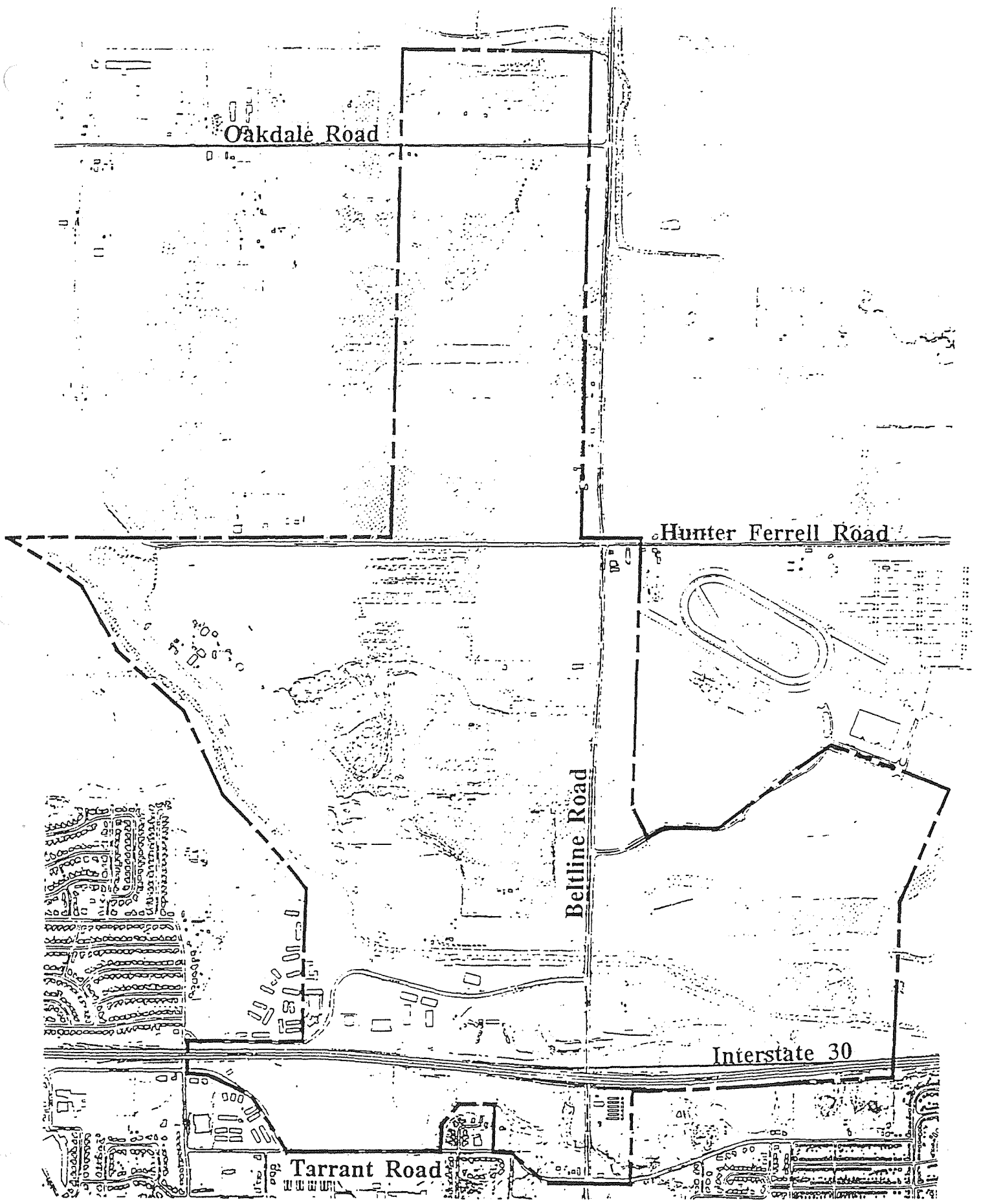
The Beltline Corridor has been identified by City Council as a special district within the City of Grand Prairie. It involves major investments by the City for economic development in both the creation of the Lone Star Park at Grand Prairie Race Track and the planning and manipulation of flood plain lands to create developable sites. A comprehensive and unified conceptual development plan, and zoning and development regulations will aid in protecting investments by the City and by property owners and new businesses in the district.

Background Issues and Opportunities

Access. Currently, the area has access mainly from Beltline Road and IH 30. Plans for the area, however, include SH 161, Trinity Parkway and the Proposed Trinity Tollway. SH 161, when constructed, will greatly increase north-south access to the site, especially where it will intersect with the Trinity Parkway, and the Proposed Trinity Tollway. SH 161 will be located on the westerly boundary of the study area. The Trinity Parkway could provide additional east-west connections to DFW Airport, and the proposed Trinity Tollway, if implemented, will provide excellent regional east-west access to the study area.

Flood Plain. Over 75% of the study area is currently in Trinity River flood plain. A joint study of the development potential along this corridor, with the City of Irving, is underway to determine the amount of land that may be reclaimed and a range of land uses along this area. A preliminary estimate of the amount of land which could be reclaimed includes a little under 50% of the land in the flood plain, excluding the floodway areas of the Trinity and its tributaries.

Major Features. The study area is covered by large stands of mature trees and grass lands, providing the feeling of being "out in the country". In addition, the area includes the Lone Star Park at Grand Prairie, the House of Wax/Ripley's believe it or Not—two major attractions for visitors to Grand Prairie. The



Oakdale Road

Hunter Ferrell Road

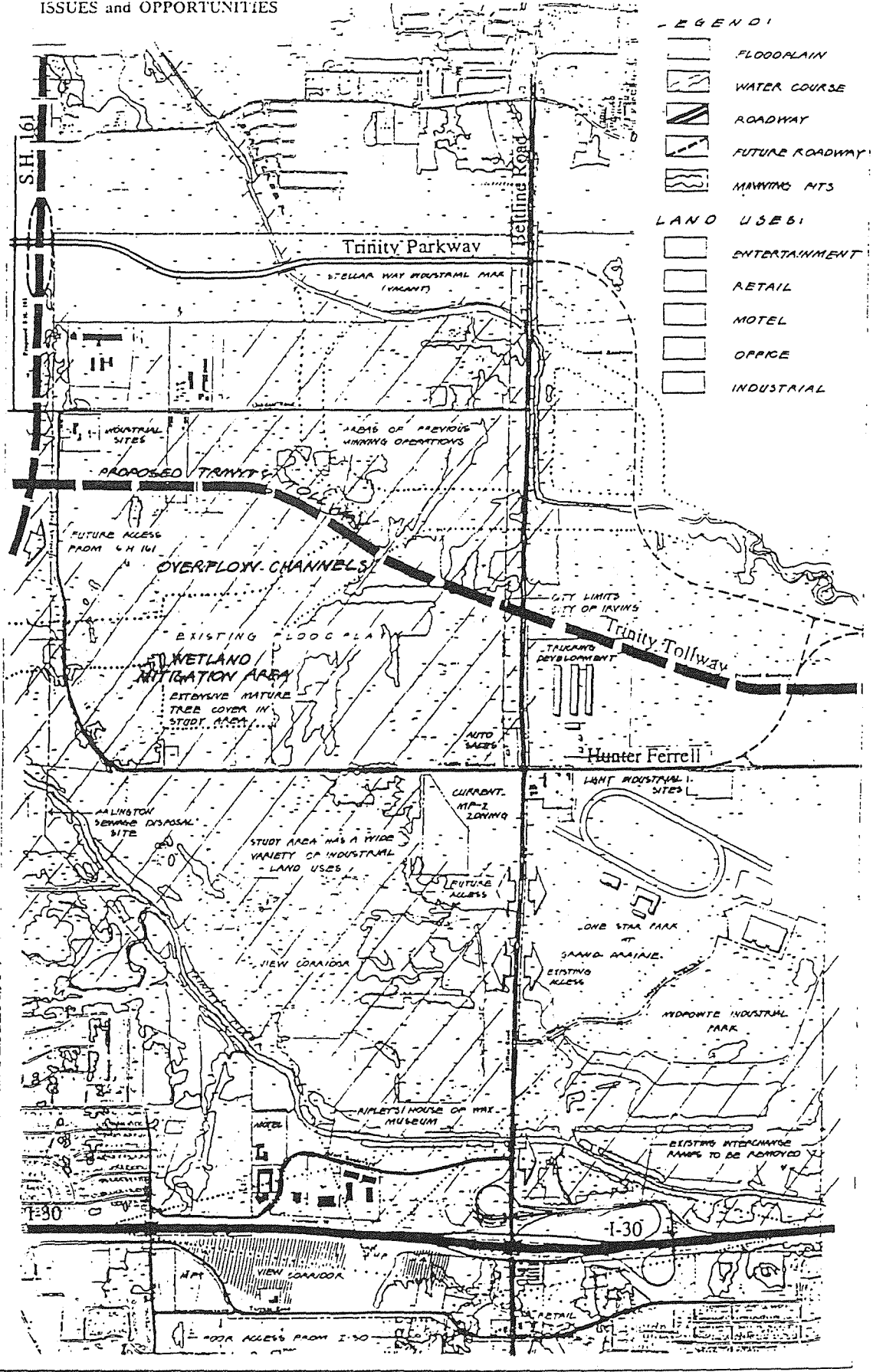
Beltline Road

Interstate 30

Tarrant Road

DISTRICT BOUNDARY

PD-217

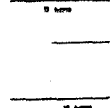


The Beltline Corridor District
Grand Prairie, Texas

J. DENNIS WILSON & ASSOCIATES
PLANNERS ARCHITECTS ENGINEERS



Scale 1" = 100' Date: June 1998



Simulcast building at Lone Star Park is currently in operation, and the race track should be operational by the end of this year.

District Concept

Area Resources. The area includes two major resources that can be built upon to create an exceptional district—Lone Star Park and the Trinity River (with its requirements for flood plain and valley storage areas). Lone Star Park at Grand Prairie can provide a style theme for development, and the flood plain, if properly configured, can provide major amenities for development sites.

Family-Oriented Entertainment and Recreation. The district should develop as a unified area with a distinct identity as a family oriented recreational destination within the Metroplex and the Southwestern United States. The district character should build on the Lone Star Park at Grand Prairie Race Track which will be a world class horse racing operation that will be known the world over; thereby establishing an identity for the area. In addition, the large amount of open space preserved as recreation areas and flood plain will establish a “park-like” environment.

Land Use. The attached Concept Plan places retail, entertainment and restaurant land uses along Beltline Road, Safari Pkwy and Tarrant Road. Other land uses in the district could include multifamily, office and light industrial.

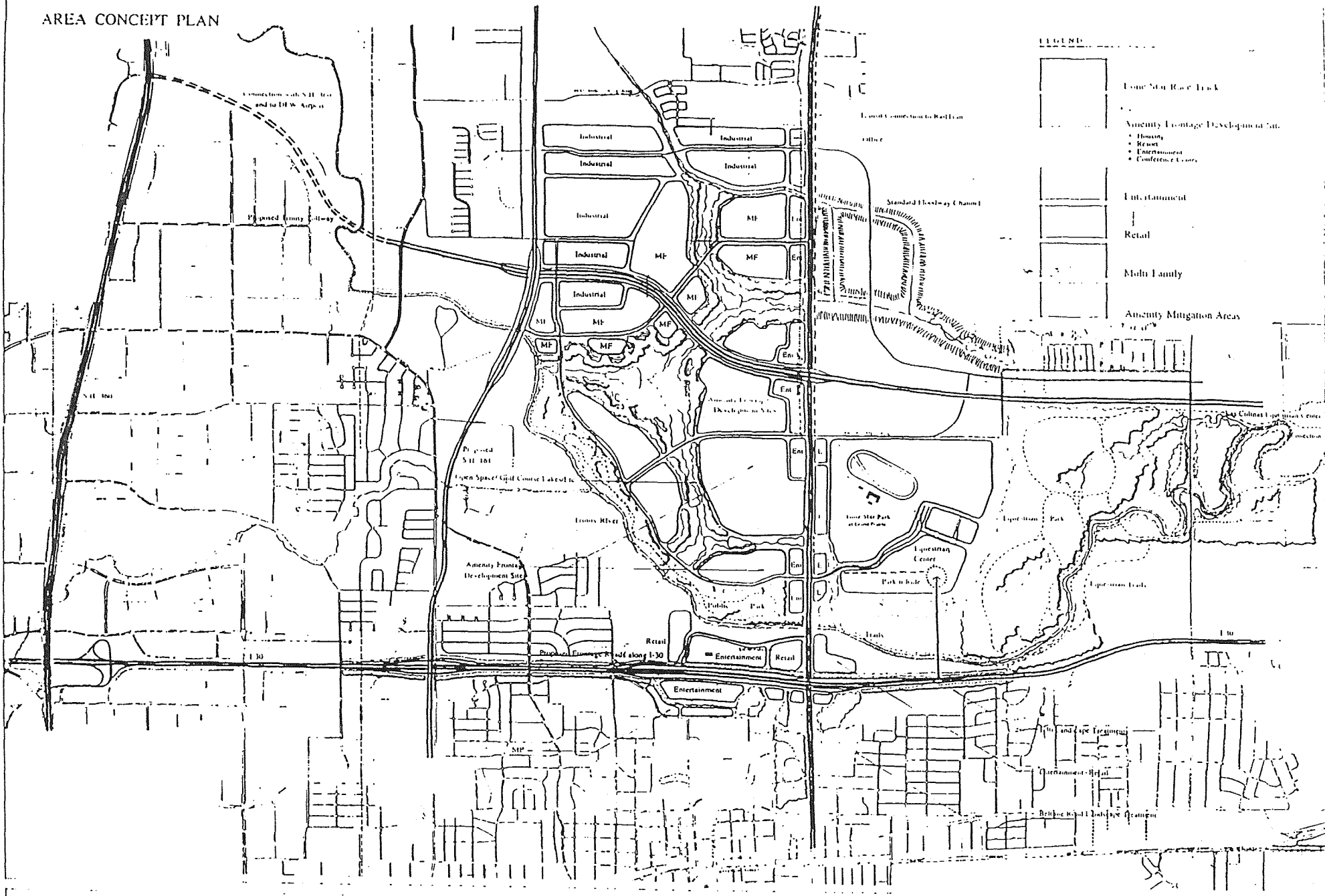
Because Lone Star Park and a large amount of flood plain land are located on the east side of Beltline, this area could be a major focus for equestrian activities such as horse training, large animal veterinarians and tack shops. The flood plain land immediately east of the Park and south of Hunter Farrell could become a regional equestrian trail-oriented park—especially if its is preserved as part of the mitigation for creating development sites.

The area west of Beltline could be oriented to golf if a major golf course is created in the flood plain. A world class course would create an attraction for the visitors as well as for offices, apartments and condos.

District Style. Lone Star Park at Grand Prairie is designed in a Spanish Revival style which is both reflective of Texas history and has proven to be a very commercially successful style. The first 3 shopping centers in the Southwest were constructed using this style and they remain very successful centers today—Country Club Plaza (Kansas City), Highland Park Village, and Casa Linda. In addition, many newer centers have adopted the theme because it is very versatile in accommodating different types of businesses and architectural designs.

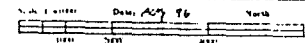
Trinity Flood Plain. There is a study currently being completed as a joint effort between the City of Grand Prairie and Irving to establish the limitations and possibilities for creating buildable land out of the flood plain. Preliminary estimates indicate that a little less than 50% of the land currently in the flood plain (exclusive of the floodway) can reasonably be removed from that designation through excavation and filling.

AREA CONCEPT PLAN



The Beltline Corridor District
Grand Prairie, Texas

J. DENNIS WILSON ASSOCIATES
Urban Planning and Green Design



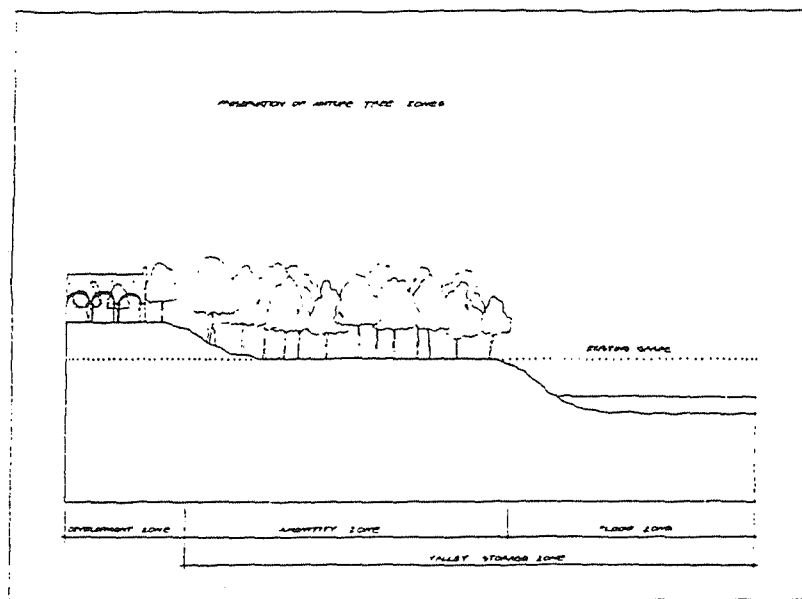
There is a need to provide for both "flood way" (movement of water) and for "valley storage" (the storage of a certain volume of water) within the flood plain. The requirements for floodway and valley storage presents a unique opportunity for the creation of open space and recreation area for development sites which are immediately adjacent and for the Metroplex. Some of the uses that could be placed in the flood plain include:

- Equestrian trails
- Polo Fields
- Horse Jumping and training facilities
- Golf courses
- Hike and bike trails,
- Canoeing and small boating in impounded water areas
- Recreational Bicycling / Competitive Training

When working with the shaping of floodway channels and wetland mitigation areas, "amenity development zones" can be created which will protect mature tree stands. These amenity development areas increase land values, enhance the area's scenic qualities and establish a framework for land development.

If designers work with existing grades and trees in identifying fill areas (for development) and cut areas (for floodways and flood plains), several mature stands of trees can be preserved.

The accompanying sketches of the development along the Flood plain show how development can take advantage of the adjacency of the flood plain and "valley storage" necessary for the Trinity River and its tributaries. Overflow parking can be placed in an area that receives only periodic flooding, and many space-consuming recreational uses can take advantage of land preserved for flood plain.



TYPICAL LAND RECLAMATION SECTION

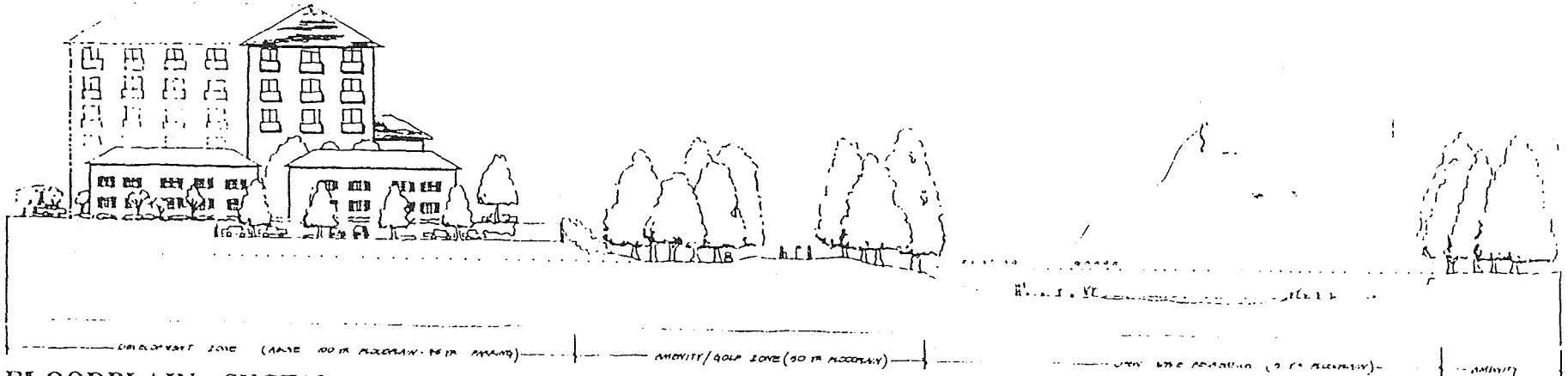
HOTEL/LOBBY AREA

MULTI-FAMILY HOUSING/AMENITY BLDG

GOLF COURSE
(RECONSTRUCTION OF EXISTING TRACKS)

VALLEY STATION & DEVELOPMENT

TRAIL AREA



FLOODPLAIN SECTION

NO. 100 OFFICE

OFFICE

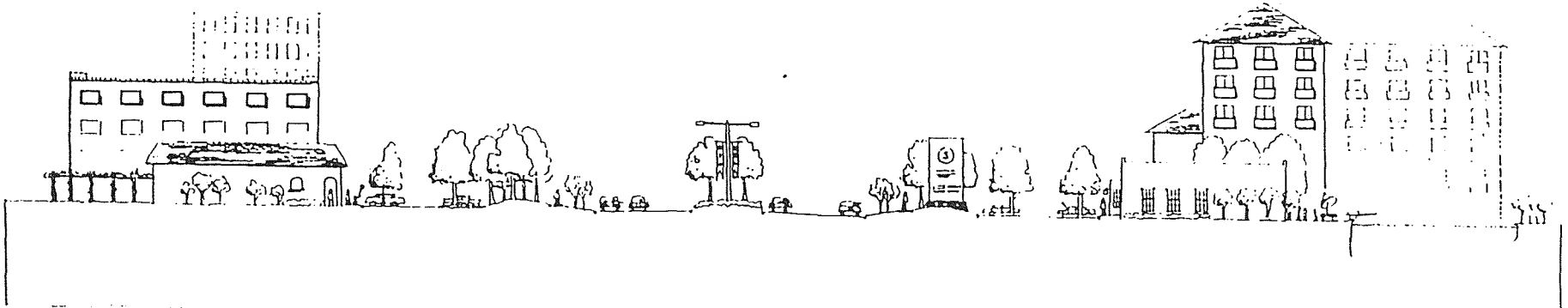
RETAIL STORE/LOBBY

LANDSCAPED HIGHWAY / ROAD LANDMARK

RESTAURANT & SERVICE

RESTAURANT

MULTI-FAMILY HOUSING/LOBBY AREA



BELTLINE ROAD SECTION

The Beltline Corridor District

Grand Prairie, Texas

J. DENNIS WILSON & ASSOCIATES
Texas Planning and Urban Design



Scale: _____ Date: July 1994

Landscape Character

The landscape character of the district should be "woody", and take advantage of the character of adjacent open space and natural areas of the Trinity flood plain. In contrast, the streetscaping of Beltline Road should be more formal with a unified treatment which includes a low level berm and a continuous row of trees lining the street. Individual development sites may be landscaped in a formal or informal manner.

The median of Beltline, the Safari Pkwy and IH 30 should be heavily landscaped with shade and ornamental trees to identify the district and to establish its character. This will serve to establish the quality and character of landscaping expected of private development.

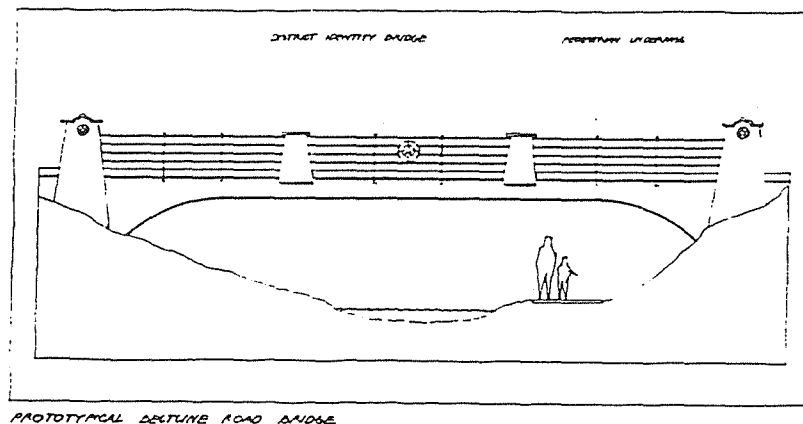
Public Infrastructure

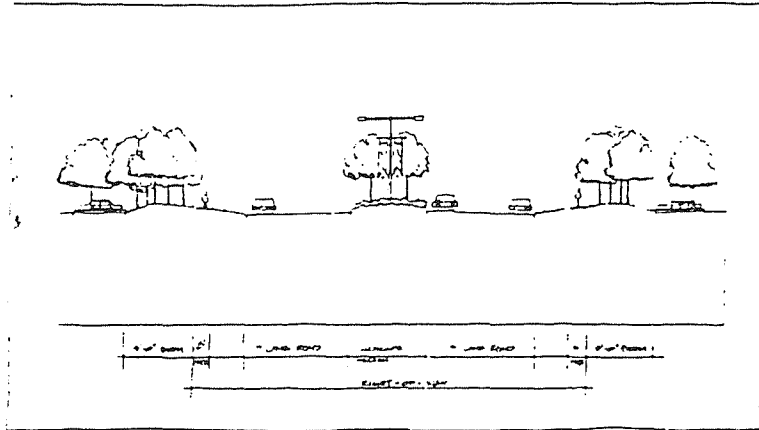
Streets. An identity for the area should be advanced by developing a theme for the naming of streets within the district, such as for famous horses or famous race tracks; or, on the west side of Beltline, for famous golf courses or famous golfers.

Beltline should include a well landscaped median and coordinated street lighting with banner arms to display banners relating to the district.

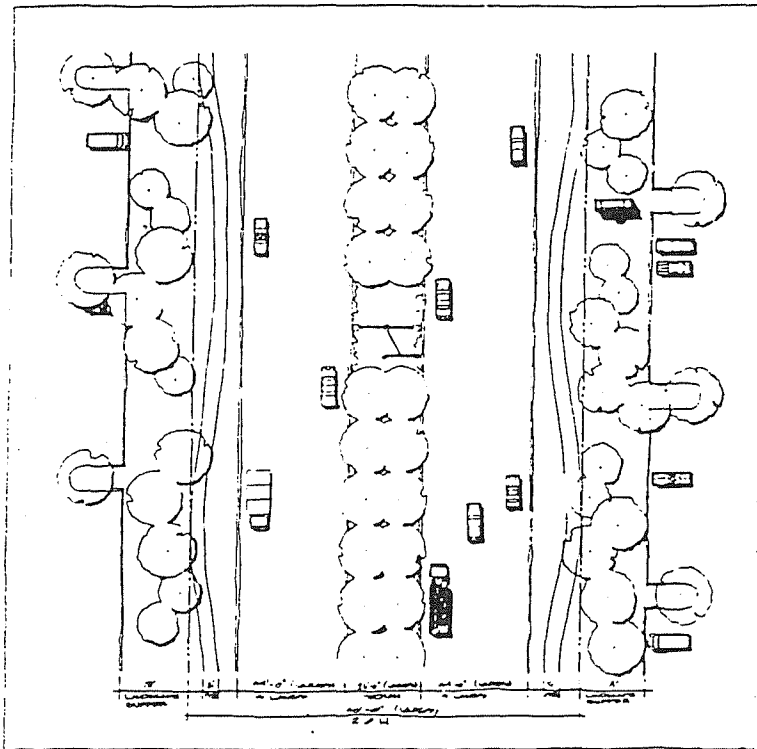
Bridges. Since all bridges in the area will need to be replaced, and several new ones built, they should reflect a unique style which is distinctive to the district, similar to the bridges next to the Ball Park at Arlington.

All new and rebuilt bridges should be constructed to allow for hike, bike and equestrian trails to continue below.





DETLINE ROAD SECTION



PROTOTYPICAL DETLINE ROAD PLAN

The Beltline Corridor District
 Grand Prairie, Texas

LUENNIS WILSON ASSOCIATES

Sidewalks. If the area is to become a recreational destination, it should contain sidewalks throughout the area. In addition, major roadways should include at least one-8 foot sidewalk to accommodate bicyclists and runners who will be circulating within the district and who will be using the regional Trinity Trail system. Alternatively, a designated roadway shoulder should be provided along the area's street for cyclists.

Code Enforcement

It is a key objective of the City to attract new investment into the area in the form of businesses and development. An important incentive for people to invest in an area is to protect their investment with a program of aggressive Code Enforcement.

Funding

Infrastructure. The creation of a high quality infrastructure and its continued maintenance, are fundamental to the success of this special district. Included in this infrastructure is the construction of a higher than normal quality of bridge structure, street lighting and landscaping. It could also include a high quality golf course designed by a well known golf course designer to broaden and intensify the entertainment "magnet".

The capital costs to create a higher than normal standard could be covered by creating a Tax Increment Financing District. All the key elements of a successful district are there—

- the area will soon be gaining national attention;
- the infrastructure could trigger development which would repay the capital costs;
- there is an emerging market for the area;
- there are property owners/developers with large property holdings who are willing to invest in high quality development; and
- the area would likely qualify for a TIF district under State legislation.

Maintenance. Ongoing maintenance of the district is as important as its original construction. If the district is not continuously maintained at a high level, it will not attract a high level of investment, and maintain its vitality as a district. The extra level of maintenance could be funded using a Tax Increment Funding vehicle or through creating a Public Improvement District.