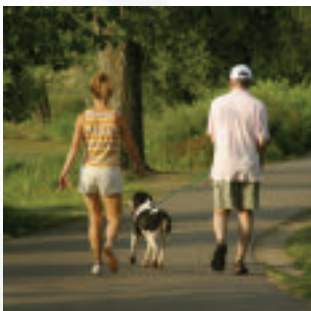




Crossroads West

SPECIFIC PLAN

Riverbank's Newest Master Planned Community



Prepared by:



April 2019

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Chapter 1

EXECUTIVE SUMMARY

1.1 Specific Plan Purpose and Process

A specific plan is a tool for the systematic implementation of the General Plan of a City. Specific Plans effectively establish a link between implementing policies of the General Plan and individual development proposals defined in a specific planning area. All specific plans are required to comply with Sections 65450-65457 of the Government Code. This Specific Plan is a tool that will facilitate zoning and land use which is consistent with the City's General Plan and will provide guidance to developers. The Crossroads West Specific Plan will supersede the City's Zoning Code and will provide the development standards that specifically apply to Crossroads West area. The creation of the 380 acre Crossroads West Specific Plan (CWSP) is highly anticipated as a welcome addition to the City of Riverbank hereinafter referred to as the "City". This plan has been prepared in cooperation with City officials to create a unique community that celebrates and enhances the distinct qualities of the City. In 2009, the City updated and adopted their 2025 General Plan in an effort to protect and preserve the small town community spirit of Riverbank while planning for future growth and development. The CWSP assists the City to implement the goals and objectives in the 2025 General Plan by (i) providing detailed direction for the future development of Crossroads West over an approximately 20-year build-out, (ii) identifying commercial uses, retail shopping options, mixed use opportunities, residential development, recreational opportunities and amenities including parks and areas for pedestrian trails, and (iii) identifying development goals, policies and implementation measures to guide the development of Crossroads West.

This document provides the implementation mechanisms and development standards by which Crossroads West will be built. The purpose of the CWSP is to create a comprehensively planned community that provides a balance of land uses, infrastructure and services to adequately serve and support development. The specific plan also provides land use information, amenities, development standards and guidelines for infrastructure, landscape and architecture. The project as proposed is consistent with the goals, policies and objectives of the City's 2025 General Plan and is in line with the City's desire to create pedestrian friendly neighborhoods that feature interconnected and walkable streets that provide easy access to neighborhood centers, parks and regional mixed use areas.

The specific plan process provides the City with a planning mechanism by which any issues of the site will be explored and policies and standards can be created to guide the build-out of the plan area. This specific plan establishes design criteria, outlines development opportunities and constraints and provides an overview of the necessary infrastructure needed to serve the project and potential funding sources for the construction of these improvements while allowing flexibility to tailor the unique vision of a particular area of the site as conditions change over time and providing the necessary consistency during the construction and build-out of the plan area.

The procedural steps for the adoption of a specific plan are identified in Section 153.309 of the City's Zoning Ordinance. The following discretionary actions will be submitted concurrently for approval:



Specific Plan – A specific plan requires adoption by resolution or ordinance, following public hearings before both the Planning Commission and the City Council. Additional entitlement applications that are necessary for the implementation of the specific plan may be made concurrently with the specific plan application.

General Plan Amendments – With the adoption of the 2025 General Plan, the City has indicated a preferred land use distribution for lands covered by the Crossroads West project site. In order to develop the site according to the Riverbank General Plan, several General Plan Amendments are necessary. First, because the Specific Plan is being developed to survive 20 years or so, the City has agreed to create a Specific Plan General Plan designation to govern the land use mix and density. This new General Plan Land Use designation will follow the land use mix envisioned with the Riverbank 2025 General Plan, but will eliminate the geographic representation of each designation area within the defined Specific Plan boundary. In this case, the Specific Plan will demonstrate the specific location of each use and document the method in which the plan area will be implemented following the density and intensity goals set by the defined Specific Plan General Plan designation. In this manner, the Crossroads West Specific Plan area will receive a General Plan designation of Specific Plan (SP). Third, the proposed Specific Plan suggests the relocation of certain planned roads identified on the General Plan Circulation Element. The adopted General Plan Circulation Element will need to be modified to conform to the certain street patterns proposed with this Specific Plan.

Prezone Application – Concurrent with the adoption of a specific plan, the planning area must be prezoned SP and assigned a number from the City. As referenced above, it may be necessary to connect the adoption of the specific plan with the adoption of other entitlements or programs such as subdivision or parcel maps, area facilities plans, design guidelines, monitoring programs, planned development permits, financing mechanisms, etc. to implement the plan. The specific plan shall define the relationship between other planning documents and the defined planning area.

Annexation Application – In addition to this specific plan and prezone request, the applicant has prepared an annexation application to the City of Riverbank who in turn will be requested annexation of the subject area through an application with Stanislaus LAFCO. Annexation of the 380+/- acres is necessary in order to receive the service benefits provided by the City of Riverbank. These include utilities, public safety protection and other City provided services such as public works and park maintenance.

Development Agreement – The applicants have suggested that a future action of the City of Riverbank would be to include the consideration of a Development Agreement to define certain terms of development timing, phased implementation obligations and reimbursement terms.

1.2 Specific Plan Organization

The Crossroads West Specific Plan has been organized into 10 chapters and they are described as follows:

Chapter 1 – Executive Summary provides an overview of the specific plan purpose and process; identifies Plan Area location and summarizes the vision for the Crossroads West development including the land use plan.



Chapter 2 – Context and Setting discusses the legal authority of the specific plan document, the policy setting methods and compatibility with the City of Riverbank General Plan and Zoning Code as well provides a brief overview of the site and the existing conditions.

Chapter 3 – Vision for Crossroads West provides a vision for the project with design and planning principles and how these principles relate to Crossroads West.

Chapter 4 – Land Use provides detail regarding the land use plan and land use designations. The land use chapter also provides development standards for each land use, which will help guide development of the plan area.

Chapter 5 – Circulation and Alternative Transportation provides a summary of the existing roadway system in the plan area and an overview of the proposed circulation system of Crossroads West. Details include street classifications and sections, bike paths and walkways, bus transit facilities and alternatives to vehicle use.

Chapter 6 – Public Services and Facilities provides information relating to the public services and facilities that will serve the plan area including law enforcement, fire protection, solid waste disposal and recycling, parks and schools.

Chapter 7 – Infrastructure provides a summary of the backbone infrastructure systems for water, sanitary sewer and storm drainage that will service the plan area. Preliminary Master Plan studies have been prepared for the site and will need periodic review and amendment as subdivision and/or parcel maps are submitted.

Chapter 8 – Design Guidelines details the architectural styles and site planning for each land use district. These Design Guidelines are intended to provide guidance and direction for all development within the plan area, creating a unique and inviting Crossroads West community.

Chapter 9 – Project Financing Plan provides funding and financing mechanisms that may be used to fund the development and construction of public facilities and improvements including roadways, parks and backbone infrastructure.

Chapter 10 – Implementation and Administration describes the process for which subsequent and future submittals are made and if necessary, how amendments will be reviewed and approved for the specific plan.



1.3 Plan Area Vicinity and Project Description

The CWSP is approximately 380 acres, which is centrally located in Stanislaus County in the heart of the San Joaquin Valley. The plan area is bound to the west by farm land and existing residential homes on large lots. Oakdale Road bounds the plan area to the east; Claribel Road bounds the plan area to the south and the Riverbank Regional Sports Park and MID Main Canal bound the plan area to the north. MID Lateral No. 6 bifurcates the plan area in the southern third of the parcel. The project area encompasses approximately 380 acres of land that has primarily been used for farming and agricultural purposes. The boundaries of the site are within the City's proposed sphere of influence and are a natural choice for annexation because of its location to existing development and existing infrastructure. Please see Figure 1 – Project Vicinity Map for a visual of the project location.

The 380 acre Crossroads West development is envisioned as a vibrant mixed use community consisting of residential, retail, commercial, office, hospitality, entertainment, recreation, mixed use and/or public and civic facilities, interconnected walking and biking paths, parks and open space. The land use plan for the Crossroad West development is located in Chapter 4 of this specific plan as Figure 5. The retail, commercial, office, hospitality, entertainment, recreation and mixed uses within the plan area will assist in job creation, further complying with the City's goals and policies of their 2025 General Plan. The site is compact in design and therefore has been planned efficiently. By increasing pedestrian connectivity to existing uses to the north and east, the small town community atmosphere desired by the City has been preserved.

There are approximately 59+/- acres of land intended for mixed use commercial, office, hospitality, entertainment, recreation, mixed uses, and/or retail/residential development. These are identified in Figure 5 as Mixed Use 1 and Mixed Use 2. The largest concentration of retail development will be located at the southern end of the plan area at the intersection of Oakdale Road and Claribel Road and is identified as Mixed Use 1. A smaller mixed use site is located near Morrill Road and Oakdale Road and is identified as Mixed Use 2. Proposed lot densities in the residential component of the CWSP are consistent with the 2025 General Plan. The residential lots will vary between low density (0-8 du/ac), medium density (8-16 du/ac) and high density residential



Existing shopping center Oakdale/Claribel.



Existing irrigating ditch



Existing Riverbanks Sports Complex



Existing Oakdale Road



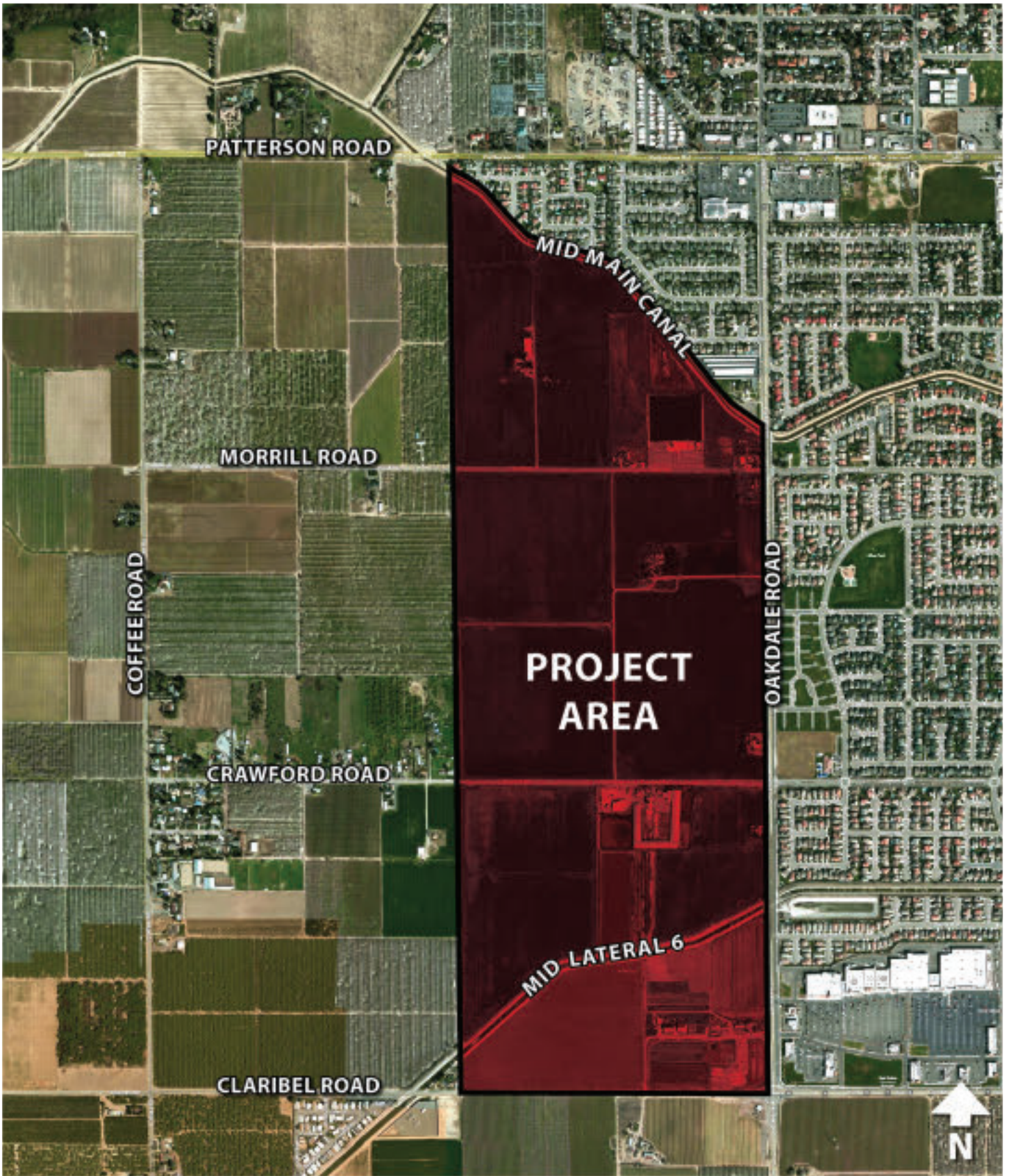


Figure 1 - Project Vicinity Map



(16 - 25+ du/ac). All residential land uses are provided in the Land Use Plan and the final lot count and ultimate build-out of the residential area will be dependent on the mix of housing provided at development. The maximum number of units are identified in Tables 1 & 2 and if exceeded in the future, subsequent environmental review will be required.

1.4 Existing Conditions Summary

The land plan for Crossroads West is a result of many factors including the site conditions in their present state, the surrounding land uses and the topography of the site. The Environmental Impact Report (EIR) prepared for Crossroads West will provide detailed information and a full discussion of the existing conditions found in the plan area.

1.4.1 Property Ownership

Please refer to Figure 2 – for a Property Owner Map that provides a visual representation of the parcels, the respective property owner, acreage and address.

1.4.2 Existing and Surrounding Land Use

The nine parcels that comprise the Crossroads West Specific Plan area are primarily used for dairy and farming operations including row crops and irrigated open land. Seven home sites exist within the plan area and many of them have accessory structures on site including storage buildings, shop buildings and barn structures.

Surrounding site conditions vary around the plan area. To the north, residential development exists; to the south, agricultural land for farming exists; to the east, the Crossroads Development which features both Mixed Use and residential development; and to the west, agricultural land and rural residential home sites exist.

The City’s 2025 General Plan has lands to the west of the CWSP designated for Low Density Residential, Medium Density Residential and Mixed Use development; parcels to the north and east of the plan area having existing residential and Community Commercial development, which is in conformance with the City’s General Plan. The CWSP area is bounded by two arterial roads, Oakdale Road to the east and Claribel Road to the south. Morrill Road and Crawford Road are designated as Collector Streets and the CWSP includes the addition of a north/south collector to provide ease of access to all of the main areas of development. Please refer to Figure 3 – Existing and Surrounding Land Use for a visual depiction of the site and its surroundings.



Existing ranchettes

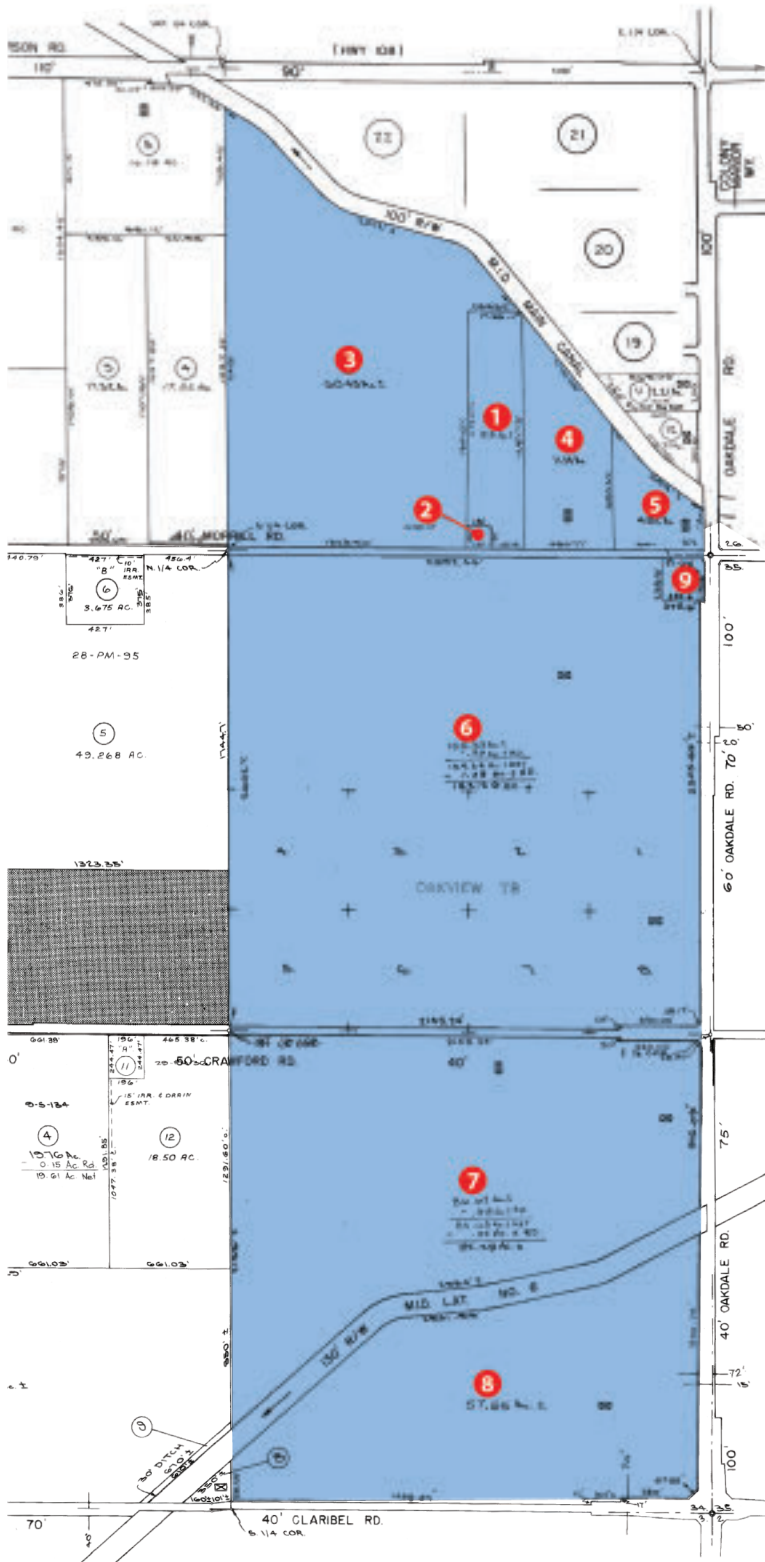


Existing site looking south from Morrill



LEGEND

 Project Area



Property Owner Information

1	Hope Homes, Inc. 0 Morrill Road Modesto, CA 074-006-022	8.9 ac
2	Mary E. York Estate 2007 Morrill Road Modesto, CA 074-006-021	0.38 ac
3	John A. Harrigfeld Trust 1901 Morrill Road Modesto, CA 074-006-016	60.43 ac
4	City of Riverbank 2119 Morrill Road Modesto, CA 074-006-014	11.0 ac
5	Albert & Susie Dadesho 2173 Morrill Road Modesto, CA 074-006-013	4.78 ac
6	Alyce Machado Parsons Trust 5525 Oakdale Road Modesto, CA 074-011-009	153.26 ac
7	Machado Properties, LTD 5413 Oakdale Road Modesto, CA 074-014-006	85.38 ac
8	Rosalie McGrane Trust 5101 Oakdale Road Modesto, CA 074-014-007	54.5 ac
9	Alyce Machado Parsons Trust 5955 Oakdale Road Modesto, CA 074-011-004	0.97 ac



Figure 2 - Property Owner Map



1.4.3 Topography and Drainage

The lands that comprise the CWSP area are relatively flat and are utilized primarily for farming of row crops and dairy operations. Based on observation and preliminary runoff data, the plan area generally slopes and drains from the northeast towards the southwest.

1.4.4 Biological Resource

The primary use of the plan area today is agricultural in nature with uses including dairy operations, row crops and fallow land. Due to the intensive agriculture use that is present on the site and the lack of a suitable habitat, it is unlikely that any special status plants or species exist on the site. Due to the historic and current use of the property, no potentially jurisdictional wetlands or Waters of the US have been observed in or surrounding the plan area.

1.5 Summary of Land Use Plan

The land use plan as proposed for Crossroads West has approximately 59 acres dedicated to mixed use development. These areas will feature a mix of commercial, retail, office, hospitality, entertainment, recreation, residential, restaurants, neighborhood and regional commercial uses, as further described in Section 4.4.1 and 4.4.2, that will help with job creation for the City and provide convenient shopping destinations for the residents of Riverbank. Residential development is permitted within the mixed use areas, identified on the Land Use Plan as Mixed Use 1 (MU 1) and Mixed Use 2 (MU 2), and discussed in more detail in Chapter 4 of this Specific Plan. The remainder of the site is dedicated to residential land uses, parks and open space and the potential for development of a school site(s). The ultimate build-out of these areas will be determined with the submittal of final development plans.

The land use plan as proposed would permit residential development within a range of between 1,524 to 2,452 residential units. For Low Density Residential (LDR) the plan assumes between 1,170 and 1,872 units, assuming a build-out at between 5 and 8 dwelling units per gross acre, after removing parks, schools, and collector and arterial road rights-of-way. Medium Density Residential (MDR) development is assumed at around 96 to 192 units, based upon a build-out at between 8 and 16 dwelling units per acre. Some MDR density development would be allowed in the LDR areas, although they would need to fall within the total LDR number unit range. High Density Residential (HDR) is assume to build-out at around 248 to 388 units on 15.5 acres, and assuming a build-out density of 16 to 25 dwelling units per acre. The maximum number of units are identified in Tables 1 & 2 and if exceeded in the future, subsequent environmental review will be required.

The Mixed Use 1 property could provide up to 550,000 square feet of MU 1 Commercial Uses (defined in Section 4.4.1.3 below), but could similarly provide less MU 1 Commercial Uses and more MU 1 Residential Uses (defined in Section 4.4.1.3 below) depending upon market demands at the timing of development (i.e. it could provide 360,000 square feet of MU 1 Commercial Uses and 350 MU 1 Residential dwelling units). Mixed Use 2 is estimated to develop with up to 27,000 square feet of retail, and about 25 to 50 residential units.



Table 1 provides a summary of the land uses proposed for Crossroads West. If any of the analyzed maximum land use intensities are exceeded, additional environmental review will be required.

Table 1 – Land Use Summary - Residential Land Use

Map Symbol	Land Use	Acreage	Range Density/S.F.	Range Units or S.F.	Average Density/S.F.	Average Units or S.F.
LDR	Low Density Residential	234	5 du/ac - 8 du/ac	1,170 - 1,872	6.5 du/ac	1,521
MDR	Medium Density Residential	12	8 du/ac - 16 du/ac	96 - 192	12 du/ac	144
HDR	High Density Residential	15.5	16 du/ac - 25+ du/ac	248 - 388	20.5+ du/ac	318
P	Parks/Open Space/ Regional Sports Park	42	N/A	N/A	N/A	N/A
	Schools	12/20	N/A	N/A	N/A	N/A

Table 1 – Land Use Summary - Non Residential/MU Land Uses

Map Symbol	Land Use	Acreage	Maximum Density/S.F.	Maximum Units or S.F.	Maximum S.F.
MU 1	Mixed Use 1 (R)	54	25 du/ac	350	0
	Mixed Use 1 (C)		0.35 FAR ¹	360,000 sf	550,000 sf
MU 2	Mixed Use 2 (R)	5	25 du/ac	50	N/A
	Mixed Use 2 (C)		0.35 FAR ¹	27,000 sf	N/A

¹Residential and Hospitality uses are exempt from the FAR maximum.





Figure 3 – Existing and Surrounding Land Use



Chapter 2

CONTEXT & SETTING

2.1 Project History

Riverbank is a mid-size City in the Central Valley with a population of approximately 24,610 residents. The City has experienced well-planned growth over the last decade with new Community Commercial and residential developments adding to the character and community-oriented town the residents desire. Crossroads West will be the perfect addition to Riverbank as it will seamlessly blend with the existing Crossroads development while providing enhanced amenities in shopping, dining and recreation. The existing Crossroads development adjacent to the CWSP and across Oakdale Road to the east has been a highly successful Commercial project. Placed in the City Sphere of Influence in 1989, planned in the 1990's and annexed in 1997, the City has focused its development efforts on the Crossroads area for over 25 years. As the Crossroads project is now basically complete, the Crossroads West development promotes orderly development because of its continuity with the existing Crossroads, the ease of access to the plan area via Oakdale Road and Claribel Road.

The CWSP provides a variety of residential and Mixed Use development opportunities that will complement the existing Crossroads project, provide room for public facilities and school sites if they are needed in the area, and provide vast areas of green space and open space as functional active and passive recreation areas. A mix of residential densities is planned which will accommodate an array of home buyers and family sizes.

2.2 Compliance and Regulatory Authority

2.2.1 Specific Plan Authority

Specific Plans are authorized and described in California Government Code Section 65450 et. seq. As set forth in the Government Code Section 65451, Specific Plans are required to contain the following information:

- (a) A specific plan shall include a text and a diagram or diagrams which specify all of the following in detail:
 - (1) The distribution, location and extent of the uses of land, including open space, within the area covered by the plan.
 - (2) The proposed distribution, location and extent and intensity of major components of public and private transportation, sewage, water, drainage, solid waste disposal, energy and other essential facilities proposed to be within the area covered by the plan and needed to support the land use described by the plan.
 - (3) Standards and criteria by which development will proceed, and standards for the conservation, development and utilization of natural resources, where applicable.
 - (4) A program of implementation including regulations, programs, public works projects and financing measures necessary to carry out paragraphs (1), (2) and (3).



The CWSP has been prepared along with supporting studies to provide text and diagrams for the items listed above. A land use plan diagram; major infrastructure and services including water, sanitary sewer and drainage; guidelines and standards; implementation and financing measures have been included.

- (b) Section 65454 states: “No specific plan may be adopted or amended unless the proposed plan or amendment is consistent with the General Plan.

Working closely with City staff, the CWSP has been found to be in substantial compliance with the 2025 General Plan. Appendix A of this Specific Plan consists of the General Plan consistency matrix utilized to determine the Specific Plan is in substantial conformance with the 2025 General Plan.

2.2.2 Entitlement Phasing and Sequence

The CWSP provides general information regarding design, phasing, financing and implementation of the plan area. The CWSP suggests land use classifications which will establish the framework for the implementation of the development guidelines and constraints identified in the Specific Plan. The Plan Area will receive an SP zoning number which will be identified on the final, adopted Specific Plan. The Specific Plan summarizes some basic phasing and financing of backbone infrastructure to serve the plan area in Chapter 9. A more in depth project Infrastructure Financing and Phasing Plan will be provided prior to the approval of any parcel map, tentative map or subdivision map, development plan review or planned development process.

2.2.3 CEQA Compliance

Projects that do not meet certain categorical exemptions are subject to a review by the California Environmental Quality Act (CEQA). The Crossroads West plan area is subject to this environmental review under CEQA. In conjunction with the specific plan, an EIR will be prepared and submitted for review. The EIR will examine and evaluate environmental impacts and provide necessary mitigation measures for the plan area. The CWSP is intended to function together with the Mitigation Monitoring and Reporting Plan contained in the EIR. Mitigation measures which are developed as part of the EIR will be incorporated as policies or standards in the CWSP if applicable or they will be placed as conditions of approval on subsequent development applications for the plan area.

2.2.4 Severability

If any portion, regulation, condition or program of the Crossroads West Specific Plan are held invalid by a California or Federal Court of competent jurisdiction, such portions shall be deemed separate, distinct and independent provisions, and the invalidity of such provisions shall not affect the validity of the remaining provisions thereof.

2.3 Policy Setting – City of Riverbank General Plan

The goals, policies and implementation measures of the City of Riverbank 2025 General Plan, adopted on April 22, 2009 and amended on February 24, 2014, govern the CWSP area. A General Plan’s function is to provide guidance to the development and management of land within the City. The 2025 General Plan contains goals, policies and objectives to which all projects submitted for review and approval to the City



must adhere. The General Plan document summarizes its policies and implementation strategies as they relate to the City’s goals and objectives. The 2025 General Plan includes the following elements: Land Use, Circulation, Community Character and Design, Economic Development, Conservation and Open Space, Safety, Noise, Public Services and Facilities, Air Quality, Housing and Legal Authority.

2.3.1 General Plan Land Use

The CWSP area is currently under the jurisdiction of Stanislaus County, but within the City of Riverbank Sphere of Influence. See Figure 4 – City of Riverbank General Plan Land Use Map of the land use densities and intensities envisioned with the City’s 2025 General Plan.

While it is clear the CWSP is in substantial compliance with the City’s General Plan Land Use Element, it does differ enough to trigger an amendment to the 2025 General Plan document that will be processed concurrently with adoption of this document. The General Plan Amendment will be considered for approval by the City Council concurrently with the annexation and rezone applications, and consist of the creation of a General Plan land use designation of Specific Plan and some Circulation Element revisions.

2.3.2 General Plan Analysis

The City of Riverbank adopted their General Plan with lands envisioned for build-out through the year 2025. The estimated build-out of these lands will be phased to ensure orderly growth and development and will be dependent on population trends and demand for new housing and amenities for residents. It is important to phase development according to the demonstrated need for residential, Mixed Use and public uses and keep the supply manageable in accordance with available City services and infrastructure.

During public outreach for preparation of the 2025 General Plan, the City encountered key land issues brought forth from the public as well as known issues amongst City staff. Major concerns from the public were traffic congestion, housing affordability, loss of open space and rural character as well as a shift away from the redevelopment of downtown to other less prominent areas.

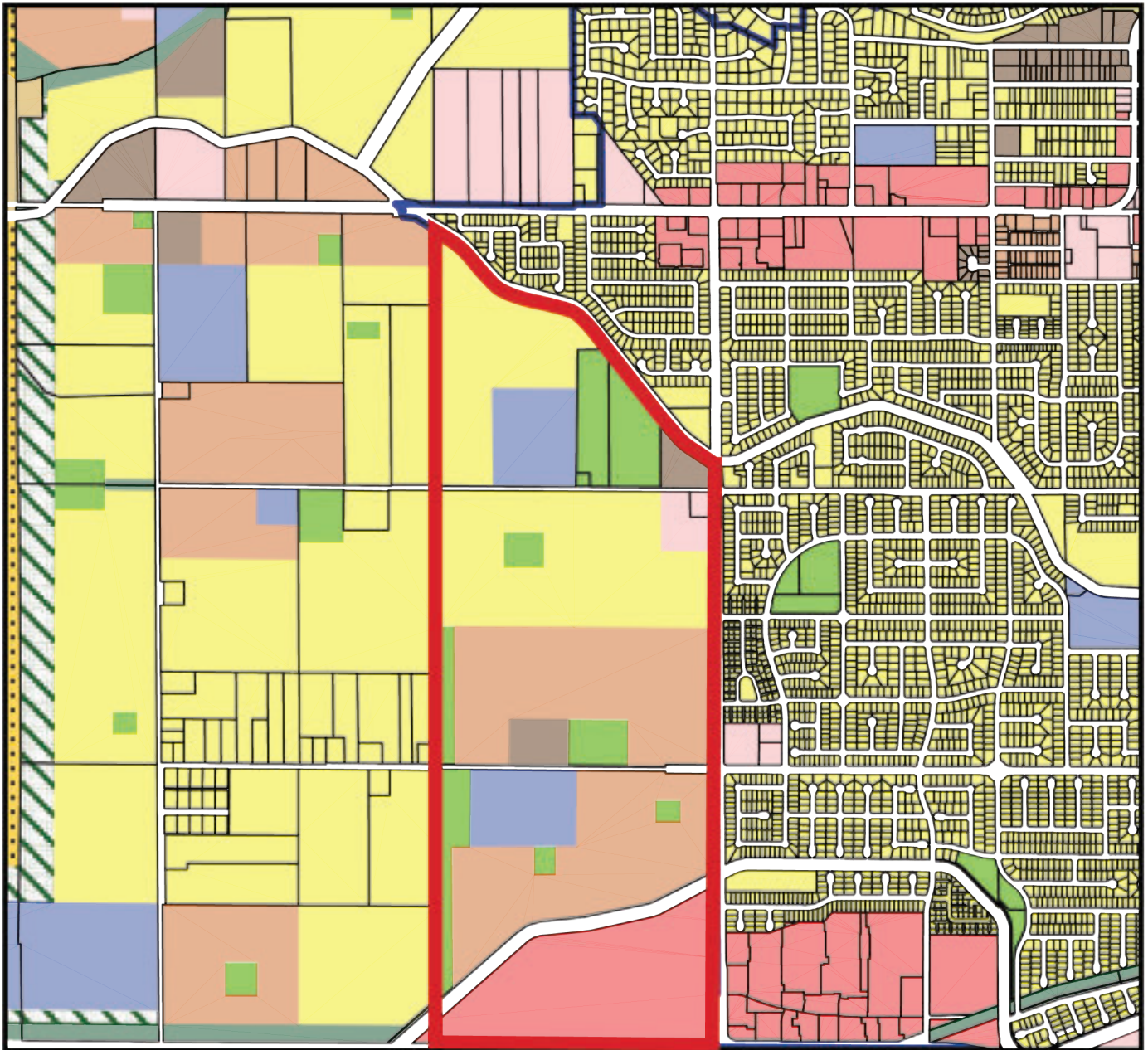
The overriding theme of the public outreach was balance. Residents of Riverbank and City staff wanted to find a balance between the revitalization of the existing downtown and the development of new retail, commercial and residential areas. Based on the goals, policies and procedures adopted in the 2025 General Plan, Crossroads West will blend seamlessly into the existing Community Commercial, mixed-use and residential areas presently found in Crossroads. Appendix A consists of a General Plan consistency matrix, and the CWSP is in conformance with the 2025 General Plan based on this analysis.

2.4 Site Analysis and Existing Conditions

Because of the existing use of the site, primarily dairy and farming operations, some constraints do exist in the plan area that will be fully addressed in the project EIR, although the constraints on the development of the site are few. The potential limitations and constraints are summarized below:

1. The Modesto Irrigation District (MID) Main Canal forms the northern boundary of the project site. While some may see this as a limiting factor to development in the area, the project applicant





LEGEND















			
AG Resource Conservation (AG)	Higher Density Residential - 16+ Units (HDR)	Mixed Use (MU)	Clustered Rural Residential - 0.2 or less units per acre (RR)
			
Buffer/Greenway/Open Space (B/G/OS)	Industrial/Business Park (I/BI ^P)	Multi-Use Recreational/Resource (MUR/R)	Reserve (R)
			
Civic (C)	Lower Density Residential - 0-8 Units (LDR)	Parks	City Limits
			
Community Commercial (C/C)	Medium Density Residential - 8-16 Units (MDR)	Reserve (R)	



Figure 4 – City of Riverbank General Plan Land Use Map



sees it as a positive attribute to the project. A Class I Bike Trail will be created along the south side of the MID Main Canal which will provide continued recreational opportunities between the existing Riverbank community and the Crossroads West Specific Plan area.



MID Main Canal at Oakdale Road

2. Oakdale Road is classified as a major arterial roadway in the City's 2025 General Plan. This type of roadway is designed as a multiple lane road which is intended to move a high volume of traffic to and from various destinations around the City. Because of the Mixed Use 1 development being proposed at Claribel and Oakdale Roads, this is actually seen as a positive factor for the CWSP. There will be ingress and egress locations along Oakdale and Claribel Roads to provide ease of access to Mixed Use 1. Smaller, collector roadways will be constructed heading north through the site as well as east and west to handle lower volume traffic to the residential areas and Regional Sports Park.
3. MID Lateral No. 6 runs east and west in the southern 1/3 of the project site. A vehicular and pedestrian crossing is proposed across this facility to provide access to the Mixed Use 1. The Specific Plan also suggests the construction of a trail along the north side of MID Lateral No. 6 again to connect the existing Riverbank community and Crossroads West Specific Plan area.
4. The western boundary of the plan area abuts agricultural land and residential estate lots which front on Crawford Road. Future urban development of the properties which front Crawford Road is unknown at this time. In the interim, the project applicant has proposed less intensive Low-Density Residential as well as a park basin along the western boundary of the plan to provide a buffer to the residents on Crawford Road.
5. Existing Crawford Road west of the project site contains numerous small ranchette style parcels. With the development of the project, traffic levels would be expected to increase on Crawford Road. While these levels would be within acceptable traffic levels of service, the residents on Crawford Road have expressed a concern about this increased traffic.

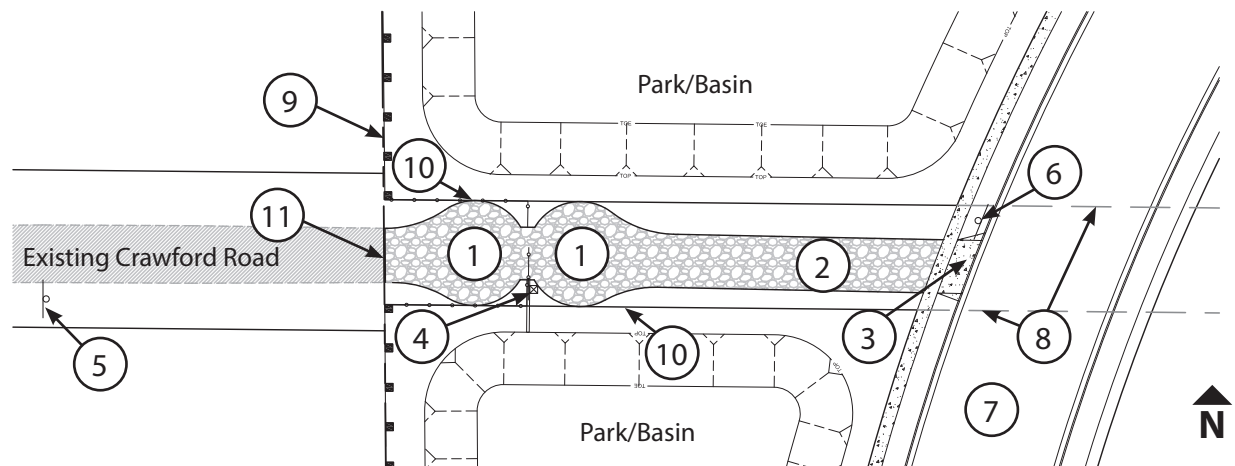


- Concurrent with the first development in Phase B, a gate will be constructed across Crawford Road, near the project's west property line, and an appropriate temporary vehicular turn around will be constructed on each side of the temporary gate. In addition, the temporary gate will be constructed in a way to allow quick access for the fire district to pass through and continue down Crawford Road, but otherwise will stay closed.

As Crawford Road is constructed with the development of Phase B, and realigned consistent with this Specific Plan, and the north/south collector road and dual use/park basin are designed, the temporary gate and turnarounds will be revised to maintain emergency access, but not daily vehicular traffic.

In the future, when the area between the project site and Coffee Road develops (it is located within the City of Riverbank sphere of influence), Crawford Road will be continued to Coffee Road, and an alternative route through the neighborhood may be proposed, or Crawford Road may become Crawford Court permanently. Such a determination will be made in the future when development plans are prepared for the area west of Crossroads West.

Preliminary Crawford Road Emergency Vehicular Access (EVA) Layout



LEGEND

- | | |
|--|--|
| 1. Aggregated Base (AB) Turn-Around | 7. Proposed Collector Road |
| 2. AB Emergency Vehicle Access (EVA) | 8. Abandon Existing Crawford Road Right of Way |
| 3. Concrete EVA Driveway | 9. Metal Split Rail Fencing or Similar |
| 4. EVA Automated Gate and Fire Department Knox Box | 10. Wrought Iron Fencing or Similar |
| 5. "Road Ends 500 ft" Signage - W31A (CA) | 11. Property Line |
| 6. "Emergency Access Keep Clear" Signage - SR46 (CA) | |



Chapter 3

VISION FOR CROSSROADS WEST

3.1 Overview

The Crossroads West Specific Plan will be the implementing document for development within the plan area and will direct the development of the project site as a comprehensively planned new community in the City of Riverbank. With the approval and construction of the adjacent Crossroads project, the City of Riverbank set a precedent for high-quality development and met a demand for first class amenities for its residents. With approval of the CWSP, the City of Riverbank will create new standards for high-quality development and amenities and establish a benchmark for future development not only in Riverbank but for surrounding areas as well. The vision of Crossroads West will be realized and expressed in the design guidelines and character elements of the project. The specific details of how this vision will be carried forward are contained in the chapters that follow.

3.2 Crossroads West within the Regional Context

The Specific Plan process was initiated for this planning area with an initial vision to improve and expand housing opportunities for all types of home buyers, integrating land uses while updating planning philosophies, improving quality of life for residents, creating a livable community where motorized and non-motorized transportation modes work in harmony and a community that balances the economics and job creation with a visually pleasing neighborhood.

The City of Riverbank has created a unique community identity for itself with the existing Crossroads development. Crossroads West intends to expand and build upon the successes of Crossroads Specific Plan by further enhancing the area and providing additional amenities for all residents of Riverbank. The proximity of the planning area to Riverbank City Limit and the major access points from Oakdale Road and Claribel Road are the main reasons for developing this portion of Riverbank. Other virtues of the CWSP area are the availability of infrastructure, proximity to the Regional Sports Park and the opportunity to improve the jobs/housing balance in the City. Development of the planning area crafts a continuous growth pattern for the City and eliminates unincorporated islands.

3.3 The Crossroads West Plan

The vision for Crossroads West is to create a unique, mixed-use community inspired by small town America. Crossroads West will be a desirable community with a diverse mix of housing types, abundant open spaces and easy access to regional shopping, dining and entertainment.



Illustration of proposed Park/Basin



The community will be centered on pedestrian friendly walkways, bike paths and landscaped corridors and will feature gateways that call attention to major intersections and neighborhoods within the planning area. The Crossroads West development is expected to be a positive addition to the City of Riverbank because it embraces and implements the City’s goals and policies as expressed in its General Plan.

The Commercial core of the community is the regional center (Mixed Use 1) located along Claribel Road. This area is envisioned as a pedestrian friendly mix of regional and neighborhood serving retail, commercial, office, residential, hospitality, entertainment, recreation, restaurants and services presented in a visually appealing manner to residents. Pedestrian access from the MID Lateral will be provided from the houses north of the center, truly creating a walkable community.

The residential components of the CWSP area are envisioned to be inviting and open. Homes will be placed near parks, retail and other amenities reinforcing the strong sense of place in each neighborhood. A mix of architectural themes is desirable for the area to promote the use of different materials, color palettes and styles.



Pedestrian friendly commercial courtyard



Class I bike trail

3.4 Development Principles

The planning and design principles in the following sections provide the physical framework and further establish the vision for Crossroads West. These principles are intended to implement the development principles focusing on sustainability.

3.4.1 Sustainability Elements

The creation of development principles is essential to create a strong sense of place, to improve the quality of life and create better places to live and work in the Crossroads West development. These development principles will help to achieve the vision for Crossroads West. The CWSP development principles are as follows:

- Promote a balance of uses in the land plan including retail, commercial, office, hospitality, entertainment, recreation and residential opportunities, schools, public facilities, parks and open space and varying density residential.
- Promote a mixed-use compact development pattern that is linked to regional amenities and transportation systems.
- Provide a variety of sidewalks and walkways throughout the project to promote connectivity, foster



- a sense of community and connect the residents of Riverbank to amenities and public facilities.
- Protect adjacent farmland by providing transitional buffers.
- Encourage energy efficiency and thoughtful use of resources through sustainable design practices and Low-Impact Design (LID) strategies.
- Promote friendly and inviting streetscapes through the use of landscape materials, street fixtures, furniture and design elements that reflect a high-quality development.
- Encourage the use of mixed architectural styles and materials.
- Reinforce existing retail corridors to the east and designate sufficient commercial land for job generating uses to improve the jobs to housing balance.
- Create a safe and accessible link between neighborhoods, community facilities and Commercial uses and residential uses within the plan area and to the surrounding neighborhoods.

3.5 Planning and Design Principles

The planning and design principles in the following sections provide the physical framework and further establish the vision for Crossroads West. These principles are intended to implement the development principles focusing on the relationship of the neighborhood and community elements. Sustainability elements, community elements and neighborhood elements provide the overarching theme of the CWSP. This CWSP is in substantial conformance with the City of Riverbank Sustainable Agricultural Strategy and the Regional Transportation Plan/Sustainable Community Plan adopted by Stanislaus Council of Governments (STANCOG) in 2014.

3.5.1 Sustainability Elements

Smart growth is a better way to build and maintain our towns and cities. Smart growth means building urban, suburban and rural communities with housing and transportation choices near jobs, shops and schools. This approach supports local economies and protects the environment. The sustainability elements contained in the CWSP provide the guidance and framework for promoting energy efficiency, creating live/work/play communities, reducing vehicle trips and employing eco-friendly building and landscape materials throughout the development.

A. Sustainable Community Design

1. *Compact Development*

The proper use of land for development encourages livability, walk-ability and transportation efficiency. Compact development involves increased density and taller buildings to improve the sustainability of land and natural resources. The CWSP meets or exceeds the development intensity identified in the City's General Plan and approved Municipal Services Review (MSR).

2. *Reduce Dependency on Automobiles*

Provide a Bicycle Network and alternative methods of transportation such as clean air transit,



bicycling and walking should be encouraged through design. By providing amenities such as shopping, employment areas and attractive walking and bicycling paths within close proximity to neighborhoods, vehicle trips can be reduced.

3. *Open Street Network and Walkable Streets*

Community connectivity is paramount when creating a walk-able neighborhood and fostering a sense of community beyond the development. All streets, sidewalks and walking/biking paths encourage use by the community at large.

Continuous sidewalks and/or bicycle trails and paths should be provided throughout the CWSP area to promote physical activity and provide an appealing and comfortable pedestrian environment.

4. *School/Community Facility Proximity*

The existing City of Riverbank General Plan identified two possible schools sites within the Crossroads West Specific Plan area, one just north of Morrill Road, and one just south of Crawford Road. These two possible school sites are identified on the Crossroads West Land Use Plan. The underlying land use designation is low density residential (LDR) for any school site, until acquired.

The Crossroads West Specific Plan projects the development of about 1,665 detached single-family units and about 310 high-density (multi-family) units. Sylvan Union School District provides housing for elementary students in K-5 elementary and 6-8 middle schools. Secondary students in grades 9 through 12 will attend schools in the Modesto High School District. The following student generation rates were provided: (i) for single-family units, 0.280 for K-5 students, 0.150 for 6-8 students and 0.176 for 9-12 students, and (ii) for multi-family units, 0.083 for K-5 students, 0.111 for 6-8 students and 0.096 for 9-12 students.

Based on the foregoing, the CWSP is projected to generate about 492 K-5 students, about 310 6-8 students, and about 323 9-12 students. Assuming school sizes of about 800 students for an elementary school, 1,200 students for a middle school, and about 2,400 students for a high school, the project would generate the need for about 62% of an elementary school, about 26% of a middle school and about 13% of a high school.

In addition, the Mixed Use 1 area is permitted to construct up to 350 additional residential units. Assuming all these 350 units are developed, and split as 175 single-family and 175 multi-family, the Mixed Use 1 area could generate up to an additional 64 K-5 students, 46 6-8 students and about 48 9-12 students.



Pedestrian area along park basin



Class I bike trail along Oakdale Road



Bus stop



5. *Access to Community and Regional Amenities*

Interconnected streets, bike paths and walking paths shall be integrated into the land plan and ultimate build out of the CWSP linking residents to the regional amenities found in the northern and southern portions of the project.

6. *Sustainable Design and Green Building Strategies*

Buildings for Mixed Use 1 and retail use and residential dwelling units within the plan area shall meet or exceed Cal Green mandatory measures at the time of approval.

B. Sustainable Landscape Design

1. *Drought Tolerant Landscape and High Efficiency Irrigation Systems*

The master developer of the CWSP is required to install drought tolerant landscapes throughout the CWSP area which will reduce water usage and comply with State and City of Riverbank Municipal Code Chapter 157: Water Efficient Landscape and Irrigation water conservation standards. (Appendix D) The CWSP will also utilize irrigation systems that are high efficiency and water conserving. The use of bubblers, drip irrigation and low flow emitters will be required.

2. *Pavement and Hardscape Areas*

A reduction in the amount of paving and hardscape will be encouraged in the CWSP area. The use of permeable surfaces such as landscaping, pavers, decomposed granite, gravel, grass and/or landscaping strips that allow water to percolate into the soil will be encouraged.

3.5.2 Community Elements

Community elements are important as unifying elements of the CWSP area. These elements will provide common features throughout the project that help to merge the different neighborhoods together and provide continuity. The community wide elements include parks and open space, streetscapes, gateways and monuments that will help define the community identity of Crossroads West.

A. Parks and Open Space Network

1. The parks and open space network throughout Crossroads West is comprised of park land, stormwater retention areas/dual-use basins and linear parks. This network of open space provides active and passive recreation areas, neighborhood parks and dual-use facilities. To further promote non-vehicular travel, the CWSP area provides bicycle lanes and walkways linked to all of the major destinations in the plan area. This community wide system of tree-lined streets, linear parks, bicycle lanes and walkways links all of the neighborhoods, mixed use areas and public facilities which promote walking and cycling as an alternative means of transportation.

B. Gateways, Monuments and Streetscape Landscaping

1. *Gateways and Monuments*

The CWSP provides the City of Riverbank an opportunity to distinguish itself through the use of Gateways and Monuments that clearly define Crossroads West and the neighboring developments along Claribel Road and Oakdale Road.



2. Streetscape Landscaping

Landscape treatments will be applied to street corridors, entry features, intersections and public spaces that will further define the character of Crossroads West and provide a clear continuity throughout the development.



Entry monumentation

C. Public Transit Linkage

1. The City of Riverbank has an established public transit system that is detailed in the existing circulation system of the 2025 General Plan. The CWSP shall incorporate areas for future bus stops or other public transit facilities as needed. Shelters, bus turnouts, benches and ticketing facilities should be planned in conjunction with the City's Development Services Department and any recommendations or requirements made through the Traffic Study.



Chapter 4

LAND USE

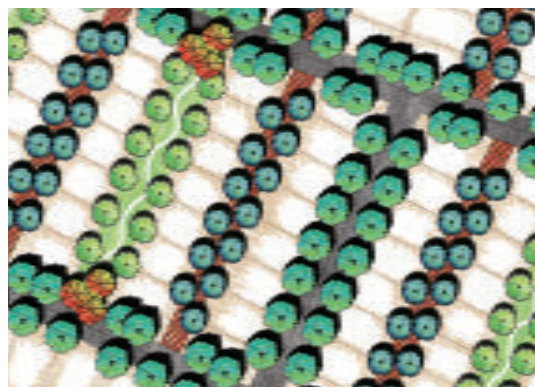
4.1 Overview

The land use chapter of a specific plan summarizes the various land uses within the project boundary and provides details about permitted uses, conditional uses and development standards. This chapter essentially provides the framework for the finished product for Crossroads West. The Crossroads West Land Use Plan and the land use categories discussed within this chapter establish the land use classifications for all of the areas within the CWSP area. The City of Riverbank's General Plan Map is being proposed to be modified according to the new land use designation to establish an overall General Plan designation for the CWSP area of Specific Plan. This plan and Figure 5 set forth the framework for the CWSP development consistent with the proposed Specific Plan land use designations. A comparison of the adopted General Plan Land Uses with the proposed General Plan Land uses for the project area is shown in Appendix A.

Development standards provided in this specific plan supersede any development standards contained in the existing Zoning Ordinance (and/or existing General Plan Map) for the City of Riverbank. For any instance where a development standard is not addressed in the CWSP, the City's Zoning Ordinance will apply.

The development standards of the CWSP will facilitate the construction of neighborhoods that are desirable, attractive and provide compatible land uses throughout the entire development. This process will allow Crossroads West to seamlessly blend in with its surroundings which include nearby residential developments and Community Commercial centers.

Formulation of the Crossroads West Land Use Plan was influenced by many factors including physical site constraints, environmental character of the property and the adjacent land uses which are discussed in greater detail in Chapter 2 Context and Setting. The Land Use Plan responds to the property owner's desires, input from the community, necessary development flexibility over time, analysis done on the project site for environmental impacts and infrastructure availability. The result is a comprehensive Land Use Plan that includes a balance of mixed land uses that have been organized and distributed throughout the site in a logical and efficient manner.



MDR example



Mixed Use Example





LEGEND/NOTES

- LOW-DENSITY RESIDENTIAL (LDR) - 5-8 DU/Acre ^{3,4}
- MEDIUM DENSITY RESIDENTIAL (MDR) - 8-16 DU/Acre
- HIGH DENSITY RESIDENTIAL (HDR) - 20+ DU/Acre
- REGIONAL SPORTS PARK (P)
- MIXED USE 1 (MU-1) ⁵
- MIXED USE 2 (MU-2)
- School (S)
- PARK/BASIN (P)
- Neighborhood Park
- ARTERIALS, COLLECTORS AND LOCAL STREETS

Acres	D.U.'s	Average D.U./Acre	S.F.
234	1,521	6.5	-
12	144	12	-
15.5	310	20	-
22	-	-	-
54	0/350	-	550,000/360,000
5	38	12	27,000
20	-	-	-
25	-	-	-
387.5 ¹	2,013/2,363		577,000/387,000

LEGEND/NOTES

- ACCESS POINTS FOR COMMERCIAL
- EXISTING TRAFFIC SIGNALS
- POSSIBLE TRAFFIC SIGNAL
- POSSIBLE SCHOOL SITE ³ (S)
- ACTIVE ADULT ²
- BIKE TRAIL
- STORM DISCHARGE
- POSSIBLE NEIGHBORHOOD PARK

NOTES:

- ¹ Total acreage does not include the existing Right-of-Ways for the MID Main Canal, Oakdale Road, MID Lateral No. 6, or Claribel Road.
- ² Active Adult is allowed anywhere within the LDR designation.
- ³ The School locations are a placeholder within the LDR.
- ⁴ LDR unit count includes Neighborhood Park land which has an underlying land use designation of LDR.
- ⁵ In Mixed Use 1, development could consist of all retail (550,000 sf) and no residential; or up to 350 units of residential and about 360,000 sf of retail.
- ⁶ Park area and school areas are included in the LDR acreage.
- ⁷ The Land Use areas and shapes as shown on this Land Use Map are conceptual. Shapes of the Land Use areas may be reconfigured based on final design.



Figure 5 – Proposed Crossroads West Land Use Map



This chapter is to be used in conjunction with other chapters that discuss the circulation system, public facilities, infrastructure, phasing and financing for Crossroads West. Given the time-frame (approximately 20 years) for the project development and the potential for changing market conditions and community desires throughout that period, this Specific Plan incorporates development flexibility over time. Such flexibility allows final decisions on uses and development siting to be made as conditions change over time within a framework that ensures the other project objectives are achieved. This process is further discussed in the Implementation and Administration chapter 10. A Design Review Process consistent with Section 10.4 of the CWSP will be performed to evaluate the consistency of development proposals with the Design Guidelines set in Chapter 8 which identifies color palette and design materials, landscape, and architectural style of buildings proposed within the CWSP.

As property owners and builders elect to develop property within the CWSP area, they will be required to submit Preliminary Development Plans (PDP's), Tentative Subdivision Maps and any other detailed plans as required by the City of Riverbank for review and approval. These maps and plans will be designed based on the policies and development standards contained in this chapter.

4.2 Main Elements of the Land Use Plan

The Crossroads West Land Use Plan (Figure 5) provides a visual representation of the land use distribution within the plan area. The Land Use Data Table (Table 2) provides a breakdown of the land uses with acreage and estimated dwelling units as well as maximum floor area ratio (FAR). This information is subject to change slightly as the land plan begins to develop according to current market conditions, consumer demands and builder direction. In no case will the total number of dwelling units nor the total square footage of anticipated development exceed the maximums set forth in this Specific Plan and the assumptions made in the Land Use Table.

Table 2 – Land Use Summary - Residential Land Use

Map Symbol	Land Use	Acreage	Range Density/S.F.	Range Units or S.F.	Average Density/S.F.	Average Units or S.F.
LDR	Low Density Residential	234	5 du/ac - 8 du/ac	1,170 - 1,872	6.5 du/ac	1,521
MDR	Medium Density Residential	12	8 du/ac - 16 du/ac	96 - 192	12 du/ac	144
HDR	High Density Residential	15.5	16 du/ac - 25+ du/ac	248 - 388	20.5+ du/ac	318
P	Parks/Open Space/ Regional Sports Park	42	N/A	N/A	N/A	N/A
	Schools	12/20	N/A	N/A	N/A	N/A

Table 2 – Land Use Summary - Non Residential/MU Land Uses

Map Symbol	Land Use	Acreage	Maximum Density/S.F.	Maximum Units or S.F.	Maximum S.F.
MU 1	Mixed Use 1 (R)	54	25 du/ac	350	0
	Mixed Use 1 (C)		0.35 FAR ¹	360,000 sf	550,000 sf
MU 2	Mixed Use 2 (R)	5	25 du/ac	50	N/A
	Mixed Use 2 (C)		0.35 FAR ¹	27,000 sf	N/A

¹Residential and Hospitality uses are exempt in Section 4.4.1.3 from the FAR maximum.



Connectivity amongst the varied land uses throughout the plan is an important component of the CWSP as it provides vehicular and pedestrian access to the Mixed Use centers of the plan as well as the residents who will reside in the dwelling units provided. Streets are designed as complete streets and allow for the efficient flow of vehicular traffic including walking, biking, and the use of public transit. These connections are shown in Figure 12 Pedestrian and Bicycle Circulation Plan in Chapter 5 of this Specific Plan.

The Circulation Element of the City of Riverbank General Plan was consulted while planning the circulation plan for Crossroads West. Claribel Road and Oakdale Road provide the existing backbone circulation system and the main access into the development. The CWSP provides for east/west collector roads as well as a north/south collector roads which provide connectivity for all of the neighborhoods to amenities and public facilities.

4.3 Land Use in Crossroads West

This section provides an in-depth look at the land use categories that make up the Crossroads West Specific Plan. Development Standards and Allowable Land Uses, Conditional Uses and Design Guidelines have been prepared for each category and are provided herein.

4.3.1 Mixed Use 1

The Mixed Use 1 (MU 1) land use designation provides the MU 1 developer flexibility for long-term implementation of the Specific Plan, consistent with the MU General Plan land use designation. Maximum development intensity (excluding residential or hospitality developments) would be at a Floor Area Ratio (FAR) no greater than 0.35, but overall development density is expected to be at a FAR of 0.25. In addition, up to 350 residential dwelling units could be constructed within the MU 1. While development of the MU 1 with all retail/commercial could result in around 550,000 square feet of development, the MU 1 plan provides the necessary flexibility for an alternative MU 1 Commercial and MU 1 Residential scenario containing up to 350 residential dwelling units and up to 360,000 square feet of retail/commercial development. The environmental impact report prepared on the CWSP analyzes impacts under both scenarios. The MU 1 development will be retail, commercial, office, hospitality, entertainment, recreational or similar uses, and residential is permitted as the market dictates. All future development within the MU 1 category will be subject to the Approval Process in Chapter 10 to ensure consistency with the Design Guidelines in Chapter 8, and Development Standards set forth in this Specific Plan.

4.3.2 Mixed Use 2

The Mixed Use 2 (MU 2) land designation similarly provides flexibility to the developer. Ultimately, the MU 2 designated land will be developed as neighborhood serving retail uses on a smaller, more intimate scale than the MU 1 area of the project. Residential uses are also permitted at medium or high density rates, with an assumed development at 20 du/acre for up to half of the MU 2 site. Office use is permitted and will further promote job creation throughout the CWSP. All future development within the MU 2 category will be subject to the Approval Process in Chapter 10 to ensure consistency with the Design Guidelines in Chapter 8, and Development Standards set forth in this Specific Plan.



4.3.3 Residential

The CWSP land use plan proposes three categories of residential land uses: LDR, Low Density Residential; MDR, Medium Density Residential; and HDR, High Density Residential. These residential designations provide varying densities that will ensure a mix of housing types and styles across the plan area. All future development within the residential land use categories will be subject to Design Review Approval to ensure consistency with the Design Guidelines and Development Standards set forth in this specific plan. The Land Use Plan in Figure 5 identifies possible locations for such land uses, and it is expected that within the LDR areas, a school site might be developed as well as a one (1) acre fire station. The final location of these public use areas will be determined by mutual consent of the land owner, City of Riverbank as well as the benefiting agency.

4.3.4 Parks and Open Space

The City of Riverbank's Westside Regional Sports Park is within the boundaries of the CWSP. As a part of the build out of Crossroads West Specific Plan, this regional sports park will be increased in size by a minimum of 11 acres. This expanded regional sports park combined with a dual-use linear park system, neighborhood parks, and linear trails make up the park and open space network for Crossroads West Specific Plan. While the acreage calculations of park land and open space are conceptual at this stage, the CWSP will meet or exceed the City's park land requirements for both Regional Parks as well as Neighborhood Parks. An initial park land dedication calculation based on the average units suggests the project obligation might be 19.6 acres. This is calculated as 2,013 units with 3.24 individuals per household, or a population estimate of approximately 6,425 people, and a regional park land requirement of 2 acre per 1,000 in population. The Neighborhood Park requirement is 3 acres per 1,000 in population, for an estimated total obligation of about 19.6 acres. Any dual use Park Basins within Phase A, B or C may receive partial park credit. The City of Riverbank has historically credited dual use park/basins at 50% towards the required City of Riverbank parkland dedication obligation. At the time of final design, a park credit may be requested and if the design provides adequate usable space, the City may grant park credit of 50% or more.

4.3.5 Interim Agricultural Uses

Interim agricultural uses limited to row crops, trees and vines will be allowed in any land use zone within the CWSP area. Existing agricultural uses (i.e. existing dairy) will be allowed to maintain existing operating functions after annexation is complete.

4.4 Land Use Regulations and Development Standards

The following sections describe planned land use designations and provide land use regulations and development standards for each zone classification within the specific plan and are illustrated on the Land Use Plan.



4.4.1 Mixed Use 1

4.4.1.1 Purpose and Scope

The MU 1 component of the CWSP area is intended to be developed as retail, commercial, office, hospitality, entertainment, recreation and/or residential uses (as further described in Section 4.4.1.3 below). The MU 1 will provide a mix of commercial services, shopping opportunities to serve the community and residential uses to provide a positive contribution to the City's economic base. In addition, the plan is intended to provide land use and implementation flexibility while providing land use and development criteria as set forth in this Specific Plan related to the MU 1 area, maintaining density caps for the MU 1 development.

4.4.1.2 Density/FAR

The City of Riverbank General Plan has analyzed development of the property with commercial uses up to an overall FAR of 0.30. Due to the shape of the property, and realistic development expectations, if the property were to develop entirely with commercial uses, a more realistic development intensity for commercial property is about 10,000 square feet per acre, or just a little under an overall site development FAR of 0.25. To recognize this, for the purposes of the Specific Plan and the accompanying EIR, a maximum commercial development intensity of 550,000 square feet for the gross 54 acre Mixed Use 1 parcel will be analyzed. At the same time, subareas of development within the Mixed Use 1 area may be developed at a higher FAR, up to 0.35, so long as the overall development stays within the upper limit of 550,000 square feet of commercial development. Residential and Hospitality developments may exceed the 0.35 FAR maximum. If this upper limit is to be exceeded in the future, subsequent environmental review will be required.

Since the Mixed Use 1 area also allows residential development at low-, medium-, and high-densities, consistent with the development standards for each residential category, the Specific Plan allows, and the EIR evaluates, an alternative that would allow up to 350 residential dwelling units to be constructed within MU 1. These residential units could be provided through horizontal integration in small development areas at low-, medium- or high- density intensities. While development of MU 1 with all retail/commercial could result in around 550,000 square feet of development, an alternative mixed use residential/retail/commercial scenario could contain up to 350 residential dwelling units and 360,000 square feet of MU 1 Commercial Use (defined below) development. The environmental impact report prepared on the CWSP analyzes impacts under both scenarios.

For the purposes of the Specific Plan, the maximum residential development option assumes about half of the property will develop commercially, up to an overall FAR of 0.30 (although sub areas development will still be allowed to develop up to an FAR of 0.35, so long as the overall limits are not exceeded. Residential and Hospitality developments may exceed the 0.35 FAR maximum. The commercial FAR has been increased for this option to the General Plan maximum standard. While the remainder of the site may be developed residentially, at a low-, medium-, or high-density intensity, vertical integration of residential development in commercial areas is preferred. If vertical integration of residential development occurs in the Mixed Use 1 area, the ground floor commercial land use will still be considered as developing commercially. As set forth above, the development of the MU



1 Property development will be retail, commercial, office, residential, hospitality, entertainment, recreation or similar uses (as further described in Section 4.4.1.3 below).

4.4.1.3 Allowed Land Uses

The allowable land uses within the community Mixed Use 1 areas (hereinafter “MU 1 Uses”) of the plan are:

- (A) All attached single-family and multi-family residential uses, including duplexes, apartments, boarding houses, multiple dwellings, dwelling groups, rooming houses, condominiums, townhouses, live-work units, accessory dwelling units, assisted living and independent living development.
- (B) Detached residential uses with a minimum net density of six units per acre, including single family homes, caretaker units, and manufactured homes.
- (C) Attached Higher Density Residential in a vertical and/or horizontal mixed use setting of 10 or more dwelling units per net acre.
- (D) Hotels and motels.
- (E) Churches (excluding open-air or tent), day care centers, vocational schools, and public swimming pool.
- (F) Clubhouse, lodge, social hall.
- (G) Office uses including administrative, professional, creative, medical, hospitals, urgent care, telecommunications, flex, research, and similar office uses that are primarily worker-occupied.
- (H) Parking lots.
- (I) Public or quasi-public utilities and buildings except those listed in Riverbank Municipal Code § 153.093(M), as amended.
- (J) Retail and/or wholesale sales and/or services including but not limited to food and specialty foods, groceries, open air markets, small household goods, pharmaceuticals and sundries, apparel, drug stores, general merchandise, art and supplies, antiques, furniture, appliances (sales and service), electronics, books, florists, flowers, hardware, toys, bicycles and sporting goods, music, bakery, camera, candy, china, clothing, confections, drapery, dry goods, ice dealer (less than five tons, sales only), jewelry, liquor store, news dealer, radio (sales and service), shoes, stationary, or other merchandise or any combination of the above unless otherwise specifically prohibited by this chapter.
- (K) Retail and/or wholesale sales of cannabis and/or services including but not limited to food and specialty foods (all cannabis uses are subject to the current requirements of Riverbank Municipal Code §120 and as amended).
- (L) Retail and/or wholesale sales of boats, building materials, or lumber yards (must be enclosed by a six foot fence), mobile homes, trucks and similar uses.
- (M) Secondhand stores or used merchandise (the operation or sale of wrecked automobiles, and junk are prohibited).
- (N) Service uses and establishments including personal and business services with walk-in customer service, such as automobile steam cleaning, bank, bar, barber shop, bath, beauty shop, spas, massage, cleaners, cafe, car wash, clinic, dance studio, delicatessen, delivery service, drapery cleaning, dressmaking, travel or real estate agents, medical clinics, child or adult care, photocopiers, mailing services, drive-in restaurant, gas station and service, equipment rental, ice cream parlor, laboratory experimental, motion picture, testing, launderette, laundry, meat locker,



mini warehouses when enclosed by a solid masonry wall at least six feet in height, museum, parking garage, photography, electronics, printing establishment, radio station, repair shops (appliance, radio, television), recreation center, restaurant, sign painting, tailor, taxi stand, television studio, theater, upholstery and veterinary. Rental uses of items such as party supplies, wedding apparel or tuxedos are allowed as service uses.

- (O) General commercial uses including entertainment and recreational uses including bars, full service restaurants, breweries and brew pubs, quick service restaurants and drive through fast food, dance and exercise studios, gyms, sports facilities, health clubs, children entertainment and recreational facilities, game arcades, and similar uses. Sales of alcoholic beverages do not require a conditional use permit unless the use is a bar, night club, or sit down restaurant that generates more than 50% of their total sales from the sale of alcoholic beverages. (Appendix F does not apply within the “Mixed Use 1” land use designation. For alcohol permitting refer to the MU-1 Conditions of Approval and Development Agreement).
- (P) Civic uses including government offices and services, libraries, community centers, conference facilities, and similar uses.
- (Q) Public infrastructure including support uses include water, storm water, sewer, and utility infrastructure, streets, sidewalks, bicycle paths, parking garages, transit stops, parks, plazas, streetscape improvements and other public facilities.
- (R) Utility facilities including communication equipment and buildings and electrical substations, together with the necessary buildings, apparatus or appurtenances thereto.
- (S) Institutional and educational uses including colleges, trade schools, day care centers, places of worship, museums, and similar uses.
- (T) Stamps, rubber or metal (manufacture or assembly).
- (U) Temporary construction building to house tools and equipment or containing supervisory offices in connection with construction projects during active construction on the same property.
- (V) Signs as allowed by this CWSP.
- (W) Garage sales as defined in Riverbank Municipal Code § 153.003 (for residential uses only), as amended.
- (X) Home occupations as defined in the Riverbank Municipal Code (for residential uses only), as amended.
- (Y) Accessory buildings normally incidental to permitted uses provided that such buildings are constructed concurrent with, or subsequent to the construction of the permitted use.
- (Z) Self storage facilities.
- (AA) New and used car lots in which all of the cars are in operable condition without major body damage. Such lots shall be paved and landscaped with an automatic irrigation system and shall include an office and rest room facilities. Used car lots need not be within an enclosed building or fence.
- (BB) Tattoo establishments or body art establishments, subject to the requirements of the Riverbank Municipal Code.

The term “hospitality” used within this Specific Plan is defined as hotel, motel, lodging, extended stay, other hospitality lodging with guest services, or similar type facilities.

The allowable land uses described in (A), (B), and (C) above are sometimes collectively referred to herein as “MU 1 Residential Uses”. The allowable land use described in (D) – (AA) above are sometimes collectively referred to herein as “MU 1 Commercial Uses”. In addition, MU 1 Residential Uses and MU 1 Commercial Uses are sometimes collectively referred to herein as “MU 1 Uses”.



Table 3 - Summary Table for All Mixed Use Development Standards¹

Mixed Use (MU)	
	Land Use Designations
	Commercial (C)
Development Requirements	
Development Intensity <ul style="list-style-type: none"> • Business or professional components • Commercial components 	N/A .25 - .35 FAR (Residential and Hospitality Uses are exempt from the FAR max)
Minimum front, side and rear setbacks <ul style="list-style-type: none"> • Front • Side⁴ • Rear⁴ • From Residential 	10 ft 0 ft 0 ft 20 ft
Maximum Building Height: <ul style="list-style-type: none"> • Within 30 ft of residential property • Within 50 ft of residential property • Within 100 ft of residential property 	40 ft 55 ft 70 ft
Development Requirements	
Minimum Landscape Coverage ²	10%
Parking (minimum) ³ <ul style="list-style-type: none"> • Retail • Restaurants/Bars • All Others 	1 per 300 sf 1 per 250 sf 1 per 300 sf
Decorative Masonry, Concrete or Metal Fencing adjacent to residential and along Mid Lateral No. 6	6 ft maximum height with 5 ft landscape strip
Footnotes: <ol style="list-style-type: none"> 1. Residential development in a mixed use area will be subject to the Development Standards for LDR, MDR or HDR development in this Specific Plan, based on the density proposed. 2. 50% shade cover in ten years for parking lot trees. 3. Minimum parking requirements are set forth above. The development can provide more parking than above as necessary in Developer's discretion. 4. There shall be a zero side yard, and rear yard setbacks where two commercial properties are adjacent to each other. 	

4.4.1.4 Development Standards

In addition to the design guidelines discussed in Chapter 8 of the CWSP, development standards within the MU 1 land use shall be those identified in Table 3.

In addition to the Design Guidelines discussed in Chapter 8 of the CWSP, development standards within the MU 1, Mixed Use 1 Land Use shall include the following:

- *Nuisances.* Operation by owner(s) and/or occupant(s) on any premises of the commercial development shall be conducted in a manner to reasonably minimize the amount of unreasonable noise, odor, dust, smoke, vibration or electrical interference off the site atypical with normal operations of a regional MU 1 Commercial Use complex. This requirement may be satisfied by recording it within a declaration or other instrument such as Covenants Conditions and Restrictions (CC&Rs) or an Operation, Easement and Access Agreement (OEA) against the applicable premises and binding future owners and/or occupants.



- *Landscaping.* A landscaping plan for the applicable portion of the development site requesting development plan approval including plant species, initial size, location, growth characteristics and method of irrigation shall be approved by the Community Development Director prior to issuance of any permit for such area. The required landscaping for the applicable portion of the development site shall be installed prior to final inspection of the building or buildings and shall be maintained by the property owner or tenant. MU 1 Commercial Use development approvals shall be based on the degree of compliance with landscape standards set forth in this Specific Plan as of project approval.
- *Trash storage area.* Trash bins for MU 1 Commercial Use development (excluding single-family dwellings, duplexes, or dwelling groups) shall be provided in a fully enclosed trash storage area. This area or areas shall be provided at locations that are readily accessible to tenants and sanitation collectors.
- *Off-street parking.* Off-street parking shall be at a minimum as provided in Table 3. For commercial uses not identified in Table 3, parking shall be a minimum of 3 parking spaces per 1,000 square feet, provided, however, development can provide more parking at the discretion of Developer. Parking shall be able to be shared among the various parcels in the project and via Shared Parking and Maintenance Agreement between the different MU 1 Uses in the MU 1 project area. This requirement may be satisfied by recording a declaration or other instrument (such as Covenants, Conditions & Restrictions (CC&Rs) or an Operation, Easement and Access Agreement (OEA) against the applicable premises and binding future owners and/or occupants.
- *Site plan review.* Proposals to construct on a parcel shall be subject to staff level site plan review by the Community Development Director prior to issuance of a building permit. This review shall be limited to compliance with Design Guidelines including, but not be limited to, comparison of the project with the Design Guidelines in Chapter 8 of the CWSP.
- In any instance where there is a conflict between the Development Standards, Design Guidelines and City Zoning Ordinance, the Development Standards and Design Guidelines from the CWSP supersede and govern development within the plan area.



Vertical Mixed Use



Horizontal Mixed Use



4.4.2 Mixed Use 2

4.4.2.1 Purpose

The Mixed Use 2 component (MU 2) of the CWSP area is intended as a pedestrian friendly environment with a mix of neighborhood serving retail/restaurant uses, entertainment uses, educational uses, and office uses. The Mixed Use 2 areas will be highly visible from the surrounding neighborhoods within the plan area and are meant to provide basic services and gathering places to the residents of Crossroads West.

4.4.2.2 Density/FAR

The maximum Floor Area Ratio (FAR) is the allowable, buildable area of the Mixed Use 2 land use. Within the CWSP, a FAR of 0.35 is permitted for retail and neighborhood serving uses. There is potential for multi-story buildings with residential units above retail and neighborhood serving uses. These residential units would conform to the Medium or High Density Residential standards with a density range of 8 to 25 dwelling units per acre with an average estimate of 16 dwelling units per acre.

4.4.2.3 Allowed Uses

The allowable land uses within the MU 2, Mixed Use 2 areas of the plan are the same as identified in 4.4.1.3.

4.4.2.4 Conditional Uses

Certain land uses may be permitted upon securing a Conditional Use Permit through the City of Riverbank Planning Department.

- (A) Residential care homes for more than seven persons, provided it is determined that the care home will not adversely affect commercial uses in the area and that commercial uses will not adversely affect the proposed care home.
- (B) Emergency shelters as defined in the Riverbank Municipal Code, as amended.
- (C) Transitional housing as defined in the Riverbank Municipal Code, as amended.
- (D) Supportive housing as defined in the Riverbank Municipal Code, as amended.

4.4.2.5 Development Standards

In addition to the Design Guidelines discussed in Chapter 8 of the CWSP, development standards within the MU 2 area shall be the same as those for MU 1 and shown in Section 4.4.1.5 – Table 3.

- *Nuisances.* No operation shall be conducted on any premises in such a manner as to cause an unreasonable amount of noise, odor, dust, smoke, vibration or electrical interference detectable off the site.
- *Landscaping.* A landscaping plan for all uses (except single-family dwellings, duplexes and triplexes) including plant species, initial size, location, growth characteristics and method of irrigation shall be approved by the Community Development Director prior to issuance of any permit. The required landscaping shall be installed prior to final inspection and shall be



maintained by the property owner. Approval shall be based on the degree of compliance with landscape standards as adopted from time to time by the City Council.

- *Trash storage area.* Trash bins (except single-family dwellings, duplexes, or dwelling groups) shall be provided in a fully enclosed trash storage area per City Standards. This area or areas shall be provided at locations that are readily accessible to residents and sanitation collectors.
- *Off-street parking.* Off-street parking shall conform as provided in Table 3. For land uses not identified in Table 3, the parking standards described in the City of Riverbank Municipal Code will govern, as amended.
- *Site plan review.* Proposals to construct on a parcel shall be subject to site plan review by the Community Development Director prior to issuance of a building permit. This review shall include, but not be limited to, comparison of the project with the approved Design Guidelines in Chapter 8 of the CWSP.



LDR Street View



Interior and Typical Lot Plot Plan

4.4.3 Low Density Residential

4.4.3.1 Purpose

The LDR designation is intended to provide detached, single-family dwellings on a variety of lot sizes and neighborhood configurations. Lot sizes will vary, and are expected to range in size from 4,500 square feet and larger. Please reference Table 4 – LDR for lot specifications and development standards.



Gated Entry Example

4.4.3.2 Density

The maximum number of units allowed on parcels designated LDR, Low Density Residential is 8 dwelling units per acre. The density range provides some flexibility and will accommodate 5 to 8 dwelling units per gross acre in the LDR designation, after deducting arterial and collector road right-of-way and any public facilities, like schools or parks. Average development density is calculated at 6.5 du/ac. The density range is provided to allow for a mix of lot sizes on different parcels throughout the plan area.

As shown on the Land Use Plan – Figure 5 – each of the LDR development areas, areas LDR-1, LDR-2, LDR-



Townhome Product



3 and LDR-4 will be required to provide a minimum development density of 5 dwelling units per acre, and can provide up to 25% of the area at MDR densities of eight (8) to sixteen (16) dwelling units per acre, as long as the entire area does not exceed 8 units per acre over the entire LDR planning area.

An example of this in LDR 3 is as follows:

Minimum Dwelling Unit (D.U.) Count:

55 acres LDR x 5/D.U.'s/acres = 275 lots

Possible Development Scenario:

13 acres MDR x 10/D.U.'s/acre = 130 D.U.'s

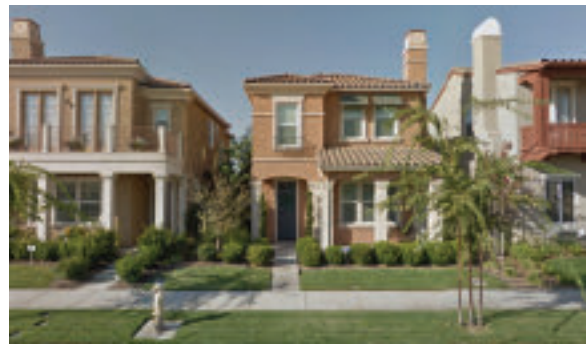
42 acres LDR x 5.4/D.U.'s/acre = 227 D.U.'s

Total D.U.'s = 357 D.U.'s

Average Density:

357 D.U.'s/55 acres = 6.49/D.U./acres

The development scenario would be allowed as it falls within the maximum LDR threshold of 8 D.U.'s/acre and does not exceed the average density of 6.5 D.U.'s/acres.



Alley Loaded Product

Table 4 - Low Density Residential Standards (LDR)

Lot Sizes:	7,000 s.f.+	6,000 - 6,999 s.f.	5,000 - 5,999 s.f.	4,500 - 4,999 s.f.
Setbacks (Minimum)				
Front Yard @ street				
Living Space	15'	15'	15'	12'
Porch	12'	12'	12'	10'
Garage Front Facing (Door)	20'	20'	20'	18'
Side-Entry Garage Wall	10'	10'	10'	10'
Side Yard				
Living Space (Interior Property Line)	5'/10'	5'	5'	4'
Living Space (Corner Property Line)	10'/15'	10'	10'	10'
Wrap Around Porch (Corner)	10'/15'	7'	7'	7'
Detached Garages/Accessory Unit	5'	5'	5'	5'
Rear Yard				
Living Space	15'	15'	10'	8'
Front Entry Attached/Detached Garage	5'	5'	5'	5'
Patio Covers (1 Story Height)	10' min.	10' min.	10' min.	8' min.
Height (Maximum to Ridge Line)				
Primary Dwelling	35' (2 Stories)	35' (2 Stories)	35' (2 Stories)	35' (2 Stories)
Detached Garage	15' (1 Story)	15' (1 Story)	15' (1 Story)	N/A
Detached Garage/Accessory Unit	25' (2 Stories)	25' (2 Stories)	25' (2 Stories)	25' (2 Stories)
Building Coverage (Single Story/Two Story)	60% / 50%	60% / 55%	60% / 55%	65% / 55%



4.4.3.3 Allowed Uses

The allowable land uses within the LDR, Low Density Residential areas of the plan conform to the City of Riverbank Land Usage Code, Title XV, Chapter 153 – Zoning, Section 153.031, R-1 Single-Family Residential District Permitted Uses located in Appendix E. In addition, up to 25% of the area may be developed to MDR, Medium Density Residential, and if this is proposed, any MDR area will need to comply with the MDR standards contained in paragraph 4.4.4 of this plan. For more information on various allowable land uses, please refer to Appendix E.



Courtyard Product

Table 5 - Medium Density Residential Standards (MDR)

Lot Size (minimum)	
Area, Interior Lot	2,500 sf
Area, Corner Lot	2,850 sf
Width, Interior	35'
Width, Corner	40'
Density Range and Average	
Overall Residential Density/ Allowable Residential Units per lot	8-16 DU/ACRE 1 primary dwelling no secondary units allowed
Setbacks (minimum)	
Front Yard	
Living Space	10'
Porch	5'
Garage (Front/Side Entry/Alley)	8' / N/A / N/A
Side Yard	
Living Space (Interior)	3' one side or 6' (zero lot line)
Living Space (Corner)	10'
Wrap Around Porch (Corner)	5'
Detached Garage/Accessory Unit	N/A
Rear Yard	
Living Space	8'
Garages with Rear Access	4'
Patio Covers	N/A
Coverage (maximum)	
Site Coverage	65% - 2/3 story
Height (maximum to ridge)	
Height Limit	45' (3 stories)
Private Open Space (per unit)	
Minimum Area	300 sf*
Parking Standards	
Off-Street Parking: minimum 2 spaces/dwelling Guest Parking: .25/unit	

*Open Space is made up of Private and Common Open Space. Private Open Space would include patios and open yard areas. Common Open Space would include amenities such as a Tot Lot, Picnic Area, Shade Structures, etc.



4.4.3.4 Development Standards

In addition to the Design Guidelines discussed in Chapter 8 of the CWSP, development standards within the LDR, Low Density Residential Land Use shall be as shown in Table 4. A Developer shall also prepare a PDP concurrent with, or prior to, tentative map and Site Plan Review approvals as described in Section 10.5 of this document.

4.4.4 Medium Density Residential

4.4.4.1 Purpose

The MDR designation is intended to provide areas within Crossroads West with smaller lot sizes for both attached and detached housing including but not limited to cluster homes, courtyard homes motor courts and townhomes. To promote a mix of lot sizes and product types, the lot sizes are expected to range from about 2,500 square feet to 4,499 square feet. Please reference Table 5 – MDR for lot specifications and development standards. Modifications will be allowed based on architecture presented through the PDP process.

4.4.4.2 Density

The maximum number of units allowed on parcels designated MDR, Medium-Density Residential is 16 dwelling units per acre. Flexibility is provided in this designation by allowing a range of densities from 8 to 16 dwelling units per gross acre in the MDR designation, after deducting arterial and collector road right-of-way and any public facilities like schools or parks. Average development density is calculated at 12 du/ac. The density range is provided to allow for a mix of lot sizes and housing product types on different parcels throughout the plan area.

4.4.4.3 Allowed Uses

The allowable land uses within the MDR, Medium Density Residential areas of the plan conform to the City of Riverbank Land Usage Code, Title XV, Chapter 153 – Zoning, Section 153.047, R-2 Duplex Residential District Permitted Uses located in Appendix E.

4.4.5 High Density Residential

4.4.5.1 Purpose

The purpose of the HDR land use designation is to provide attached, higher density residential options to residents of Crossroads West. Please reference Table 6 – HDR for lot specifications and development standards.

4.4.5.2 Density/FAR

The maximum density allowed within the HDR designation is 25 dwelling units per acre. This will be achieved through the approval and construction of multi-story buildings containing condominiums and



Table 6 - High Density Residential Standards (HDR) – Multi-Family/Apartments

FAR Standards	
Single-story building	FAR 0.60
Two-story building	FAR 0.80
Three-story building	FAR 1.20
Permitted Density (per HDR land use are and individual lot)	
HDR Density Range: 16 to 25 dwelling units per Gross Acre	HDR Average Density: 20.5 DU/ Gross Acre
Setbacks (minimum)	
Front Yard	
Living Space (First/Second story)	10'
Porch	10'
Garage (Front/Side Entry/Alley)	N/A
Side Yard	
Living Space (Interior)	5'
Living Space (Corner)	10'
Distance Between Buildings	10'
Open Space (minimum)	
300 Sq. feet/unit Private and Common Open Space*	
Coverage	
Site Coverage/Landscape Coverage	N/A
Height (maximum)	
Height Limit	75'
Projections	
Building elements such as chimneys, bay windows, roof overhangs, and other projections may extend no greater than 24” into a front or side yard setback. Roof overhangs on porches may extend no greater than 24” beyond face of porch columns.	
Parking Standards	
1 space per studio/1 bedroom, 1.5 spaces per 2-3 bedrooms and 2.5 spaces per 4+ bedrooms. Visitor parking 1 space/4 units.	

*Open Space is made up of Private and Common Open Space. Private Open Space would include patios and open yard areas. Common Open Space would include amenities such as a Tot Lot, Picnic Area, Shade Structures, etc.

apartment homes. Flexibility is allowed in terms of density in the HDR designation with a range of 16 to 25 dwelling units per gross acre.

4.4.5.3 Allowed Uses

The allowable land uses within the HDR, High Density Residential areas of the plan conform to the City of Riverbank Land Usage Code, Title XV, Chapter 153 – Zoning, Section 153.060, R-3 Multi-Family Residential District Permitted Uses located in Appendix E.

4.4.5.4 Compatible Transition from Higher Density Development to Less Intensive Uses

Appropriate transitional methods should be considered at all locations where high density development abuts less intensive land uses. In general, transitions between different types of intensities of land use should be made gradually, particularly where natural or man-made buffers are not available. The CWSP strives to meet the following objectives when compatible transition is necessary:



a. Site Orientation:

- (1) Site design should be oriented so that less compatible uses such as recreational facilities are located in the interior of the development and not adjacent or in close proximity to low-density residential neighborhoods.
- (2) Site access should be off of collector or thoroughfare streets.
- (3) Where appropriate, streets may be used as boundaries between different intensities of land uses.

b. Land Features:

- (1) Promote the retention of stands of trees, or natural vegetation whenever possible to separate higher-density residential development from a less intensive land use.
- (2) Where possible, use existing differences in topography to naturally separate higher-density developments and less intensive land uses.

c. Buffering and Landscaping:

- (1) Encourage the creative and extensive use of landscaping and berming techniques for effective buffering between differing intensities of land uses and to increase neighborhood privacy and security.
- (2) When using fencing around the perimeter of a multi-family development it should be used in combination with landscaping and possible berming.
- (3) Encourage the use of high quality materials in the construction of fencing and landscaping to decrease long-term maintenance costs and to make it less likely that neglected, unsightly areas will occur.

d. Lighting:

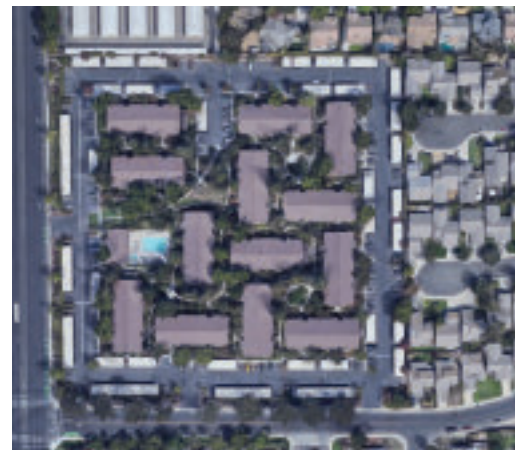
- (1) Any lighting used to illuminate parking areas, signs or other structures shall be arranged so as to deflect light away from any adjoining property or from public streets through fixture type, height, and location.
- (2) Exterior lighting of buildings shall be limited to low level incandescent spotlights and similar illuminating devices hooded in such a manner that the direct beam of any such light source will not glare upon adjacent property or public streets.



HDR elevation example



HDR Aerial View



Transition from HDR to LDR

4.4.5.5 Development Standards

In addition to the Design Guidelines discussed in Chapter 8 of the CWSP, development standards within the HDR, High Density Residential Land Use shall be as shown in Table 6.



4.4.6 Parks and Open Space

4.4.6.1 Purpose

Parks and Open Space are an integral part of any neighborhood and with the diverse vision for Crossroads West, the parks and open space element will be the constant theme carried through from each land use designation creating a cohesive community. The purpose of parks and open space is to provide visual interest to the residents and visitors; provide connectivity amongst neighborhoods and land uses; create gathering places for active and passive recreation; and promote walk-able, pedestrian-friendly neighborhoods.

The main Park and Open Space areas consist of the Central linear Park Basin identified on Figure 6, and a Conceptual design for an expanded Riverbank Regional Sports Park identified on Figure 7.

4.4.6.2 Allowable/Required Area

The Crossroads West development will be required to dedicate and improve park land and open space. The amount of this parkland dedication will be determined during the PDP review process, as defined



4-5 acre neighborhood park example



4-5 acre illustrative neighborhood vignette



Picnic Tables and BBQ



Linear Park Basin example



Park equipment



Bicycle rack



Shaded picnic area

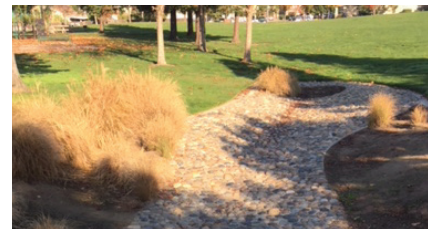


Half court basketball





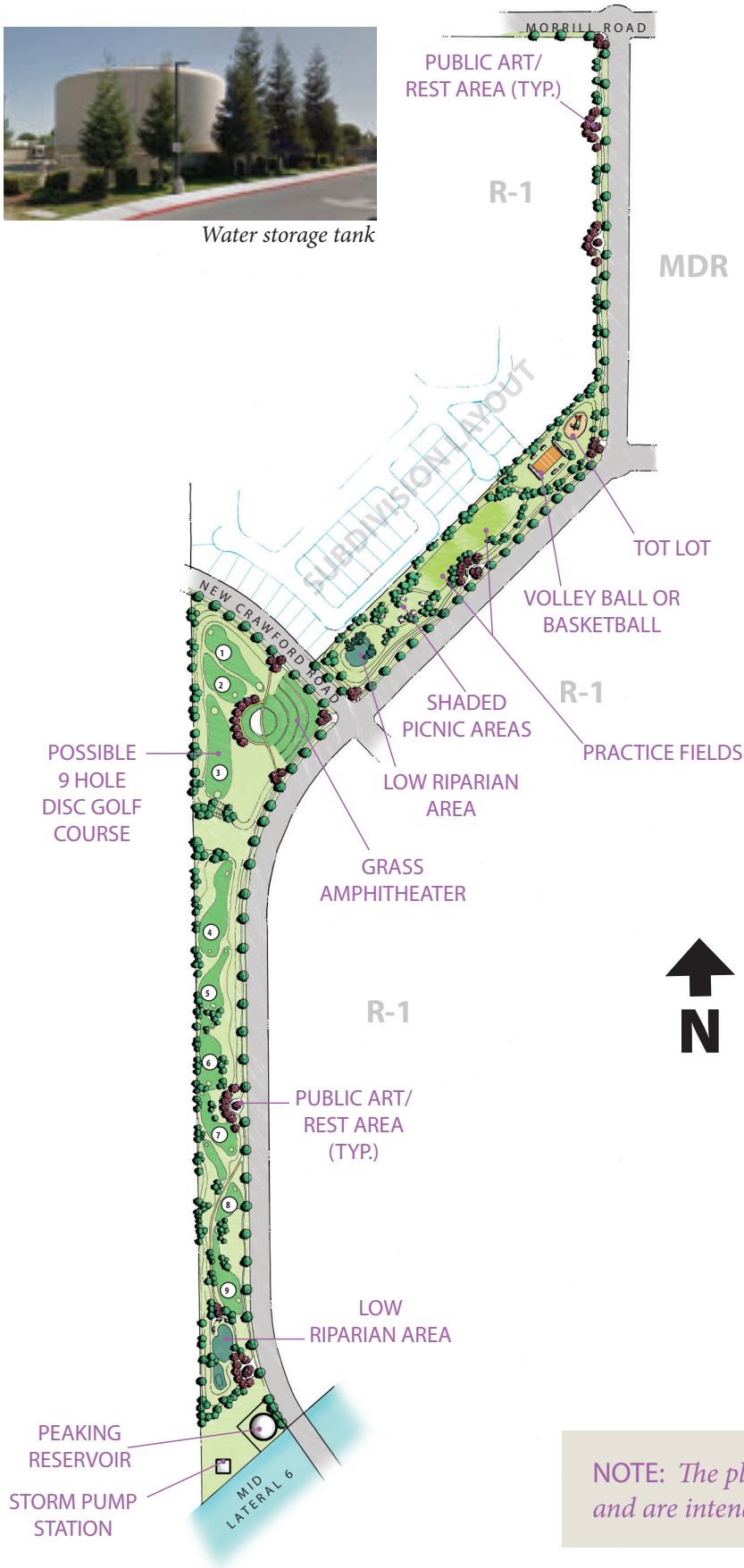
Water storage tank



LID Swale



Storm pump station



AREA OF DETAIL

NOTE: The plans shown on this page are conceptual and are intended to be for illustrative purposes.

Figure 6 – Linear Park Basin



in Section 10.5. and will be based on the number of residential lots proposed. An initial park land dedication calculation based on the average units suggests the project obligation might be 19.6 acres. This is calculated as 2,013 units with 3.24 individuals per household, or a population of approximately 6,522, and a regional park land requirement of 2 acres per 1,000 in population. The Neighborhood Park requirement is 3 acres per 1,000 in population, for an estimated total obligation of about 19.6 acres. Any dual use Park Basins within Phase A, B or C may receive partial park credit. The City of Riverbank has historically credited dual use park/basins at 50% towards the required City of Riverbank parkland dedication obligation. At the time of final design, a higher level of park credit may be requested and if the design provides adequate usable space, the City may grant park credit over 50%. To achieve parkland credit the dual use park/basin will need to be designed with City standard park elements which could include usable space not subject to inundation, play areas, restrooms or parking areas, a par course, and uses in inundation areas such as soccer fields, disc golf or similar uses. At the time of design, such park features will be taken into consideration, as will the amount of land subject to inundation, and the time of such inundation. A final park credit amount, which may be over or under the historic credit of 50% will be determined by the City in consultation with the developer. Since the 20 acre Linear Park Basin is a preliminary design, and its effective park credit is yet to be calculated, two additional neighborhood park (4-5 acre) locations have been shown in LDR-1 and LDR-4, if needed to meet the minimum park requirement. Depending on the credits granted for the linear dual use park/basin, these neighborhood parks can flex up or down in size. Any required parkland not provided within CWSP Area will be offset by paying an “In Lieu” park fee. Crossroads West Specific Plan area is home to the City’s west side Regional Sports Park, which will be expanded and improved with development of the CWSP. Please see Figure 7 - Conceptual Regional Sports Park.

4.4.6.3 Allowed Land Uses

Permitted uses in the Parks and Open Space designation will be limited to neighborhood parks, linear parks and dual use park basin facilities, water wells, tanks and other public facilities, as appropriate. Accessory buildings or structures built in conjunction with a park are also permitted in the Parks and Open Space designation. (See Figure 6)

4.4.6.4 Conditional Uses

Because of its specific function within the plan area, park land, landscape areas and open space are the only permitted uses for this designation. There are no conditional uses permitted.

4.4.6.5 Development Standards

In addition to the Design Guidelines and Landscape Guidelines discussed in Chapter 8 of the CWSP, development standards within the Parks and Open Space Land Use shall be subject to the Site Plan Review approval in conjunction with new neighborhood.

In any instance where there is a conflict between the Development Standards, Design Guidelines, Landscape Guidelines and City Zoning Ordinance, the Development Standards, Design Guidelines and Landscape Guidelines from the CWSP supersede and govern development within the plan area.





Figure 7 – Regional Sports Park Master Plan



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Chapter 5

CIRCULATION AND ALTERNATIVE TRANSPORTATION

5.1 Overview

This chapter describes the existing road network and proposed circulation system that will provide vehicular access in and around the plan area of Crossroads West. In addition to the circulation, transportation alternatives will be discussed to encourage the use of bicycles, public transit and other methods for moving through the plan area.

The implementation of the CWSP will provide additional roadways, bus transit, bicycle lanes and pedestrian connectivity between the plan area and other nearby developments, which improves the overall connectivity of the City. The Circulation Plan shows the relationship between existing roads and the future roads as identified in the CWSP and the City of Riverbank Circulation Element of the General Plan. These connections provide regional and local accessibility between land uses within and adjacent to the plan area.

The phasing of roadway improvements and new construction will be set by the master developer and the City to assure the improvements are completed in a timely manner. Further discussion on the phasing and financing of the circulation and alternative transportation improvements can be found in Chapter 9 – Project Financing Plan.

5.2 Existing Circulation System

The CWSP area is bound by Oakdale Road running north and south to the east, Claribel Road running east and west to the south. The closest main roadway to the west of the plan area is Coffee Road, which runs north and south. The plan area is bifurcated east to west by Morrill Road in the northern area of the site and Crawford Road running east to west midway of the site. Regional access to the project site is via State Highway 99, which is approximately 7 miles west via Claribel Road.

Oakdale Road and Claribel Road are classified as arterial streets in the City of Riverbank's General Plan. Arterial streets are designed to handle a higher volume of traffic and are typically placed on external boundaries of a project such as this to keep traffic moving and to prevent motorists from using the local streets.

Morrill Road and Crawford Road are classified as collector streets in the City of Riverbank's General Plan. Collector streets are designed to funnel traffic onto arterials and other major roadways. Improvements are planned on Morrill Road and Crawford Road so that they are able to handle the proposed traffic generated by the Crossroads West project.



5.3 Proposed Circulation System

The Crossroads West plan area is well suited for development because of the existing circulation system and roadways that exist in the vicinity. The CWSP proposes the construction of one additional collector street, running north and south through the plan area. This internal collector street will provide additional funneling of traffic through the project site without a reliance on local streets. This new internal north and south collector will intersect Claribel Road although its exact location has not been established, and it may be adjusted east or west during the tentative map approval process. Several new local streets will also be constructed for the neighborhoods within Crossroads West. The exact location of these streets is unknown at this time as they will be determined during the tentative map approval process.

In addition to new roadways, the CWSP proposes to install bicycle paths and lanes within the development. These lanes will be connected where possible to existing City of Riverbank Bicycle Lanes to provide optimal connectivity to non-motorized modes of transportation.

Working together, this network of roadways, bicycle lanes and walkways will provide convenient and safe access to all neighborhoods within the plan area as illustrated in Figure 8 – Proposed Crossroads West Circulation Plan. Construction of the roadway network will adhere to the adopted City Standards and Street Cross Sections, unless modified by this Specific Plan.

5.3.1 Street Classifications

5.3.1.1 Arterial Streets

Arterial streets feature 4 travel lanes, sidewalks on both sides of the street, bike lanes and landscape strips with the exception of the south side of Claribel, which is not the responsibility of the project. Arterial streets are intended to function like boulevards or thoroughfares and therefore do not allow on-street parking. Please refer to Figure 9 – Typical Street Cross Sections.

5.3.1.2 Collector Streets

Collector streets are smaller than arterials and typically only have 2 travel lanes, sidewalks on both sides of the street, bike lanes where applicable and landscape strips. On-street parking is allowed on collector streets as it is not uncommon for businesses to front onto collectors. Please refer to Figure 10, 11 and 12 – Typical Street Cross Sections for an illustrative view.

5.3.1.3 Local Residential Streets

These smaller streets are designed to handle small volume, neighborhood traffic with low speeds. The local residential streets being proposed for Crossroads West will feature 2 travel lanes, curb, gutter and sidewalk on both sides. On-street parking is permitted on local residential streets to provide additional parking for the residents of the neighborhoods within the plan area. Optional street sections are provided with variable width parkstrips and travel lane widths. The choice of which LDR/MDR street cross-section to be used in any development is at the discretion of the developer. Please refer to Figure 11– Typical Street Cross Sections for an illustrative view.



5.3.1.4 Cul-de-sacs and Roundabouts

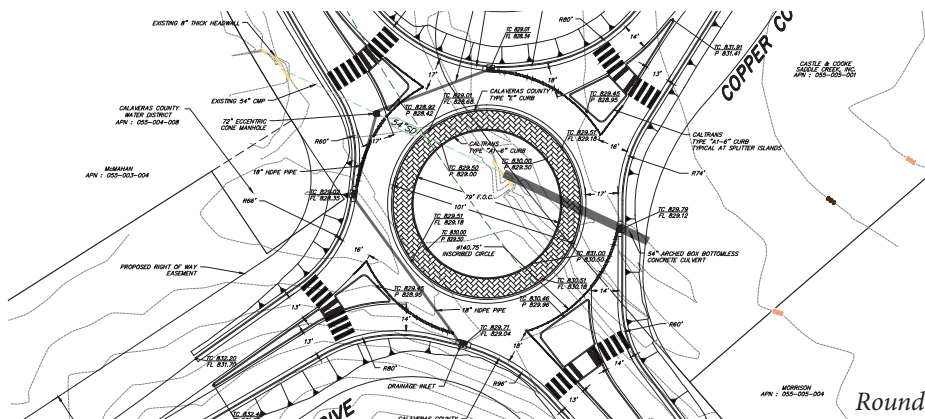
Cul-de-sacs are a popular street treatment in residential neighborhoods and are appealing to families with children because they provide a closed end street that does not allow through traffic. Cul-de-sacs proposed within the CWSP area will be designed and built according to City of Riverbank Street Standards and will provide adequate turning radius for emergency vehicles.

Roundabouts are often used as a method of traffic calming in neighborhoods and on collector streets and as focal points at the entrance of communities.

The Crossroads West Proposed Circulation Plan shows the possible location of roundabouts within the residential neighborhoods of the plan area and will not be included in the MU 1 property area. Roundabouts provide for the slowing of traffic on otherwise straight through street where speeds can escalate. Roundabouts also provide a place for public art displays, landscape treatments and traffic signage. All roundabouts shall be designed to the U.S. Department of Transportation Federal Highway Administration Roundabouts: An Informational Guide (Latest Edition Pub#FHWA-RD-00-067) and Caltrans Roundabouts.



Crawford Road roundabout



Roundabout design plan



Roundabout aerial view



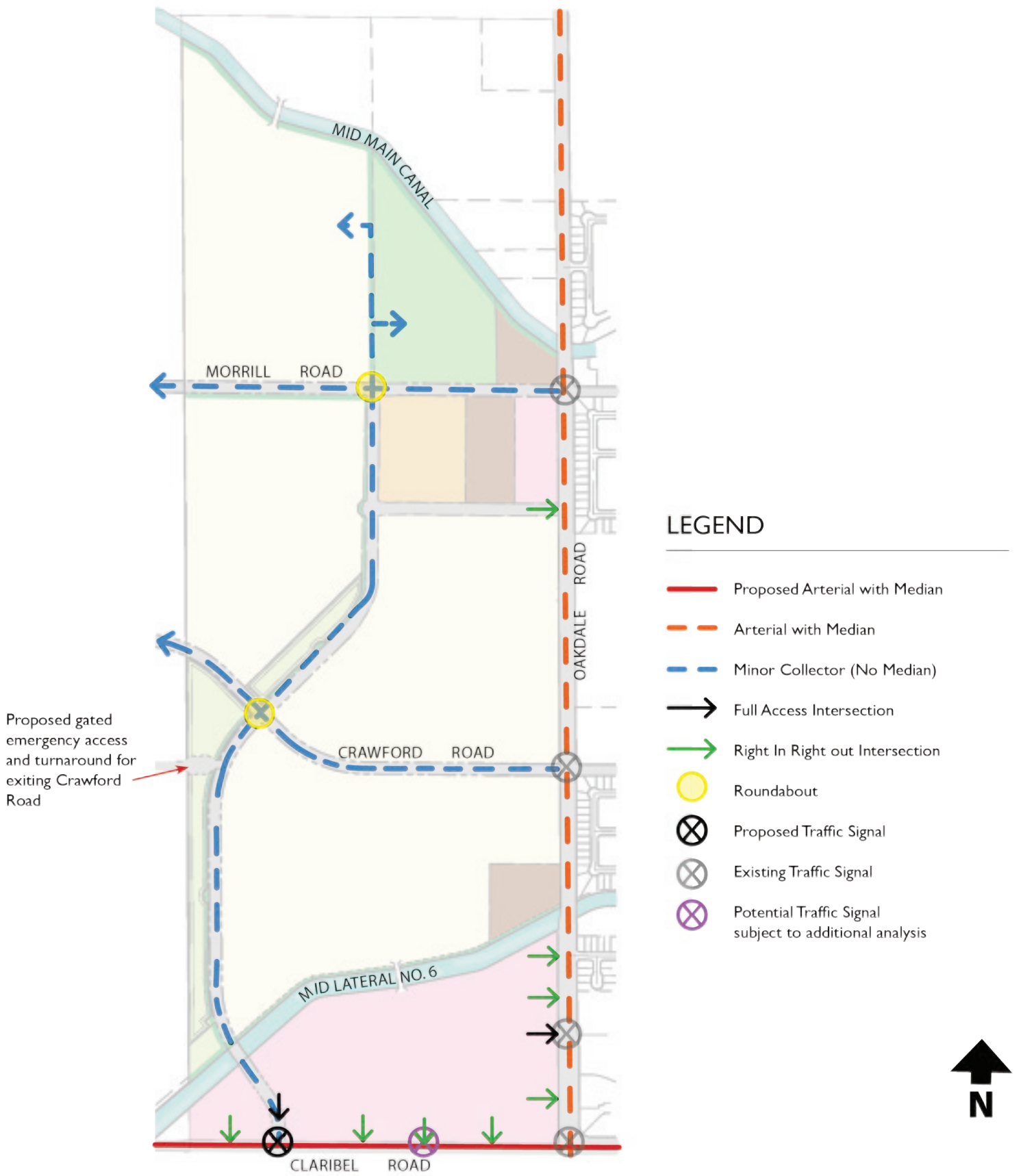
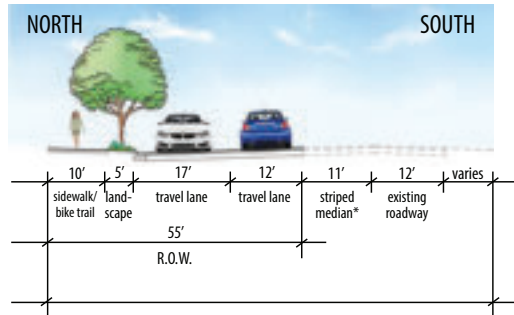


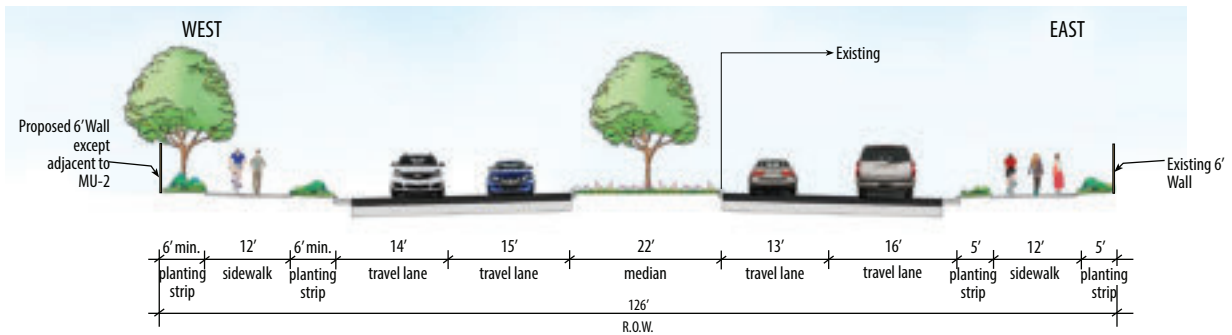
Figure 8 – Proposed Crossroads West Circulation Plan





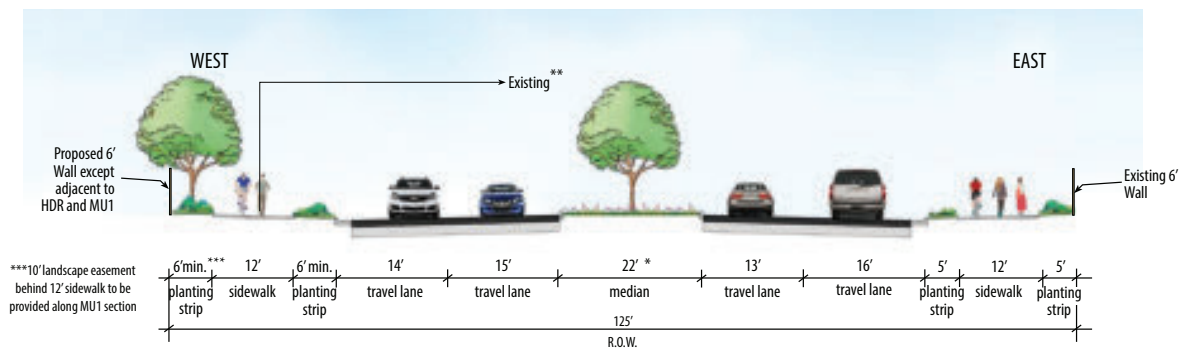
Claribel Road (R.O.W. varies)

*Width of striped median will vary to accommodate left turn lanes



Oakdale Road Section (North of Morrill Road to Crawford Road)

*Left turn pockets reduce planting strip



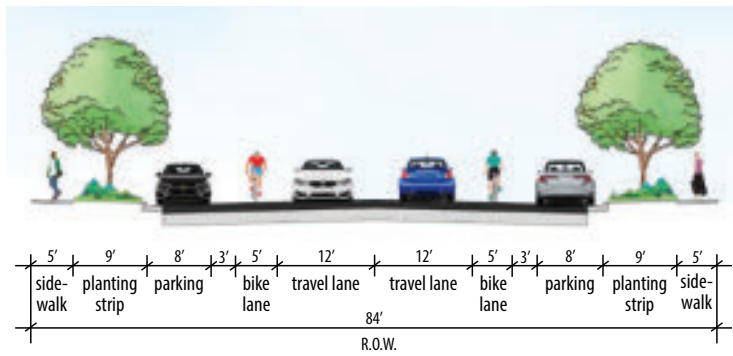
Oakdale Road Section (South of Crawford Road to MID Lateral No. 6)

*Left turn pockets reduce planting strip

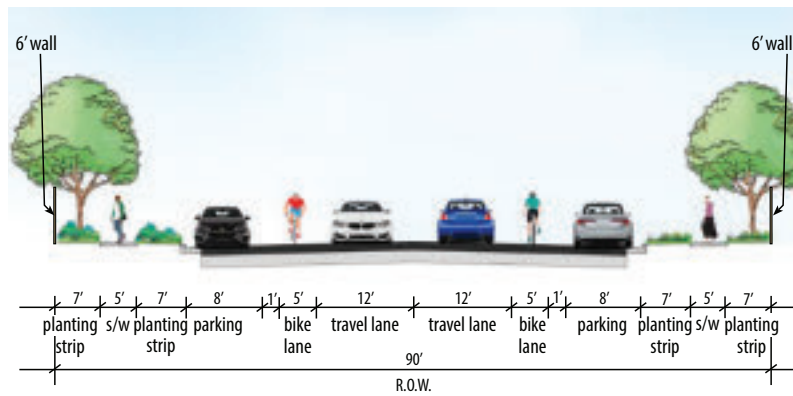
**All improvements are existing in this area except 12' sidewalk and landscaping on west side

Figure 9 – Typical Street Cross Sections

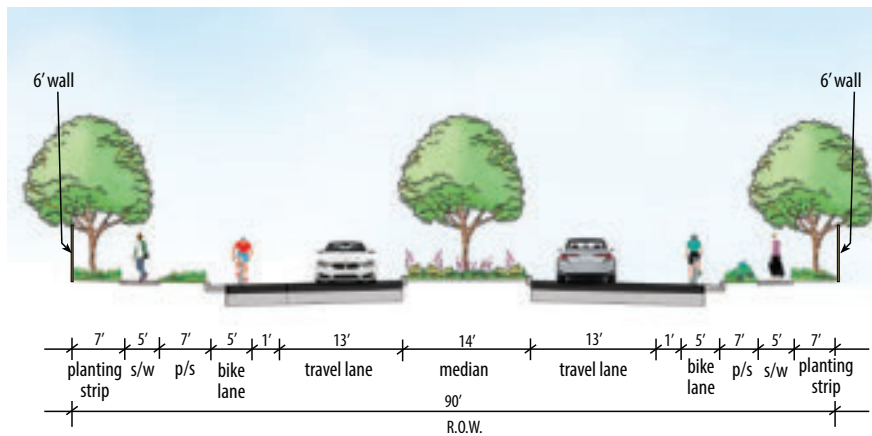




Collector Street Section without walls
(Morrill Road from Oakdale Road to west edge of project)



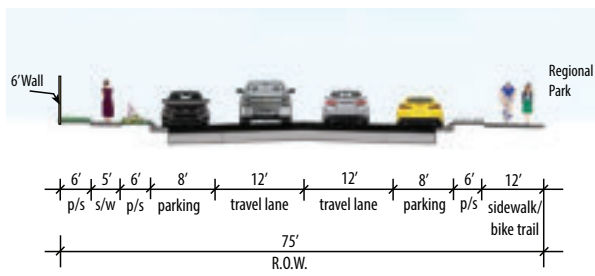
Collector Street Section with wall condition
(Morrill Road from Oakdale Road to west edge of project)



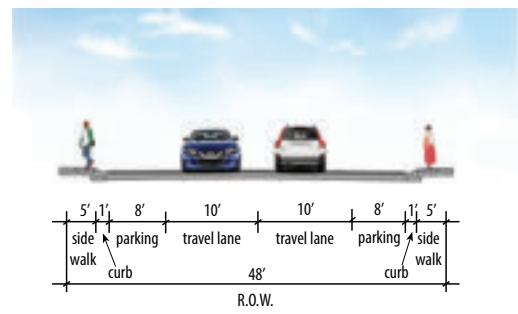
Collector Street Section with wall condition
(Crawford Road from Oakdale Road to west edge of project)

Figure 10 – Typical Street Cross Sections



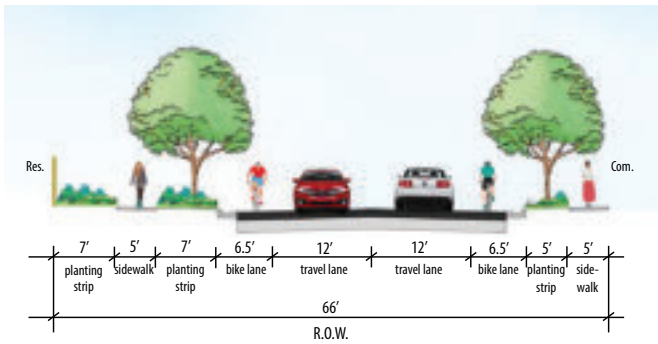


Minor Collector adjacent to LDR-1 and Regional Sports Complex

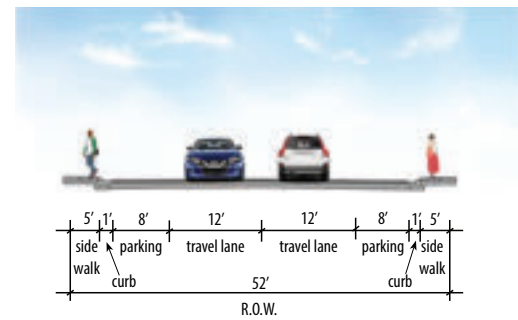


*LDR/MDR Street Cross Section

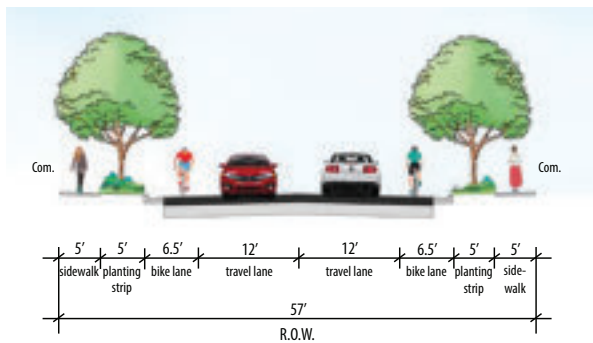
*The choice of which LDR/MDR street cross-section to be used in any development is at the discretion of the developer.



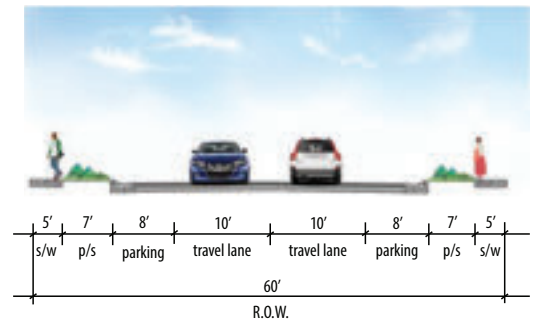
North/South Collector Street Section South of MID Lateral 6
(Residential/Commercial Condition)



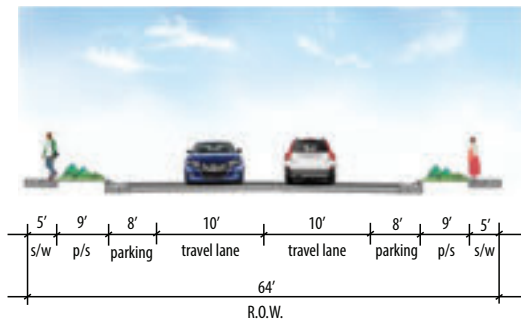
*Optional LDR/MDR Street Cross Section



North/South Collector Street Section South of MID Lateral 6
(Commercial condition (both sides))



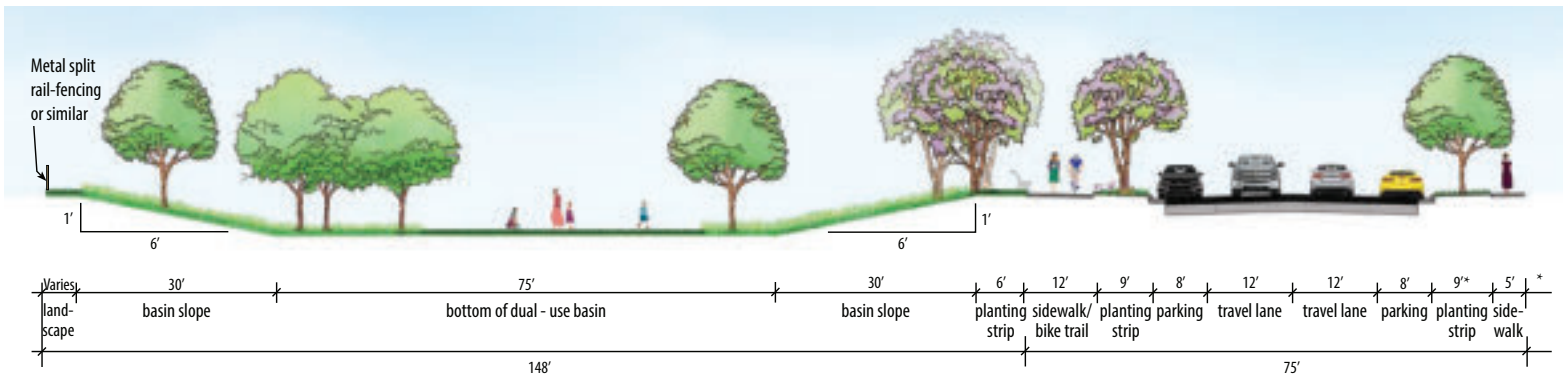
*Optional LDR/MDR Street Cross Section



*Optional LDR/MDR Street Cross Section

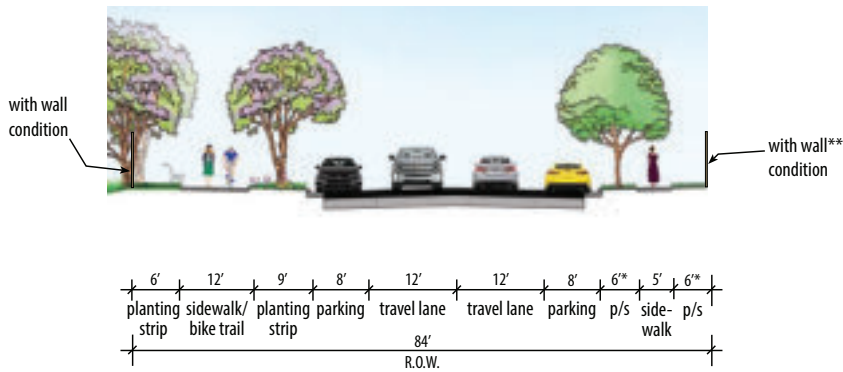
Figure 11 – Typical Street Cross Sections





Collector at Dual-Use Basin without wall (North/South Collector from Morrill to north side of MID Lateral No. 6)

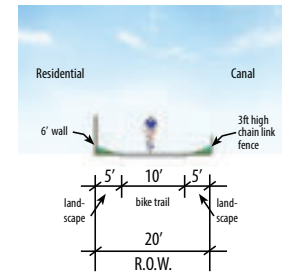
*Planting strip reduced to 6' with wall condition and 6' of landscape required on the back side of the sidewalk for a 78' R.O.W.



North/South Collector north of Dual-Use Park Basin to Morrill

*Planting strip increases to 9' and 6' planting strip behind sidewalk is removed without wall condition.

**Walls are optional along MDR section.



Class I Bike Trail along Canal adjacent to MID Lateral 6 and the Main Canal

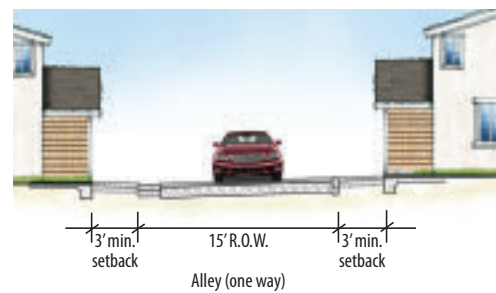
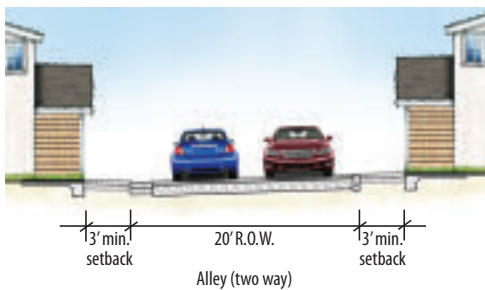


Figure 12 – Typical Street Cross Sections



5.4 Alternative Transportation

Alternative Transportation includes bicycle lanes, pedestrian walkways, bus routes and other means of public transportation. With growing concerns over the environment, motor vehicle use is on the decline as more and more residents are using other transportation methods to get to school, work and other amenities in their neighborhoods.

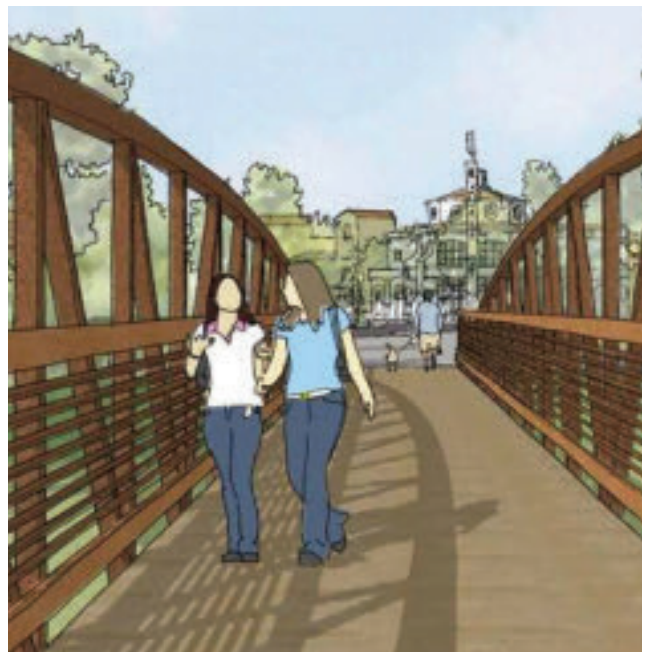
Class I and Class II Bike Lanes will be integrated into the Circulation Plan of Crossroads West. The likelihood that bike lanes will be utilized is dependent on their ability to provide direct routes to certain areas and amenities in the neighborhoods such as parks, employment centers and school sites. Safe, well-lit bicycle facilities that have been planned for efficiency and ease of access further lessen the use of motorized vehicles in the community.



Class I bike trail Oakdale Road

Pedestrian walkways and sidewalks will be provided throughout Crossroads West to encourage walking and jogging. Separated sidewalks provide a safe and pleasant walking experience in neighborhoods and provide connectivity to all major areas of the development including public facilities and shopping. When residents are provided with walk-able streets and pedestrian paths that provide ease of access to the main areas of Crossroads West, the development is encouraging an alternative to vehicular traffic. See Figure 13 – Pedestrian and Bicycle Circulation Plan

Bus routes and other public transit options will be planned for within the confines of Crossroads West. Riverbank Dial-a-Ride operates routes throughout the City and will likely add additional routes as new development occurs. Stanislaus Regional Transit operates loop Route 60 through Riverbank and into Oakdale. These public transit providers will be integral partners in developing suitable bus turnout locations and bus shelter facilities within the plan area. To support the use of public transit, it is likely these facilities will be placed near the highest intensity uses in the plan area including high-density residential units and the regional Mixed Use 1 areas. Figure 14 – Proposed Public Transit Locations shows where these bus turnouts and shelters could be placed in Crossroads West.



Pedestrian bridge



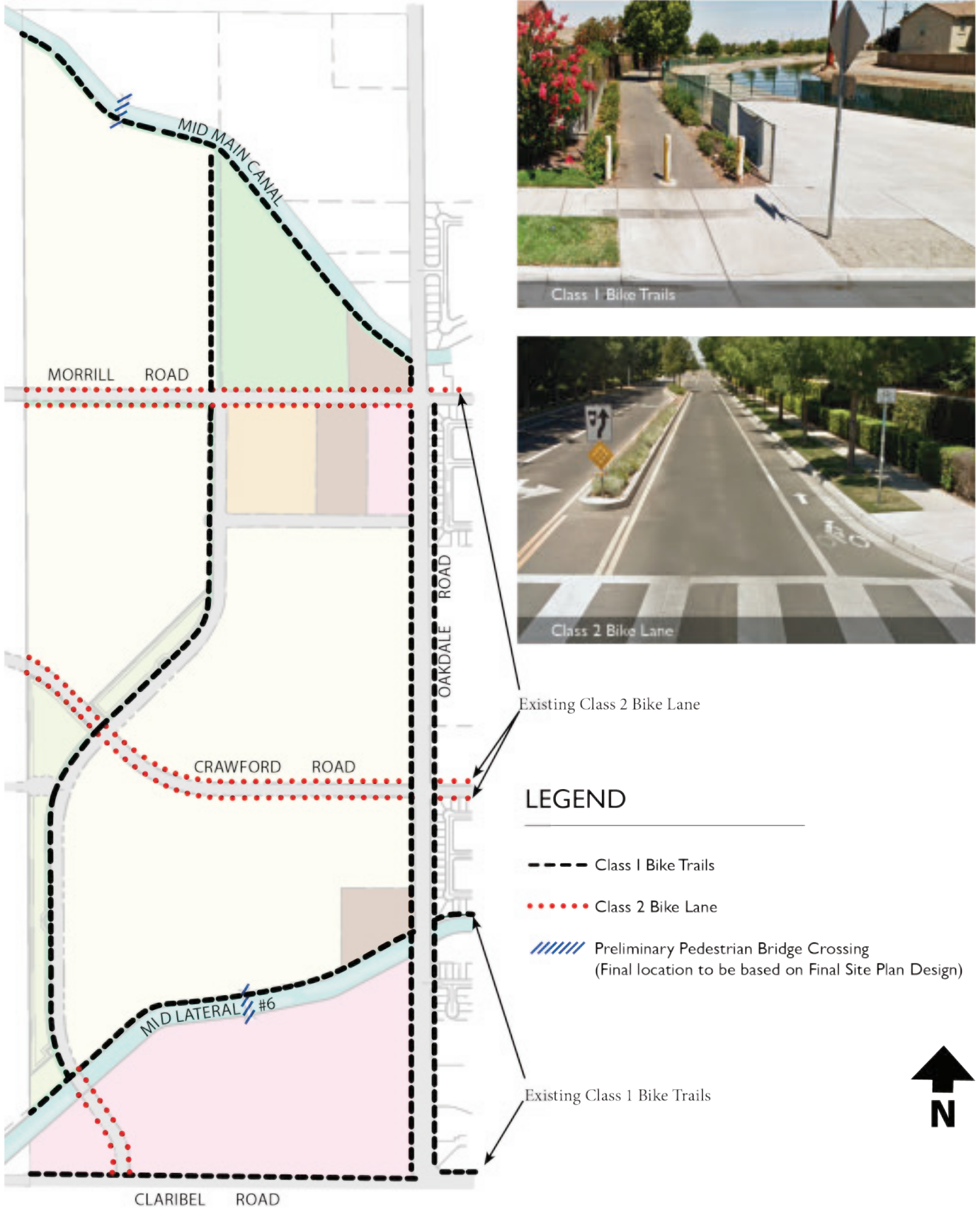


Figure 13 – Pedestrian and Bicycle Circulation Plan





LEGEND

- Public Transit Locations



Figure 14 – Proposed Public Transit Locations



5.5 Air Quality

5.5.1 Overview

The City of Riverbank and the CWSP area are located within the boundaries of the San Joaquin Valley Air Basin and are under the jurisdiction of the San Joaquin Valley Air Pollution Control District (SJVAPCD). The Air District has developed and implemented rules and policies to mitigate air pollution in the valley and lessen the accumulation of air pollution over time. The CWSP area will be subject to the rules and policies established by SJVAPCD. Air Quality standards are provided herein as they relate to air pollution caused by vehicle traffic and a more detailed discussion of possible pollutants and mitigation measures will be available in the Project EIR.

5.5.2 Air Quality Measures and Standards

Because ozone and particulate matter pollution levels in the San Joaquin Valley are some of the highest in the nation, the CWSP encourages people to walk, bicycle or use public transit for a significant number of their daily trips.

During phases of construction, materials and practices should be used that reduce direct and indirect air pollutant emissions. This includes dust control measures and proper disposal of construction debris.

To minimize health risks to residents of Crossroads West, incompatible land uses shall be avoided. This includes proper placement of industrial and manufacturing uses in areas buffered from residential neighborhoods to eliminate industrial waste and hazardous air emissions.

For buildings utilized by the public, the CWSP shall provide outdoor electrical outlets and encourage the use of electric powered maintenance equipment to reduce gas emissions. The use of electric vehicles shall be encouraged per Cal-Green Standards.



Chapter 6

PUBLIC SERVICES AND FACILITIES

Public Services and Facilities will be required to serve the future population generated by the development of Crossroads West. This chapter will review the necessary public services including law enforcement (police services), fire protection, solid waste disposal and recycling and parks that will be required as well as some possible facilities that may be developed within the plan area including school sites. The construction of any new public facilities will be at the discretion of the agency such as the school district or City of Riverbank and will be included on an as needed basis.

6.1 Law Enforcement

Law enforcement will be provided to Crossroads West area by the Stanislaus County Sheriff’s office who provides contract police services to the City of Riverbank Police Department. The Chief of Police for the City is responsible to protect and serve the residents, businesses and visitors to the plan area as well as provide emergency response services as a second responder to the Fire Department. The Crossroads West project will be required to create a Community Facilities District to generate revenue to pay for the extra police services required for the new annexation area.



City of Riverbank Police Department

6.2 Fire Protection

Fire protection services are provided to the City of Riverbank through a contract with Stanislaus Consolidated Fire Protection District. This department is the first responder to all medical emergency calls and 911 calls where fire services are requested.



Fire Station

Currently, the fire department operates out of Station #36, which is located in downtown Riverbank on Topeka Street.

This station is manned 24 hours per day. The need for an additional fire station will be evident with the development of the CWSP area. A new station is proposed to be on a site of 1-2 acres located along Oakdale Road near the intersection of Crawford Road and Oakdale Road at a location mutually agreed on by the City, developer, and the Stanislaus Consolidated Fire Protection District.

6.3 Solid Waste Disposal and Recycling

Garbage or solid waste disposal and recycling services are currently provided to residents and businesses in Riverbank through a franchise agreement with Gilton Solid Waste. Gilton provides each resident and



business with two collection bins upon signing up for services with the City. Solid waste, yard waste and recycling are all collected and sorted at the Gilton facility. The CWSP area is entirely within Gilton's service area and the specific routes and schedule will be established upon completion of the project.

6.4 Parks

The City of Riverbank prides itself on having abundant park land and open space for its residents to enjoy. Currently, the City has 15 parks ranging in size from small pocket parks to large, regional sports parks. Parks, open space and recreation areas are essential to creating a neighborhood environment within the Crossroads West community. The Land Use Plan is envisioned to be an inter-connected network of bike trails, walking trails and linear parks that provide easy access to all main areas within the plan including the Mixed Use 1 and Mixed Use 2 areas. CWSP proposes a dual-use park basins which are designed to handle peak storm water flow as well as be designed for recreation and gathering.

It is important to note that the park areas and open space areas shown on the Land Use Plan are approximate and final determination on size, location and amenities will occur with the submittal of a PDP. A Community Facilities District will be created to generate revenue for parks maintenance.

6.5 Schools

The existing City of Riverbank General Plan identified two possible schools sites within the Crossroads West Specific Plan area, one just north of Morrill Road, and one just south of Crawford Road. The school sites are identified on the Crossroads West Land Use Plan. The underlying land use designation is low density residential (LDR) for any school site, until acquired by the Sylvan Union School District.

The Crossroads West Specific Plan projects the development of about 1,665 detached single-family units and about 310 high-density (multi-family) units. Sylvan Union School District provides housing for elementary students in K-5 elementary and 6-8 middle schools. Secondary students in grades 9 through 12 will attend schools in the Modesto High School District. The following student generation rates were provided: (i) for single-family units, 0.280 for K-5 students, 0.150 for 6-8 students and 0.176 for 9-12 students, and (ii) for multi-family units, 0.083 for K-5 students, 0.111 for 6-8 students and 0.096 for 9-12 students.



Existing school

Based on the foregoing, the CWSP is projected to generate about 492 K-5 students, about 310 6-8 students, and about 323 9-12 students. Assuming school sizes of about 800 students for an elementary school, 1,200 students for a middle school, and about 2,400 students for a high school, the project would generate the need for about 62% of an elementary school, about 26% of a middle school and about 13% of a high school.



In addition, the Mixed Use 1 area is permitted to construct up to 350 additional residential units. Assuming all these 350 units are developed, and split as 175 single-family and 175 multi-family, the Mixed Use 1 area could generate up to an additional 64 (K-5) students, 46 (6-8) students and about 48 (9-12) students.

6.6 Maintenance Community Facilities District

A Maintenance Community Facilities District (CFD) will be created for the project to operate and maintain facilities including street lights, parkway landscaping, parks and other landscape areas, storm drainage systems, street maintenance as well as provide funding for police protection. At the time of development the developers within each phase of development (A, B, or C) will either be required to annex to an existing CFD, or create a new CFD specific to the service demands of the CWSP area by defined benefit.



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Chapter 7

INFRASTRUCTURE

7.1 Overview

The following chapter summarizes the existing utilities and infrastructure that exists within the plan area as well as provides the basic infrastructure requirements needed to adequately serve Crossroads West. The City of Riverbank's backbone infrastructure including domestic water, sewer and drainage systems will be discussed as well as build-out solutions and possible interim solutions that will allow efficient phasing of development.

It is important to note that the information provided for the infrastructure plan in this chapter is presented at a conceptual level, and, to the extent feasible, intended to follow the adopted City of Riverbank Master Plans. This Chapter 7 is meant to provide a general overview of the size, type and location of infrastructure. As the land is entitled through tentative subdivision maps or site plan review permits, the engineer of record will be required to certify the exact locations and sizing of all new infrastructure improvements.

In the event of a change in land use, the infrastructure master plans will be allowed revisions at the administrative level as long as they are found in substantial compliance with the Crossroads West Specific Plan and it is determined that the service levels are met. Revisions made to the land use plan or master infrastructure plans will require the review and approval of the City of Riverbank Planning Department, the City Engineer and the Public Works Department.

7.2 Water System

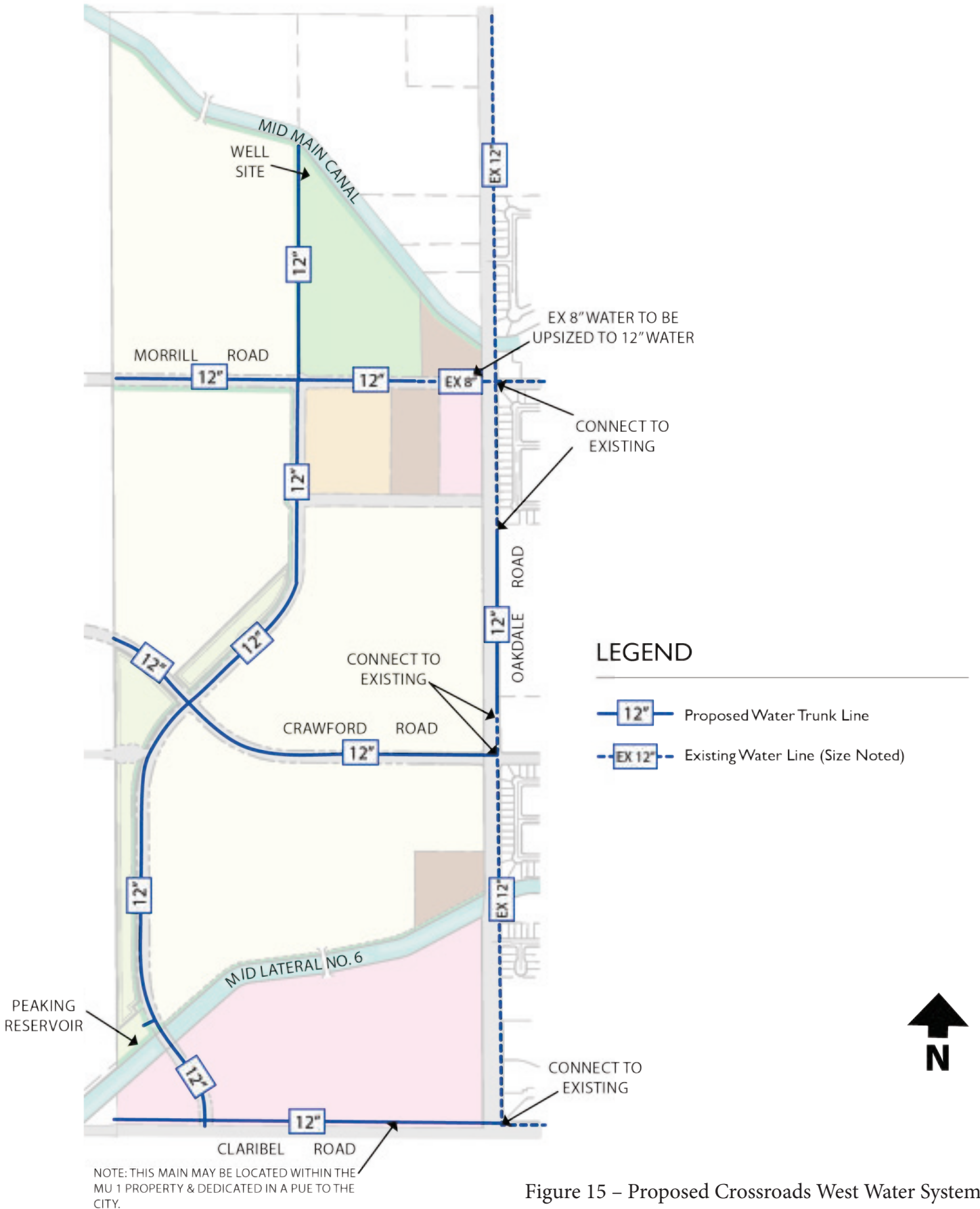
7.2.1 Existing Conditions

The City of Riverbank Domestic Water System provides four connection points for the Crossroads West project. There is an 8" line existing in Morrill Road just west of Oakdale Road that was constructed to serve the Riverbank Sports Complex in the northern end of the plan area. The 8" line connects to the City's existing water system at the intersection of Oakdale Road and Morrill Road and ultimately contributes to the residential water supply system east of Oakdale Road and two 12" stubs from the existing shopping center that will connect across Oakdale Road to the MU 1 property. Those two connections are available to provide water service to the MU 1 area if the MU 1 area is first to develop. This line was placed during construction of the existing Crossroads development in anticipation for future growth to the west.

7.2.2 Proposed Water System

Domestic water service will be provided to Crossroads West through the installation of water mains in the proposed arterial and collector roadways. Each land use will be connected to these main lines through an interconnected master water system. The installation of a 12" water main line from the existing stub at Crawford Road will service a portion of the plan area. Flows and demands for that portion of the plan





area will be determined at the design stage of development. This 12” line will serve as the initial supply for the first phase of development north of MID Lateral No. 6, which is likely to occur on the east side of the project. The existing water stubs are available for the MU 1 area and will come from the two 12” stubs in Oakdale Road.

With the development of the MU 1 at the corner of Claribel Road and Oakdale Road, connections will be made through connecting to existing water lines in Oakdale Road and to a new water line constructed along the Claribel frontage of/or within MU 1. These lines will be looped through the project to serve development. As part of the development of the property north of MID Lateral No. 6, when the new north south collector road is constructed and as part of that development, the water lines north of MID Lateral No. 6 will be stubbed to the south side of MID Lateral No. 6 and connected to the water line within MU 1 at the time the property north of the MID Lateral No. 6 is developed. A water balance and consumption report will be prepared with the development of MU1 to determine if additional water line improvement are required to provide domestic and fire flow service.

In addition to the installation of water main lines, the City of Riverbank Water Master Plan provides, and the CWSP proposes construction of a 1.69 million gallon Peaking Reservoir water tank to be located in the linear park near MID Lateral No. 6. A booster pump station will be constructed in conjunction with the water tank to distribute water to areas that will not be adequately served by the 12” main line. The ultimate water system build out will feature a tie-in to the existing 12” line which will provide uniform water distribution for the balance of the plan area. A new water well is proposed to be located in the Regional Park expansion area near the MID Main Lateral in the northern portion of the Specific Plan area. This well will be used to supplement the overall water system for Crossroads West. Please see Figure 15 – Proposed Crossroads West Water System for a conceptual domestic water system plan.

The timing of the construction of the new Peaking Reservoir water tank and well will be determined by a water balance and consumption report prepared at the time of the residential property development north of MID Lateral No. 6. All water improvements shown are part of the City’s Master Water Plan and are funded through the payment of City capital fees known as System Development Fees. If an adequate amount of fee revenue has not been collected when the well and/or tank are required, the CWSP developer’s will be required to front the cost of the master water improvements, subject to reimbursement through fee offsets, and/or repayments as fees are collected from other areas in the City.

The City of Riverbank Public Works Department will be responsible for the operation and maintenance of the proposed water supply, transmission main lines, water storage tank and well site upon approval and certification of the Improvement Plans submitted by the master developer.

7.2.3 Water Standards and Guidelines

The City of Riverbank requires all new residential, commercial mixed use or industrial development to be served by a public water system. The proposed water system for Crossroads West will be designed and constructed to operate at levels established by the City’s Public Works Department. Standards and Guidelines for the Domestic Water System shall include:

- Ensure the construction of a water system with adequate supply, transmission and storage to meet the needs of the CWSP.
- Minimum water pressure shall be provided based on standards established by the City; this pressure



- shall be adequate throughout the day and during peak hour demands. Minimum fire flows must be provided based on standards established by the City's Fire Department and Public Works Department.
- Individual water meter will be provided to all new parcels within the MU 1 Development. The MU 1 Development may elect in its sole discretion to have one or more water meters per parcel and/or development, or one or more water meters and/or private individual or multiple sub-meters per parcel and/or development as long as each parcel has its own City domestic water meter.

7.3 Sanitary Sewer System

7.3.1 Existing Conditions

The CWSP intends to tie-in to the City of Riverbank's existing sanitary sewer system at two different locations. An 8" line in Morrill Road, west of Oakdale Road, was installed to service the Regional Sports Park in the northern portion of the plan area. This line ties into the sewer manhole at the intersection of Oakdale Road and Morrill Road and is ultimately a part of the residential sewer collection system. An 18" line runs across Oakdale Road to Crawford Road, which was planned for the future development of Crossroads West. This stub is an extension of the main trunk line that services the existing Crossroads development to the east and extends to Roselle Avenue.

7.3.2 Proposed Sanitary Sewer System

To adequately service the CWSP area, new sewer main lines and an extension of the 18" trunk line will be constructed in the new arterial and collector roads in the project. These improvements will service the majority of the plan area however a portion of development south of Crawford Road will be required to utilize a sewer pump station that will be placed in the southwest portion of the site, near the Mixed Use 1 land use.

The Proposed Crossroads West Sanitary Sewer System, is shown in Figure 16. An 18" line in Crawford Road; a 10" line in Morrill Road; and an 8" line where Crawford Road intersects the westerly boundary of the plan area. All new sewer lines will be installed at varying slopes to provide the best service for the project. Should LDR 1 develop prior to the necessary sewer improvements or trunk line extension, this flow may utilize a temporary lift station that connects to the existing 8" line in Morrill Road.

The development of the MU 1 property may require the construction of an interim sewer connection to the existing sewer line at the intersection of Oakdale Road and Crawford Road. If the MU 1 site proposes to develop in advance of the property north of MID Lateral No. 6, the MU 1 site will connect to the existing City of Riverbank sewer system utilizing either the following scenarios at the MU 1 Developer's discretion:

- Scenario 1 – Interim Sewer Lift Station Only: The MU 1 site will construct a private interim sewer lift station which will be constructed on the east side of the collector street, directly south of MID Lateral No. 6. The entire project site will discharge to this lift station. Effluent from the interim sewer lift station will discharge utilizing force main to Oakdale Road, and connect to the existing 12" sewer line located on the north side of the existing Crossroads Shopping Center.
- Scenario 2 – Permanent Gravity System and Private Interim Sewer Lift Station: The MU 1 site will



construct a 12” sewer line extending from the existing 18” sewer stub at Crawford Road and Oakdale Road to the MU 1 site along Oakdale Road. A majority of the MU 1 site will gravity discharge to this 12” sewer line. An interim sewer lift station will be constructed on the east side of the collector street, directly south of MID Lateral No. 6 to serve areas of the MU 1 site that cannot gravity discharge to Oakdale Road. Effluent from the interim sewer lift station will discharge to the proposed gravity system constructed with the MU 1 site.

The final sewer scenario will be dependent on market conditions, project needs, and cost at the time of development of the MU 1 site as determined by the MU 1 developer in consultation with the City of Riverbank.

At the time the residential development occurs north of MID Lateral No. 6, and concurrent with the construction of the north south collector roadway through the project site and the construction of the bridge over MID Lateral No. 6, the sewer line will be extended to the south side of MID Lateral No. 6 to allow for a gravity connection from within the Mixed Use 1 development area. If this occurs in advance of development of the Mixed Use 1 property, then this connection will be available to serve the Mixed Use 1 site. If the Mixed Use 1 site has constructed a temporarily lift station and/or connection along Oakdale Road outlined in Scenario 1 or Scenario 2, at the time the new sewer connection becomes available, the temporary pump station and force main will be abandoned and connected to the new gravity sewer line.

A preliminary analysis was performed on the downstream system in Roselle, north of the Crawford Road Lift Station (CRLS). The existing flows from the CRLS are greater than the capacity in the stretch of 18” from CRLS to Talbot Lift Station (TLS) and from TLS to First Street. Therefore, a force main or a new and larger gravity main would need to be extended to a point downstream where the existing gravity sewer has adequate capacity.

The reduction of the CRLS flows from the TLS flows would be $3,272 - 2,100 = 1,172$ gpm. This flow is less than the 80% full capacity of the 18” line it currently ties into. Therefore, the existing line could remain and be utilized by the TLS. As mentioned above, the CRLS would need to have a force main extended past the TLS to a point where the gravity line could accept the flow plus any additional flow due to future upgrades to the CRLS. A proposed solution to the lack of capacity would be to extend a 16” force main from CRLS to the existing 30” sewer main near First Street. This improvement will provide additional capacity for areas outside of the CWSP, and will be subject to reimbursement or fee offsets.

7.3.3 Sanitary Sewer Standards and Guidelines

The City of Riverbank Sewer Collection System Master Plan provides the design criteria required for all new gravity flow sewer systems constructed within the City. These criteria will apply to the sanitary sewer system installed for Crossroads West.

- Ensure the construction of a sanitary sewer system with adequate transmission and storage to meet the needs of the CWSP.
- Sewers are required to be sized to meet minimum flows of 40 to 70 percent full.
- Maximum depth for sewer trunk lines is 30 feet; minimum depth for trunk lines is 3 feet.
- All future sewer lines, which will be incorporated into the sewer collection network will be



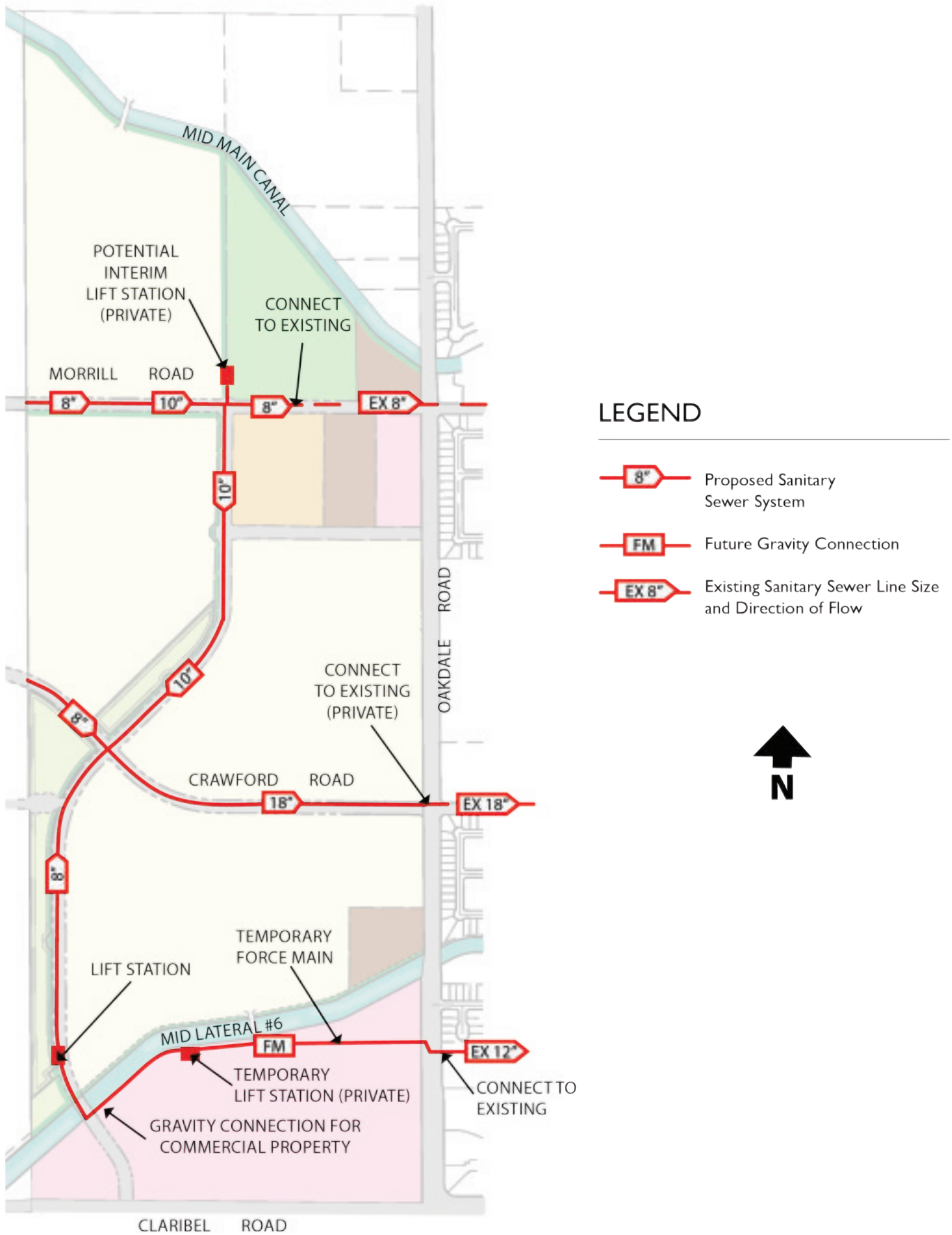


Figure 16 – Proposed Crossroads West Sanitary Sewer System



required to be comprised of mainly 8” lines and where applicable, 6” lines, in some cases, on-site private sewer service lines shall be as small as 4”.

7.4 Storm Drainage System

Storm water discharge regulations continue to become more and more stringent as urban development continues. Water quality is of the utmost importance for local municipalities therefore storm water must be properly retained, treated and discharged to promote optimal water quality. Storm water runoff from storm events collects and transports soil, pet waste, pesticides, fertilizer, oil and grease, leaves, litter, and other potential pollutants that ultimately wind up in the local water supply system. Thus, it is of critical importance to design and build efficient, low-maintenance storm drainage systems to help treat, retain and release storm water to the municipal water supply. The following subsections provide details and guidelines which show the adherence to the City of Riverbank’s Low Impact Development (LID) Practices, MS4 Permit Regulations and CASQA compliance.

7.4.1 Existing Conditions

Currently, the Regional Sports Park located at the northern end of the plan area and the MU 1 area are the only areas within the CWSP boundary that have drainage facilities to accommodate storm water runoff. The facilities there were developed as part of the overall plan for the Regional Sports Park tie into the existing City of Riverbank facilities located in Morrill Road and Oakdale Road. MU 1 has the ability to tie into the existing storm basin on the east side of Oakdale Road. The remaining project areas do not have a formal storm drainage system. Natural runoff generally flows to the south and west conforming to the site’s natural topography.

A full Geotechnical Report has been prepared for the site. Soil conditions vary slightly from north to south and east to west. The majority of the land is comprised of two main soil types, Greenfield Sandy Loam and Madera Sandy Loam. Greenfield sandy loam features slopes of 0 to 3 percent and has a depth of 38” to 54”.

7.4.2 Proposed Storm Drainage System

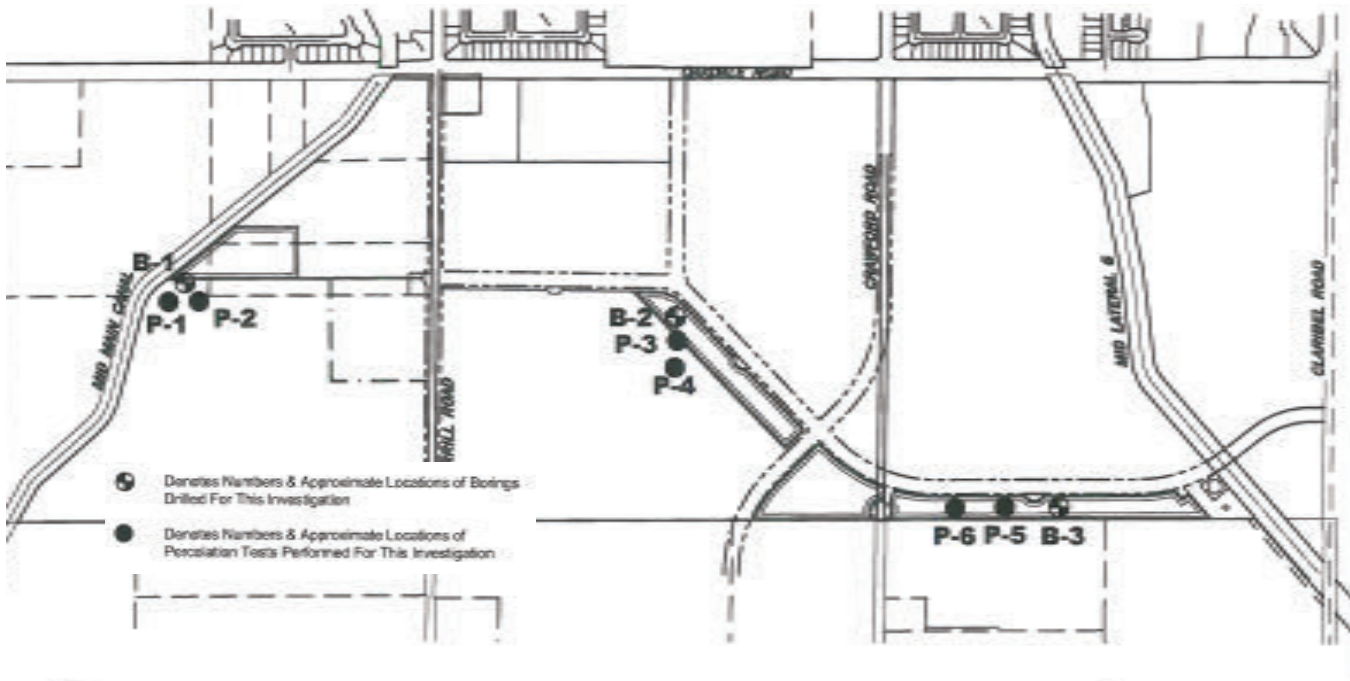
7.4.2.1 Low Impact Development Practices (LID)

In January 2013, the City of Riverbank and AECOM partnered to develop and adopt a Low Impact Development Design and Specifications Manual to assist developers in meeting State and Local mandates for storm water drainage. Negative impacts to the Stanislaus River, the San Joaquin Delta and regional wildlife have prompted many municipalities to design and adopt LID Practices and Guidelines. The CWSP is identified as a Greenfield/rural residential property in the LID manual and does not have any other land data available due to it being outside the current City Limit line.

The CWSP area (other than MU 1) is proposing a standalone drainage system that will filter, percolate and in some cases detain all storm water runoff for ultimate discharge into one of two adjacent irrigation laterals. The MU 1 shall use the remaining capacity in the drainage system basin at Crossroads Shopping Center and/or, if needed, some additional new on-site drainage facilities to be determined at the time of development of MU 1. Because of our Greenfield/rural residential designation within the LID manual, maintaining existing hydrological conditions by conserving natural areas and existing drainage features



is an important consideration where possible. Impervious hardscape surfaces (conventional roofs and paving) should be designed to discharge to pervious areas to help filter and infiltrate the stormwater runoff. To further aid infiltration, native soil compaction in landscaped areas should be minimized. Land planning for Crossroads West and the preliminary drainage studies and design are integrated which emphasize water conservation, protect water quality, help reduce flooding and improve the overall watershed health. The LID Practices devised are appropriate for the local and existing conditions found on the project site.



Boring and percolation testing locations

The property north of MID Lateral No. 6 in Crossroads West proposes to construct and use three major storm water detention basins. The first proposed basin may be located in a portion of the 11 acre expansion of the Riverbank Regional Sports Park, and drain the areas north of Morrill Road. The two remaining detention basins will be located north and south of Crawford Road on the west side of the proposed North/South collector. Kleinfelder was hired to perform boring and percolation testing in the locations of the proposed detention ponds. Each pond had two percolation tests performed for a total of six along with one deep boring at each pond to classify the deeper underlying soil. The percolation tests were performed at a depth that would be consistent with the proposed bottom of the proposed detention ponds. These percolation rates will be used as recommended in the report for design and sizing of the basins. The deeper tests may be utilized for the design of an absorption trench to percolate any nuisance water that may occur. LID practices can greatly improve storm water quality by encouraging processes (such as sedimentation, filtration, or evapotranspiration) which reduce the pollutants present in urban and suburban runoff.



The CWSP will utilize the City of Riverbank model standards and specifications for low impact development practices to ensure better water quality management, and recharging of ground water supplies, where feasible and reduce community infrastructure costs. While the City of Riverbank collects fees for regional water collection and disposal, the CWSP area will be exempted from these fees because the system proposed with this Specific Plan is considered a stand alone system. This exemption is appropriate as the CWSP will construct all necessary storm water collection and disposal facilities to serve the entire project area, as well as set up a Community Facilities District (CFD) financing district to maintain the system.

Best Management Practices (BMP'S) go hand in hand with LID guidelines to help address significant water quality issues and hydrologic concerns that developments create. Several design goals are required by the City and they include:

- conserve natural areas and drainages;
- minimize impervious surfaces, drain to previous area;
- minimize soil compaction;
- mitigate peak runoff and associated erosion; and
- treat runoff in storm water BMPs

Construction of Crossroads West is anticipated to be phased and will be directed by demand and need. Because of this, temporary basins will be needed to handle storm water runoff until the permanent facilities are constructed.



Bioretention areas are shallow, landscaped areas that receive and treat stormwater.

The MU 1 area of the CWSP has the right to utilize on-site storage and/or transmission to the existing off-site basin in the existing Crossroads development or a combination of both. Preliminary calculations for the site and existing grades helped to determine that the existing basin just east of Oakdale Road and south of MID Lateral No. 6 has approximately 8 acre-feet of additional storage capacity available to serve the MU 1 area.



Vegetated Basin

The MU 1 area shall have the right to use an on-site basin in conjunction with underground storage of storm water, surface water storage in parking areas and landscaped swale areas or use a combination of both and the existing basin in Crossroads Shopping Center. The design and construction of these improvements will adhere to capacity constraints described above and the adopted City of Riverbank LID Practices. No impact and/or storm drainage fees for the Crossroads West project shall be required as it is providing its own drainage facilities and/or using existing drainage facilities.

The MID Discharge Agreement currently on file for Crossroads will need to be modified to accommodate Crossroads West development. The agreement currently permits the discharge of storm water out of



existing basins easterly of Oakdale Road into the MID Lateral No. 6. These pre-existing contracts will need to be modified to add the additional discharge from Crossroads West. On-site percolation of storm water is anticipated to reduce the peak and long-term discharge volumes to the MID facilities.

All new construction projects in the City of Riverbank are classified in the LID Practices Manual based on their intended use, i.e. residential, MU 1 and 2, parking areas, etc. The following Design Standards must be implemented for all project classifications:

Mitigate peak run-off flow rates

- Conserve and create natural areas
- Minimize storm water pollutants of concern
- Protect slopes and channels
- Provide storm drain stenciling and signage
- Properly design outdoor material and trash storage areas
- Provide proof of ongoing BMP practices and maintenance
- Incorporate treatment control BMP's for water quality

LID Practices are most effective when they are dispersed throughout a development project. The CWSP area has been designed with this in mind and features linear park drainage basins running north and south throughout the project area. Treatment and attenuation of flows throughout the project site can be achieved by draining sidewalks to vegetated swales, constructing parking lots with bio-retention basins and/or permeable pavement and piping of roof leaders to the surface of a bio-retention area.

7.4.2.2 Municipal Separate Storm Sewer Systems (MS4) Regulations

The Clean Water Act was amended in 1987 and established the NPDES storm water program in an effort to improve water quality. The Act, implemented in two phases addressed the most significant sources of pollution in storm water runoff in Phase 1 and additional sources to protect water quality in Phase 2. The permit process is also set in 2 phases. Phase 1 MS4 Permits are required for all medium and large municipalities and Phase 2 MS4 permits are required for small municipalities in urbanized areas. The City of Riverbank is in Phase 2 of the MS4 Permit. Upon successful completion of the annexation of CWSP into the City of Riverbank, the project site will be included in the cities MS4 Permit obligations. All new development will be required to comply MS4 permit obligation at the development.



Basins that are thoughtfully designed and planted can manage stormwater from a larger area, while still offering aesthetic appeal.



Vegetated swales are shallow stormwater conveyance channels with vegetation covering the side slopes and bottom.



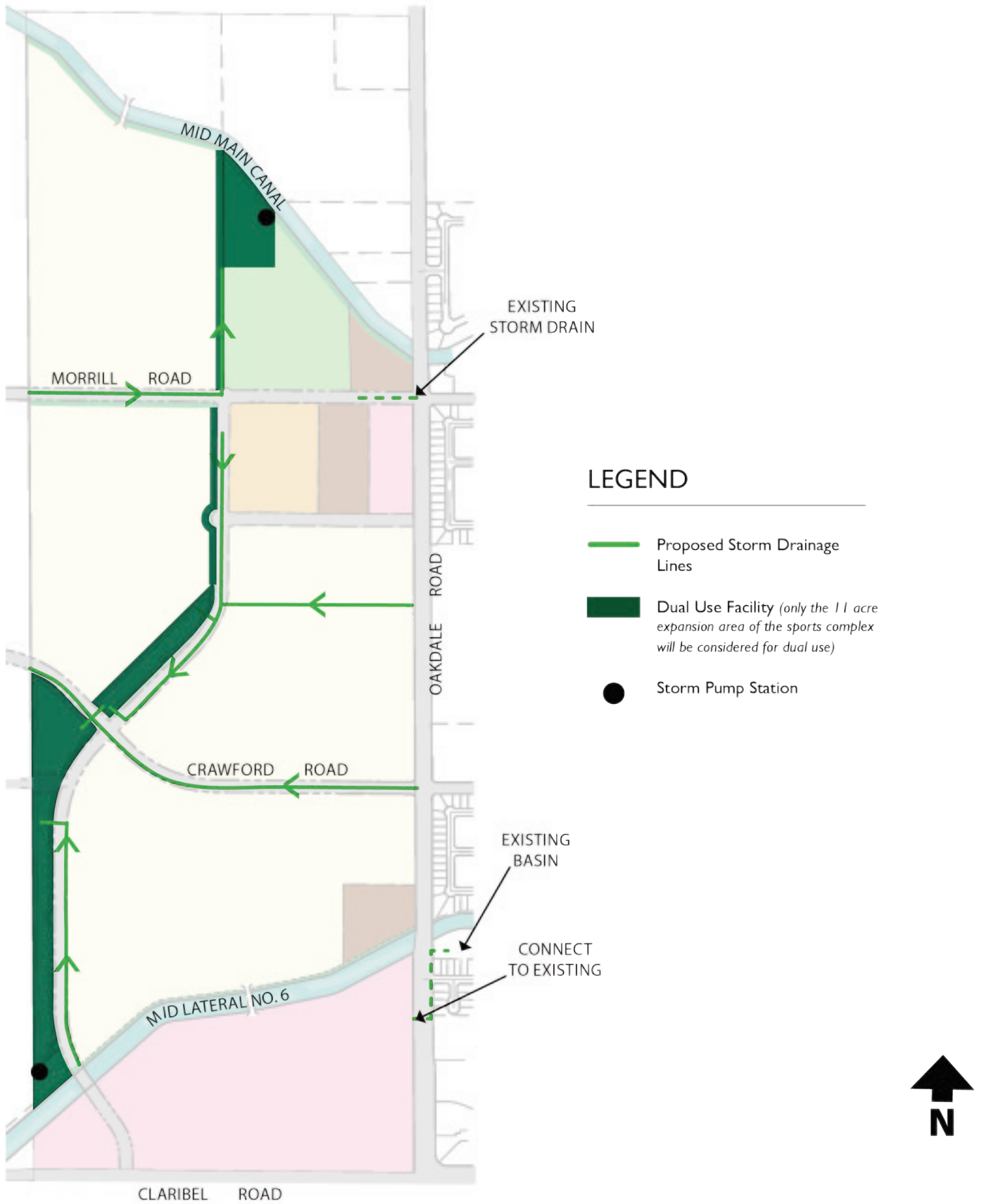


Figure 17 – Proposed Crossroads West Storm Drainage System



7.4.2.3 California Storm Water Quality Association (CASQA)

CASQA is comprised of professional members dedicated to the advancement of storm water quality. Since 1989, CASQA has provided education, implementation guidance, regulatory review, collaboration with local, state and federal agencies and assessment to ensure water quality is at the forefront of all new development. The City of Riverbank requires adherence to CASQA's mission which is largely to help California storm water permittees in developing, implementing and maintaining effective storm water quality management programs.

CASQA assists developers and projects like Crossroads West to be in compliance with the National Pollutant Discharge Elimination System (NPDES) mandates which are governed in conjunction with the Clean Water Act.

7.4.3 Storm Drainage Standards and Guidelines

Figure 17 provides an illustrative of the Proposed Crossroads West Storm Drainage System. This system will be designed to adhere to the City of Riverbank Storm Drain System Master Plan, the LID Standards and Specifications, the MS4 Permit Regulations governed by the California Department of Transportation – Storm Water Management Division and the CASQA recommendations and guidelines. The developer for CWSP is charged with designing and maintaining a system that will provide an adequate storm drain system that will function according to City established levels of service.

Guidelines for construction of the storm water drainage system shall include but are not be limited to:

- The overall storm drain system and storm drainage facilities shall be designed to handle and properly carry a 10-year storm event.
- Dual-use basins north of the MID Lateral No. 6 shall be constructed for the majority of the CWSP to function both during a storm event and a non-storm event. The maximum depth for these dual-use basins shall be 6 feet with a maximum water depth of 4 feet with 2 feet of freeboard.
- The majority of the CWSP storm drainage system will be designed as a linear park system beginning at the Regional Sports Park and continuing throughout the development until it's connection point at the Mixed Use 1/Mixed Use 2 components.
- The Mixed Use 1 development will utilize available capacity in the existing basin on the east side of Oakdale Road, just north of the existing shopping center combined with on-site detention.
- Appropriate accommodations for ADA access will be required for all dual-use storm basin concepts.
- All standards and guidelines, where appropriate, from the LID Standards and Specifications adopted by the City of Riverbank in January 2013, shall be implemented into the design and construction of the Storm Drainage System.
- All regulatory requirements of the MS4 Permit for the City of Riverbank shall be adhered to during the design and construction of the Storm Drainage System.



7.5 Dry Utilities

Dry utilities for the plan area include electricity, natural gas and telecommunication services. These services are not typically provided by the City and therefore rely on outside service providers. This section provides details on the dry utility providers who will service the CWSP area.

7.5.1 Electricity

Electricity service is available from Modesto Irrigation District (MID) and they have confirmed adequate supply for the area. New power lines will be installed underground, which conforms to the City development standards. Each user, tenant or residential unit will be individually metered for their electricity use.

7.5.2 Natural Gas

The entire CWSP area falls within the service boundary for PG&E and therefore they will be the natural gas provider. Similar to electricity service, new transmission lines will be installed underground for Crossroads West, which meets City requirements. Individual connections for retail tenants and residential units will be established for usage and billing purposes.

7.5.3 Telecommunications

Telecommunications services include phone service, fiber optics and cable television. AT&T Residential Division will be the primary phone and fiber optic provider for residents of Crossroads West. Charter Communications will be the primary cable television provider. AT&T Business Division will be the primary provider for the retail, Mixed Use 1 and civic uses within the plan area. As with the other utilities, all new transmission lines will be constructed underground to meet the requirements of the City.

7.5.4 Dry Utility Standards and Guidelines

- Tentative subdivision maps and/or development plans shall be submitted to the City and appropriate utility companies to confirm the location, sizing and availability of service to the Crossroads West development.
- Builders within the CWSP area shall coordinate with all dry utility providers to ensure proper design guidelines and criteria are met when preparing improvement plans.
- Telecommunication services including fiber optics if available shall be provided to every residential unit within the plan area to enhance the opportunity for telecommuting and home based businesses thereby reducing the impacts on the transportation system and air quality.
- All new transmission lines for electricity, natural gas and telecommunications shall be constructed underground per City of Riverbank requirements unless otherwise noted and approved by the City and the utility provider or otherwise economically infeasible to install underground. This excludes all transformers and secondary boxes in commercial areas only. Switch gear boxes shall be planned for and properly screened from public view. Special attention shall be made to prevent any utility box from being a target for Graffiti.
- Existing overhead transmission lines shall be placed underground where practical.
- Service standards for dry utilities providers are established and enforced by the California Public Utilities Commission.



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Chapter 8

DESIGN GUIDELINES

8.1 MU 1 Overview

The purpose of these Design Guidelines is to set out the permitted architectural theme and future development plans for the retail commercial development within Mixed Use 1 and Mixed Use 2. If low, medium or high density residential is proposed, it will be designed consistent with the design guidelines starting in Section 8.3. These guidelines are intended to direct the future development of this site ensuring architectural compatibility, site design, landscape features, intensity of development and signage program. These development guidelines are intended to provide the necessary flexibility (i) to allow creative freedom and diversity, (ii) for the project to be developed as a phased development, (iii) for tenants and future tenants to be and remain attracted to the regional MU 1 Commercial Use facility developed, such as a regional and/or neighborhood Shopping Center and/or commercial uses, (iv) for potential future changes and/or additional changes in retail and/or other permitted MU 1 Commercial Uses going forward, and the development of the best possible project. This document outlines the guidelines under which the MU 1 Commercial Uses, MU 1 Residential Uses, and other permitted mixed-uses can successfully coexist.

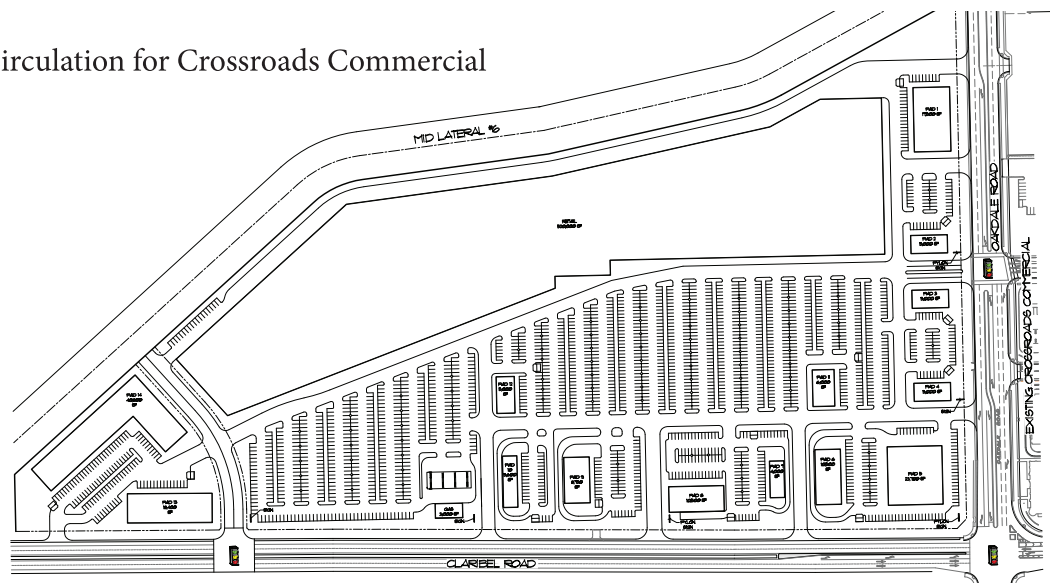
8.1.1 MU 1 Site Design

The commercial development plan consists of one or more retail areas, separated by private vehicular roadways which bisect the site from Claribel Road to Oakdale Road, and may include one or more “commercial use” areas and/or mixed-use residential areas.

One main and four secondary entries are planned in Claribel Road. The main entry provides a North/South Collector road connecting Claribel Road to MID Lateral No. 6. The North/South Collector provides the access to the retail and the future residential to the North MID Lateral No. 6 and is intended to be signalized.

Figure 18 - Proposed Circulation for Crossroads Commercial

For illustrative purposes only. Building areas, parking, driveways, and landscape areas on the site plan may be relocated throughout the development site and square footage can be reconfigured. The North/South Collector intersection with Claribel Road is preliminary and may shift during final design.



8.1.1.1 Placement and Orientation

The location of the main parking field for the retail/commercial use shall be substantially consistent with the manner shown in the PDP to be presented for the project in the future, but parking and landscape areas can be moved to the front, side or rear as necessary to accommodate development and/or tenant requirements.

All building structures along Claribel and Oakdale Roads shall respect a 20' minimum setback, however drive through lanes and necessary structures ancillary to the building's like patios, walking paths, trash enclosures, exit walkways and other pedestrian type enclosures may be located in the 20' set back area. For all interior parcel lines (internal to the shopping center) building structures shall not have a setback requirement.

To allow the project to attract a variety of uses, no minimum or maximum lot size shall be required.

8.1.1.2 Parking and Circulation

Special paving or pavement markings shall be provided at key on-site intersections and/or key entries.

Pedestrian walks shall be provided connecting retail structures to the parking areas. Walkways which cross traffic lanes shall have special markings.

90 degree or angled parking shall be permitted. In addition, diagonal parking shall also be permitted.

Cross access parking easements shall be provided throughout the retail development and to the extent required by developer in the mixed-use development or in the form of CC&Rs/OEA or shared parking declaration. With these cross access agreements, shared parking, and peak/off-peak shared parking arrangements shall be allowed to meet parking requirements through MU-1 (for example, if one area is under-parked and another area has excess parking, such excess parking areas shall be allowed to supplement the under-parked areas), consideration shall also be given to shared parking and in the case where the need for parking does not meet City of Riverbank code. Additionally, when MU 1 Residential Uses are adjacent or in close proximity to MU 1 Commercial Uses, the various uses within the MU 1 property may share parking based on peak and off-peak parking demand.

Emergency vehicle access shall be provided.

Proposed improvements within this development may, at developer's discretion, be phased over time. Improvements constructed within each phase shall provide the services necessary to support the uses constructed and open to the public including utilities, roadways, parking, landscaping, compatible retail, and signage.

The parking areas shall be designed to include provisions for pedestrian connection from the parking lots in the front of the anchor building line located in the rear portion of the site for access to building entrances. Walkways that cross traffic lanes shall have special striping or other delineations. Walkway



Special pavers example



strips shall be intermittently provided through landscaped areas to protect landscaping from foot traffic damage.

Reciprocal access and shared parking between retail properties and owners of MU-1 shall be used if approved by the MU 1 developer, whenever possible. Cross access easements may be provided in the mixed-use property at developer's discretion.

All commercial site designs may include loading areas and raised docks which are screened from view from the public right of way by landscaping or a screening structure. If a screen wall is used, it shall be architecturally treated. Screen structures may be made of wood, post and wire, masonry, landscaping, or various other compatible materials.

Drive aisles shall allow for circulation within the development, with sufficient width for emergency vehicles.

Provisions for connecting driveways and walkways between adjacent properties on the Retail/Mixed-Use Site are to be provided.

8.1.1.3 Landscape, Lighting, Signs and Site Amenities

The general characteristics of the landscape plan palette for the development shall be a combination of year-round color and textural interest. Developer shall select plants on the basis of color combinations, growth patterns, low maintenance, water conservation characteristics, and compatibility with the area. At time of installation, all trees within the landscape setback on Claribel and Oakdale shall have a container size of 15 gallons, otherwise trees shall have a minimum container size of 15 gallons.

Canopy-providing trees shall be planted in the parking lot planter islands to produce shade and shall result in a fifty (50) percent surface shading within ten (10) years. Minimum planting will consist of one fifteen (15) gallon tree for every ten (10) parking spaces.

A landscaped median may be provided at the main signalized entry from Claribel at the North/South collector. Shrubs or vine planting shall be provided to screen utilities and trash enclosures. All landscape areas provided (with the exception of areas adjacent to public streets, patios, walkways and the rear MID Canal) shall have a minimum average width of five (5') feet. This includes but is not limited to landscaping adjacent to buildings, alongside rear property lines, and finger planters within parking lots.

Where possible, landscaping shall be used to soften the appearance of fences and blank walls.

The design of the exterior building lighting poles and sconces shall be compatible with the overall architectural style of the development.

Parking lot lighting shall be consistent throughout the development. The maximum pole height of the lighting shall be thirty eight (38') feet measured from top of pole base.

All lighting shall be LED and shall be directed and shielded in such a manner so as not to directly cast light on neighboring properties.



This development shall allow one 50' high sign at the Claribel / Oakdale intersection and three (3) 35' high monument sign(s) along Claribel Road. Two 35' high monument signs shall be allowed on Oakdale Road. Developer may be allowed to have additional monument signs (not to exceed one (1) per parcel as long as they do not exceed 10' in height), gas station pricing signs and other signs required by state law or governmental requirements shall be allowed notwithstanding anything contained herein.

Tenant identification signs will be allowed on all faces of buildings including those that are visible on Claribel, Oakdale, and MID Lateral No. 6 and north/south connection road.

A uniform sign program identifying locations of signs is attached to and part of the design guidelines, Appendix C. Individually mounted, channel letters shall be utilized for building attached signs. Illumination shall be individually illuminated, internal, halo, or direct (gooseneck lighting from above or up-lit from below, mounted on hard metal canopies). Building attached signs for uses less than 2,000 square feet shall be limited to the name and logo of the business except for signs related to drive through uses, directional signs, etc. The guidelines provide flexibility so that signs can be consistent with the proposed Tenant's prototypical design, quantity, and colors and will be architecturally compatible.

Trash enclosures shall be constructed of solid material including without limitation wood, metal, concrete and masonry material, and shall be a minimum of six feet in height, with solid view obstructing gates.

Fences and walls shall be designed to be compatible with the surrounding landscape and architectural style of the development.

8.1.2 Architecture

The architectural theme shall be complementary to the Crossroads Shopping Center on the east side of Oakdale Road but also establishing its own identity while creating a campus of retail structures which vary in character, massing, materials, and colors. Each structure shall be complementary but shall maintain its own uniqueness as though designed and constructed at different times. If entertainment uses are included, building design may include some or all of the following: neon tower, marquee, special lights and statement lobby glazing. Facades shall contain primary and secondary features to vary the scale and offer flexible leasing opportunities. The concepts for architectural theme are to use rural architectural forms and treatments balancing off



Trash enclosure example



Architectural example



Architectural example



the agricultural heritage of the Central Valley of California with some vineyard influence. The theme may contain, at applicant's discretion, architectural treatments found within the Crossroads Shopping Center on the east side of Oakdale Road.

8.1.2.1 Style and Design

The development shall include tower features, variable heights, undulating facades, and a variety of materials, colors and storefronts. Awnings, banners, outdoor patios, light sconces, site lighting, and/or street furniture shall be included.

The general character of all building exteriors shall be consistent with the style of theme established for the development.

Any outdoor storage of goods, materials or equipment shall be limited to twenty feet in height with setbacks equal to height and located in an enclosed fenced area. The fencing shall be designed as an integral part of the building design and site layout and consistent with the architecture of the shopping center. Chain link fencing with wood inserts is not an acceptable manner of screening outdoor storage of goods. Sidewalk sales and temporary sales (no greater than 60 days at a time) shall be permitted for the common areas without the requirement for fencing or special City approvals.

8.1.2.2 Massing, Scale and Form

No retail building within the development shall exceed 75 feet in height except for tower elements or elevation enhancement covering no more than 50% of the frontage. Height exceeding these allowances must be as otherwise authorized by the Community Development Director.

Rooflines shall vary in height between different buildings and may include a blend of sloping and flat rooflines as shown in the Illustration A.

Large, continuous structures shall incorporate breaks in horizontal planes by varying architectural features and designs and recessing windows and entrances or other building features, to provide articulation and scale. Windows shall be enhanced by use of various sizes and shapes. The design shall be complementary to the architectural style of the development.



Architectural example



Architectural example



Varied roof lines example



Scale & Massing example





Illustration A

On facades which front on Claribel and Oakdale Roads, architectural treatment shall be applied to the building elevations.

No mechanical equipment shall be exposed on the wall surface of any building.

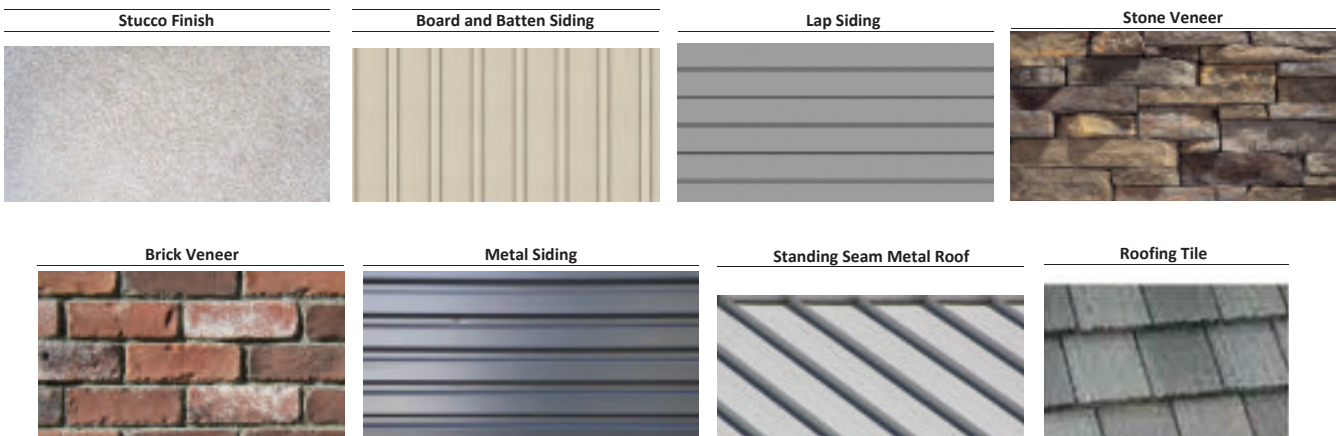
Mechanical and utility equipment shall be located below the roof line or parapet wall and out of public view from the immediately adjacent parking lot. All roof-mounted mechanical equipment or ductwork which project vertically above the roof or parapet shall be screened by an enclosure which is consistent or compatible with the building's architectural design.

Gutters and downspouts located on the exterior wall shall be treated or painted to blend into the facade to which it is attached, unless used as a design element or architectural feature, in which case the color shall be consistent with the color schemes of the building or the intended architectural feature.

8.1.2.3 Site Design

The rural architectural theme for the MU 1 property will include similarly themed building architecture and materials as set forth in the below photos.

8.1.2.4 Materials



Use of Standing Seam Metal Roof, Stucco, Wood Board & Baton Siding and Block



Varying use of materials



Awning projections and Facade Projections

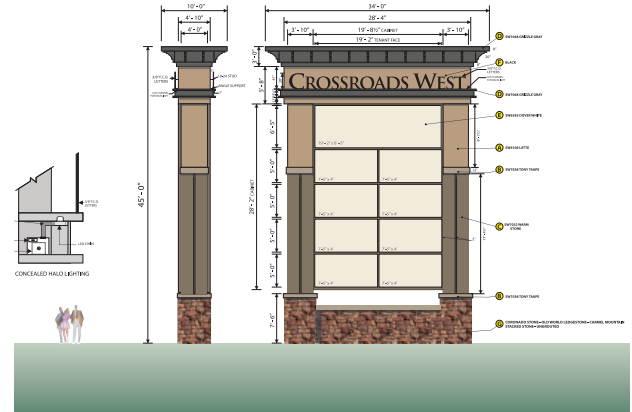


Varying use of materials

8.1.2.5 Signage

This development shall allow one 50' high and three 35' high monument sign along Claribel Road. Two 35' high monument sign shall be allowed on Oakdale Road. Gas station pricing signs or other monument signs required by State laws and/or other governmental requirements shall be allowed and not restricted by this CWSP.

Tenant identification signs will be allowed on the faces of buildings that are visible on Claribel, Oakdale, and MID Lateral No. 6.



Pylon Sign & Monument Sign Rendering

A uniform sign program identifying locations of signs shall be prepared for review and approval of the Community Development Department as part of the Architectural and Site Review process. Individually mounted, channel letters shall be utilized for building attached signs. Illumination shall be internal, halo, or direct (gooseneck lighting from above or uplift from below, mounted on hard metal canopies). Building attached signs for uses less than 2,000 square feet shall be limited to the name and logo of the business only except for signs related to drive through uses.

Signs shall have design elements and colors consistent with the architecture proposed. (see Appendix C: Master Sign Program)

Address numbers shall contrast with their background and shall be a minimum of six inches in height.

8.2 Mixed Use 2 Overview

Building on the characteristics and style of the regional Mixed Use 1 portion of the CWSP area, the Mixed Use 2 parcels are intended to provide smaller scale, localized amenities and conveniences to the residents of Crossroads West all while keeping a succinct theme within the development.

The design standards and guidelines contained in this section were developed to ensure Mixed Use 2 development is thoughtfully planned, well designed and compatible with adjacent land uses. The desire of the mixed-use land is to create pedestrian oriented places that are easily maneuvered, vibrant and contribute to the overall character of the neighborhoods.





Metal Bench



Bike Racks



Light Fixture



Parking Lot Light Fixture

8.2.1 Mixed Use 2 Site Design

8.2.1.1 Placement and Orientation

The orientation of buildings should be on primary streets with sidewalks and landscape treatments. The primary or front elevation should be the most pronounced and be oriented towards the main pedestrian thoroughfare.

8.2.1.2 Parking and Circulation

The design and layout of parking areas is critical to the Mixed Use 2 neighborhood experience. Front facing parking lots that block the building's primary elevations are discouraged. Side load or rear load parking facilities are encouraged and will help minimize the intrusion of vehicles on the streetscape. A shared parking facility is encouraged in the mixed-use development to lessen the amount of area dedicated to parking.

Circulation within the parking areas should incorporate safety features for pedestrians including lighting, narrowed crosswalks and banded or textured paving. These features will help to highlight pedestrian connections and visibility as well as provide a buffer to pedestrians from the street. Drive aisles shall be clearly delineated and provide adequate space for vehicles to pass one another while maneuvering through the parking area.

8.2.2 Architecture

8.2.2.1 Style and Design

The architectural style and design of the mixed-use development shall incorporate colors and materials that are complimentary to the surrounding neighborhoods. The majority of lands dedicated to Mixed Use 2 development is on or near corners of the project and therefore provide an opportunity for architectural details including tower features, enhanced window treatments, awnings, cornices, varied roof lines and other vertical elements.



Mixed Use



Guidelines and standards have been established for the Mixed Use 2 development areas and include:

- Primary elevations shall be oriented onto major street frontages that provide pedestrian accessibility.
- Secondary elevations shall compliment the primary elevation and can be oriented towards the parking area or street frontages that cater to vehicle traffic.
- Horizontal massing should be broken up to provide visual interest and character to buildings.
- A cohesive color palette shall be used on all buildings within the Mixed Use 2 development and should be complimentary to the regional Mixed Use 1 center.
- Storefronts should be eye catching and appealing to pedestrians and visitors to the site. The use of awnings, shutters, stone work and other decorative elements should be incorporated into the design.
- Glass frontages are encouraged to provide attractive views into stores but should not be the only element incorporated into the storefront. Decorative entry ways, landscape treatments and site furniture are all encouraged.

8.2.2.2 Massing, Scale and Form

The scale and mass of buildings within the Mixed Use 2 development should be pedestrian scale and not overpower the site. Large, continuous expanses of blank wall should be avoided. Multiple story buildings should provide relief in roof lines and varying heights should be integrated to break up the mass and form of buildings.

8.2.2.3 Architectural Style

Architecture, color and design of the buildings in the mixed-use areas are important because it sets the tone for the neighborhoods and provides a basis for the architectural style of the remainder of the CWSP area. The buildings can incorporate many design elements from different architectural styles and utilize colors from a vast color palette to create unique and memorable neighborhoods.

The architecture of the mixed-use areas shall be consistent to the residential homes in the area to create a cohesive and complimentary neighborhood. Details and guidelines of the architectural styles are provided in future sections of this chapter.

8.3 Residential Overview

Residential Site design and architecture are the focus of this section. The design guidelines establish the framework for which the residentially designated areas of the CWSP will be constructed and provides a distinctive and diverse character to the community. The new neighborhoods within the Crossroads West project will draw from elements of classic Riverbank neighborhoods communicated to the design team.

8.3.1 Low Density Residential

The low density residential (LDR) land use designation is characterized by single-family, detached homes on varied lot sizes. The CWSP showcases ±234 acres of land for this type of housing density. The amount of land designated for LDR will be reduced by the site selection of public use areas such as schools, fire station and neighborhood parks.



8.3.2 Medium Density Residential

Medium density residential areas are characterized by smaller, compact lot sizes that can accommodate detached or attached single-family homes. The CWSP shows one areas containing ±12 acres south of Morrill Road that will be dedicated to medium density construction.

8.3.3 High Density Residential

High density residential areas are characterized by compact, multi-family development, most notably apartment complexes, townhomes, or condominiums. The CWSP shows three (3) areas containing 15.5+ acres west of Oakdale Road. The corner of Morrill Road and Oakdale Road will be a vibrant, neighborhood spot with the combination of the high density and medium density residential and the Mixed Use 2 development. The third location is illustrated as south of Crawford Road adjacent to the MID Lateral No. 6.



Massing, Scale and Form

8.3.4 Residential Site Design

The proper siting of homes in Crossroads West is critical to the overall flow and continuity of the development. The residential parcels have been thoughtfully planned to provide transitional buffers between land uses and ease of access to shopping, dining and community amenities. The following standards and guidelines intend to carry-out the vision for the CWSP and promote quality design and cohesive neighborhoods.

8.3.4.1 Placement and Orientation

- The placement and orientation of buildings is important in creating a vibrant streetscape and in facilitating the interaction of residents. The following standards and guidelines shall apply to the residential areas of Crossroads West:
- Variations to front setbacks shall be incorporated into master development plans for all densities of residential homes. No more than two consecutive homes shall have the same front setback.
- A cohesive mix of lot sizes and product types shall be incorporated into residential areas of the CWSP. Homes should be oriented towards the street to create a safe, pleasant and active neighborhood.
- Courtyard, cluster or alley-loaded homes may be considered to lessen the appearance of garage fronts on residential streets.

8.3.4.2 Parking

Parking is a necessary component of any residential development and good design principles are necessary to keep the parking areas from dominating the view along public streets. Off-street parking will be provided for the CWSP area per the standards set forth in Chapter 4, Tables 4, 5 and 6.

- All visitor parking, resident parking, handicap accessible parking and clean-air vehicle parking shall be clearly marked and identified.



- In multi-family complexes, the majority of the parking lots shall be placed to the rear or side of buildings or evenly distributed throughout and out of public view. Parking lots shall not dominate the streetscape of multi-family residential areas.
- If feasible, a combination of on-street and off-street parking shall be utilized in residential areas to provide a buffer from street to sidewalk.
- Parking lots shall be located in a way that provides short, direct access to the main building

8.3.5 Architecture

Architecture is the most important element of a development project. In the case of the residential areas of Crossroads West, the architecture sets the tone for the type of development and promotes quality design that will help to enhance the City of Riverbank.

8.3.5.1 Style and Design

Various architectural styles will be encouraged in Crossroads West. Different architectural styles will help to create unique neighborhoods amongst the mixed-use and residential areas. The following design parameters are meant to establish minimum standards for the residential areas, regardless of what architectural style is selected. Homebuilders/Architects are encouraged to build upon the architectural styles presented here to develop new and differing architectural styles like California Modern, Western Regional Farmhouse, Savannah and other styles.



Alley loaded product lessen garage front

- Architectural design themes shall be varied throughout the CWSP to establish unique neighborhood project identities.
- Discourage the placement of consecutive elevations, floor plans and color palette on homes within a subdivision.
- 4-sided architecture is encouraged when adjacent to public view sheds to avoid flat, unfinished walls.
- Each production builder shall offer a variety of alternate floor plans and building elevations to provide sufficient variations of homes within each subdivision, or portion thereof. The minimum range of different exterior house designs, derived from a combination of different floor plans and different elevations for each floor plan according to the following table:

Number of Lots	Number of house types (models with different elevations)
Under 50 lots	9 (3 models/3 elevations)
51+ lots	12 (4 models/3 elevations)

8.3.5.2 Massing, Scale and Form

Massing, scale and form are used in development to encourage residential areas that are visually appealing, provide streetscape variety, are at an appropriate pedestrian scale and are compatible with surrounding neighborhoods and land uses.



- Building articulation shall be incorporated including the use of overhangs, balconies, porches and patios and dormers to help reduce building mass.
- Varied roof lines shall be integrated into the residential home design such as hipped roofs, gabled roofs, pitched roofs and roof dormers.
- Residential dwellings shall be constructed at a pedestrian scale.

8.3.5.3 Architecture Style

The residential communities within the CWSP area are envisioned as traditional, single-family neighborhoods with some of the qualities of the older, established neighborhoods of Riverbank. Diversity of architecture is encouraged throughout the residential areas and each dwelling unit should have a distinct identity and relation with one style of architecture. If a particular architectural style is not depicted here, additional styles may be proposed to the City which adhere to the visions of this Specific Plan.



8.3.5.3.1 Craftsman

Southern California is the birth place of Craftsman architecture which was inspired by the English arts and crafts movement of the late 19th century. The Craftsman style is distinct and easily recognizable featuring shallow pitched roofs with deep overhangs and broad porches that display rafters, brackets, beams and columns.



Craftsman homes combine a mixture of materials to add visual interest including stone near the foundation, shingled roofs and wood siding. A statement front door and abundant casement windows with grid patterns are also classic Craftsman elements. The color palette is varied and includes earth tone base colors with complimentary trim colors.



The front entry porch is probably the most notable design element in the Craftsman style of architecture. Porches typically extend the entire front expanse of a Craftsman home and are anchored by large, sturdy columns on stone or brick piers.



Craftsman Style

ELEMENTS	MINIMUM EMBELLISHMENTS	ENHANCED EMBELLISHMENTS
Outward Appearance	Asymmetrical one- and two- story boxy forms; low lines with simple, wide projecting roofs	Horizontal character through the use of boxed massing with vertical and horizontal offsets
Roof Elements	Low-pitched gable roofs, occasionally hipped roofs; low roof slopes; wide projecting eaves with exposed rafters and beams; shingles or flat tiles	Varied porch roof styles; overhangs range from 18” to 26”; concrete shake materials
Exterior Wall Elements	A blend of stucco and siding with accents of rock, stone or brick	Authentic clapboard sidings; wood shingles
Window Elements	Casement windows that are double hung; large front windows with grid patterns	Bands of continuous windows often under one sill
Additional Embellishments	Exposed structural elements; prominent front porches with gabled roofs or columns; decorative shutters; decorative beams; exposed eaves	Heavy square columns with stone or brick trim; Arts and Crafts style light fixtures; dormers
Exterior Wall Elements	Earth tone color palette with contrasting, complementary trim colors	Same
Outdoor Living Spaces	Grand front porches a minimum of 4 feet deep	Same



8.3.5.3.2 Traditional (American)

The American Traditional style of architecture is found throughout the Country rather than a particular region or State. The style is simple and classic featuring open, columned porches, symmetrical one and two story stacked homes and dominant, gabled roofs.



American Traditional architecture draws inspiration from the colonial period as well as Greek-inspired architecture with the introduction of moldings and decorative borders. Primarily, this home style features a stucco exterior with the addition of brick facades but clapboard siding is also a classic choice for this style of home.



The color palette of Traditional architecture is neutral and light. Off-whites, light beige, creams and tans dominate the exterior colors and trim colors are complimentary. Window shutters and brick often complete the front elevation of a classic American Traditional.



Traditional (American) Style

ELEMENTS	MINIMUM EMBELLISHMENTS	ENHANCED EMBELLISHMENTS
Outward Appearance	Symmetrical one- and two- story stacked homes	Same
Roof Elements	Dominant gable roof with one intersecting gable; moderate pitch and slopes; predominantly flat tile	Gambrel roof form
Exterior Wall Elements	Stucco and/or siding with stone or brick trims and details	Authentic clapboard siding with brick veneers and wainscot trim; decorative shingles
Window Elements	Symmetrical window placement on front elevation; vertical multi-paned windows; standard single-hung windows	Decorative bay windows on front elevation
Additional Embellishments	Colonial details; porticos; decorative shutters; decorative attic vents; decorative door trims and Greek-inspired moldings	Decorative cornice gable-end trims; decorative doors with side lights
Exterior Wall Elements	Primarily light color palette with off-whites, creams and tans with contrasting and complementary trims	Same
Outdoor Living Spaces	Enclosed front yards with decorative fencing and gates porches	Same



8.3.5.3.3 Italianate

The Italianate style of architecture emerged in the mid-1800's in the United States and was predominantly found in port cities like San Francisco and along the northeastern seaboard. It is another architectural style that is easily recognized for incorporating columns, marble, stone and cornice treatments. These dwelling units typically feature low pitched or flat roofs, tall arched windows, smooth stucco exteriors and dramatic entryways flanked by columns.

Curved tile roofs are the predominant choice for an Italianate style home. Shingle roofs are also permissible and should feature a variegated color scheme. The exterior walls of Italianate style homes are typically finished in smooth or textured stucco and sometimes incorporate marble or stone embellishments. The color palette is classic and soft featuring tan, soft yellows and off-white shades with contrasting and complimentary trim colors.



Italianate Style

ELEMENTS	MINIMUM EMBELLISHMENTS	ENHANCED EMBELLISHMENTS
Outward Appearance	Symmetrical one- and two- story stacked homes	Same
Roof Elements	Low Pitched roofs with curved tiles; shingles; flat roofs with shingles or composite	Dormers with additional windows
Exterior Wall Elements	Smooth stucco	Textured stucco with marble and/or stone embellishments
Window Elements	Tall, narrow windows with decorative trim for separation	Slightly arched windows; multi-pane windows
Additional Embellishments	Greek-inspired moldings; projecting eaves, cornices and columns	Cupolas, glazed doors
Exterior Wall Elements	Primarily light color palette with off-whites, creams and tans with contrasting and complementary trims	Sunny hues of yellow, orange
Outdoor Living Spaces	Porticos, courtyards and fountains	Same



8.3.5.3.4 Spanish Colonial

The Spanish Colonial style of architecture gained popularity around 1915 and is still a widely used design in home construction today. Historic missions all throughout California played a part in creating the famous style we know today. Homes constructed in the Spanish Colonial style feature low-pitched roofs of Spanish tile or red clay, the use of arched doorways and window treatments, stucco exterior walls, textured and decorative wooden doors and ironwork details.

Homes of the Spanish Colonial style are typically a square or rectangular shape with design features on the exterior elevations to provide visual interest. These design features are distinct and easily recognizable. The use of wrought iron details, balconies, recessed windows with arched frames and the signature mission style parapet or dormer over the front door are the most widely used symbols of Spanish architecture.



Spanish Colonial Style

ELEMENTS	MINIMUM EMBELLISHMENTS	ENHANCED EMBELLISHMENTS
Outward Appearance	Asymmetrical two-story homes with square and rectangular plan designs	Square or rectangular plan designs with internal courtyard
Roof Elements	Side gabled with some hipped or flat roofs; low pitch with wide overhangs; clay or concrete tiles	Mission-shaped dormers for parapet
Exterior Wall Elements	Stucco exterior finishes	Stucco, smooth or custom-troweled finishes
Window Elements	Decorative arched windows; grid patterns; accent bay windows	Recessed windows; window groupings with decorative trim and column separation
Additional Embellishments	Arched doorways and entry-ways; use of decorative wrought iron, clay ceramic tiles; decorative shutters; stucco embellishments on exterior finishes	Sculptured chimneys
Exterior Wall Elements	Primarily light color palette with deeper contrasting trim colors	Same
Outdoor Living Spaces	Internal courtyards	Same



8.3.5.3.5 French

In the 19th Century, French influences on residential architecture began to surface in the United States. This style is recognizable for its heavy use of steep, hip roofs that are high pitched and often constructed of wood shingles to add to the classic, rustic look of French architecture. The details of this home style are delicate and feature many Tudor elements such as large, grand entry ways that are arched or pronounced and the use of stone, brick and stucco that add visual interest to the homes’ exterior elevations.

The color palette of French architecture is natural and earth toned, which helps to play up the details and use of stone and brick finishes. Often times, pale blues or gray trim is added for a slight color variation and additional visual appeal.



French Style

ELEMENTS	MINIMUM EMBELLISHMENTS	ENHANCED EMBELLISHMENTS
Outward Appearance	Asymmetrical two-story homes with a strong Tudor and Norman influence	Larger, single-story home
Roof Elements	Mansard roofs; steep, high pitched roofs with wood shingles or composite shingles	Overhangs – minimum 18”; concrete shake roof
Exterior Wall Elements	Smooth stucco or masonry; wood siding, shingles or clapboard embellishments; heavy stone and brick accents	Authentic clapboard or wood siding as the primary exterior treatment; heavy brick and stone use on trims
Window Elements	Multi-pane windows; slightly arched with pop-outs	Decorative bay window on front elevation
Additional Embellishments	Covered entry; full porches and/or balconies; French doors; alcoves	Extra-wide porch with trims
Exterior Wall Elements	Earth-tone color palette with contrasting trims; blue/gray color palette	Same
Outdoor Living Spaces	Courtyard of stone; gardens with water element	Same



8.4 Streetscape Guidelines and Character

8.4.1 Paving and Sidewalks

The pedestrian network throughout the CWSP area is an important element for purposes of providing safe and attractive methods of maneuvering the community. Paving will be found throughout the development in the form of sidewalks, bicycle paths and multi-use trails all with the same goal of urging residents to use alternate forms of transportation.

Sidewalks throughout Crossroads West will be a minimum of 5' wide. All Class I and Class II bicycle lanes will be clearly marked and designed according to the CWSP and the City's Bike Plan. Pedestrian trails and bicycle lanes are a required amenity that provides connectivity to the Mixed Use 2 and regional shopping areas suggested in Mixed Use 1 as well as a connection to public use sites and the Regional Sports Park north of Morrill Road.

In the regional Mixed Use 1 and Mixed Use 2 areas of the community, it is envisioned that decorative paving of a distinctive color and pattern will be used at entrances to clearly provide a sense of arrival to these destinations.

Street paving and sidewalks are not only a safety element but also provide a wayfinding and place making element to the Crossroads West project.

8.4.2 Lighting

The lighting used throughout the CWSP provides two benefits, that of illumination and safety. Well-lit streets, pedestrian trails, neighborhood centers and regional shopping areas are inviting and attractive.

All poles and fixtures for street lighting will be selected from the approved master lists of MID, who will ultimately provide service to the plan area. Light poles and fixtures shall be sized appropriately to provide illumination without being intrusive. In residential areas, lighting is subject to the approval of the design review committee and the City and shall utilize the following guidelines:

- Light poles and fixtures should complement the surrounding buildings, pedestrian walkways, trails and the amenities they illuminate.
- If proposed decorative lighting may be placed predominantly at entry ways, gateways, near bicycle lanes and pedestrian trails to provide clear visibility.
- All residential units shall have illuminated house numbers to provide visibility to emergency personnel.
- All Mixed Use 1 buildings are required to have building and/



Decorative paving



Expanded concrete sidewalk with tree wells



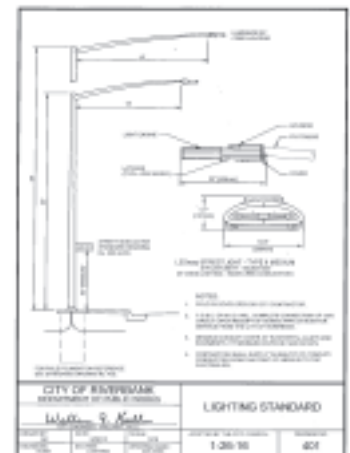
Concrete sidewalk with tree wells and seating



Concrete Class I bike trail



Paved Class I bike trail



City of Riverbank Lighting Standard



or pole mounted lighting for use at night to assist in illuminating the pedestrian walkways.

- Bollard lights are an acceptable form of lighting that can be used at intersections and at pedestrian trails to provide illumination.
- Any lighting issue not clearly defined in these guidelines will be subject to the City of Riverbank's Lighting Standards.

8.4.3 Street Furniture

Street furniture describes all of the finishing touches to a neighborhood and overall community. In Crossroads West, an eclectic mix of furnishings should be used to create a cohesive and distinctive flair in all pedestrian friendly areas.

The street furniture elements that should be incorporated include but are not limited to:

- Trash receptacles
- Decorative planters, tree grates
- Bicycle racks
- Kiosks / Directories
- Newspaper/magazine racks

8.4.4 Public Art

Public art is a wonderful way to provide character and a personal touch to a new community. This amenity is encouraged and welcomed in Crossroads West, but not required. Public art can be any number of things from murals, sculptures, fountains or gallery walks, public patios/gathering places. When displaying and exhibiting public art, the significance of the piece should have one or more of the following aspects:

- The art pieces should be commissioned from a local craftsman.
- The art pieces should depict a historical element or time period of the area, or compliment the architectural or aesthetic theme of the project.
- When possible, the art pieces should utilize local, natural materials.
- Public art should be sought from unlikely sources including schools, community centers and senior centers.



Planters



Benches



Newspaper stand



Public art



Public art



Public art



8.4.5 Walls and Fences

Walls and fences are an integral part of the streetscape and the community. Walls and fences are elements that provide safety, security, privacy, property definition, and noise attenuation. When poorly designed or maintained, walls and fences become targets for vandalism, degrading the overall quality and character of a neighborhood. Walls and fences shall be regulated to ensure that they contribute to the quality of the community (See Figure 19). The following guidelines shall be applied in the design of walls and fences within public viewsheds:

- a. When a wall is needed, its scale and design shall be compatible with the surrounding landscape and architectural materials found in the neighborhood.
- b. Walls and fencing shall be constructed of high quality, long lasting, low maintenance and vandal resistant materials.
- c. Perimeter walls shall be constructed of solid material (i.e. decorative masonry, split face concrete block, plaster, stucco, or brick).
- d. Residential yard fencing shall not exceed seven (7) feet in height and shall be constructed from redwood, cedar or comparable material.
- e. Edge fencing along the west edge of the parkway basin shall be split rail or similar fencing (2-3 rails) and shall be four (4) feet in height, typically.
- f. Where a LDR/MDR land use is adjacent to a HDR or MU commercial land use acoustic walls shall be installed and enhanced with plantings and wall articulation to avoid monotony. The walls shall be constructed of masonry or similar type materials.
- g. Walls shall be built on private property such that the face of wall is the edge of row.



Masonry wall example



Decorative Wall



Separated sidewalk



Landscape median

8.5 Landscape Guidelines

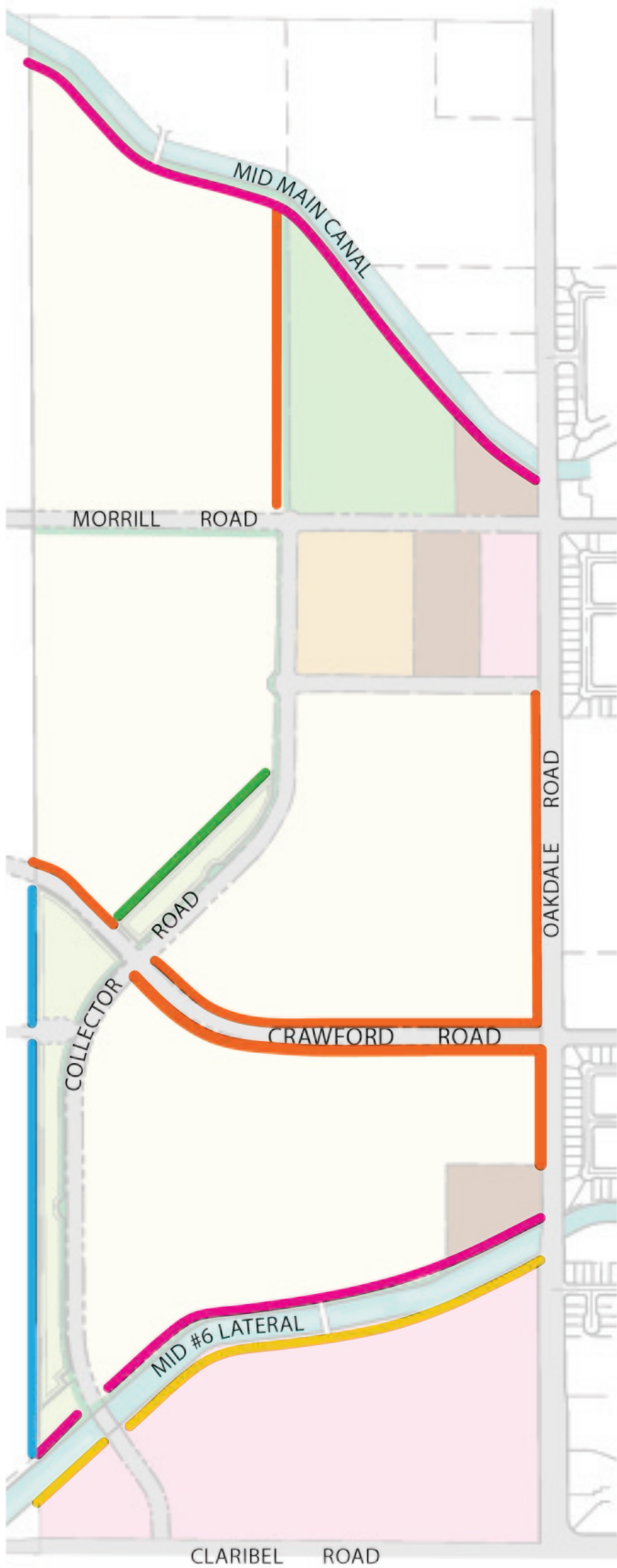
8.5.1 Street Landscape

The street hierarchy within the CWSP area will be defined by landscape treatments. Landscape corridors shall remain consistent throughout the development with regard to plant and shrub selection, hardscape materials, lighting and other site furnishings. Improvement plans prepared by a licensed Landscape Architect will be required with each submittal for development and are subject to the Design Review Process identified in Section 10.4 of the CWSP.



Streetscape





LEGEND

- 6' masonry wall except where lots front on Morrill Road, walls on private property and North/South Collector*
- 6' masonry wall along lots unless lots front onto bike trail;
- 6' Decorative Masonry, Concrete or Metal Fencing adjacent to residential and along Mid Lateral No. 6;
- 3' Wall with 3' wrought iron unless lots front onto park/basin
- Metal split rail fence or similar

* No wall will be required if a school is developed adjacent to Crawford Road, the North/South Collector or Morrill Road.

Figure 19 – Walls and Fences

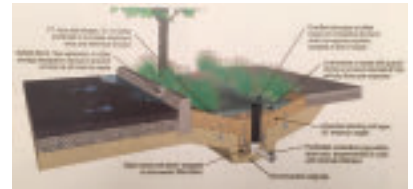


Center median landscape will be featured on Oakdale Road and Crawford Road could can incorporate grass or artificial grass, ground cover, trees and shrubs. The median landscape should provide adequate planting area for trees. In the area adjacent to the MU 1 Property, tree spacing may be relaxed and pruning allowed to promote building architecture and signage visibility in the project. Tree spacing is important as it ensures visibility, proper root development and shade canopy. In addition to center median landscape treatments, arterial streets will also be landscaped on either side of the street near the sidewalk to provide a buffer between pedestrians and vehicular traffic.



Center median landscape

Collector and neighborhood streets will be constructed as two lane roads with on-street parking permitted on both sides of the street. Landscape strips on the collector streets will be on both sides and feature ground cover, grass, and trees. The landscape strip may also serve as a storm water collection and treatment device. Separated curb and sidewalk again provides a buffer between pedestrians and vehicular traffic.



LID swale design

All street landscape trees shall be planted with proper spacing and with root deflectors and deep watering device to prevent sidewalk and road damage. The materials chosen for street landscape should be drought tolerant and weather resistant meeting current local and state codes. In addition, the irrigation systems installed to maintain the street landscape should be water conserving.



Root barrier

Public street landscaping throughout Crossroads West will be maintained through the creation of a Community Facilities District established to collect funds necessary for proper care and maintenance of the landscaping installed throughout the Specific Plan area. This district will be responsible for the long-term maintenance and upkeep of street landscape and for the repair or replacement of damaged or expired items.



Public open space

8.5.2 Public Open Space Landscape

The Mixed Use 1 and Mixed Use 2 areas within the CWSP area shall contribute to the public open space atmosphere by providing deliberately designed spaces for residents and visitors such as outdoor plazas and gathering areas. Pedestrian plazas are an excellent opportunity to provide decorative benches and lighting, landscape spaces and decorative planters and pots. The landscape in these areas shall be well planned to provide buffers between buildings and parking as well as focal features to enhance the overall pedestrian experience. Public Open Space within the residential areas consists of center street medians, street trees, bike paths along the MID Laterals and unique urban landscape features to be built with each neighborhood. The



Park walkway



next sections provide examples to implement creative park landscaping as well as other Public Open Space areas.

8.5.3 Parks and Trails Landscape

The park system for Crossroads West is made up of several elements: an expansion of the Regional Sports Complex, a linear park/storm drainage feature, up to two centrally located neighborhood parks and trail features along the MID Main Lateral as well as MID Lateral #6. Park land in the plan area has been dispersed throughout the project. This linear park extends from the Regional Sports Park through the project and forms a connection to the regional Mixed Use 1 center in the southern most regions of the plan area. By incorporating linear parks within properties north of MID Lateral No. 6 with a multi-modal trail system, the emphasis of a pedestrian focused community is further strengthened.

Dual-use park basins make up the linear park lands in Crossroads West. The Regional Sports Park encompasses ± 22 acres with a basin in the northwest corner of the park. The remaining ± 18 acres of neighborhood parks will be provided in the linear park, based in the park calculation formula, and if additional parkland is required it will be provided with 4 to 5 acre parks located within residential areas, currently shown on the land plan, Figure 5, in LDR-1 and LDR-4. The linear park basins will be landscaped for use as passive recreational areas and some active recreational activities. Sod or native grasses, ground covers, scattered trees and shrubbery will be used to provide visual interest, buffers to the existing residential areas along Crawford Road and the planned residential areas of the community. Multi-modal trails for walking, jogging and bicycling will be integrated into the park basin areas.

8.5.4 Entry Monuments and Roundabouts Landscape

Primary entry monumentation will establish the design and character of the neighborhoods within Crossroads West. Entry signage shall be clearly visible and constructed of high quality building materials reminiscent of the materials and architectural styles used in the neighborhoods. Primary entry monumentation shall be the focal point of the Community. Color through materials and plantings shall be integrated into the entry monument at various points and the landscape shall complement the signage. Primary entry monumentation may be located at Crawford and Oakdale Road and Morrill Road and the North/South Collector.



Park walkway



Riverbank Sports complex



Class I bike trail



Primary Monument Signage



Secondary entries and monumentation offer individual communities within CWSP an opportunity to make visitors and residents aware of the arrival into their own individual community. The monumentation and landscaping shall be less prominent in size and scale than the primary entry monumentation, however the general improvements should be consistent.

Roundabouts are used throughout Crossroads West north of MID Lateral No. 6 that echo the roundabouts used in the existing Crossroads Development. These roundabouts are blank canvases for public art and landscaping that will help to unify Crossroads West and create a distinct neighborhood feel. Like those in the existing Crossroads north of MID Lateral No. 6, the roundabouts in the CWSP area shall be sized appropriately and provide landscaping trees if applicable and color through shrubs and flowers.

The landscaped areas at entry monuments and roundabouts at the residential property north of the MID Lateral No. 6 will be maintained via two mechanisms: the formation of a Maintenance CFD or the creation of an HOA. Landscaped areas at entry monuments on the MU 1 Property will be maintained by the affected property, via CC&Rs or Maintenance CFD at the discretion of the MU 1 developer.



Secondary Monument Signage



Landscaped roundabout



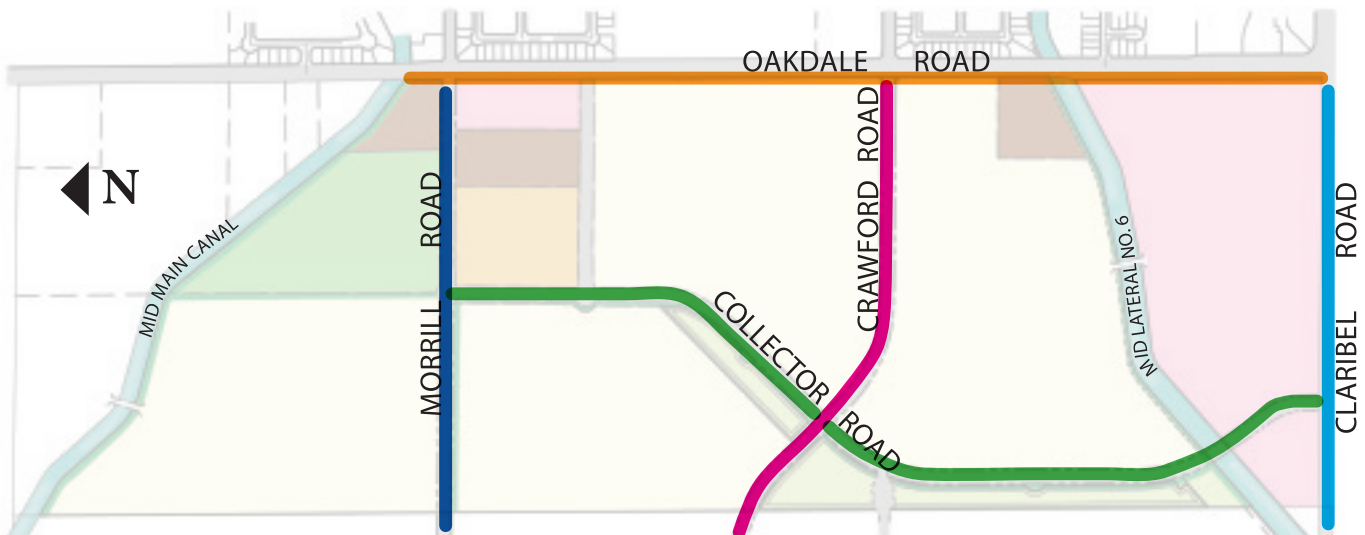
Landscaped roundabout



8.5.5 Master Street Tree Plan

The City of Riverbank’s Master Street Tree List (Appendix G) is a list of street tree species appropriate for use within the City of Riverbank. The list is a guide for choosing street tree species for development projects. The below exhibit is a proposed street tree theme for the various major and collector roads.

Street Tree Themes		
Botanical Name	Common Name	Name of Road
Quercus virginiana	Cork Oak	Oakdale Road 
Acer rubrum	Red Maple	Collector Road 
Quercus ruba	Northern Red Oak	Morrill Road 
Pistacia chinensis	Chinese Pistache	Crawford Road 
Koelreuteria paniculata	Golden Rain Tree	Claribel Road 



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Chapter 9

PROJECT FINANCING PLAN

9.1 Overview

The construction of public improvements and/or facilities may be financed using a number of funding sources and programs. This chapter identifies the obligations of the CWSP and the funding sources that may be used by the developer of each Phase of development, identified in paragraph 9.2, to assure timely construction and maintenance of these improvements. As discussed below these three (3) phases, Phase A, B and C, have been crafted to allow development, in any order, partially or completely, in response to market conditions.

While there are several ways in which to finance improvements and facilities, this section is intended to only provide an overview. A detailed Financing Plan will be reviewed and approved by the City and Developer prior to recordation of any final map within the Specific Plan area. The detailed Financing Plan shall outline the preferred methods of financing. Developer of Phase A may complete its Financing Plan independently for the commercial portion of the development only. It is anticipated that any residential development associated with Phase A would have associated improvement items which might rely on Phase B and Phase C infrastructure like the regional sewer lift station to be built north of MID Lateral No. 6. In this later case, residential development associated with Phase A would be expected to participate in a focused financing plan with Phase B and C.

9.2 Phasing

The phasing plan for Crossroads West will be developed to ensure timely completion of public facilities and improvements that coincide with development. The plan will also ensure that each phase of development has the infrastructure necessary to meet the demands of the new construction. At this time, it is anticipated that the regional Mixed Use 1 Property will develop first followed by various areas of residential and Mixed Use 2, which are located north of MID Lateral No. 6. The plan can be distinguished by three (3) development phases representing each major landholding. See Figure 20.

This phased development plan represents only today's best estimate as to how the Specific Plan area could be phased and should not be construed as the final phasing plan, as project development will be influenced by several factors, including general economic conditions, demographics; occupancy rates, construction schedule, construction costs, the emergence of other competitive projects, and possible changes in the regional infrastructure and public facilities, including the possibility of the North County Corridor (NCC) Project. These three phases have been designed, to the extent feasible, for independent utility and development. Each phase could be able to develop and connect to sewer, water, and storm drainage facilities without relying on adjacent phases to develop. Each phase can provide adequate circulation within the phase.

The following subsections of Section 9.2 are only a summary of the general infrastructure and/or service requirements for each phase, while a more detailed description of the funding sources, financial strategy,



financial obligations, allocation of responsibilities, and specific infrastructure required for each phase are more specifically stated in the Section 9.6 tables.

9.2.1 Phase A

Phase A may connect to existing sewer, water, and storm drainage facilities to the east in, or across, Oakdale Road. Phase A will get transportation access from the adjacent roadways (Oakdale Road and Claribel Road). Because it is expected that Phase A will develop before sewer lines are extended from Phase B across MID Lateral No. 6 to serve Phase A, Phase A may need an on-site, privately owned sewer lift station to connect sewer lines into Oakdale Road, however Phase A may either (i) connect into existing sewer lines on Oakdale Road and use a temporary sewer lift station, or (ii) extend the sewer line south of MID Lateral No. 6 and connect. With the future construction of the roadway across the MID canal, water and sewer lines will be extended to Phase A by the Phase B and Phase C properties. Developers have submitted preliminary engineering studies for review by the City, the final infrastructure studies shall be approved by the City to demonstrate which infrastructure items are necessary to serve Phase A.

Phase A storm drainage will consist of some on-site storage and conveyance of the remainder of the storm water to the existing storm drain basin in the Crossroad neighborhood just east of Oakdale Road. This proposed storm water discharge into the MID facilities must be approved by MID as well as the City of Riverbank.

9.2.2 Phase B

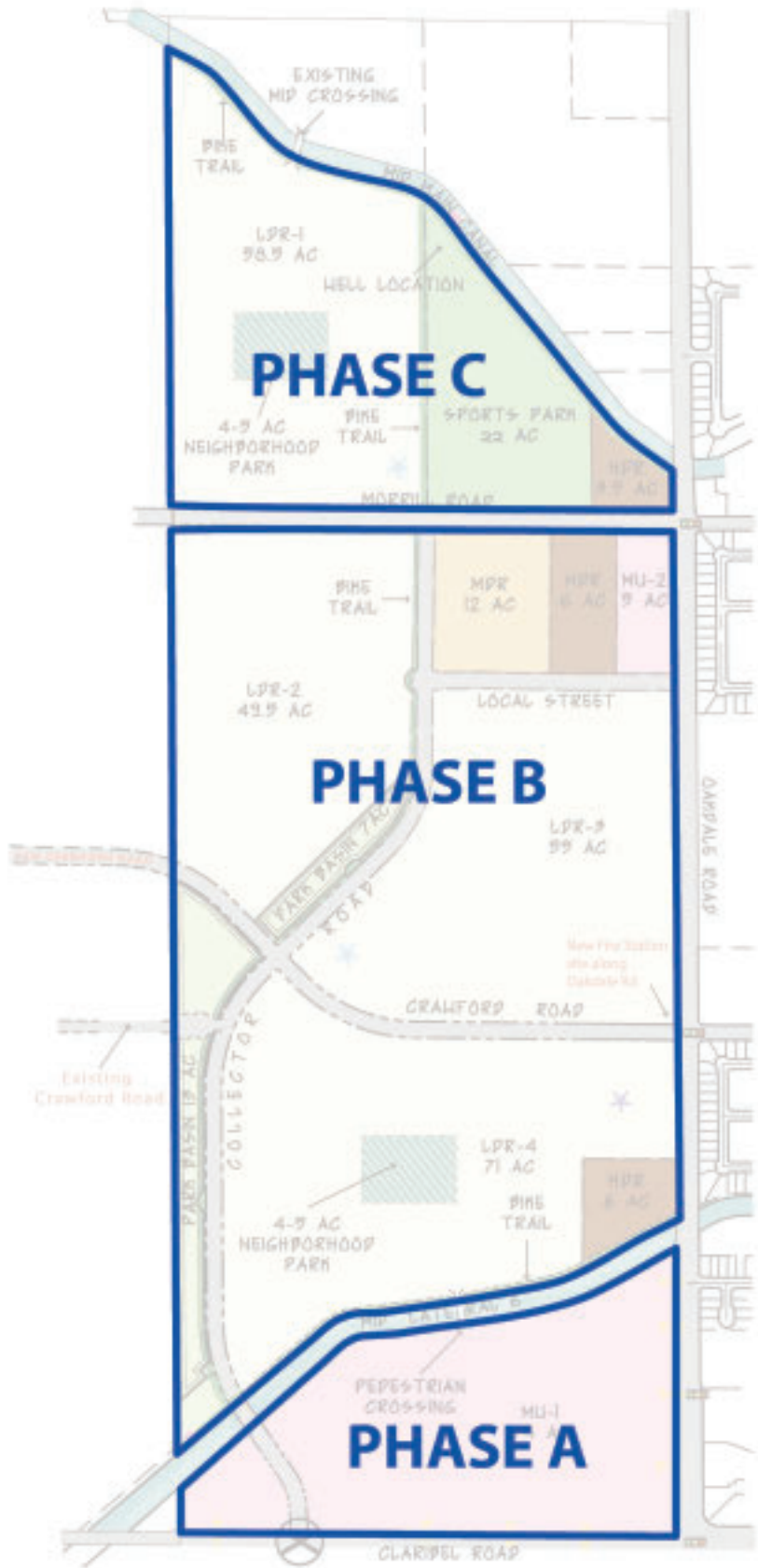
Phase B will connect to sewer, water, and get transportation access from the adjacent roadways (Oakdale Road, Crawford Road and Morrill Road). Phase B will have a storm drain system consisting of on-site storage in the Central Park Basin that conveys runoff through a pump into the MID Lateral No. 6 located at the south end of Phase B. Sewer will be provided by the extension of the existing sewer line at the intersection of Oakdale and Crawford Roads. Water will be provided by a looped system connecting water lines in Oakdale Road to water lines in Morrill Road.

Phase B will be connected to Phase A by the roadway across the MID Lateral #6, which shall be constructed by and as a part of Phases B and C.

9.2.3 Phase C

Phase C can connect to sewer, water, and transportation access from the adjacent Morrill Road. Phase C will ultimately be served by sewer lines coming up from the south through Phase B. If the Phase B sewer lines are not available, Phase C can install a temporary sewer lift station, to be owned, operated and maintained by the developer of Phase C, and connecting to existing sewer lines in Morrill Road, until such time as the sewer line in Phase B at Morrill Road becomes available. Phase C will have a storm drain system consisting of a storage basin at the northwest corner of the Regional Sports Park with a pump connecting the basin to the adjacent MID Main Lateral.





NOTE: Phasing may be adjusted to accommodate market demand.



Figure 20 – Preliminary Phasing Plan



9.2.4 Major Back Bone Infrastructure

Major Back Bone Infrastructure within the CWSP is that infrastructure which serves the entire project area, or which is required by the currently adopted City of Riverbank Infrastructure Master Plans. Examples of Major Back Bone Infrastructure would be Morrill Road improvements, a new City Master Plan water well and peaking reservoir, off-site sewer upgrades to sewer lines downstream of the Crawford Road Lift Station, Traffic Signal at Claribel and the new North/South Collector and the bridge crossing of the MID Lateral #6. Items which are Specific Plan exclusive master improvements, such as Crawford Road, dual use parks/drainage basins, and similar improvements that serve the CWSP development exclusively, are not Back Bone Infrastructure Improvements.

Paragraph 9.6 Capital Improvement Program identifies the Major Back Bone infrastructure needed to serve each Phase of Development. To the extent an improvement is contained within the City of Riverbank Systems Development Fee program, full reimbursement through fee offset or by reimbursement from fees collected, will be available. The developer of each Phase may also negotiate a Development Agreement for the Phase that may include City participation or offsets for other improvements not in the City fee program.

9.2.4.1 Water Well and Peaking Reservoir

The City of Riverbank Water Master Plan requires construction of a new water well and peaking reservoir in the project area to serve not only the project, but a larger area. These facilities will be located within the regional park or the dual use park/basin. Concurrent with development, a more detailed engineering analysis will be performed to determine the timing of construction of the well and the peaking reservoir. As these are City of Riverbank Master Planned facilities, the project fair share responsibility for these facilities is included in the currently adopted City of Riverbank System Development Fee program. If the stated facilities are needed in advance of adequate water funds being collected, advanced developer contribution may be required. The timing of these improvements will be determined by the City Engineer. If the developers are required to make advanced contributions, the City of Riverbank will negotiate reimbursement through collected fees or by way of System Development Fee offsets.

9.2.4.2 Off-Site Sewer Improvements

The City of Riverbank Wastewater Master Plan requires upgrading of the sewer force main and lift station pumps in Roselle Avenue at the Crawford Road Lift Station to serve not only the project area, but areas to the west of the project. Prior to development, a more detailed engineering analysis will be performed to determine the timing of construction of the upgraded force main and pumps. As these are City of Riverbank Master Planned facilities, the project fair share responsibility for these facilities is included in the currently adopted City of Riverbank System Development Fee program. If the stated facilities, as clarified by Chapter 7 of this Specific Plan, are needed in advance of adequate sewer funds being collected, advanced developer contribution may be required. The timing of these improvements will be determined by the City Engineer. If the developers are required to make advanced contributions, the City of Riverbank will negotiate reimbursement through collected fees or by way of System Development Fee offsets.



9.2.4.3 Morrill Road

Morrill Road is a major road serving the project area. Morrill Road will serve the development on north end of the plan area as well as the current and expanded Regional City Park. As such, the timing of Morrill Road construction and its completion is important to the City. Property owners adjacent to Morrill will be expected to dedicate the needed right-of-way for the road construction, consistent with the Specific Plan roadway cross-section, without reimbursement. At the time of development, each adjacent landowner will be required to perform frontage improvements. The City may require improvements in front of adjacent landowner's property to complete roadway segments. Offsite improvements designed and constructed by new development may be subject to reimbursement as a Specific Plan Improvement item through the collection of a Specific Plan Fee affecting Phase B and Phase C only. As examples, if the southwest corner of Morrill Road and Oakdale Road develops, the City may request pavement and curb and gutter improvements on the north side of Morrill Road as well. These offsite improvements could be subject to a reimbursement agreement for improvements constructed north of the centerline of Morrill Road.

9.2.4.4 Bridge Crossings at MID Lateral No. 6

The North/South Collector Road Bridge crossing at MID Lateral No.6 is expected to be required with development of LDR-4, Phase B. The costs associated with this bridge crossing complete with wet and dry utilities will be shared by all development in Phases B and C as a Specific Plan Fee affecting development north of the MID Lateral No. 6. A Specific Plan fee will be adopted to be paid by the developments north of MID Lateral No. 6 for the North/South Collector Road Bridge crossing, and any pedestrian crossings over the MID Lateral.

9.2.4.5 Crawford Road Connection

Existing Crawford Road west of the project site contains numerous small ranchette style parcels. With the development of the project, traffic levels would be expected to increase on Crawford Road. While these levels would be within acceptable traffic levels of service, the residents on Crawford Road have expressed a concern about this increased traffic.

Concurrent with the first development in Phase B, a gate will be constructed across Crawford Road, near the project's west property line, and an appropriate temporary vehicular turn around will be constructed on each side of the temporary gate. In addition, the temporary gate will be constructed in a way to allow quick access for the fire district to pass through and continue down Crawford Road, but otherwise will stay closed. The cost of these improvements will be borne by developments in Phase B and Phase C. As Crawford Road is constructed with the development of Phase B, and realigned consistent with this Specific Plan, and the north/south collector road and dual use/park basin are designed; the temporary gate and turnarounds will be revised to maintain emergency access, but not allow daily vehicular traffic westerly of the Specific Plan area via Crawford Road. In the future, when the area between the project site and Coffee Road develops (it is located within the City of Riverbank sphere of influence), Crawford Road will be continued to Coffee Road, and an alternative route through the neighborhood may be proposed, or Crawford Road may become Crawford Court permanently. Such a determination will be made in the future when annexation plans are promoted for the area west of the Crossroads West Specific Plan area.



9.2.5 Infrastructure Specific to the MU - 1 Property

MU - 1 property is expected to be an extension of the existing Crossroads Shopping Center east of Oakdale Road. Similar to the existing Crossroads Shopping Center, it is anticipated that the MU 1 developers will negotiate a Development Agreement. The infrastructure listed below could be eligible for reimbursement in the MU 1 Development Agreement, but the final determination as to which improvements are included, and the level of their inclusion in the Development Agreement will be based on future negotiations between the City of Riverbank and the developer of the MU 1 Property.

9.2.5.1 Oakdale Road (if necessary to make modifications to the existing completed improvements)

- Land dedication
- Traffic Signal modification/improvement at Freddi Lane
- Paving, curb, gutter and sidewalk, and lane modifications (including bike paths and/or lanes)
- Landscaping and irrigation between sidewalk and curb and in right of way
- Street lighting or modifications to street lighting
- New and/or existing under-grounding power and communication lines
- Main gas and electrical lines
- Water, storm, and Sewer Infrastructure (including any public water loop and fire hydrants)
- Street markings and signage
- Any bus shelter improvements as outlined in the Specific Plan

9.2.5.2 Claribel Road

- Land dedication
- Traffic Signal at Claribel Road and Oakdale Road
- Sidewalk / Bikeway along Claribel Road
- Street lighting or modifications to street lighting
- New and/or existing under-grounding power and communication lines
- Main gas and electrical lines
- Water, storm, and Sewer Infrastructure (including any public water loop and fire hydrants)
- Paving, curb, gutter, and sidewalk and lane modifications
- Street markings and signage
- Landscape and irrigation in right of way and between sidewalk and street curb

9.2.5.3 New North/South Collector Road / On-site Public Improvements

- Land dedication
- Traffic Signal at North/South Collector Road and Claribel Road.
- Sidewalk / Bikeway along new connector road
- Street lighting
- Under-grounding power and communication extensions / backbone power extensions
- Main gas and electrical lines
- Water, storm, and Sewer Infrastructure (including any public water loop and fire hydrants)
- Concrete curbing, gutter, and sidewalk and paving
- Street markings and signage
- Landscaping and irrigation in right of way between street curb and sidewalk



All hard and soft costs (i.e., Architecture and Engineering expenses, testing and inspections, Geo-Tech, Environmental testing and third party consulting charges) associated with the above improvements listed in Sections 9.2.5.1, 9.2.5.2, and 9.2.5.3.

9.3 Financing Strategies

In order to properly design, build and maintain the public facilities and infrastructure for Crossroads West, several funding sources and financing strategies will be utilized. The following subsections detail possible sources for financing, but it should be noted that the final mix of financing strategies will be determined in the implementation process.

Since each Phase, Phase A, B, and C, can develop independently, it is expected that each Phase will establish their own financing strategy. There are common infrastructure items which benefit a combination of Phase areas. In this case, shared financing strategies amongst Phases may be necessary to capture all costs based on benefits received. This would be the case for fee reimbursements, oversizing, fee offsets, maintenance districts, community facilities districts, and similar items. At the same time, if a Phase developer chooses to join another Phase in any of these districts, mechanisms, or agreements, they may at the developer's discretion.

9.3.1 Area Specific Impact Fees

Specific Plan fees may be developed as part of the Financing Strategy discussed above. In this case, fees will be calculated for the CWSP and levied against all new development within the Specific Plan area based on benefit to facilitate construction of public facilities and infrastructure to serve the Plan area. Specific Plan fees may be imposed as a result of the CWSP Financing Plan and may result in fee credits and/or fee waivers. Possible Specific Plan Fees which would benefit all Phases A, B and C might include: Neighborhood Parks as it relates to planned residential development and CFD formation costs.

Possible Specific Plan Fees which would benefit Phases A and B might include: a permanent sewer lift station north of MID Lateral No. 6 and force main to gravity sewer.

Possible Specific Plan Fees which benefit Phases B and C might include: Morrill Road; Crawford Road; North/South Collector Road north of MID Lateral No.6, Bridge over MID Lateral No. 6 complete with wet and dry utilities; Crawford EVA closure at western edge of the project; storm drainage improvements, Oakdale Road; sewer over-sizing on-site; and, the Pedestrian crossing over MID Lateral No. 6.

The Subdivision Map Act allows Cities to require developers to dedicate land or make cash payments for backbone infrastructure and public facilities required for their project. Land dedications are most often done for road rights-of-way and utility easements, park sites and other public facilities.

9.3.2 Assessment Districts and Special Tax Districts

The State of California has laws and procedures in place to levy assessments against properties benefiting from improvements and to issue tax-exempt bonds to finance said improvements. The districts must be initiated by the governing body (City Council) and are subject to a majority protest of the property



owners. Assessments are distributed based on the benefits received by each property and act as a lien against the property. The assessments are typically a fixed dollar amount and may be prepaid however; most property owners or developers pay them back over time with accrued interest.

9.3.2.1 Community Facilities District (CFD's)

California's Mello-Roos Community Facilities Act of 1982 allows the creation of special districts authorized to levy a special tax and issuance of tax exempt bonds to finance public facilities and services. Because CFD's use non-resource bonds that eliminate risk to the municipality, they are an attractive funding source for large projects like Crossroads West. The creation of a CFD can be initiated by the governing body (City Council) or by the property owner. The property owner must submit a petition and garner a 2/3 vote of all registered voters living in the area.

Bonds issued to a CFD provide greater flexibility to developers because a broader range of improvements can be funded through a CFD. These bonds also provide flexibility with the timing of improvements because the monies are available as-needed and not on a pay as you go system. At this time, the City of Riverbank has stated their preference that a bonded CFD not be established for Crossroads West, although one is not prohibited by this Specific Plan.

Developers in the Crossroads West Specific Plan area will be required to create a Maintenance CFD for road, storm water systems, street lighting, parks, and landscaping as well as extended police protection services. The CFD will likely have a number of zones of benefit to reflect actual services provided to each Phase by land use type which will result in a tiered assessment approach.

9.3.3 Private Funding

Private funding also referred to as developer funding simply means the improvements associated with the CWSP area will be the responsibility of the developer and/or property owners. A developer is free to seek out private funding sources, equity or debt financing of his choosing to complete the necessary improvements to service Crossroads West. Should private funding be the mechanism chosen, the developer assumes the most risk initially due to the high up-front costs associated with construction of the improvements. Reimbursement Agreements will be necessary between the developer and the City to assure the developer is reimbursed for the cost of constructing the backbone infrastructure and any other public facilities or improvements. For this funding source, the City acts as the bookkeeper and is responsible for collecting monies and issuing payments to the master developer.

9.3.4 Other Funding Sources

Other funding sources include public and private financing through grants, bonds and overrides. Some of the most popular are Federal and State Grants, General Obligation Bonds, Revenue Bonds, School Bonds and Sales Tax Overrides.

Cities are sometimes eligible for grants from other public agencies like the Federal or State Government. While limited, these grants should be considered as a valid funding source for certain improvements. Grants from the Federal or State government will be project specific and only valid for the improvement for which the grant is issued. Further research will be required to determine the availability of these grants.



Proposition 46 was passed in 1986 which permitted Cities, Counties and School Districts to issue general obligation bonds. These bonds may be used to finance the acquisition of land for capital improvements and are repaid with the revenues generated from increased property taxes. A general obligation bond can only be established and issued upon securing a 2/3 voter approval. Sales tax overrides are an additional sales tax that is above the local level currently being collected. Jurisdictions may elect to submit a sales tax override measure to be approved by the voters. If approved, the additional sales tax would be collected and used to fund infrastructure improvements and capital projects.

The Land Use Plan for Crossroads West shows two possible school sites that can be developed subject to approval by the Sylvan Union School District. Senate Bill 50 authorizes school districts to collect development impact fees to fund a portion of new school construction. While these fees are solely for use by the school district to fund limited school construction and improvements, the impact fees are valid source of funding and should be considered.

9.4 Establishing Financing Plan

9.4.1 Financing Principles and Policies

The following principles and policies will govern the funding and financing strategies for the CWSP area. These principles and policies will help guide the decisions on the formation of financing entities, adopting financing mechanisms and approving the project.

1. A complete financing analysis should be prepared to identify the infrastructure needed to properly serve each phase of the Crossroads West Specific Plan area.
 - **Policy 1** – Financial analysis shall be conducted that encompasses the entire CWSP area, identifying all infrastructure costs and financing mechanisms proposed. The analysis shall account for project-specific circumstances, allow for a changing market and provide refined facility and cost data as it becomes available. The Mixed Use 1 area and its financing plan will be limited to only the infrastructure needed to serve the site, but includes infrastructure that benefits Phase B, Phase C, and the City, and therefore all such parties will participate in a financial resolution.
2. New development within Crossroads West should pay the full costs for backbone infrastructure and other public facilities needed to serve the project.
 - **Policy 2** – The existing residents surrounding the CWSP area shall not be burdened with new taxes or assessments to pay for public infrastructure or facilities that they do not benefit from.
 - **Policy 2.1** – As a part of the Financing Plan to be submitted for approval, evaluate the City's existing development impact fee program to determine the relation with backbone infrastructure and other public facilities required for the CWSP area.
 - **Policy 2.2** – Should any property outside of the CWSP area receive benefit from new infrastructure or facilities, an area of benefit shall be established and said property owners will be assessed a fair share of cost.
3. The total cost for infrastructure improvements and public facilities should be kept to industry standards and minimums and not exceed projected costs.
 - **Policy 3** – New construction of infrastructure and public facilities shall utilize local resources



when possible to keep costs low.

- **Policy 3.1** – Encourage measures that minimize costs for new construction in the CWSP area. Such measures can include construction and reimbursement agreements with the developer, which can lower costs generally associated with these improvements.
4. If a property owner within the boundaries of Crossroads West is required to dedicate land or make improvements to serve the project that are in excess of the benefit the landowner would receive, the excess value should be reimbursed from other properties who receive a benefit.
 - **Policy 4** – Require dedication of land for road improvements and construction of public improvements consistent with City policies.
 - **Policy 4.1** – The City shall provide credits against City fees to landowners or developers who build infrastructure of broader City-wide benefit.
 - **Policy 4.2** – The Financing Plan shall include options to incentivize properties to develop at the designated land use and density provided for in the CWSP.
 5. Financing mechanisms should be identified or established to ensure the on-going maintenance of public facilities and improvements.
 - **Policy 5** – The City shall assume responsibility for coordination of landowners and developers in establishing the appropriate financing mechanisms. The mechanisms shall be incorporated into the Financing Plan and should be interpreted broadly to include all possible funding sources. The City shall support the developers of Crossroads West to create their own separate maintenance of infrastructure, or in combination with one or more of the phases of Crossroads West.

9.4.2 Implementation of Financing Plan

The implementation of the Financing Plan shall be a joint effort between the City of Riverbank and the developers of each specific phase of the Crossroads West Specific Plan area. The schedule of implementation has been designed to accommodate different circumstances including changes to the infrastructure and the overall intensity of the development.

Action 1 – Identify the Final Public Facilities

Prior to the adoption of the Financing Plan, the City must provide direction concerning what public facilities are needed to adequately serve the plan area. This decision shall require input from the Police Department, Stanislaus Consolidated Fire Protection District, Modesto City Schools and Sylvan Union School District on the proposed facilities outlined in the Land Use Plan.

Action 2 – Complete Final Cost Estimates

The interested parties shall compile a list of final infrastructure costs associated with the CWSP area. The cost estimates will be incorporated into the Financing Plan and will be considered preliminary and for planning purposes only.

Action 3 – Establish Infrastructure Phasing Based on Development Priorities

It is the assumption of the CWSP that the regional Mixed Use 1 area will begin construction first and therefore the necessary infrastructure and tie-ins for this area will be phased first. The remaining land plan will need to establish a sequence of phasing for improvements to assure demands are met with new construction.



Action 4 – Formalize any City and Other Agency Funding Commitments

Based on the outcomes of the previous action steps, the City shall select the preferred financing strategy or strategies desired for the construction of the Crossroads West project.

9.5 City Services Provision

In addition to the construction costs associated with new infrastructure and public facilities, the City shall ensure that new development within Crossroads West Specific Plan will fully fund the ongoing operations and maintenance costs associated with the new project. The City of Riverbank will require the formation of and annexation to a Maintenance CFD, which shall be dependent on the developers' election, to maintain the infrastructure independently, or in combination with the other developers.

An analysis of the cost of maintenance of items identified shall be prepared prior to the recordation of the First Final Map. The analysis shall focus on the potential benefits each phase shall receive based on land use type and respective public service benefit. This analysis shall provide a framework for subsequent detailed financial planning and implementation for the costs of ongoing maintenance, operation and public services. The analysis will identify the types and estimated costs of ongoing operations and maintenance costs that new development has on City support services.

9.6 Draft Capital Improvement Program

The following Capital Improvement Program (CIP) identifies the Major Back Bone Infrastructure that is expected for each Phase of development as well as the responsible party and maintenance obligations. This is a “draft” plan as until development is actually proposed, and engineering analysis is prepared, it is not clear that all of the proposed items will be needed up front. Some items may be able to be deferred and as required in Paragraph 9.1, prior to development of any Phase a more detailed Financing Plan will be prepared. The funding sources which have been denoted as “To be Determined” (TBD) is meant to imply that the City and Developer are in the midst of negotiating possible funding sources, which will be determined in the future by City Council action. It is likely these items denoted would be part of a package of negotiated conditions which would be formalized in a City Council approved Agreement.



9.6.1 Phase A - Capital Improvement Program

Phase A

Improvement	Responsible Party	Funding Sources	When Provided	Maintenance Responsibility
<p>Water Lines 12 inch water line in Claribel Road and Oakdale Road, adjacent to Phase A will be constructed incrementally as Phase A develops. Water line and all appurtenances may be constructed in a PUE adjacent to Claribel Road or interior of the project site provided the responsibilities for this improvement have been approved by the City of Riverbank.</p>	Developer	Systems Development Fee	Concurrent with Development	City
<p>Off-Site Sewer Upgrade The Crawford at Roselle Road Lift Station and downstream improvements to Force Main and Trunk Sewer will need to be upgraded. If it is determined, based on engineering calculations, some capacity is available; the City will allow commercial development on Phase A only prior to requiring the sewer upgrades. The upgrade may be phased if engineering calculations show discrete components can be constructed to increase capacity, or Phase A developments may increase onsite storage capacity and meter flows. Any initial available capacity shall be reserved for use in Phase A for commercial development only. This excludes any residential development planned for Phase A.</p>	Developer	Systems Development Fee ^{<1>}	Based on Engineering Analysis	City
<p>Sewer Line (Under MID, Lateral No. 6) Phases B and C will share the cost for extending the sewer line under MID Lateral No. 6 to serve Phase A at the time of construction of the north/south Collector Road over MID Lateral No. 6.</p>	Developer of Phase B & C	Developer of Phase B & C	When required by City	City
<p>Sewer Lines (South of MID Lateral No. 6, external, adjacent and internal to the Project) Internal sewer lines serving Phase A will be constructed by one of the methods identified in this CWSP.</p>	Developer	TBD	Concurrent with Development	City/ Developer ^{<3>}
<p>Water Lines Onsite public water main lines of 12" serving multiple parcels.</p>	Developer	TBD	Concurrent with Development	City
<p>Storm Drain Lines Main storm drain lines serving multiple parcels which will drain towards the existing system in the Crossroads Development East of Oakdale Road.</p>	Developer	Developer	Concurrent with Development	City
<p>Temporary Sewer Lift Stations Phase A has the option to construct a temporary lift station to serve the western portion of the development in the event development occurs in advance of gravity sewer lines being available north of MID Lateral No. 6. Construction of a temporary lift station is allowed at developers' cost and discretion.</p>	Developer	Developer	Concurrent with Development if gravity lines are not available	Developer



Major Road Frontage Widening Oakdale Road widening to occur adjacent to development. Reimbursements to be determined at the time of development or through a Development Agreement.	Developer	TBD	Concurrent with Development	City
Claribel Road widening Including underground of any overhead power/ telecom lines, bike lane, landscaping and sidewalks. Reimbursements to be determined at the time of development or through a Development Agreement.	Developer	TBD	Concurrent with Development	City
North/South Collector Road Bridge (Over MID Lateral No. 6) Bridge construction including wet and dry utilities, when required, is the responsibility of Phases B and C.	Developer of Phase B & C	Developer of Phase B & C	When required by City	City
North/South Collector Road/Claribel Road Traffic Signal The traffic signal at the north/south Collector Road and Claribel Road intersection will be installed when warranted.	Developer	SDF	When Warranted	City
North/South Collector Road Water, sewer, storm drain, gas, electrical, street lights, landscaping, curbs, gutter, sidewalks, and paving from Claribel Road to southern edge of the Bridge over the MID Lateral No. 6.	City	TBD	Concurrent with Development timing to be determined by the City Engineer	City
Oakdale Road and Freddi Lane Traffic signal and intersection modification, Work may be required at the existing Riverbank Crossroads and Phase A driveway including signal, traffic loops, curb ramps, sidewalks, pavement, striping, and landscaping.	Developer	TBD	When Warranted	City
Neighborhood Parks Neighborhood Parks will be constructed concurrent with development of residential units and/or provided through the payment of park fee offsets.	Developer	System Development and Parkland Dedication Fees (Affects Residential Development only)	Concurrent with Development	CFD

¹If Improvement is required before adequate Systems Development Fees have been collected; Developer may be required to front the shortfall and receive reimbursement over time.

²If City has collected adequate fees from development in the CWSP prior to the need for the facility; the City will construct the improvement on its schedule.

³Permanent City facilities will be maintained by City. Temporary facilities will be maintained by Developer/Landowner.



9.6.2 Phase B - Capital Improvement Program

Phase B

Improvement	Responsible Party	Funding Sources	When Provided	Maintenance Responsibility
<p>Water Lines 12 inch water lines in Oakdale Road, Morrill Road, Crawford Road, and north/south Collector will be constructed incrementally based on a Water Analysis prepared at the time of development.</p>	Developer	Systems Development Fee ¹	Concurrent with Development	City
<p>Water Well (And Site) Prior to development in Phases B or C, a Water Analysis will be prepared to determine the timing of construction of the new water well.</p>	Developer or City ²	Systems Development Fee ¹	Based on Engineering Analysis	City
<p>Peaking Reservoir (And Site) One approximately 2.0 MG Peaking Reservoir and booster pump is proposed north of MID Lateral No.6. Timing of construction will be based on a Water Analysis prepared in advance of development in Phases B and C.</p>	Developer or City ²	Systems Development Fee ¹	Based on Engineering Analysis	City
<p>Off-Site Sewer Upgrade The Crawford Road at Roselle Lift Station and downstream improvements to Force Main and Trunk Sewer will need to be upgraded. The upgrade may be phased if engineering calculations show discrete components can be constructed to increase capacity.</p>	Developer	Systems Development Fee ¹	Based on Engineering Analysis	City
<p>Sewer Lines (North of MID Lateral No. 6) Prior to development, a reimbursement program will be prepared to share costs between Phases B and C for sewer lines over ten (10) inches in size.</p>	Developer	Developer	Concurrent with Development	City
<p>Sewer Line (Under MID, Lateral No. 6) Phases B and C will share the cost for extending the sewer line under MID Lateral No. 6 to serve Phase A at the time of construction of the north/south Collector Road over MID Lateral No. 6.</p>	Developer	Developer	When required by City	City
<p>New Sewer Lift Station A new sewer lift station is required to serve the southerly area of Phase B. Prior to construction of this lift station, an analysis will be prepared to determine if the lift station can serve areas west of the CWSP, and if so, a reimbursement mechanism will be established.</p>	Developer	Developer	When Development Warrants	City
<p>Major Road Frontage Widening Oakdale Road widening to occur adjacent to development. Reimbursements to be determined at the time of development or through a Development Agreement.</p>	Developer	TBD	Concurrent with Development	City



<p>Collector Roads Crawford Road, Morrill Road and the north/south Collector Road to be constructed at the time of adjacent development. City may require discrete sections which extend beyond development frontage, but such improvement would be subject to reimbursement by agreement or Development Agreement when in front of a benefiting landowner.</p>	Developer	Developer	Concurrent with Development	City
<p>North/South Collector Road Bridge (Over MID Lateral No. 6) Bridge construction including all wet and dry utilities, when required, is the responsibility of Phases B and C.</p>	Developer	Developer	When required by City	City
<p>Pedestrian Bridge over MID Lateral No. 6 Pedestrian Bridge, if constructed, are the responsibilities of Phases B and C</p>	Developer	Developer	When required by City	City
<p>Regional Park Expansion City will construct the Regional Park expansion.</p>	City	Systems Development Fee ^{1and 4}	When Determined by City	City
<p>Neighborhood Parks Neighborhood Parks will be constructed concurrent with development of residential units or provided through the payment of park fee offsets.</p>	Developer	Systems Development and Parkland Dedication Fees ^{1and 4}	Concurrent with Development	City
<p>Storm Drainage Each Phase of development will provide its own storm drainage system, supported by engineering calculations. Payment of City storm drainage fees will not be required.</p>	Developer	Developer	Concurrent with Development	CFD

¹If Improvement is required before adequate Systems Development Fees have been collected, Developer may be required to front the shortfall and receive reimbursement over time.

²If City has collected adequate fees from development in the CWSP prior to the need for the facility, the City will construct the improvement on its schedule.

³Permanent City facilities will be maintained by City. Temporary facilities will be maintained by Developer/Landowner.

⁴Applies to residential development only



9.6.3 Phase C - Capital Improvement Program

Phase C

Improvement	Responsible Party	Funding Sources	When Provided	Maintenance Responsibility
<p><u>Water Lines</u> 12 inch water lines in Oakdale Road, Morrill Road, Crawford Road, and north/south Collector will be constructed incrementally based on a Water Analysis prepared at the time of development.</p>	Developer	Systems Development Fee ¹	Concurrent with Development	City
<p><u>Water Well (And Site)</u> Prior to development in Phases B or C, a Water Analysis will be prepared to determine the timing of construction of the new water well.</p>	Developer or City ²	Systems Development Fee ¹	Based on Engineering Analysis	City
<p><u>Peaking Reservoir (And Site)</u> One approximately 2.0 MG Peaking Reservoir and booster pump is proposed north of MID Lateral No.6. Timing of construction will be based on a Water Analysis prepared in advance of development in Phases B and C.</p>	Developer or City ²	Systems Development Fee ¹	Based on Engineering Analysis	City
<p><u>Off-Site Sewer Upgrade</u> The Crawford Road at Roselle Lift Station and downstream improvements to Force Main and Trunk Sewer will need to be upgraded. If it is determined, based on engineering calculations, some capacity is available; the City may allow some development prior to requiring the upgrade. The upgrade may be phased if engineering calculations show discrete components can be constructed to increase sewer capacity.</p>	Developer	Systems Development Fee ¹	Based on Engineering Analysis	City
<p><u>Sewer Lines (North of MID Lateral No. 6)</u> Prior to development, a reimbursement program will be prepared to share costs between Phases B and C for sewer lines over ten (10) inches in size.</p>	Developer	Developer	Concurrent with Development	City
<p><u>Sewer Line (Under MID, Lateral No. 6)</u> Phases B and C will share the cost for extending the sewer line under MID Lateral No. 6 to serve Phase A at the time of construction of the north/south Collector Road over MID Lateral No. 6.</p>	Developer	Developer	When required by City	City
<p><u>Temporary Lift Stations</u> Phase C has the option to construct a temporary lift station to serve the development in the event development occurs in advance of gravity sewer lines being available. Construction of such temporary lift station is allowed at developer's cost and discretion.</p>	Developer	Developer	Concurrent with Development if gravity lines are not available	Developer



Major Road Frontage Widening Oakdale Road widening to occur adjacent to development. Reimbursements to be determined at the time of development or through a Development Agreement.	Developer	TBD	Concurrent with Development	City
Collector Roads Crawford Road, Morrill Road and the north/south Collector Road to be constructed at the time of adjacent development. City may require discrete sections which extend beyond development frontage, but such improvement would be subject to reimbursement by agreement or Development Agreement when in front of a benefiting landowner.	Developer	Developer	Concurrent with Development	City
North/South Collector Road Bridge (Over MID Lateral No. 6) Bridge construction including wet and dry utilities, when required, is the responsibility of Phases B and C.	Developer	Developer	When required by City	City
Pedestrian Bridge Over MID Lateral No. 6 Pedestrian Bridge, if constructed, are the responsibilities of Phases B and C	Developer	Developer	When required by City	City
Regional Park Expansion City will construct the Regional Park expansion.	City	Systems Development Fee ^{1 and 4}	When Determined by City	City
Neighborhood Parks Neighborhood Parks will be constructed concurrent with development of residential units or provided through the payment of park fee offsets	Developer	Systems Development and Parkland Dedication Fees ^{1 and 4}	Concurrent with Development	CFD
Storm Drainage Each Phase of development will provide its own storm drainage system, supported by engineering calculations. Payment of City storm drainage fees will not be required.	Developer	Developer	Concurrent with Development	CFD

¹If Improvement is required before adequate Systems Development Fees have been collected, Developer may be required to front the shortfall and receive reimbursement over time.

² If City has collected adequate fees from development in the CWSP prior to the need for the facility, the City will construct the improvement on its schedule.

³ Permanent City facilities will be maintained by City. Temporary facilities will be maintained by Developer/Landowner.

⁴Applies to residential development only.



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Chapter 10

IMPLEMENTATION AND ADMINISTRATION

10.1 Overview

California Government Code Sections 65450 through 65457 guide the formation, approval and administration of specific plans for municipalities. This chapter describes the Development Plan Review process, Specific Plan Interpretation and amendment process, Design Review Process, Plan enforcement and other mechanisms to implement the Crossroads West Specific Plan (CWSP).

Riverbank Municipal Code (RMC) states that a specific plan must be adopted by the City Council either by resolution or ordinance and that the specific plan shall be prepared, adopted and amended in the same manner as a general plan. The only difference, a specific plan document may be amended as many times as deemed necessary by the legislative body.

10.1.1 City Documents

General Plan

The City of Riverbank 2025 General Plan serves as the long-term policy guide for the physical and economic development of the City. The City's core values are the foundation of the General Plan and the underlying basis for its vision and direction.

The Specific Plan implements the goals and policies of the General Plan and supplements these goals and policies by providing specific direction to reflect conditions unique to the Specific Plan area. Prior to the initial approval of this Specific Plan, a comprehensive citywide update to the General Plan was approved, which incorporated the appropriate land use and policy framework to reflect the Specific Plan development program. The Specific Plan is consistent with the City's 2025 General Plan as adopted.

Zoning & Other Codes

The City of Riverbank Municipal Code is one of the primary tools for implementing the General Plan. Included in the Municipal Code is the City's Zoning Code, which will be used in conjunction with the Specific Plan to implement the development program. Other related components of the Municipal Code include the Subdivision Ordinance, Sign Ordinance, and Tree Ordinance, all of which will be used to implement the Specific Plan where applicable.

The Specific Plan modifies the permitted uses, development standards, and other regulations of the City's Municipal Code. In these cases, the standards and regulations provided in the Specific Plan supersede those in the Municipal Code. Where the Specific Plan is silent, regulations in the Municipal Code remain applicable. If a conflict occurs between the requirements of the Specific Plan and the City's Municipal Code, the requirements of the Specific Plan shall prevail.



Standard Specifications

The City of Riverbank Standard Specifications provide detailed drawings and design standards to guide public improvement projects within the City. Topics addressed include safety, streets, lighting, water, sewer, storm drains, parking, walls, fire standards, utilities, landscaping, irrigation, standard measures, backflow and cross connections, and bike and pedestrian trails.

The Specific Plan includes plans, standards and guidelines that supplement the City's adopted Standard Specifications. If a conflict occurs between the requirements of the Specific Plan and the City's Standard Specifications, the requirements of the Specific Plan shall prevail.

10.1.2 Specific Plan Documents

Environmental Impact Report

An Environmental Impact Report (EIR) was certified for the Specific Plan by the City Council through the adoption of Resolution No. _____ on _____. The EIR, prepared in accordance with CEQA, examines the potential direct and indirect environmental effects of the project and identifies appropriate mitigation measures, where feasible, to reduce impacts determined to be potentially significant.

This EIR serves as the base environmental document for purposes of evaluating subsequent development entitlements, discretionary permits, and ministerial approvals. Included is the Mitigation Monitoring and Reporting Program to be used by the City and project developers in ensuring compliance with adopted mitigation measures as the Specific Plan area builds out. The Specific Plan Mitigation Monitoring and Reporting Program is included as Appendix A.

10.2 Effectuation of Entitlements

Concurrent with adoption of this Specific Plan, the City will prezone all properties within the Specific Plan Area to SP-4 pursuant to Section 153.309 of the Riverbank Municipal Code. In order to fully effectuate land use and zoning entitlements, and allow development on any property with the Specific Plan Area to proceed, the following subsequent approvals may be required:

Development Agreement

Development Agreements are anticipated for one or more of the development areas for Crossroads West Specific Plan.

Key topics which might be addressed in development agreement for properties include, as applicable:

- Vested rights (subject to subsequent project entitlements), and applicable laws and regulations
- Affordable housing requirements, participation, and subsidies (if any).
- Outside agency approvals, permits and responsibilities.
- Public improvements (on-site and off-site) to be dedicated constructed or financed by Specific Plan area and individual properties.
- Park and ride lots.
- Parks, open space and trail dedication, thresholds, construction, and financing.



- Police, fire, and general fund contributions.
- Community wide financing mechanisms formation and obligations (infrastructure, services and maintenance).
- Credits and reimbursements.
- Project phasing/sub-phasing, including “large lot” parcelization and/or subdivisions.
- Defaults, remedies and termination.
- Hold harmless agreement.
- Other relevant provisions.

A Development Agreement must be approved and executed prior to or concurrent with final City approval of any subsequent development entitlements for an applicable property. Development Agreements are to be processed in accordance with the Riverbank Municipal Code, and run with the land.

Project Entitlements in Riverbank

Individual development projects within the Specific Plan are subject to review and approval of subsequent permits and entitlements by the City. Typical entitlements may include architectural and site plan review, PDP’s, use permits, parcel maps, subdivision maps, and/or other permits. Application and processing requirements, with the exception of the Site Plan Review and PDP’s, shall be in accordance with the City’s Zoning Code and other regulations, unless otherwise modified by this Specific Plan.

All subsequent development projects, public improvements, and other activities shall be consistent with the provisions of the Specific Plan, applicable development agreements, and pertinent City of Riverbank policies, requirements and standards. In acting to approve a subsequent project or permit, the City may impose conditions as are reasonably necessary to ensure that the project is in compliance with the Specific Plan and all then applicable plans and regulations.

Outside Agency Approvals

Other permits and approvals may be required by federal, state and/or regional agencies. These agencies may include the U.S. Army Corps of Engineers, U.S. Fish and Wildlife Services, California Department of Fish and Game, California Department of Transportation, Regional Water Quality Control Board, San Joaquin Valley Air Pollution Control District, Stanislaus County, Modesto Irrigation District and others as applicable.

Subsequent Environmental Review

Each subsequent development project shall be reviewed to ensure compliance with the California Environmental Quality Act (CEQA). The project specific EIR, certified for the Crossroads West Specific Plan serves as the base environmental document for subsequent entitlements within the Specific Plan area. Development applications are to be reviewed on a project-by-project basis to determine consistency with the EIR.

The EIR prepared for the Specific Plan identifies specific mitigation measures to be applied to projects within the Specific Plan as applicable. Some of the mitigation measures necessitate additional study or



review to be undertaken prior to a project moving forward.

See the Specific Plan Mitigation Monitoring and Reporting Program (Appendix B) for a full listing of applicable mitigation measures.

Existing Uses

At the time of Specific Plan approval, there are a number of existing homes and related agricultural structures within the Specific Plan Area. While some of these uses may remain either indefinitely or for an extended period, it is expected that many will ultimately be replaced to accommodate new development consistent with Specific Plan land uses.

Certain existing land uses that were legal under prior land use regulations may not conform to the standards specified in this Specific Plan. Since the Specific Plan is oriented towards the future, it is intended that these nonconforming uses will eventually be phased out consistent with the City's Zoning Code. Permits should not be granted for expansion of structures to accommodate nonconforming uses, but routine interior and exterior maintenance is permitted under certain circumstances. Routine interior and exterior maintenance shall be determined by the Community Development Director. This Specific Plan does not establish a specific time frame for non-conforming uses to comply.

10.3 Amendment Process

The CWSP represents the master plan for the Crossroads West development area. Individual project applications will be reviewed by the Community Development Director or its designee, without requiring public notice and/or public hearing, to determine consistency with the Specific Plan and other regulatory documents. Such applications could include Large Acreage Tract Maps, Parcel Maps, Tentative Subdivision Maps, Development Plans for Commercial or Mixed Use projects, Design Review Applications and Conditional Use Permits.

Applications for any entitlement within the CWSP shall be submitted to the City of Riverbank Development Services Department for review and action, which shall be conducted by the Community Development Director or its designee. Upon submittal of a complete application package, the applicant will be notified of any deficiencies and/or requests for additional information needed to process the requested action. To the extent an owner and staff cannot agree, owner may appeal staff's decision to the Planning Commission for approval, and if the owner disagrees with such Planning Commission decision, owner may appeal to the City Council.

10.4 Architectural and Site Plan Review Process

All applications for residential, commercial and mixed use developments in the Specific Plan will be under obligation to present architectural drawings to the Community Development Director to demonstrate how the proposed projects complies with and implement the Crossroads West Specific Plan. The City shall develop processing forms and charge appropriate fees for the administrative review of Design Review Permits. It will be expected that sufficient evidence is presented along with the applications being presented to allow the Community Development Director the opportunity to fully understand the project being proposed in order to determine, without requiring public notice and/



or public hearing, that the design is in substantial conformance with the Specific Plan. The review and approval of the Design Review Permit shall be by the Community Development Director, unless appealed to the Planning Commission for approval in accordance with the Riverbank Municipal Code, and if the owner disagrees with such Planning Commission decision, owner may appeal to the City Council.

10.5 Preliminary Development Plan Process

A Developer shall also prepare a Preliminary Development Plan (PDP) concurrent with, or prior to, tentative parcel map, tentative subdivision map and Site Plan Review approvals as described in Section 10.5 of this document.

Scope: The minimum area to be addressed in the PDP is the development area as indicated in Figure 5 as distinct development areas (e.g., LDR-1, LDR-2, LDR-3, LDR-4, MU-1 and MU-2) and shall be a schematic plan covering the entire development area. The PDP shall illustrate how the proposed project implements the CWSP and show tentative locations for future circulation and infrastructure connection points.

Purpose: The purpose of the PDP will be to assist City Staff in reviewing and conditioning the development applications consistent with the CWSP.

PDP's shall be subject to the following provision of this Specific Plan:

- a) The density of development shall be consistent with the Specific Plan land use designations.
- b) The number of residential units or commercial square footage proposed shall be within the minimum and maximum range of the Specific Plan for that land use category.
- c) The PDP must be approved by the Planning Commission prior to consideration of a Site Plan Approval by Staff or concurrently with the Tentative Map approval.
- d) Residential units allocated by the Specific Plan may be reallocated within a distinct development area through a Preliminary Development Plan if: (i) the minimum numbers of medium and high density units are achieved, (ii) the average number of units allowed in the district is not exceeded and (iii) PDP complies with the CWSP design guidelines. Compliance must be demonstrated with a numerical comparison of low, medium, and high density units required in the Specific Plan and the proposed low, medium, and high density units.
- e) Residential units allocated by the Specific Plan may be transferred to another planning area when two or more distinct development areas are the subject of one PDP. The reallocation of units between distinct development areas shall be deemed consistent with the Specific Plan if: (i) the minimum numbers of medium and high density units are achieved in each district, (ii) the average numbers of units allowed in each district is not exceeded and (iii) the design guidelines are followed. Compliance must be demonstrated with a numerical comparison of low, medium, and high density units required in the Specific Plan and the proposed low, medium, and high density units by district.

10.6 Administration

The City of Riverbank is responsible for and shall cause its staff to perform the administration, implementation, and enforcement of the Specific Plan, without requiring public notice and/or public hearing, provided the design is in conformance with the Specific Plan. Implementation of the Specific



Plan is anticipated to occur over several years, and over time conditions may change that necessitate interpretation, minor modification, and possible amendment to the Specific Plan. It is intended that the Specific Plan be administered with flexibility and creativity to allow the City and property owners to react quickly to changes in the market place within the intent of the Specific Plan. To the extent an owner and staff cannot agree, owner may appeal staff's decision to the Planning Commission for approval, and if the owner disagrees with such Planning Commission decision, owner may appeal to the City Council.

10.7 Interpretations

Interpretations of any standards, guidelines, and requirements included within this Specific Plan are to be made at the discretion of the Community Development Director (Director). In making an interpretation, the Director shall determine that the interpretation is consistent with, and will not otherwise obstruct the attainment of, the Specific Plan. The Director may forward any interpretation directly to the Planning Commission for determination. All interpretations of the Director may be appealed to the Planning Commission for approval in accordance with the Riverbank Municipal Code, and if the owner disagrees with such Planning Commission decision, owner may appeal to the City Council.

For purpose of interpretation the terms "shall", "will", "must", "are to", and "is to" are mandatory. "Should" and "encouraged" are highly recommended, and "may" is permissive.

10.8 Alternative Development & Design Standards

The Specific Plan allows for the approval of alternative development and design standards that differ from those specified in the Specific Plan. The intent is to provide flexibility to respond to evolving market conditions, promote innovation design, and consider the merits of individual projects. Alternative standards may be approved by the Director, without requiring public notice and/or public hearing, provided the alternative development and/or design standards are in substantial conformance with the Specific Plan, and such alternatives shall not require amendment to the Specific Plan when they:

- Provide for efficient, diverse, and innovative development.
- Encourage the mixing of uses.
- Enhance pedestrian orientation, connectivity, and social interaction.
- Promote attractive and active streetscapes.
- Support high quality and district design that strengthens community character and identity.
- Respect the character and quality of adjacent uses.
- Respond to unique natural, cultural, and/or scenic resources.
- Are consistent with the intent and character of the Specific Plan and the 2025 General Plan.
- Achieve the original design intent to a same or better level.
- Respond to changes and evaluation ongoing in marketplace in MU 1 Uses.

A request for alternative standards is to be submitted and approved concurrent with the subdivision map or site plan review application required for the applicable project. The request shall include explanation and illustration of the proposed alternative standards, how they achieve the above criteria and any other information deemed necessary for evaluation by the Director. In reviewing a request for alternative standards, the Director and/or its designee, without requiring public notice and/or public hearing, shall consider whether the application complies substantially with the above criteria. To the extent an owner



and staff cannot agree, owner may appeal staff's decision to the Planning Commission for approval, and if the owner disagrees with such Planning Commission decision, owner may appeal to the City Council.

10.9 Amendment Process

Large projects like Crossroads West adopt specific plans often with phased, lengthy development time-frames and multiple developers/builders. Situations may arise where an amendment to the specific plan is warranted including changing market conditions, unforeseen circumstances or development standards changes through the City.

Amendments to the specific plan shall be classified as minor or major and the Community Development Director will be the primary decision maker. To the extent an owner and staff cannot agree, owner may appeal staff's decision to the Planning Commission for approval, and if the owner disagrees with such Planning Commission decision, owner may appeal to the City Council.

10.9.1 Minor Specific Plan Modifications

The Specific Plan allows for approval of minor modifications to the Specific Plan. The Community Development Director shall determine whether a proposed revision is minor, and may act upon a minor modification administratively without amendment to the Specific Plan. A minor modification may be approved if determined by the Director to be in substantial conformance with:

- The vision and overall intent of the Specific Plan;
- Applicable development agreement(s);
- The City of Riverbank General Plan;
- The Specific Plan Environmental Impact Report.

Examples of minor modifications include, but are not limited to:

- Minor adjustments to land use boundaries and street alignments where the general land use pattern is maintained.
- Changes to the provision of public infrastructure and facilities that do not impact the level of service provided or affect the development capacity in the Specific Plan area.
- Other modifications determined by the Director to be in substantial conformity with the Specific Plan.

A request for a minor modification is to be submitted to the Community Development Director. The request shall include explanation of how the request is in substantial conformance with the above, and any other information deemed necessary for evaluation by the Director. The Director, without requiring public notice and/or public hearing, may make a determination on the project or may forward any minor modifications to the Planning Commission for determination. All minor modifications to the Specific Plan may be appealed to the Planning Commission in accordance with Riverbank Municipal Code. To the extent an owner and staff cannot agree, owner may appeal staff's decision to the Planning Commission for approval, and if the owner disagrees with such Planning Commission decision, owner may appeal to the City Council.



10.9.2 Major Specific Plan Amendments

When it is determined that a specific development project or requested interpretation, alternative standard, or minor modification is not in substantial conformity with the Specific Plan, an amendment to the Specific Plan will be required. A Specific Plan amendment may be initiated by a developer, property owner, or the City. Specific Plan amendments are to be processed in the same manner as the initial Specific Plan adoption, requiring review by the Planning Commission and action by the City Council as specified in Section 153.309 of the Riverbank Municipal Code. Depending upon the nature of a Specific Plan amendment, a concurrent amendment to the General Plan, Municipal Code, Development Agreement, or other related City and Specific Plan documents may be required.

All requests for a Specific Plan amendment and related documents shall include text (red-line/strike-out), graphics and other materials suitable to replace or augment the sections being amended. The graphic format and style of the original documents is to be followed for ease of incorporation and consistency. All amendments to the Specific Plan and related documents are subject to compliance with CEQA.

10.10 Specific Plan Enforcement

Any violation of the requirements of this Specific Plan is to be enforced in the same manner as a violation of the Municipal Code.

10.11 Severability

If any section, subsection, phrase, regulation, condition, program or portion of this Specific Plan, or any future amendment, is for any reason held to be invalid or unconstitutional by any court or competent jurisdiction, such decision shall not affect the validity of the remaining portion of this Specific Plan, or any future amendment.



APPENDIX A

GP Consistency Matrix



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Appendix A: GP Consistency Matrix

The following table summarizes the General Plan consistency with the Crossroads West Specific Plan (CWSP).

For the Purposes of efficiency and brevity, only those goals within the Riverbank General Plan that are relevant to the CWSP are itemized and described in the Table. The column shown as “General Plan Goals and Policies” contains in many cases only partial quotes from relevant sections of the General Plan. The Table is organized sequentially with the City’s General Plan document and formatted by section and subject heading. Since the Riverbank General Plan is organized in a more “narrative” format, the Table is similarly structured showing General Plan page numbers to facilitate location of the various items being addressed.

Riverbank General Plan Goals	General Plan Page	General Plan Compatible	Compatibility Analysis
Goal LAND-1: Managed Urban Growth that Benefits the Entire Community	LAND-15	Yes	The CWSP includes Mixed Use Commercial Retail Land Uses, Neighborhood and Community Parks. Most notably, the CWSP includes the expansion of the City’s existing Regional Sports Park. In addition, the CWSP includes potential future school sites and a future fire station. These land uses will benefit the entire community.
Goal LAND-2: Balanced and Diverse Uses of Land ...Approved specific plans shall include a variety of land uses...Approved specific plans that include proposed parks, schools, and other civic uses.	LAND-15	Yes	The CWSP includes a mixture of land uses consistent with those identified in the City of Riverbank General Plan, and will provide residential and commercial development at intensities projected by the General Plan. The CWSP includes 261 acres of residential land uses, including low, medium and high density residential. In addition, 59 acres of commercial mixed use is included in the Plan, which allows for up to .35 Floor Area Ratio and 25 dwelling units per acre. Parks and schools are planned for at levels necessary to serve the projected residential development. Parks contemplated in the CWSP includes open space, trails, neighborhood parks and expansion of the regional sports complex – 42 acres. Future school sites in the plan area might include a ten (10) acre Elementary School as well as a twenty (20) acre Middle School.



Appendix A: GP Consistency Matrix

<p>Goal LAND-3: Development Patterns that encourage Alternatives to Vehicular Travel.</p>	<p>LAND-16</p>	<p>Yes</p>	<p>The CWSP includes a bike trails along the Modesto Irrigation District (MID) Main lateral as well as Lateral 6, Pedestrian Crossing of MID Lateral 6 is proposed to access the Mixed Use 1 Land Use area to the south of the plan area. Bike trails are proposed along the planned north-south Collector Road which will connect various neighborhoods and land uses to the new development areas as well as the balance of the City of Riverbank to the north and east.</p>
<p>Goal CIRC-1: Riverbank’s Circulation Network Provides Convenience and Choice among All Modes of Transportation...New growth areas shall provide a fully connected network of smaller roadways that provide many alternative routes.</p>	<p>CIRC-13</p>	<p>Yes</p>	<p>The CWSP provides a hierarchy of roadways from major roadways like Claribel Road and Oakdale Road, to collector roads like Morrill and Crawford. These roadways serve to link each neighborhood through a series minor streets that provide a roadway network which promotes alternative transportation methods such as buses, biking and walking. In addition, the Plan includes a new north-south collector in the heart of the project that will connect the various land uses within the Plan Area to Claribel Road to the South.</p>
<p>Goal CIRC-2: The City’s Urban Development Pattern Supports All Locally Available Modes of Transportation...New growth areas will provide an appropriate balance of higher-activity land uses, such as schools, parks, retail and commercial services, small offices, civic uses, apartments, in accessible neighborhood centers.</p>	<p>CIRC-15</p>	<p>Yes</p>	<p>The CWSP includes a mixture of land uses including residential (low, medium and high density), commercial mixed use, and parks/open space. These land uses are balanced throughout the project area, with commercial and higher density land uses located adjacent to arterial and collector streets.</p>
<p>Goal CIRC-3: Increase the Availability and Use of Transit...accommodate transit facilities consistent with transit agency planning.</p>	<p>CIRC-16</p>	<p>Yes</p>	<p>The CWSP provides bus turnout locations along Oakdale Road as well as along the new north-south collector.</p>



Appendix A: GP Consistency Matrix

<p>Goal DESIGN-1: Street and circulation patterns that encourage walking, bicycling, transit use, and reduce traffic congestion...new growth areas shall arrange streets in an interconnected block pattern...ensure frequent street and trail connections between new residential developments and established neighborhoods.</p>	DES-3	Yes	<p>The proposed circulation system includes arterial streets, collector streets, local residential streets and cul-de-sacs and roundabouts. Arterial and collector streets include Complete Streets principles, allowing for all modes of transportation. The proposed roadways interconnect with the proposed land uses, such as residential to commercial, commercial and residential to parks/open space.</p>
<p>Goal DESIGN-2: Amenities and Features along Neighborhood Residential and Commercial Streets that Accommodate All Travel Modes...visually attractive streetscapes with street trees and sidewalks on both sides of street.</p>	DES-5	Yes	<p>Arterial and Collector roadways will be constructed with separated sidewalk and landscape strips to allow for street trees. Internal residential streets will provide for trees behind sidewalks in front yards. Street trees will be consistent with the street tree palette and theme presented in the CWSP. The CWSP also includes neighborhood entry signs and an opportunity for street furniture and public art, further enhancing the design of the Plan area.</p>
<p>Goal DESIGN-3: Neighborhoods are Oriented to the Pedestrian and Foster a Sense of Community...discourage residential development that backs up to parks or other open space.</p>	DES-7	Yes	<p>The Linear Park Basin (20 acres) will be located on the western edge of the CWSP area and will feature passive recreational open space, volley ball court, shaded picnic areas, and possible 9-hole disc golf course. In addition to the linear park basin, two (2) 4.5 acre neighborhood parks could be planned within the low density residential land use areas to accommodate the planned population. These recreation areas will be open to the surrounding development. The CWSP includes the opportunity for an eleven (11) acre expansion of the City's Regional Sports Complex which further provides recreational opportunities to the entire City of Riverbank.</p>



Appendix A: GP Consistency Matrix

<p>Goal DESIGN-4: High Quality Residential Site Design and Architecture...provide diversity among dwelling units in the use of color, building materials, floor plan layouts, square footages, and roof-lines.</p>	DES-11	Yes	<p>The CWSP provides residential architectural design guidelines intended to provide the diversity among units, but these guidelines are also flexible enough for the City to consider other architectural styles. The CWSP includes five (5) main architecture styles: Craftsman, Traditional (American), Italianate, Spanish Colonial, and French. In addition, home builders/architects are encouraged to build upon the architectural styles suggested by the Specific Plan or offer differing architectural styles such as California Modern, Western Regional Farmhouse, Savannah and other styles.</p>
<p>Goal DESIGN-5: Surveillance and Visibility of Public and Semi-Public Places to Improve Safety and Encourage a Sense of Community Ownership...encourage the use of porches, stoops, and other elements that provide a place to comfortably linger and thereby provide “eyes on the street.”</p>	DES-12	Yes	<p>As shown in Chapter 8: Design Guidelines, the CWSP includes a variety of architecture styles which include the use of porches and stoops that provide surveillance on the street and improve safety by providing “eyes on the street.”</p>
<p>Goal DESIGN-6: Multi-Family Architecture and Site Design Reflects Positive Features Consistent with Single-Family Homes...encourage multi-family housing to incorporate building forms and architectural features common to adjoining adjacent single-family houses.</p>	DES-12	Yes	<p>High density residential is characterized by compact, multi-family development and includes architecture forms such as traditional apartments, townhomes and condominiums. Three (3) areas containing 15.5 acres are planned for multi-family residential development. These areas are adjacent to planned Mixed Use Development along Oakdale Road; the City’s Regional Sports Complex and has access to other areas of the specific plan via transit and bicycle/PED. Various architecture styles and features are encouraged in the medium and high density residential areas that will incorporate architectural elements of single-family homes</p>



Appendix A: GP Consistency Matrix

<p>Goal DESIGN-10: New and Existing Commercial Areas, Mixed-Use Areas, and Neighborhood Centers Accommodate Pedestrians, Bicyclists, Transit Users, and Motor Vehicles</p>	DES-17	Yes	<p>The CWSP include two (2) areas designated for Mixed Use and Commercial development that include design elements that promote elevations oriented onto major street frontages that provide pedestrian accessibility and exposure while also providing efficient and safe access for vehicular and bicycle traffic.</p>
<p>Goal DESIGN-11: Urban Design of Commercial Projects and Neighborhood Centers Enhance the Character of the City</p>	DES-20	Yes	<p>The CWSP includes commercial design guidelines to make sure commercial developments are of the highest quality, and in keeping with the existing high quality Crossroads Shopping Center lying immediately east of the CWSP site. The CWSP includes design guidelines for Mixed Use Commercial that promotes pedestrian and vehicular visibility and architectural themes consistent with the existing Crossroad Shopping Center to the east.</p>
<p>Goal Design-15: Adequate, Safe, Well-Located Public Open Spaces, Parks, Facilities, and Access to Features of the Natural Environment</p>	DES-24	Yes	<p>The CWSP provides park and dual use park basins adjacent to collector streets. This form of design promotes a high level of visibility for these open space areas. As shown in Figure 5 of the CWSP, a dual Park/Basin is proposed along the north-south collector road in the heart of the project. The City of Riverbank is also encouraged about the opportunity to expand the Regional Sports Complex Morrill Road and possibly add future neighborhood parks (4.5 acres each) within the residential neighborhoods.</p>
<p>Goal DESIGN-17: Environmental Sensitivity and Low-Impact Development Principles in the Design and Construction of All Projects</p>	DES-25	Yes	<p>Low Impact Development (LID) Principles are mandatory and will be incorporated into project drainage facilities. The CWSP will utilize LID guidelines and specifications throughout the plan to ensure water quality management and recharge of groundwater resources. The CWSP area is proposing a standalone drainage system that will filter, percolate and in some cases detain all</p>



Appendix A: GP Consistency Matrix

			storm water runoff for ultimate discharge into one of two adjacent irrigation laterals. For instance, the expansion of the City’s Regional Sports Complex includes a dual-use facility that will detain storm water runoff from the project area as well as the expanded regional park. In addition, the CWSP includes dual-use linear park basin along the new north-south Collector Road that will detain storm water runoff.
Goal DESIGN-18: Renewable Resource Use and Energy Efficiency In Site and Architectural Design	DES-26	Yes	Development in the CWSP will be required to comply with the current version of the California Building Code (CBC) that identifies requirements for Energy Efficiency, use of recyclable materials and the like. In addition, development is required to adhere to the California Green Building Standards Code (CALGreen), also known as Title 24.
Goal DESIGN-19: Water Quality is Protected Throughout the Development Process and Occupation of the Site...Promote the use of rain gardens, open ditches or swales, and pervious driveways and parking areas in site design to maximize infiltration of storm water.	DES-27	Yes	The CWSP includes Low Impact Development (LID) Standards that promote storm water solutions to capture and filter all storm water runoff as a result of the development of the CWSP area. Dual-use facilities, such as the linear park along the north-south collector street will capture and filter storm water runoff prior to discharge into the MID irrigation laterals.
Goal ED-1: Continue to make economic development a priority in Riverbank	ED-5	Yes	The CWSP includes two (2) Mixed Use Commercial areas that will promote regional and neighborhood commercial opportunities. The Mixed Use 1 area is located on the corner of Claribel Road and Oakdale Road and will expand on regional and community commercial oriented uses similar to the existing Crossroad Shopping Center. The Mixed Use 2 will provide for neighborhood oriented commercial activities in combination with high density residential (multi-family) opportunities.



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<p>Goal ED-2 Strengthen Riverbank’s economic base</p>	<p>ED-5</p>	<p>Yes</p>	<p>The CWSP provides opportunities for retail and small scale office development. The Mixed Use 1 area provides for regional and community commercial oriented uses including office, retail, and restaurant uses. The Mixed Use area provides for neighborhood commercial oriented uses, such as small-scale retail, boutique stores, etc. The two (2) Mixed Use commercial areas will strengthen Riverbank’s economic base and provide opportunities for additional retail and sales tax generating uses.</p>
<p>Goal ED-5: Proactively create and maintain a positive business climate</p>	<p>ED-8</p>	<p>Yes</p>	<p>As discussed above, the CWSP includes two (2) Mixed Use areas that allow for commercial and high density residential land uses. The goal is to continue the retail climate that has been created by the existing Crossroads Shopping Center.</p>
<p>Goal ED-7: Continue to increase Riverbank’s base of regional commercial uses, while addressing market opportunities with locally-oriented commercial uses.</p>	<p>ED-10</p>	<p>Yes</p>	<p>The CWSP provides opportunities for regional and community serving retail uses, while the Highway 108 corridor and the Downtown Specific Plan continue to provide opportunities for unique and local-serving commercial development.</p>
<p>Goal ED-9: Ensure that development patterns can be feasibly sustained when accounting for the fiscal benefits and costs associated with different land uses</p>	<p>ED-11</p>	<p>Yes</p>	<p>The CWSP, through the provision for future retail development, and participation in maintenance CFD’s to cover ongoing costs associated with landscaping, lighting, roads; police services, storm water maintenance, will assure the project is sustainable.</p>
<p>Goal ED-10: Plan for a diversity of housing that will make the community attractive to a variety of workers</p>	<p>ED-11</p>	<p>Yes</p>	<p>The CWSP includes a variety of land uses and densities, including low-, medium- and high density residential as well as two (2) mixed use areas that allow for high density residential. Low, Medium and High Density Residential will provide a wide opportunity for housing at various price points and rents.</p>



Appendix A: GP Consistency Matrix

<p>Goal CONS-3: Support the Practice of Agricultural and the Resources Associated with Farming in the Riverbank Planning Area and Beyond</p>	<p>CONS-5</p>	<p>Yes</p>	<p>As part of this goal, Policy CONS-3.2 states that “ongoing agricultural practices on fertile lands in the western portion of the Riverbank Planning Area shall be protected from encroachment of urban use through the use of buffers.” The CWSP includes a dual use park basin along a portion of the western boundary of the plan area that serves as an interim agricultural buffer between the CWSP and ongoing agricultural practices to the west. The General Plan suggests a permanent agricultural buffer west of Coffee Road.</p>
<p>Goal CONS-6: Maintain or Increase Surface and Groundwater Quality and Supply</p>	<p>CONS-8</p>	<p>Yes</p>	<p>The CWSP includes areas for stormwater retention and groundwater infiltration through use of stormwater basins and collection at two (2) key locations: the Linear Park Basin (20 acres) and the expansion of the City’s Regional Sports Park. These areas will include basin areas that are designed to capture storm water runoff. The CWSP will feature Low Impact Development (LID) standards that include swales and storm water capture and filtration systems at each building site.</p>
<p>Goal CONS-8: Minimize the Use of Energy through Sustainable Development Patterns, Construction Practices, and Construction Materials</p>	<p>CONS-9</p>	<p>Yes</p>	<p>Development within the CWSP will be required to adhere to the California Building Code (CBC) and CALGreen regulations related to the use of energy.</p>
<p>Goal SAFE-2: Provide Adequate Access for Emergency Response</p>	<p>SAFE-4</p>	<p>Yes</p>	<p>The CWSP roadway system provides looped internal access to all project areas. The CWSP includes a location for a future Fire Station for the Stanislaus Consolidated Fire Protection District (SCFPD) at a key location defined by emergency response mapping provided by SCFPD.</p>



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<p>Goal NOISE-1: Create Land Use Patterns and Transportation Networks that Minimize Noise Problems</p>	<p>NOISE-3</p>	<p>Yes</p>	<p>The Program EIR prepared for the CWSP has evaluated land uses and noise impacts. Along major noise producing roadways, such as Oakdale and Claribel, residential uses will be buffered from noise by way of sound walls. Internal to the project, noise issues are treated through typical design methods, unless modified by mitigation measures suggested Mitigation Monitoring and Reporting Program (MMRP) of the Program EIR.</p>
<p>Goal NOISE-2: Minimize Noise Impacts Associated with Development Projects and other Land Use Change</p>	<p>NOISE-4</p>	<p>Yes</p>	<p>Development within the CWSP will be required to adhere to the City's Noise Standards (Chapter 93 of the Riverbank Municipal Code) and the Mitigation Monitoring and Reporting Program (MMRP) of the Program EIR.</p>
<p>Goal PUBLIC-1: Public Service and Infrastructure Provision to Meet or Exceed Level of Service Standards Consistent with other Community Goals</p>	<p>PUBLIC-2</p>	<p>Yes</p>	<p>The CWSP includes provisions and analysis of the existing and proposed infrastructure for the CWSP area. Domestic water will be provided to Crossroads West through the installation of water mains in the proposed arterial and collector roadways as well as a new water well and water storage tank. Sanitary sewer will be provided via new sewer main lines and an extension of the 18" trunk line in Crawford Road. Stormwater will be provided via LID development standards and include a standalone storm drain system that will filter, percolate and in some cases detain all stormwater runoff for ultimate discharge into one (1) of two (2) adjacent MID irrigation laterals.</p>
<p>Goal PUBLIC-2: Adequate Supply of Quality Water to Serve Existing and Future Projected Development Needs</p>	<p>PUBLIC-3</p>	<p>Yes</p>	<p>As discussed in the response to Goal CONS-6, the Water Supply Assessment prepared for the project shows that an adequate supply of water exists to serve the project site. Water infrastructure, including wells, tanks and water lines will be constructed as development occurs and will be consistent with the City's Water Master Plan and future water modeling projections.</p>



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<p>Goal PUBLIC-3: Adequate Wastewater Service to Meet Existing and Future Projected Development Determined in the General Plan</p>	<p>PUBLIC-4</p>	<p>Yes</p>	<p>It has been determined, through the analysis in the Program EIR and by evaluation of the City's Wastewater Master Plan, that adequate wastewater treatment capacity exists to serve the entire project site.</p>
<p>Goal PUBLIC-4: Storm Drainage Systems that Protect Public Safety, Preserve Natural Resources, and Prevent Erosion and Flood Potential</p>	<p>PUBLIC-4</p>	<p>Yes</p>	<p>The CWSP handles all of its on-site storm water runoff through internal collection infiltration and disposal systems. Storm drainage infrastructure, including swales, basins, percolation fields with storm water outlets to adjacent MID facilities.</p>
<p>Goal PUBLIC-7: Fire Protection Services, Staffing, and Deployment Adequate to Serve the Needs of Existing and Planned Development</p>	<p>PUBLIC-7</p>	<p>Yes</p>	<p>Development in the CWSP will pay all adopted fire fees and service fees. The CWSP also provides a location for a future Fire Station along Oakdale Road according to emergency fire response mapping conducted by the Stanislaus Consolidated Fire Protection District.</p>
<p>Goal PUBLIC-8: Police Enforcement Services, Staffing and Deployment Adequate to Serve the Needs of Existing and Planned Development</p>	<p>PUBLIC-8</p>	<p>Yes</p>	<p>Development will be required to pay Impact Fees and participate in a Community Facilities District for on-going police services.</p>
<p>Goal PUBLIC-9: School Facilities that Serve Existing and Future Needs and Complement Our Neighborhoods</p>	<p>PUBLIC-8</p>	<p>Yes</p>	<p>The CWSP includes possible locations for future school sites, centrally located within the project pursuant to the busing criteria established by Sylvan Union School District. It will be up to the school district to negotiate with the landowners for site acquisition. It is anticipated that these areas could be mapped residentially if the District chooses not to proceed with site purchase. The CWSP will mitigate its impact on school facilities by paying adopted school fees. As development occurs, additional school sites may need to be secured to construct future school sites.</p>



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<p>Goal PUBLIC-11: Develop a Diversifies Park System in a Variety of Scales and Environments to Meet Existing and Future Needs</p>	<p>PUBLIC-9</p>	<p>Yes</p>	<p>The CWSP is designed to meet its park requirements, consistent with the City General Plan, and includes community and neighborhood park locations as well as a dual use park basin facility. The CWSP includes a dual use linear park/basin adjacent to the new north-south collector street, the expansion of the City’s Regional Sports Part along Morrill Road and neighborhood oriented parks within the Low Density Residential areas. These will provide a variety of parks and open space opportunities for future residents to utilize.</p>
<p>Goal PUBLIC-15: Encourage Participation by Youth in a Variety of Community Service and Public Policy Activities</p>	<p>PUBLIC-12</p>	<p>Yes</p>	<p>The development of CWSP includes the expansion of the existing Riverbank Regional Sports Park and will include opportunities for expansion of youth sports and community activities.</p>
<p>Goal PUBLIC-19: Work with the Riverbank Unified and Sylvan School Districts and Private School Operators to Provide for Public Schools and Educational Facilities that Serve as Neighborhood Focal Points and Maintain a Quality Learning and Recreational Environment</p>	<p>PUBLIC-13</p>	<p>Yes</p>	<p>The CWSP includes two (2) potential locations for schools. Ongoing discussions and coordination with the appropriate Sylvan Union School District will ensure that adequate educational facilities will developed as part of this development.</p>
<p>Goal PUBLIC-20: Plan for the Costs of New School Facilities when Planning for Specific New Residential Development</p>	<p>PUBLIC-13</p>	<p>Yes</p>	<p>Financing of new school sites will be provided as development occurs through the payment of School Impact Fees in compliance with State Law (SB50/AB1575). Two (2) possible school sites are identified within the CWSP.</p>
<p>Goal PUBLIC-21: Construct New Public Schools to Meet the Needs of Residential Growth</p>	<p>PUBLIC-15</p>	<p>Yes</p>	<p>As discussed above, there are two (2) potential locations for schools within the CWSP. As the CWSP develops, the financing and construction of school sites will occur. Based on generation rates, the CWSP is projected to generate about 492 K-5 students, about 310 6-8 students and about 323 9-12 students.</p>



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<p>Goal AIR-1: Create and Enhance Development Patterns that Encourage People to Walk, Bicycle, or Use Public Transit for a significant Number of Their Daily Trips</p>	<p>AIR-3</p>	<p>Yes</p>	<p>The CWSP includes a variety of land uses and densities. The circulation system includes Complete Streets principles to allow for the movement of all modes of transportation in an efficient and safe manner. Commercial development at the two (2) Mixed Use sites will incorporate design elements to enhance pedestrian, bicycle and vehicular visibility and movement.</p>
<p>Goal AIR-2: Construction Practices and Materials Used in Riverbank Minimize Direct and Indirect Air Pollutant Emissions</p>	<p>AIR-6</p>	<p>Yes</p>	<p>The CWSP will be consistent with the San Joaquin Valley Air Pollution Control District Rules and Regulations and the MMRP of the Program EIR related to Air Quality and Greenhouse Gas Emissions.</p>
<p>Goal AIR-3: Avoid Land Use Incompatibility that Causes Local Exposure to Harmful and Hazardous Air Pollutants</p>	<p>AIR-6</p>	<p>Yes</p>	<p>The CWSP land uses are not those land uses typically considered as causing local exposure to harmful or hazardous materials. Existing laws related to these items, and any mitigation measures identified in the Program EIR will be applied to the project.</p>



APPENDIX B

Mitigation Monitoring and Reporting Program (MMRP)



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FINAL MITIGATION MONITORING AND REPORTING PROGRAM

4.0

This document is the Final Mitigation Monitoring and Reporting Program (FMMRP) for the Crossroads West Specific Plan Project (Project). This FMMRP has been prepared pursuant to Section 21081.6 of the California Public Resources Code, which requires public agencies to “adopt a reporting and monitoring program for the changes made to the project or conditions of project approval, adopted in order to mitigate or avoid significant effects on the environment.” A FMMRP is required for the proposed Project because the EIR has identified significant adverse impacts, and measures have been identified to mitigate those impacts.

The numbering of the individual mitigation measures follows the numbering sequence as found in the Draft EIR, some of which were revised after the Draft EIR were prepared. These revisions are shown in Chapter 3.0 of the Final EIR. All revisions to mitigation measures that were necessary as a result of responding to public comments and incorporating staff-initiated revisions have been incorporated into this FMMRP.

4.1 MITIGATION MONITORING AND REPORTING PROGRAM

The FMMRP, as outlined in the following table, describes mitigation timing, monitoring responsibilities, and compliance verification responsibility for all mitigation measures identified in this Final EIR.

The City of Riverbank will be the primary agency responsible for implementing the mitigation measures and will continue to monitor mitigation measures that are required to be implemented during the operation of the Project.

The FMMRP is presented in tabular form on the following pages. The components of the FMMRP are described briefly below:

- **Mitigation Measures:** The mitigation measures are taken from the Draft EIR in the same order that they appear in that document.
- **Mitigation Timing:** Identifies at which stage of the Project mitigation must be completed.
- **Monitoring Responsibility:** Identifies the agency that is responsible for mitigation monitoring.
- **Compliance Verification:** This is a space that is available for the monitor to date and initial when the monitoring or mitigation implementation took place.



4.0 FINAL MITIGATION MONITORING AND REPORTING PROGRAM

TABLE 4.0-1: MITIGATION MONITORING AND REPORTING PROGRAM

ENVIRONMENTAL IMPACT	MITIGATION MEASURE	MONITORING RESPONSIBILITY	TIMING	VERIFICATION (DATE/INITIALS)
AESTHETICS AND VISUAL RESOURCES				
Impact 3.1-3: Project implementation may result in light and glare impacts.	Mitigation Measure 3.1-1: A lighting plan shall be prepared for each phase of development. The lighting plan shall demonstrate that the lighting systems and other exterior lighting throughout the phase of development has been designed to minimize light spillage onto adjacent properties to the greatest extent feasible. Use of LED lighting or other proven energy efficient lighting shall be required for facilities to be dedicated to the City of Riverbank for maintenance.	City of Riverbank Development Services Department	Prior to the approval of the Site Plan review for each phase	
AGRICULTURAL RESOURCES				
Impact 3.2-1: The proposed Project has the potential to result in the conversion of Farmlands, including Prime Farmland, Unique Farmland, and Farmland of Statewide Importance, as shown on the maps prepared pursuant to the Farmland Mapping and Monitoring Program of the California Resources Agency, to non-agricultural uses.	Mitigation Measure 3.2-1: Prior to the issuance of grading permits, building permits, or final map approval on the subject residential property, the Project applicant shall secure permanent protection of offsite farmland based on a 1:1 ratio to the amount of gross Farmland converted as a result of Project development, consistent with the requirements of the City's Sustainable Agricultural Strategy. The acreage requiring agricultural mitigation shall be equal to the portion of the project site dedicated to residential uses which would be subject to the discretionary development entitlement and lands designated as Prime Farmland, Farmland of Statewide Importance, or Unique Farmland. Permanent preservation shall consist of the purchase of agricultural conservation easements granted in perpetuity from willing seller(s), enforceable deed restrictions, purchase of banked mitigation credits, or other conservation mechanisms acceptable to the City. Land set aside for permanent preservation shall: (1) be of equal or better soil quality, have a dependable and sustainable supply of irrigation water, and be located within Stanislaus County; and (2) not be previously encumbered by a conservation easement of any nature. The permanent protection of farmland shall be accomplished by either: (1) the landowner/developer working directly with an established farmland trust or similar organization, such as the Central Valley Farmland Trust, and providing certification satisfactory to the City that such lands have been permanently preserved at the specified ratio; or (2) it is the City's intent to work with a qualified land trust or similar organization, such as the Central Valley Farmland Trust, to establish a fee for agricultural land conservation easements.	City of Riverbank Development Services Department	Prior to the issuance of grading permits, building permits, or final map approval on the subject residential property	



FINAL MITIGATION MONITORING AND REPORTING PROGRAM

4.0

ENVIRONMENTAL IMPACT	MITIGATION MEASURE	MONITORING RESPONSIBILITY	TIMING	VERIFICATION (DATE/INITIALS)
<p>Impact 3.2-3: The proposed Project has the potential to result in conflicts with adjacent agricultural lands or indirectly cause conversion of agricultural lands.</p>	<p>Mitigation Measure 3.2-2: Prior to the conversion of agricultural lands in the Plan Area, the Project applicant shall participate in the Stanislaus LAFCo's Agricultural Preservation Policy (as amended on March 25, 2015), consistent with the City's Sustainable Agricultural Strategy. The Project applicant shall prepare a "Plan for Agricultural Preservation", which shall include information such as the Project's direct and indirect impacts to agricultural resources, the availability of other lands in the City of Riverbank's existing boundaries, and relevant General Plan policies. The Plan shall also specify the method or strategy proposed to minimize the loss of agricultural lands. The information provided in the Plan shall be consistent with the environmental documentation prepared by the City.</p> <p>Mitigation Measure 3.2-3: Prior to approval of any Final Maps, "Right to Farm" language shall be presented to the City for approval and recordation against the affected property. The proposed language shall contain the following statement: "All persons purchasing lots within the boundaries of this approved map should be prepared to accept the inconveniences associated with agricultural operations, such as noise, odors, flies, dust or fumes. Stanislaus County has determined that such inconveniences shall not be considered to be a nuisance if agricultural operations are consistent with accepted customs and standards."</p>	<p>Stanislaus LAFCo</p>	<p>Prior to the conversion of agricultural lands in the Plan Area</p>	
<p>AIR QUALITY</p> <p>Impact 3.3-1: Project operation has the potential to conflict with or obstruct implementation of an applicable air quality plan, cause a violation of an air quality standard, or contribute substantially to an existing or projected air quality violation.</p>	<p>Mitigation Measure 3.3-1: The Project proponent shall submit an Air Impact Assessment (AIA) application to the San Joaquin Valley Air Pollution Control District in accordance with District Rule 9510 Indirect Source Review (ISR) to obtain AIA approval from the District for the phase or Project component that is to be constructed. Prior to the issuance of a building permit of each individual phase or Project component, the Project proponent shall incorporate mitigation measures into the proposed Project and demonstrate compliance with District Rule 9510 including payment of all fees.</p> <p>Mitigation Measure 3.3-2: Prior to the approval of improvement plans, the Project proponent shall incorporate measures that reduce vehicle emissions. The measures will be implemented through project design, conditions of approval, noticing and disclosure statements, or through the City's plan check and inspection process. This mitigation measures is intended to ensure that the best available and practical approaches are used to reduce operational emissions. Appropriate measures shall be selected by the City in</p>	<p>City of Riverbank Development Services Department</p>	<p>Prior to approval of any Final Maps</p>	
		<p>San Joaquin Valley Air Pollution Control District</p>	<p>Prior to final approval of improvement plans for each phase</p>	
		<p>City of Riverbank Development Services Department</p>	<p>Prior to the approval of improvement plans for each phase</p>	



4.0 FINAL MITIGATION MONITORING AND REPORTING PROGRAM

ENVIRONMENTAL IMPACT	MITIGATION MEASURE	MONITORING RESPONSIBILITY	TIMING	VERIFICATION (DATE/INITIALS)
	<p>consultation with SJVAPCD, and shall include, at a minimum, the following features into the applicable Project plans (e.g. site, engineering, landscaping, etc.):</p> <ul style="list-style-type: none"> • Provide bus turnouts and transit improvements where requested by the San Joaquin RTD. • Design streets and trails to maximize pedestrian and bicycle connectivity, safety, and access to transit lines, including pedestrian and bicycle signalization, signage and safety designs at signalized intersections. • Provide traffic calming measures on all streets and intersections. Traffic calming features may include marked crosswalks, count-down signal timers, curb extensions, speed tables, raised crosswalks, raised intersections, median islands, tight corner radii, narrow roadways, traffic circles, on-street parking, planter strips with streets trees, chicanes/chokers, or other improvements designed to reduce motor vehicle speeds and encourage pedestrian and bicycle trips. • Provide street lighting along internal roadways and bike lanes/paths, sidewalks. • Provide vanpool parking only spaces and preferential parking for carpools to accommodate carpools and vanpools in employment areas. • Provide bicycle parking areas near the entrance of commercial establishments. • Provide pedestrian signalization, signage and safety designs at signalized intersections. • Require shade trees to shade sidewalks in street-side landscaping areas. <p>Mitigation Measure 3.3-3: Prior the approval of improvement plans, the Project proponent shall prepare and implement, and/or require the implementation of, high-efficiency lighting throughout all portions of the Plan Area (for example: metal halide post top lights, or LEDs, as opposed to</p>	City of Riverbank Development Services	Prior to the approval of improvement	





FINAL MITIGATION MONITORING AND REPORTING PROGRAM

4.0

Appendix B: Mitigation Monitoring and Reporting Program (MMRP)

ENVIRONMENTAL IMPACT	MITIGATION MEASURE	MONITORING RESPONSIBILITY	TIMING	VERIFICATION (DATE/INITIALS)
<p>Impact 3.3-2: Project construction has the potential to cause a violation of an air quality standard or contribute substantially to an existing or projected air quality violation.</p>	<p>typical mercury cobrahead lights).</p> <p>Mitigation Measure 3.3-4: Prior to the approval of improvement plans, the Project proponent shall prepare and implement, and the City shall require the implementation of, the following additional mitigation measures:</p> <ul style="list-style-type: none"> • Use low-VOC paint (indoor and outdoor, for both residential and non-residential uses). • Use only natural gas hearths (or no hearths). • Apply a Water Conservation Strategy to achieve reductions in outdoor water usage through installation of water-efficient irrigation systems, and landscaping with native and drought-tolerant plants that also reduce the need for gas-powered landscape maintenance equipment. • Require all flat roofs on non-residential structures to have a white or silver cap sheet to reduce energy demand. • Install low flow bathroom faucets. • Install low-flow kitchen faucets. • Install low-flow toilets. • Install low-flow showers. • Use water-efficient irrigation systems. <p>Mitigation Measure 3.3-5: To reduce construction-related emissions, the following measures shall be implemented:</p> <ul style="list-style-type: none"> • Prior to year 2025, construction contracts for development in the Plan Area shall specify use of off-road construction equipment that achieves fleet average emissions equal to or less than the Tier III emissions standard of 4.8 NOx grams per horsepower-hour (g/hp-hr). The fleet average can be achieved through any combination of uncontrolled engines complying with Tier III and above engine standards. Beginning in 2025, construction contracts for development in the Plan Area shall specify use of off-road construction equipment that achieves fleet average emissions equal 	<p>Department</p> <p>City of Riverbank Development Services Department</p>	<p>plans</p> <p>Prior to the approval of improvement plans</p>	
<p>Impact 3.3-2: Project construction has the potential to cause a violation of an air quality standard or contribute substantially to an existing or projected air quality violation.</p>	<p>Mitigation Measure 3.3-5: To reduce construction-related emissions, the following measures shall be implemented:</p> <ul style="list-style-type: none"> • Prior to year 2025, construction contracts for development in the Plan Area shall specify use of off-road construction equipment that achieves fleet average emissions equal to or less than the Tier III emissions standard of 4.8 NOx grams per horsepower-hour (g/hp-hr). The fleet average can be achieved through any combination of uncontrolled engines complying with Tier III and above engine standards. Beginning in 2025, construction contracts for development in the Plan Area shall specify use of off-road construction equipment that achieves fleet average emissions equal 	<p>City of Riverbank Development Services Department</p> <p>San Joaquin Valley Air Pollution Control District</p>	<p>Prior to issuance of a grading or building permit</p>	

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ENVIRONMENTAL IMPACT	MITIGATION MEASURE	MONITORING RESPONSIBILITY	TIMING	VERIFICATION (DATE/INITIALS)
	<p>to or less than the Tier IV emissions standards of NOx g/hp-hr. The fleet average can be achieved through any combination of controlled engines complying with Tier IV and above engine standards.</p> <ul style="list-style-type: none"> • Prior to issuance of a grading or building permit, the project applicant shall submit a Fugitive Dust Control Plan to SJVAPCD for review and approval. The Fugitive Dust Control Plan shall reduce emissions, during construction of PM₁₀ and PM_{2.5} and shall include the following: <ul style="list-style-type: none"> ○ Names, addresses and phone numbers of persons responsible for the preparation, submission and implementation of the plan. ○ Description and location of operations. ○ Listing of all fugitive dust emissions sources included in the operation. ○ The following dust control measures shall be implemented: <ul style="list-style-type: none"> ▪ All on-site unpaved roads shall be effectively stabilized using water or chemical stabilizers that can be determined to be as efficient as or more efficient for fugitive dust control than California Air Resources Board approved soil stabilizers, and that shall not increase any other environmental impacts including loss of vegetation. ▪ All material excavated or graded will be sufficiently watered to prevent excessive dust. Watering will occur as needed with complete coverage of disturbed areas. The excavated soil piles will be watered as needed to limit dust emissions to less than 20 percent opacity or covered with temporary coverings. ▪ Construction activities that occur on unpaved surfaces will be discontinued during windy conditions when winds exceed 25 miles per hour and those activities cause visible dust plumes. 			





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ENVIRONMENTAL IMPACT	MITIGATION MEASURE	MONITORING RESPONSIBILITY	TIMING	VERIFICATION (DATE/INITIALS)
	<p>Construction activities may continue if dust suppression measures are used to minimize visible dust plumes.</p> <ul style="list-style-type: none"> ▪ Track-out debris onto public paved roads shall not extend 50 feet or more from an active operation and track-out shall be removed or isolated such as behind a locked gate at the conclusion of each workday. ▪ All hauling materials should be moist while being loaded into dump trucks. ▪ All haul trucks hauling soil, sand and other loose material on public roads shall be covered (e.g., with tarps or other enclosures that would reduce fugitive dust emissions). ▪ Soil loads shall be kept below 6 inches of the freeboard of the truck. ▪ Drop heights should be minimized when loaders dump soil into trucks. ▪ Gate seals should be tight on dump trucks. ▪ Traffic speeds on unpaved roads shall be limited to a maximum of 15 miles per hour. ▪ All grading activities shall be suspended when visible dust emissions exceed 20 percent. ▪ Other fugitive dust control measures as necessary to comply with SJVAPCD Rules and Regulations. ▪ Disturbed areas should be minimized. 			
BIOLOGICAL RESOURCES				
<p>Impact 3.4-2: The potential to have direct or indirect effects on special-status reptile and</p>	<p>Mitigation Measure 3.4-1: The project proponent shall implement the following measures to avoid or minimize impacts on western pond turtle:</p>	<p>City of Riverbank Development</p>	<p>Prior to commencement of any</p>	

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Appendix B: Mitigation Monitoring and Reporting Program (MMRP)

ENVIRONMENTAL IMPACT	MITIGATION MEASURE	MONITORING RESPONSIBILITY	TIMING	VERIFICATION (DATE/INITIALS)
<p>Impact 3.4-3: The potential to have direct or indirect effects on special-status bird species.</p>	<p>surveyed for giant garter snake. The survey shall be repeated if a lapse in construction activity of 2 weeks or greater has occurred. If a giant garter snake is encountered during construction, activities within 200 feet of the irrigation ditches shall cease until appropriate corrective measures have been completed or it is determined by the qualified biologist and City staff, in coordination with USFWS and CDFW, that the giant garter snake shall not be harmed. Any sightings shall be reported to USFWS and CDFW immediately.</p> <ul style="list-style-type: none"> A biological onsite monitor will be present during initial ground-disturbing activities within 200 feet of any irrigation ditch or potential GGS habitat within the CWSP Plan Area Construction vehicles would require low-speed limits within such sites to lessen the probability that the species could be run over by vehicles and equipment. Any aquatic habitat for the snake that is dewatered shall remain dry for at least 15 consecutive days after April 15 and before excavating or filling of the dewatered habitat. If complete dewatering is not possible, potential snake prey (e.g., fish and tadpoles) will be removed so that snakes and other wildlife are not attracted to the construction area. Giant garter snake aquatic habitat to be avoided (i.e. irrigation ditches) within or adjacent to construction areas will be fenced and designated as environmentally sensitive areas. These areas shall be avoided by all construction personnel. 	<p>Qualified Biologist U.S. Fish and Wildlife Service</p>		
<p>Impact 3.4-3: The potential to have direct or indirect effects on special-status bird species.</p>	<p>Mitigation Measure 3.4-3: The project proponent shall implement the following measure to avoid or minimize impacts on western burrowing owl:</p> <ul style="list-style-type: none"> No less than 14 days before initiating ground disturbance activities, a qualified biologist shall complete an initial take avoidance survey using the recommended methods described in the Detection Surveys section of the March 7, 2012, CDFW Staff Report on Burrowing Owl Mitigation (CDFW 2012). Implementation of avoidance and minimization measures (as presented in the March 7, 2012, CDFW Staff Report on Burrowing Owl Mitigation) would be triggered if the initial take avoidance survey results in positive owl presence in 	<p>City of Riverbank Development Services Department Qualified Biologist California Department of</p>	<p>No less than 14 days before initiating ground disturbance activities</p>	

Appendix B: Mitigation Monitoring and Reporting Program (MMRP)

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ENVIRONMENTAL IMPACT	MITIGATION MEASURE	MONITORING RESPONSIBILITY	TIMING	VERIFICATION (DATE/INITIALS)
	<p>the Plan Area where project activities shall occur. If needed, the development of avoidance and minimization approaches shall be developed in coordination with CDFW.</p> <p>Mitigation Measure 3.4-4: The project proponent shall implement the following measures to avoid or minimize impacts on Swainson's hawk:</p> <ul style="list-style-type: none"> No more than 30 days before the commencement of construction, a qualified biologist shall perform preconstruction surveys for nesting Swainson's hawk and other raptors during the nesting season (February 1 through August 31). Appropriate buffers shall be established and maintained around active nest sites during construction activities to avoid nest failure as a result of project activities. The appropriate size and shape of the buffers shall be determined by a qualified biologist, in coordination with CDFW, and may vary depending on the nest location, nest stage, and construction activity. The buffers may be adjusted if a qualified biologist determines it would not be likely to adversely affect the nest. Monitoring shall be conducted to confirm that project activity is not resulting in detectable adverse effects on nesting birds or their young. No project activity shall commence within the buffer areas until a qualified biologist has determined that the young have fledged or the nest site is otherwise no longer in use. Before the commencement of construction, the project proponent shall provide compensatory mitigation for the permanent loss of Swainson's hawk foraging habitat. Mitigation shall be at the CDFW specified ratios, which are based on distance to nests. The Plan Area's distance to the closest nest currently falls within the range of "within 5 miles of an active nest tree but greater than 1 mile from the nest tree." As such, the Project shall currently be responsible for 0.75 acres of each acre of urban development authorized (0-75:1 ratio). The project proponent shall either provide lands protected through fee title acquisition or conservation easement (acceptable to the CDFW) on agricultural lands or other suitable habitats which provide foraging habitat for Swainson's hawk. <p>Mitigation Measure 3.4-5: The project proponent shall implement the following measure to avoid or minimize impacts on other protected bird</p>	<p>Fish and Wildlife</p> <p>City of Riverbank Development Services Department</p> <p>Qualified Biologist</p> <p>California Department of Fish and Wildlife</p> <p>City of Riverbank Development</p>	<p>No more than 30 days before commencement of construction</p> <p>Prior to ground disturbing</p>	

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ENVIRONMENTAL IMPACT	MITIGATION MEASURE	MONITORING RESPONSIBILITY	TIMING	VERIFICATION (DATE/INITIALS)
<p>Impact 3.4-4: The potential to result in direct or indirect effects on special-status mammal species.</p>	<p>species that may occur on the site:</p> <ul style="list-style-type: none"> • Preconstruction surveys for active nests of special-status birds shall be conducted by a qualified biologist in all areas of suitable habitat within 500 feet of project disturbance. Surveys shall be conducted within 14 days before commencement of any construction activities that occur during the nesting season (February 15 to August 31) in a given area. • If any active nests, or behaviors indicating that active nests are present, are observed, appropriate buffers around the nest sites shall be determined by a qualified biologist to avoid nest failure resulting from project activities. The size of the buffer shall depend on the species, nest location, nest stage, and specific construction activities to be performed while the nest is active. The buffers may be adjusted if a qualified biologist determines it would not be likely to adversely affect the nest. If buffers are adjusted, monitoring will be conducted to confirm that project activity is not resulting in detectable adverse effects on nesting birds or their young. No project activity shall commence within the buffer areas until a qualified biologist has determined that the young have fledged or the nest site is otherwise no longer in use. 	<p>Services Department Qualified Biologist</p>	<p>activities</p>	
<p>Impact 3.4-6: The potential to result in direct or indirect effects on special-status mammal species.</p>	<p>Mitigation Measure 3.4-6: The project proponent shall implement the following measures to avoid or minimize impacts on special-status bats:</p> <ul style="list-style-type: none"> • If removal of suitable roosting areas (i.e. buildings, trees, shrubs, bridges, etc.) must occur during the bat pupping season (April 1 through July 31), surveys for active maternity roosts shall be conducted by a qualified biologist. The surveys shall be conducted from dusk until dark. • If a special-status bat maternity roost is located, appropriate buffers around the roost sites shall be determined by a qualified biologist and implemented to avoid destruction or abandonment of the roost resulting from habitat removal or other project activities. The size of the buffer shall depend on the species, roost location, and specific construction activities to be performed in the vicinity. No project activity shall commence within the buffer areas until the end of the pupping season (August 1) or until a qualified biologist 	<p>City of Riverbank Development Services Department Qualified Biologist</p>	<p>If removal of suitable roosting areas (i.e. buildings, trees, shrubs, bridges, etc.) must occur during the bat pupping season (April 1 through July 31)</p>	



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ENVIRONMENTAL IMPACT	MITIGATION MEASURE	MONITORING RESPONSIBILITY	TIMING	VERIFICATION (DATE/INITIALS)
<p>Impact 3.4-6: The potential to effect protected wetlands and jurisdictional waters.</p>	<p>conforms the maternity roost is no longer active.</p> <p>Mitigation Measure 3.4-7: If construction activities would disturb a ditch/canal/basin within the Plan Area, the property owner/applicant proposing the activity shall verify that the facility qualifies under the agricultural ditch exemption. If the facilities do not qualify for the exemption and are determined to be jurisdictional by the regulatory agencies, any fill activity would require authorization for fill from the regulatory agencies (USACE-404 permit, RWQCB-401 certification, 1600 Streambed Alteration Agreement). All requirements of a permit shall be adhered to throughout the construction phase.</p>	<p>City of Riverbank Development Services Department Qualified Biologist</p>	<p>If construction activities would disturb a ditch / canal / basin within the Plan Area</p>	
CULTURAL AND TRIBAL RESOURCES				
<p>Impact 3.5-1: Project implementation has the potential to cause a substantial adverse change to a significant historical resource, as defined in CEQA Guidelines §15064.5, or a significant tribal cultural resource, as defined in Public Resources Code §21074.</p>	<p>Mitigation Measure 3.5-1: Prior to ground disturbing activities for each phase of the Project that would potentially affect one or more of the listed resources below, the resources shall be evaluated for their potential architectural and/or historic importance by a Qualified Architectural Historian, at the cost of the Project applicant. The potentially historic resources within the Project site include the following:</p> <ul style="list-style-type: none"> • Buildings or building complexes located northwest of the Oakdale Road / Morrill Road intersection, east of the existing Riverbank Sports Complex (on APN 074-006-013); • Buildings or building complexes located southwest of the Oakdale Road / Morrill Road intersection, approximately 0.18 miles south of the Riverbank Sports Complex (on APN 074-011-009); • Buildings or building complexes located northwest of the Oakdale Road / Crawford Road intersection, located along Oakdale Road (on APN 074-011-009); • Buildings or building complexes located southwest of the Oakdale Road / Crawford Road intersection, located 0.14 to 0.27 miles west of Oakdale Road (on APN 074-014-006); and • The MID Lateral No. 6 that crosses the southern portion of the Project site. <p>Work shall not continue at the above-listed site(s) until the Qualified</p>	<p>City of Riverbank Development Services Department Qualified Architectural Historian</p>	<p>Prior to ground disturbing activities for each phase of the Project that would potentially affect one or more of the listed resources</p>	





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ENVIRONMENTAL IMPACT	MITIGATION MEASURE	MONITORING RESPONSIBILITY	TIMING	VERIFICATION (DATE/INITIALS)
	<p>Architectural Historian conducts sufficient research and data collection to determine if the above-listed site(s) is eligible for listing on the NRHP or CRHR; or not a significant Public Trust Resource. Should the site(s) be determined to not be significant or eligible, no further action is required. Should the site(s) be determined to be significant or eligible, the Project applicant shall work with the Registered Professional Historian to develop a cultural resource plan for the site(s).</p> <p>If a building or building complex is determined to be important under the criteria of the California Register of Historical Resources, and the buildings cannot be preserved, then it is recommended that the buildings be documented through the preparation of the DPR 523 forms with large scale "HABS-like" photographs taken. Sets of these photographs shall be placed with the County museum or a suitable archival facility and the Central California Information Center, thereby preserving information on early architecture for future researchers.</p> <p>Mitigation Measure 3.5-2: All construction workers shall receive a cultural resources sensitivity training session before they begin site work in order to identify any potentially significant cultural or similar resources that may result during construction. The sensitivity training session shall be instructed by a professional archaeologist. The sensitivity training shall inform the workers of their responsibility to identify and protect any cultural resources, including prehistoric or historic artifacts, or other indications of archaeological resources, within the Plan Area. The sensitivity training shall cover laws pertaining to cultural resources, examples of cultural resources that may be discovered in the Plan Area, and what to do if a cultural resource, or anything that may be a cultural resource, is discovered.</p> <p>If any cultural resources, including prehistoric or historic artifacts, or other indications of archaeological resources, are found during grading and construction activities during any phase of the Project, all work shall be halted immediately within a 200-foot radius of the discovery until an archaeologist meeting the Secretary of the Interior's Professional Qualifications Standards in prehistoric or historical archaeology, as appropriate, has evaluated the find(s).</p> <p>Work shall not continue at the discovery site until the archaeologist conducts sufficient research and data collection to make a determination that the resource is either 1) a significant find; 2) not cultural in origin; or 3) not potentially significant or eligible for listing on the NRHP or CRHR; or 4)</p>	<p>City of Riverbank Development Services Department Qualified Archaeologist</p>	<p>Prior to ground disturbance</p>	

Appendix B: Mitigation Monitoring and Reporting Program (MMRP)

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ENVIRONMENTAL IMPACT	MITIGATION MEASURE	MONITORING RESPONSIBILITY	TIMING	VERIFICATION (DATE/INITIALS)
<p>Impact 3.5-2: Project implementation has the potential to cause a substantial adverse change to a significant archaeological resource, as defined in CEQA Guidelines §15064.5.</p>	<p>not a significant Public Trust Resource.</p> <p>If a significant finding is made, a plan must be developed for this inadvertent finding. Measures to potentially address a subsurface finding could include one or more of the following depending upon the nature of the find: recordation of the finding; further efforts to define the extent and nature of the resource; preservation in place, and re-design to ensure long-term preservation of the resource; and/or data recovery excavations.</p> <p>If Native American resources are identified, a Native American monitor, following the Guidelines for Monitors/Consultants of Native American Cultural, Religious, and Burial Sites established by the Native American Heritage Commission, may also be required and, if required, shall be retained at the Project applicant's expense.</p>	See Mitigation Measure 3.5-2	See Mitigation Measure 3.5-2	
<p>Impact 3.5-3: Project implementation has the potential to directly or indirectly destroy a unique paleontological resource.</p>	<p>Implement Mitigation Measure 3.5-2.</p> <p>Mitigation Measure 3.5-3: If paleontological resources are discovered during the course of construction during any phase of the Project, work shall be halted immediately within 50 meters (165 feet) of the discovery, the City of Riverbank shall be notified, and a qualified paleontologist shall be retained to determine the significance of the discovery. If the paleontological resource is considered significant, it should be excavated by a qualified paleontologist and given to a local agency, State University, or other applicable institution, where the resource could be curated and displayed for public education purposes.</p>	City of Riverbank Development Services Department Qualified Paleontologist	If paleontological resources are discovered during the course of construction	
<p>Impact 3.5-4: Project implementation has the potential to disturb human remains, including those interred outside of formal cemeteries.</p>	<p>Mitigation Measure 3.5-4: If human remains are discovered during the course of construction during any phase of the Project, work shall be halted at the site and at any nearby area reasonably suspected to overlie adjacent human remains until the Stanislaus County Coroner has been informed and has determined that no investigation of the cause of death is required. If the remains are of Native American origin, either of the following steps will be taken:</p>	City of Riverbank Development Services Department	If human remains are discovered during the course of construction during any phase of the	





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	<ul style="list-style-type: none"> • The Coroner shall contact the Native American Heritage Commission in order to ascertain the proper descendants from the deceased individual. The coroner shall make a recommendation to the landowner or the person responsible for the excavation work, for means of treating or disposing of, with appropriate dignity, the human remains and any associated grave goods, which may include obtaining a qualified archaeologist or team of archaeologists to properly excavate the human remains. • The landowner shall retain a Native American monitor, and an archaeologist, if recommended by the Native American monitor, and rebury the Native American human remains and any associated grave goods, with appropriate dignity, in a location that is not subject to further subsurface disturbance when any of the following conditions occurs: <ul style="list-style-type: none"> ○ The Native American Heritage Commission is unable to identify a descendant. ○ The descendant identified fails to make a recommendation. ○ The City of Riverbank or its authorized representative rejects the recommendation of the descendant, and the mediation by the Native American Heritage Commission fails to provide measures acceptable to the landowner. 	San Joaquin County Coroner	project	
GEOLOGY AND SOILS				
Impact 3.6-2: Implementation and construction of the proposed Project may result in substantial soil erosion or the loss of topsoil.	<p>Mitigation Measure 3.6-1: Prior to clearing, grading, and disturbances to the ground such as stockpiling, or excavation for each phase of the Project, the Project proponent shall submit a Notice of Intent (NOI) and Storm Water Pollution Prevention Plan (SWPPP) to the RWQCB to obtain coverage under the General Permit for Discharges of Storm Water Associated with Construction Activity (Construction General Permit Order 2009-0009-DWQ amended by 2010-0014-DWQ & 2012-0006-DWQ). The SWPPP shall be designed with Best Management Practices (BMPs) that the RWQCB has deemed as effective at reducing erosion, controlling sediment, and managing runoff. These include: covering disturbed areas with mulch, temporary seeding, soil stabilizers, binders, fiber rolls or blankets, temporary vegetation, and permanent seeding. Sediment control BMPs, installing silt fences or</p>	City of Riverbank Development Services Department	Prior to clearing, grading, and disturbances to the ground such as stockpiling, or excavation for each phase of the Project	

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ENVIRONMENTAL IMPACT	MITIGATION MEASURE	MONITORING RESPONSIBILITY	TIMING	VERIFICATION (DATE/INITIALS)
Impact 3.6-3: The proposed Project has the potential to be located on a geologic unit or soil that is unstable, or that would become unstable as a result of Project implementation, and potentially result in landslide, lateral spreading, subsidence, liquefaction or collapse.	<p>placing straw wattles below slopes, installing berms and other temporary run-on and runoff diversions. These BMPs are only examples of what should be considered and should not preclude new or innovative approaches currently available or being developed. Final selection of BMPs will be subject to approval by City of Riverbank and the RWQCB. The SWPPP will be kept on site during construction activity and will be made available upon request to representatives of the RWQCB.</p> <p>Mitigation Measure 3.6-2: Prior to earthmoving activities for each phase of the Project, a certified geotechnical engineer, or equivalent, shall be retained to perform a final geotechnical evaluation of the soils at a design-level as required by the requirements of the California Building Code Title 24, Part 2, Chapter 18, Section 1803.1.1.2 related to expansive soils and other soil conditions. The evaluation shall be prepared in accordance with the standards and requirements outlined in California Building Code, Title 24, Part 2, Chapter 16, Chapter 17, and Chapter 18, which addresses structural design, tests and inspections, and soils and foundation standards. The final geotechnical evaluation shall include design recommendations to ensure that soil conditions do not pose a threat to the health and safety of people or structures, including threats from liquefaction or lateral spreading. The grading and improvement plans, as well as the storm drainage and building plans for each phase of the Project shall be designed in accordance with the recommendations provided in the final geotechnical evaluation.</p>	City of Riverbank Development Services Department Certified Geotechnical Engineer	Prior to earthmoving activities for each phase of the Project	
Impact 3.6-4: Potential for expansive soils to create substantial risks to life or property.	<p>Implement Mitigation Measure 3.6-2.</p>	See Mitigation Measure 3.6-2	See Mitigation Measure 3.6-2	
GREENHOUSE GASES AND CLIMATE CHANGE				
Impact 3.7-1: Potential to generate greenhouse gas emissions, either directly or indirectly, that may have a significant impact on the environment or potential to conflict with an applicable plan, policy, or regulation adopted for the purpose of reducing the	<p>Mitigation Measure 3.7-1: The City shall require GHG reduction measures in connection with tentative subdivision maps submitted for approval, including but not limited to the following:</p> <ul style="list-style-type: none"> • Actions included in Mitigation Measures 3.3-1 through 3.3-5 (see Section 3.3: Air Quality) that also reduce GHG emissions; • Actions that further improve energy efficiency, such as requiring that all buildings exceed Title 24 energy-efficiency requirements by a certain percentage, requiring on-site renewable energy 	City of Riverbank Development Services Department	Prior approval of tentative subdivision maps	

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emissions of greenhouse gases.	<p>production to meet a specified percent of the subdivision's electricity needs, etc.</p> <ul style="list-style-type: none"> • Actions that further reduce vehicle miles traveled, such as providing transit hubs that would be accessible by local and regional transit routes and community multimodal paths and trails; providing general pedestrian connectivity throughout the project, etc. • Payment for GHG offsets, as determined to be feasible by the City. 			
HAZARDS AND HAZARDOUS MATERIALS				
<p>Impact 3.8-1: Project implementation has the potential to create a significant hazard through the routine transport, use, or disposal of hazardous materials or through the reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment.</p>	<p>Implement Mitigation Measure 3.6-1.</p> <p>Mitigation Measure 3.8-1: Prior to the approval of any map, Preliminary Development Plan, or site plan, the City shall review the 2017 Phase I ESA (Geocon Consultants, Inc., July 2017) cited in the Draft EIR for the CWSP to determine if it is still applicable. After July 1, 2020, the City shall require an updated Phase I ESA for the specific property. The Phase I ESA shall evaluate the specific property proposed to be developed, to ensure that no material changes have occurred since preparation of the 2017 Phase I ESA (Geocon Consultants, Inc., July 2017).</p> <p>Mitigation Measure 3.8-2: The applicant shall hire a qualified consultant to perform additional soil and site testing for the areas identified in this EIR to have potential hazardous conditions present prior to any mapping approvals. The following areas have been deemed to have potential hazardous conditions present:</p> <ul style="list-style-type: none"> • The residential units and adjoining structures. • The remnant construction and/or farming materials (i.e. remnant pipes, etc.). • The soils in the area where farming equipment and above ground tanks have been stored, including, but not limited to, the following: <ul style="list-style-type: none"> ○ The parcels associated with the Alexander Dairy (APNs 	<p>See Mitigation Measure 3.6-1</p> <p>City of Riverbank Development Services Department</p> <p>City of Riverbank Development Services Department</p>	<p>See Mitigation Measure 3.6-1</p> <p>Prior to the approval of any map, Preliminary Development Plan, or site plan</p> <p>Prior to any mapping approvals</p>	

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	<p>074-011-009 and 074-014-006).</p> <ul style="list-style-type: none"> ○ The parcels associated with the properties located at 5817 Oakdale Road, 5525 Oakdale Road, and 2054 Crawford Road. ○ The Harrigfeld property located at 1901 Morrill Road. ○ All parcels located south of Morrill Road. <p>The intent of the additional testing is to investigate whether any of the buildings, facilities, or soils in any of the above parcels contain hazardous materials. If asbestos-containing materials and/or lead are found in the buildings, a California Occupational Safety and Health Administration (Cal/OSHA) certified asbestos containing building materials (ACBM) and lead based paint contractor shall be retained to remove the asbestos-containing materials and lead in accordance with EPA and Cal/OSHA standards. In addition, all activities (construction or demolition) in the vicinity of these materials shall comply with Cal/OSHA asbestos and lead worker construction standards. The ACBM and lead shall be disposed of properly at an appropriate offsite disposal facility.</p> <p>Mitigation Measure 3.8-3: If the site investigation required by Mitigation Measure 3.8-2 indicates a probability that hazardous materials may be found on any parcel, the applicant for that parcel shall submit a Phase II ESA, which shall further evaluate on-site conditions. The Phase II ESA shall address the likely presence of hazardous substances and/or petroleum products identified in the previous Phase I ESA (Geocon Consultants, Inc., 2017) prepared for the Plan Area.</p> <p>In addition, due to the past agricultural operations in the Plan Area, a soil sampling program shall be implemented to assess potential agricultural (including pesticides, herbicides, diesel, petrochemicals, etc.) impacts to surface soil within the Plan Area, as follows:</p> <p>A soil sampling and analysis workplan shall be submitted for approval the Stanislaus County Department of Environmental Resources. The sampling and analysis plan shall meet the requirements of the Department of Toxic Substances Control Interim Guidance for Sampling Agricultural Properties (2008), and the County Department of Environmental Resources Recommended Soil and Groundwater Sampling for Underground Tank Investigations (2013). The soils in the area where farming equipment and</p>	<p>City of Riverbank Development Services Department</p>	<p>If the site investigation required by Mitigation Measure 3.8-2 indicates a probability that hazardous materials may be found on any parcel</p>	





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Appendix B: Mitigation Monitoring and Reporting Program (MMRP)

ENVIRONMENTAL IMPACT	MITIGATION MEASURE	MONITORING RESPONSIBILITY	TIMING	VERIFICATION (DATE/INITIALS)
	<p>tanks have been stored, including, but not limited to, the following, should be included in the soil sampling and analysis workplan:</p> <ul style="list-style-type: none"> • The parcels associated with the Alexander Dairy (APNs 074-011-009 and 074-014-006). • The parcels associated with the properties located at 5817 Oakdale Road, 5525 Oakdale Road, and 2054 Crawford Road. • The Harrigfeld property located at 1901 Morrill Road. • All parcels located south of Morrill Road. <p>If the sampling results indicate the presence of agrichemicals that exceed commercial screening levels, a removal action workplan shall be prepared in coordination with Stanislaus County Department of Environmental Resources. The removal action workplan shall include a detailed engineering plan for conducting the removal action, a description of the onsite contamination, the goals to be achieved by the removal action, and any alternative removal options that were considered and rejected and the basis for that rejection. A no further action letter shall be issued by Stanislaus County Department of Environmental Resources upon completion of the removal action. The removal action shall be deemed complete when the confirmation samples exhibit concentrations below the commercial screening levels, which will be established by the agencies.</p> <p>If any stained soil or odor-impacted areas are encountered during the Phase II ESA, then soil sampling of these areas shall be included in the above soil sampling workplan, and depending upon the sampling results, included in the removal action workplan as well.</p> <p>Mitigation Measure 3.8-4: Prior to bringing hazardous materials onsite, the applicant shall submit a Hazardous Materials Business Plan (HMBP) to the Stanislaus County Division of Environmental Resources (CUPA) for review and approval. If during the construction process the applicant or any subcontractors generates hazardous waste, the applicant must register with the CUPA as a generator of hazardous waste, obtain an EPA ID# and accumulate, ship and dispose of the hazardous waste per Health and Safety Code Ch. 6.5. (California Hazardous Waste Control Law).</p> <p>Mitigation Measure 3.8-5: Prior to initiation of any ground disturbance</p>	<p>Stanislaus County Division of Environmental Resources</p> <p>City of Riverbank</p>	<p>Prior to bringing hazardous materials onsite</p> <p>Prior to initiation of any</p>	

4.0 FINAL MITIGATION MONITORING AND REPORTING PROGRAM

ENVIRONMENTAL IMPACT	MITIGATION MEASURE	MONITORING RESPONSIBILITY	TIMING	VERIFICATION (DATE/INITIALS)
	<p>activities within 50 feet of a well, the applicant shall hire a licensed well contractor to obtain a well abandonment permit from Stanislaus County Department of Environmental Resources, and properly abandon the on-site wells, pursuant to review and approval of the City Engineer and the Stanislaus County Department of Environmental Resources.</p> <p>Implement Mitigation Measures 3.8-1, 3.8-2, 3.8-3, 3.8-4, and 3.8-5.</p>	<p>Development Services Department</p> <p>See Mitigation Measures 3.8-1, 3.8-2, 3.8-3, 3.8-4, and 3.8-5</p>	<p>ground disturbance activities within 50 feet of a well</p> <p>See Mitigation Measures 3.8-1, 3.8-2, 3.8-3, 3.8-4, and 3.8-5</p>	
NOISE				
<p>Impact 3.11-1: Construction of the proposed Project may generate significant noise.</p>	<p>Mitigation Measure 3.11-1: Construction activities shall not occur between 6:30 p.m. and 6:00 a.m. on weekdays or 5:00 p.m. and 8:00 a.m. on weekends and legal holidays, as required by the City of Riverbank Municipal Code. This requirement shall be noted in the improvements plans prior to approval by the City's Public Works Department.</p> <p>Mitigation Measure 3.11-2: In an effort to comply with the City General Plan standards contained in Table 3.11-6 (Table N-3 of the General Plan), all equipment shall be fitted with factory equipped mufflers, and in good working order. In addition, all staging areas shall be located as far as feasibly possible from residential areas. This requirement shall be noted in the improvements plans prior to approval by the City's Public Works Department.</p>	<p>City of Riverbank Public Works Department</p> <p>City of Riverbank Public Works Department</p>	<p>During construction activities</p> <p>During construction activities</p>	
<p>Impact 3.11-4: The proposed Project may result in traffic noise at new sensitive receptors.</p>	<p>Mitigation Measure 3.11-3: The Project applicant(s) shall determine the appropriate methods for reducing traffic noise levels at the Project site to within the City of Riverbank noise level criteria. It is expected that traffic noise levels could exceed the City standards at residential areas adjacent to Oakdale Road, Morrill Road and Claribel Road. Mitigation can take the form of sound walls, berms, a combination of walls and berms, setbacks and shielding from building facades. The effectiveness of the proposed mitigation shall be documented by acoustical analyses. The appropriate mitigation will be determined prior to the approval of tentative maps or site plans, and subject to review and approval by the City of Riverbank.</p>	<p>City of Riverbank Public Works Department</p>	<p>Prior to approval of tentative maps or site plans</p>	



FINAL MITIGATION MONITORING AND REPORTING PROGRAM

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ENVIRONMENTAL IMPACT	MITIGATION MEASURE	MONITORING RESPONSIBILITY	TIMING	VERIFICATION (DATE/INITIALS)
<p>Impact 3.11-5: The proposed Project may result in noise from on-site activities at sensitive receptors.</p>	<p>Mitigation Measure 3.11-4: The center of the play fields shall be located at a minimum distance of 275-feet from the nearest residences. This requirement shall be noted in the improvements plans prior to approval by the City's Public Works Department.</p> <p>Mitigation Measure 3.11-5: Use of the play fields shall be restricted to the daytime hours of 7:00 a.m. to 10:00 p.m. This requirement shall be noted in the improvements plans prior to approval by the City's Public Works Department.</p> <p>Mitigation Measure 3.11-6: When school site plans have been developed, a detailed analysis of school site noise impacts shall be identified and appropriate mitigation measures shall be included in the project designs. The City shall review and approve the analysis of school site noise impacts, as well as any mitigation measures resulting from the analysis.</p>	<p>City of Riverbank Public Works Department</p> <p>City of Riverbank Public Works Department</p> <p>City of Riverbank Public Works Department</p>	<p>Prior to approval of improvement plans</p> <p>Prior to approval of improvement plans</p> <p>Prior to approval of improvement plans</p>	
PUBLIC SERVICES AND RECREATION				
<p>Impact 3.12-1: The proposed Project has the potential to require the construction of police department facilities which may cause substantial adverse physical environmental impacts.</p>	<p>Mitigation Measure 3.12-1: Prior to the City recording a "Final Map" for each Project within the Plan Area, the owner of the project/map shall either annex the mapped property into a Community Facilities District ("CFD"), or create a new CFD for the mapped property, which will include funding for operational services with the Riverbank Police Department (Stanislaus County Sheriff).</p>	<p>City of Riverbank Development Services Department</p>	<p>Prior to the City recording a "Final Map" for each Project within the Plan Area</p>	
<p>Impact 3.12-5: The proposed Project has the potential to require the construction of park and recreational facilities which may cause substantial adverse physical environmental impacts.</p>	<p>Mitigation Measure 3.12-2: Prior to the recording of any Final Maps, or in connection with any other final approvals for the MU-1 "Mixed Use Retail" area dedicated to residential development, the project developer shall dedicate and finance the improvement of sufficient park land in accordance with a park improvement plan, subject to approval by the City, or pay sufficient in lieu fees in accordance with the Quimby Act and the City's General Plan policy, to develop at least five acres of parkland per 1,000 residents. If sufficient park area is not provided for in the subdividable lands in accordance with the Quimby Act and City Ordinances, the Project applicant shall demonstrate where the parkland dedication may occur and provide surety of its dedication and improvement. This dedication requirement shall include development of full park improvement plans to be approved by the Director of Parks and Recreation. The timing of the park improvements shall be negotiated with the developer unless stipulated in a Development</p>	<p>City of Riverbank Development Services Department</p>	<p>Prior to the recording of any Final Maps, or in connection with any other final approvals for the MU-1 "Mixed Use Retail" area dedicated to residential development</p>	



Appendix B: Mitigation Monitoring and Reporting Program (MMRP)

4.0 FINAL MITIGATION MONITORING AND REPORTING PROGRAM

ENVIRONMENTAL IMPACT	MITIGATION MEASURE	MONITORING RESPONSIBILITY	TIMING	VERIFICATION (DATE/INITIALS)
	Agreement or Subdivision Improvement Agreement.			
TRANSPORTATION AND CIRCULATION				
Impact 3.13-1: Under Existing conditions, the proposed Project would result in a significant impact at the Patterson Road / Coffee Road intersection.	Mitigation Measure 3.13-1: Prior to issuance of any Building Permits for the CWSP Project, each project applicant in the Plan Area shall pay the applicable City of Riverbank Impact Fee towards the improvement of the Patterson Road / Coffee Road intersection in order to satisfy their fair share obligation.	City of Riverbank Public Works Department	Prior to issuance of any Building Permits for the CWSP Project	
Impact 3.13-2: Under Existing conditions, the proposed Project would result in a significant impact at the Claribel Road / Oakdale Road intersection.	Mitigation Measure 3.13-2: Prior to the approval of a Final Map or improvement plans, each applicant within the CWSP Project shall be responsible for the project's fair share impacts towards the cost of widening Oakdale Road to provide a second southbound travel lane that continues beyond Claribel Road a distance sufficient to accommodate efficient intersection traffic operations and a transition back to a single lane, as well as a northbound right turn lane. The distance needed to accommodate the auxiliary through lane and transition back to a single lane is roughly ¼ mile. This roadway improvement shall be noted on the project improvement plans. The sum of each project applicant's fair share cost shall be equal to the total cost to construct the entire improvement, and the sum of the fair share costs shall be used by the developer(s) to construct the entire improvement. The specific segments of roadway which would be widened shall be completed as determined by the City Engineer based on the level of development being proposed at the time.	City of Riverbank City Engineer	Prior to the approval of a Final Map or improvement plans	
Impact 3.13-3: Under Existing conditions, the proposed Project would result in a significant impact at the Claribel Road / N-S Collector intersection.	Mitigation Measure 3.13-3: Prior to approval of a Final Map or improvement plans, each applicant within the CWSP Project shall be responsible for the project's fair share impacts towards the cost of constructing a traffic signal and ancillary lanes at the Claribel Road / N-S Collector intersection. When warranted, construction of the traffic signal shall be required, to the satisfaction of the City of Riverbank City Engineer. The additional ancillary lanes shall be completed as determined by the City Engineer based on the level of development being proposed at the time. When warranted, this roadway improvement shall be noted on the improvement plans for such project. The sum of each project applicant's fair share cost shall be equal to the total cost to construct the entire improvement, and the sum of the fair share costs	City of Riverbank City Engineer	Prior to approval of a Final Map or improvement plans	

4.0-22 Final Environmental Impact Report – Crossroads West Specific Plan





FINAL MITIGATION MONITORING AND REPORTING PROGRAM

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Appendix B: Mitigation Monitoring and Reporting Program (MMRP)

ENVIRONMENTAL IMPACT	MITIGATION MEASURE	MONITORING RESPONSIBILITY	TIMING	VERIFICATION (DATE/INITIALS)
Impact 3.13-5: Under Existing conditions, the proposed Project would result in a significant impact at the segment of Patterson Road from McHenry Avenue to Coffee Road.	shall be used by the developer(s) to construct the entire improvement. Mitigation Measure 3.13-4: Prior to issuance of any Building Permits for each project in the Plan Area, each project applicant shall pay the applicable City of Riverbank Impact Fee towards widening of SR 108 to four-lanes in order to satisfy their fair-share obligation.	City of Riverbank Public Works Department	Prior to issuance of any Building Permits for each project in the Plan Area	
Impact 3.13-6: Under Existing conditions, the proposed Project would result in a significant impact at the segment of Claribel Road from McHenry Avenue to Coffee Road.	Mitigation Measure 3.13-5: Prior to issuance of any Building Permits for each project in the Plan Area, each project applicant shall pay the applicable County RTIF fee towards construction of the North County Corridor in order to satisfy their fair-share obligation.	Stanislaus Council of Governments	Prior to issuance of any Building Permits for each project in the Plan Area	
Impact 3.13-7: Under Existing conditions, the proposed Project would result in a significant impact at the segment of Claribel Road from Oakdale Road to Claus Road.	Mitigation Measure 3.13-6: Prior to issuance of Building Permits for the Project, each project applicant in the Plan Area shall pay the applicable City of Riverbank Impact Fee and County RTIF fee towards the improvement of Claribel Road from Oakdale Road to Claus Road in order to satisfy their fair share obligation.	Stanislaus Council of Governments	Prior to issuance of Building Permits for the Project	
Impact 3.13-9: Under Existing conditions, the proposed Project would result in a significant impact at the Oakdale Road between Morrill Road and Crawford Road segment.	Mitigation Measure 3.13-7: Prior to issuance of any Building Permits each project within the Plan Area, each project applicant shall be responsible for contributing the fair share contribution towards the costs of widening Oakdale Road to four lanes by providing a second southbound through travel lane between Morrill Road and Crawford Road. The applicant shall be responsible for widening Oakdale Road when determined by the City Engineer.	City of Riverbank City Engineer	Prior to issuance of any Building Permits each project within the Plan Area	
Impact 3.13-11: The proposed Project would adversely affect pedestrian and bicycle facilities.	Mitigation Measure 3.13-8: Each project applicant in the Plan Area shall work with City of Riverbank staff to identify applicable pedestrian crossing features and shall install the features, when warranted, to the satisfaction of the City of Riverbank City Engineer. Mitigation Measure 3.13-9: Each project applicant in the Plan Area shall monitor pedestrian, bicycle, and motor vehicle safety conditions as development proceeds. Any identified safety conditions as a result of this	City of Riverbank City Engineer City of Riverbank City	When warranted, as determined by the City Engineer When warranted, as	

Appendix B: Mitigation Monitoring and Reporting Program (MMRP)

4.0 FINAL MITIGATION MONITORING AND REPORTING PROGRAM

ENVIRONMENTAL IMPACT	MITIGATION MEASURE	MONITORING RESPONSIBILITY	TIMING	VERIFICATION (DATE/INITIALS)
	monitoring shall be installed to alleviate these concerns, as applicable, to the satisfaction of the City of Riverbank City Engineer.	Engineer	determined by the City Engineer	
Impact 3.13-12: The proposed Project would adversely affect transit services or facilities.	Mitigation Measure 3.13-10: The project applicants in the CWSP Area shall install the transit elements included in the CWSP. The project applicants shall work with Stanislaus Regional Transit staff to identify applicable on-site transit facilities and features in order to ensure that transit facilities are incorporated into the project. The transit facilities and features may include, but would not be limited to, bus turnouts, bus stops, and signage. The project applicants shall install the features, when warranted, to the satisfaction of the City Engineer.	City of Riverbank City Engineer Stanislaus Regional Transit	When warranted, as determined by the City Engineer	
Impact 3.13-13: Under EPAP conditions, the proposed Project would result in a significant impact to queue lengths.	Mitigation Measure 3.13-11: Prior to approval of a Final Map or improvement plans, each project applicant within the CWSP Area shall be responsible for lengthening the available storage in left turn lanes at the Oakdale Road / Crawford Road, Oakdale Road / Freddi Lane, and Oakdale Road / Claribel Road intersections. The applicants shall be responsible for lengthening specific turn lanes when determined by the City Engineer. These roadway improvements shall be noted on the project improvement plans.	City of Riverbank City Engineer	Prior to approval of a Final Map or improvement plans	
Impact 3.13-14: Under EPAP conditions, the proposed Project would result in a significant impact at the proposed mixed use retail area access.	Mitigation 3.13-12: Prior to approval of a Final Map or improvement plans for the "MU-1 Mixed Use Retail" area, the project applicant shall be responsible for providing a design for vehicular access to the satisfaction of the City of Riverbank City Engineer when development of the "MU-1 Mixed Use Retail" area proceeds. This roadway design shall be noted on the project improvement plans.	City of Riverbank City Engineer	Prior to approval of a Final Map or improvement plans for the "MU-1 Mixed Use Retail" area	
Impact 3.13-15: Under EPAP conditions, the proposed Project would result in a significant impact at the McHenry Avenue / Kiernan Avenue / Claribel Avenue intersection.	Mitigation Measure 3.13-13: Prior to issuance of Building Permits for each project in the Plan Area, each project applicant shall pay the applicable County RTJF fee towards construction of the North County Corridor in order to satisfy their fair-share obligation.	Stanislaus Council of Governments	Prior to issuance of Building Permits for each project in the Plan Area	
Impact 3.13-16: Under Cumulative (Year 2042) conditions, the proposed Project would result in a significant impact at the Patterson Road /	Implement Mitigation Measure 3.13-1.	See Mitigation Measure 3.13-1	See Mitigation Measure 3.13-1	





FINAL MITIGATION MONITORING AND REPORTING PROGRAM

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Appendix B: Mitigation Monitoring and Reporting Program (MMRP)

ENVIRONMENTAL IMPACT	MITIGATION MEASURE	MONITORING RESPONSIBILITY	TIMING	VERIFICATION (DATE/INITIALS)
Coffee Road intersection.				
Impact 3.13-17: Under Cumulative (Year 2042) conditions, the proposed Project would result in a significant impact at the Coffee Road / Morrill Road intersection.	Mitigation Measure 3.13-14: Prior to approval of a Final Map or improvement plans, each applicant within the CWSP Project shall be responsible for the project's fair share impacts towards the cost of installing a traffic signal at the Coffee Road / Morrill Road intersection. When warranted, construction of the traffic signal shall be required, to the satisfaction of the City of Riverbank City Engineer. When warranted, this roadway improvement shall be noted on the improvement plans for such project.	City of Riverbank City Engineer	Prior to approval of a Final Map or improvement plans	
Impact 3.13-18: Under Cumulative (Year 2042) conditions, the proposed Project would result in a significant impact at the Coffee Road / Relocated Crawford Road intersection.	Mitigation Measure 3.13-15: Prior to approval of a Final Map or improvement plans, each project applicant shall be responsible for its fair share of the cost of installing traffic signal at the Coffee Road / Relocated Crawford Road intersection. The signal shall be installed when conditions warrant, as determined by the City of Riverbank City Engineer.	City of Riverbank City Engineer	Prior to approval of a Final Map or improvement plans	
Impact 3.13-19: Under Cumulative (Year 2042) conditions, the proposed Project would result in a significant impact at the Claribel Road / N-S Collector intersection.	Implement Mitigation Measure 3.13-3.	See Mitigation Measure 3.13-3	See Mitigation Measure 3.13-3	
Impact 3.13-20: Under Cumulative (Year 2042) conditions, the proposed Project would result in a significant impact at the Claribel Road / Oakdale Road intersection.	Mitigation Measure 3.13-16: Prior to approval of a Final Map or improvement plans, each applicant in the Plan Area shall be responsible for the project's fair share impacts towards the cost of adding a second northbound left turn lane at the Claribel Road / Oakdale Road intersection, as determined by the City of Riverbank City Engineer. When warranted, the addition of a second northbound left turn lane shall be required, to the satisfaction of the Stanislaus County Road Commissioner. When warranted, this roadway improvement shall be noted on the improvement plans for such project.	City of Riverbank City Engineer	Prior to approval of a Final Map or improvement plans	
Impact 3.13-24: Under Cumulative (Year 2042) conditions, the proposed Project would result in a significant impact at the segment of Coffee	Mitigation Measure 3.13-17: Prior to approval of a Final Map or improvement plans, each project applicant in the Plan Area shall be responsible for the fair share of the cost of improving Coffee Road from Morrill Road to the relocated Crawford Road intersection to provide the functional equivalent of a two-lane arterial street standard, as determined by	City of Riverbank City Engineer	Prior to approval of a Final Map or improvement plans	

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4.0 FINAL MITIGATION MONITORING AND REPORTING PROGRAM

ENVIRONMENTAL IMPACT	MITIGATION MEASURE	MONITORING RESPONSIBILITY	TIMING	VERIFICATION (DATE/INITIALS)
Road between Morrill Road and the relocated Crawford Road.	the City of Riverbank City Engineer.		plans	
Impact 3.13-25: Under Cumulative (Year 2042) conditions, the proposed Project would result in a significant impact at the segment of Coffee Road between the relocated Crawford Road and the realigned Claribel Road intersection.	Mitigation Measure 3.13-18: Prior to approval of a Final Map or improvement plans, each project applicant in the Plan Area shall be responsible for contributing its fair share to the cost of improving Coffee Road from the relocated Crawford Road intersection to the realigned Claribel Road intersection to the equivalent of a four-lane arterial street standard, as determined by the City of Riverbank City Engineer.	City of Riverbank City Engineer	Prior to approval of a Final Map or improvement plans	
Impact 3.13-26: Under Cumulative (Year 2042) conditions, the proposed Project would result in a significant impact at the segment of Coffee Road between the realigned Claribel Road intersection and NCC.	Mitigation Measure 3.13-19: Prior to approval of a Final Map or improvement plans, each project applicant in the Plan Area shall be responsible for contributing its fair share fee to the cost of improving Coffee Road from the realigned Claribel Road intersection to NCC to a four-lane arterial street standard.	City of Riverbank City Engineer	Prior to approval of a Final Map or improvement plans	
UTILITIES				
Impact 3.14-6: The proposed Project has the potential to require or result in the construction of new storm water drainage facilities or expansion of existing facilities, the construction of which could cause significant environmental effects.	Mitigation Measure 3.14-1: Prior to the issuance of a building or grading permit, the Project applicant shall submit a drainage plan to the City of Riverbank for review and approval. The plan shall include an engineered storm drainage plan that demonstrates attainment of pre-Project runoff requirements prior to release and describes the volume reduction measures and treatment controls used to reach attainment consistent with the Riverbank Low Impact Development Design and Specifications Manual, the Riverbank Storm Drain System Master Plan, and the Crossroads West Specific Plan.	City of Riverbank City Engineer	Prior to the issuance of a building or grading permit	



APPENDIX C

MU-1 Master Sign Program



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MU-1 MASTER SIGN PROGRAM



Appendix C: MU-1 Master Sign Program

MASTER SIGN PROGRAM

Crossroads West Shopping Center

PURPOSE AND INTENT

This criteria has been established for the purpose of maintaining a continuity of quality and aesthetics throughout the shopping center for the mutual benefit of all tenants, and to comply with the regulations of the City of Riverbank.

The intent of the sign criteria is to offer each tenant design flexibility and to incorporate common design elements. The signs specified will offer optimum tenant identity when designed in conformance with the design standards.

Conformance will be strictly enforced, and any installed non-conforming or unapproved signs will be removed at tenant's expense.

GENERAL REQUIREMENTS

- Prior to fabrication or applying for city approvals, each tenant shall submit to the landlord, for written approval, sign drawings indicating the location, size and style of lettering, materials, type of illumination, installation, details, color selection and logo design, and method of attachment.
- All signs shall be reviewed by the landlord for conformance with the established sign criteria. Approval or disapproval of sign submittals shall remain the sole and absolute right of the landlord.
- All permits and fees for signs and their installation shall be obtained by the tenant or its representative.
- All signs shall be constructed at the tenant's sole cost and expense.
- Tenant shall be responsible for the installation and maintenance of its sign. Should tenant's sign require maintenance or repair, landlord shall give tenant thirty (30) days written notice to affect the maintenance or repair. Should tenant fail to do so, landlord shall undertake repairs and tenant shall reimburse landlord within ten (10) days from the receipt of invoice.

DESIGN CRITERIA

1. STYLE

- Tenant signs must be self-illuminated individual pan channel metal letters, minimum 5" deep, with plexi-glas face.
- If the tenant does not have an established exterior sign identity, the style of lettering shall be similar to Helvetica Medium.



Appendix C: MU-1 Master Sign Program

2. NUMBER

- Each in-line non end-cap shop tenant will be permitted one (1) sign unless the premises has a building frontage on more than one side, in which case landlord must approve the number of signs, not to exceed two (2) total.
- Each end-cap shop tenant will be permitted one (1) sign for each building frontage. For example, if an end-cap tenant's suite contains three (3) frontages, such tenant will be allowed signs on its three (3) frontages.
- Freestanding and stand alone building tenants (i.e., individual stand alone restaurants, financial institutions, retail establishments) with four frontages will be allowed one (1) sign on each building frontage.

3. SIZE OF LETTERS

- Maximum letter height for in-line and end-cap tenants less than 3,000 square feet shall be 30" (using either solely upper case letters or solely lower case letters) with the maximum length not to exceed 80% of lineal frontage. Tenants utilizing a combination of upper and lower case letters will be allowed a total letter height not to exceed 36". Total square footage allowed is 1 ½ square feet per each lineal foot of store frontage.
- Maximum letter size for in-line, end-cap or freestanding tenants with a building size in excess of 3,000 square feet shall be 36" with maximum length not to exceed 80% of lineal frontage. Total square footage allowed is 1 ½ square feet per each lineal foot of store frontage.
- Freestanding and stand alone building tenants must conform to calculations of frontage x 1 ½ square feet and an individual letter height not to exceed 48".
- Major in-line or freestanding tenants in excess of 5,000 square feet ("Major Tenants"), please refer to the "Major Tenants" Section below for wall signage requirements.

4. SIGN COPY

- Excepting Major Tenants, wording of signs for tenants shall not include the product sold except as part of the tenant name, insignia, or logo.

5. COLOR

- A variation of color from one tenant to another is encouraged. All letters of the sign shall be one color. Logo colors may differ from sign color. The trim caps and returns are to be dark bronze or such other compatible color. Major Tenants are an exception to the provisions contained within this paragraph and are allowed to utilize their nationally identified colors.



Appendix C: MU-1 Master Sign Program

6. LOGOS

- Tenant will be permitted to display one (1) logo at each sign location subject to landlord's approval. The logo area shall be included within the sign area limitation. The logo shall be defined as a graphic symbol commonly used by the Tenant for identification.

CONSTRUCTION REQUIREMENTS

- Excepting Major Tenants, sign construction shall be individual aluminum pan channel letters, 5" deep aluminum returns and 3/4" trimcap. Letter faces shall be 3/16" thick plexiglas as manufactured by Acrylite or Rohm & Haas for outdoor use. Interior neon tubing will be 3" on center or less. Neon the same color as plexiglas face. No labels visible on signs, except UL label and County sticker; no sign company name to be visible from ground. All signs shall be fabricated using full welded construction.
- All sign bolts, fastenings and lips shall be hot-dipped galvanized iron, stainless steel, aluminum, brass, bronze, nickel or cadmium plated. No black iron materials of any type will be permitted.
- All cabinets, conductors, crossovers, transformers, wiring and other equipment must be concealed behind the sign fascia.
- Excepting Major Tenants, the face of the channel letters and logos shall be acrylic plastic (3/16" thick minimum) fastened to the metal frame by a trim cap matching the color of the letter returns.
- All signs and their installation must comply with all local building and electrical codes and bear a U.L. label if required by the local governing agency.
- The stroke of the letter will dictate the number of rows of neon tubing and the milliamp requirements for internal illumination. The tubing is to be installed and labeled in accordance with the "National Board of Fire Underwriters Specifications".

INSTALLATION

- Excepting Major Tenants, all signs are to be installed under the direction of the project superintendent and/or landlord. Sign work is not to commence on tenant's premises unless the project superintendent has reviewed a signed copy of the approved shop drawings. Each sign will be inspected for conformance with the detailed shop drawings.
- Tenant's sign contractor shall completely install and connect the sign display.
- The sign contractor shall repair any damage to the building caused by its work. All penetrations of the building shall be neatly sealed in a watertight condition and shall be patched to match the adjacent finish to landlord's satisfaction. Damage to the building that is not repaired by the sign contractor shall be corrected at tenant's expense.
- Landlord will inspect tenant's sign installation and require tenant to have any discrepancies and/or code violations corrected at tenant's expense.



Appendix C: MU-1 Master Sign Program

GUARANTEE

The entire sign display will be guaranteed for one (1) year against defects in material and workmanship. Defective parts shall be replaced without charge.

INSURANCE

The sign company shall carry workmen's compensation and public liability insurance against all damage suffered or done to any and all personal and/or property while engaged in the construction or installation of signs in an amount not less than \$1,000,000.00 per occurrence.

UNDER CANOPY SIGNS

The tenant is responsible, at tenant's discretion, for providing an under canopy sign, to hang no lower than 8 feet from ground. The maximum size is to be 6 square feet and the copy is to be business name only. Design and copy to be approved by landlord and City before installation.

STORE FRONT AND RECEIVING DOOR SIGNAGE

- Tenant shall be permitted to place a sign of not more than 144 square inches at the bottom of the window panel adjacent to the store front entry door. The sign shall be decaled or adhered lettering indicating hours of business, emergency phone numbers, approved credit cards, etc. No other window signage will be permitted at any time.
- Each tenant who has a non-customer door for receiving merchandise may have uniformly applied on said door in location, as directed by the Landlord, in block letters not to exceed 8", the tenant's name and address. Where more than one tenant uses the same door, each name and address shall be applied.

PROHIBITED SIGNAGE

- Except as provided herein, no advertising placards, external display, decorative lighting, flags, balloons, banners (excepting grand opening banners which shall be removed by tenant at tenant's cost no later than 30-45 days from tenant's opening date), pennants, names, insignia, trademarks or other descriptive material, shall be affixed or maintained upon either the interior or exterior glass panes and supports of the show windows and doors or upon the exterior walls of the building.
- Signs on or affixed to trucks, automobiles, trailers or other vehicles which advertise, identify, or provide direction to a use or activity not related to its lawful making of deliveries of sales or merchandise or rendering of services from such vehicles is prohibited.

EXPIRATION OF LEASE TERM

At the expiration or a sooner termination of tenant's lease term, tenant shall remove its sign and patch and repair the sign fascia including repairing any damage to the sign fascia and the replacement of any building materials damaged by tenant's installation or removal of tenant's sign. Additionally, tenant will be required to re-paint tenant's sign fascia to match the adjacent finish to the landlord's satisfaction.



Appendix C: MU-1 Master Sign Program

MAJOR TENANTS

The provisions of this Master Sign Program shall not be applicable to identification signs of occupancies designated by the landlord as Major Tenants (any tenant in excess of 5,000 square feet or any national or chain tenant having 10 or more retail stores in the United States). These tenants may install their standard signage, which appears on buildings operated by them in a majority of locations in California or the western United States, provided the signs are architecturally compatible, are fabricated and installed in first class condition and have been approved by the landlord.

FREE STANDING SIGNS

The intent of Crossroads West is to incorporate Major Tenants and regional tenants onto four (4) free standing center pylon signs as further depicted on the attached Exhibit “E-1”. Additionally, freestanding single pad tenants or shops tenants within a multi-tenant pad building may have the right to install (at their sole cost and expense) a small monument sign for their individual parcel as shown on the attached Exhibit “E-2”, not to exceed six feet (6’) in height and a total of ten feet (10’) in width with a sign area not to exceed 30 square feet per side. The individual tenant panel to be installed by tenant must be approved and permitted by landlord and the City prior to installation. The tenant may have their standard copy style and colors as long as such copy style and colors are architecturally compatible with the pylon sign as determined by landlord and the City in their sole discretion. Tenant will not be allowed representation on the pylon sign or monument sign unless allowed by landlord in its sole discretion. To the extent tenant is allowed representation on the pylon sign or monument sign, tenant will reimburse landlord its pro-rata share of the costs associated with the initial design, permitting, construction, fabrication, installation and electrical connections of the sign in common with the other tenants located on the sign based on tenant’s sign panel area as the numerator and the total tenant signage contained on the sign as the denominator.

EXHIBITS

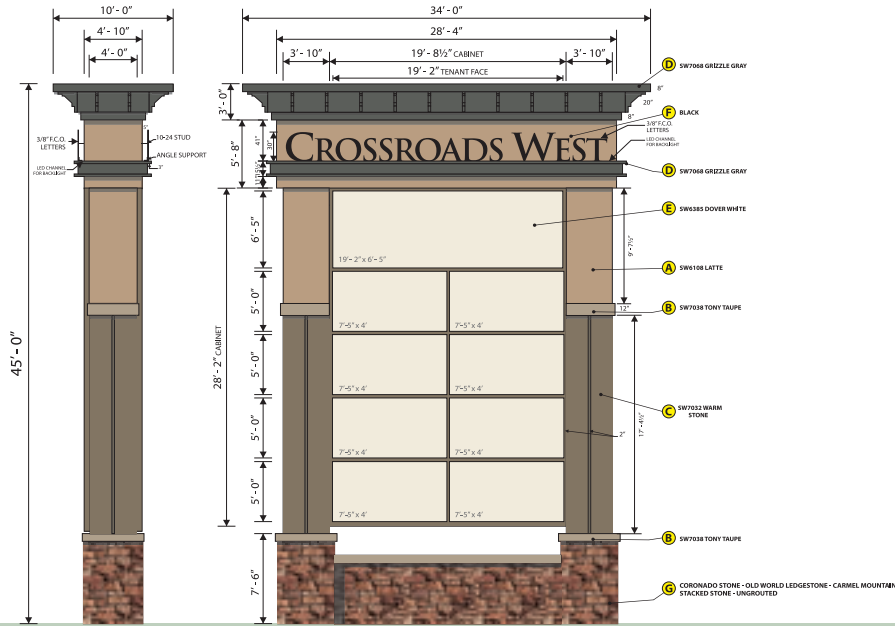
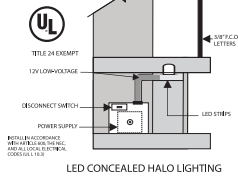
Exhibit “A-1” Pylon Sign & Monument Sign Rendering
Exhibit “A-2” Site Plan



Appendix C: MU-1 Master Sign Program

**BROWMAN DEVELOPMENT
CROSSROADS WEST
RIVERBANK, CA**

45' PYLON SIGN



SCALE 1/8" = 1' - 0"



6221 ENTERPRISE DRIVE, DIAMOND SPRINGS, CA 95619
Phone 916.933-3765 • 530.622-1420 Fax 530.622-9367
E-Mail design@westernsign.com Website www.westernsign.com

SALESPERSON Keith Willis
ILLUSTRATION Perry Wilson
LOCATION Riverbank, CA
DATE 12/21/17
REVISED

CUSTOMER APPROVAL _____ DATE _____

COLORS ON THIS PRINTOUT MAY VARY SLIGHTLY FROM FINAL MATERIALS COLORS. APPROVAL OF THE ILLUSTRATION INCLUDES APPROVAL OF THE FINAL MATERIALS COLORS AS DESCRIBED IN THE WRITTEN CALLOUT. WRITERS CANNOT TAKE ON THE ILLUSTRATED DIMENSIONS. ILLUSTRATIONS SHOWING BUILDING ELEVATIONS ARE CONCEPTUAL AND TO APPROXIMATE SCALE ONLY THEY ARE NOT TO BE CONSIDERED AN EXACT RANGE OF HOW THE FINISHED SIGN WILL APPEAR. THIS IS AN ORIGINAL UNPUBLISHED DRAWING CREATED BY WESTERN SIGN CO., INC. IT IS SUBMITTED FOR YOUR APPROVAL. IT IS NOT TO BE SHOWN TO ANYONE OUTSIDE YOUR ORGANIZATION NOR IS IT TO BE REPRODUCED, COPIED OR EXHIBITED IN ANY FASHION WITHOUT EXPRESSED WRITTEN PERMISSION OF WESTERN SIGN CO., INC.

LANDLORD APPROVAL _____ DATE _____

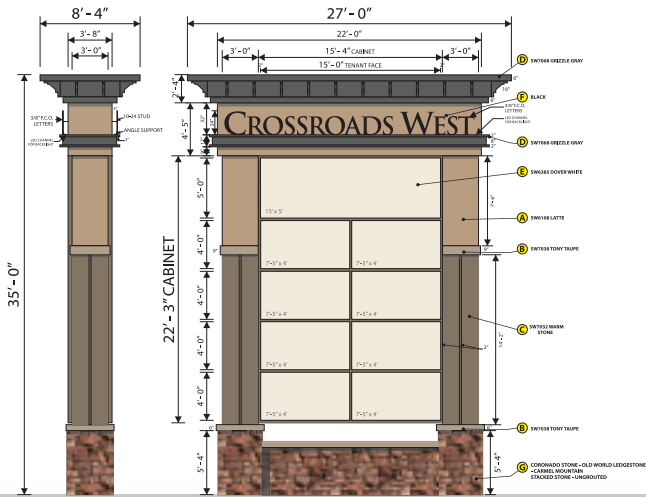
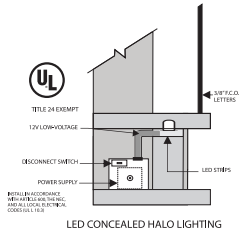
SALESPERSON SIGNOFF _____ DATE _____



Appendix C: MU-1 Master Sign Program

**BROWMAN DEVELOPMENT
CROSSROADS WEST
RIVERBANK, CA**

35' PYLON SIGN



SCALE 1/8" = 1'-0"



6221 ENTERPRISE DRIVE, DIAMOND SPRINGS, CA 95619
Phone 916.933-3765 • 530.622-1420 Fax 530.622-9367
E-Mail design@westernsign.com Website www.westernsign.com

SALESPERSON Keith Willis
ILLUSTRATION Perry Wilson
LOCATION Riverbank, CA
DATE 12/21/17
REVISED

CUSTOMER APPROVAL _____ DATE _____

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LANDLORD APPROVAL _____ DATE _____

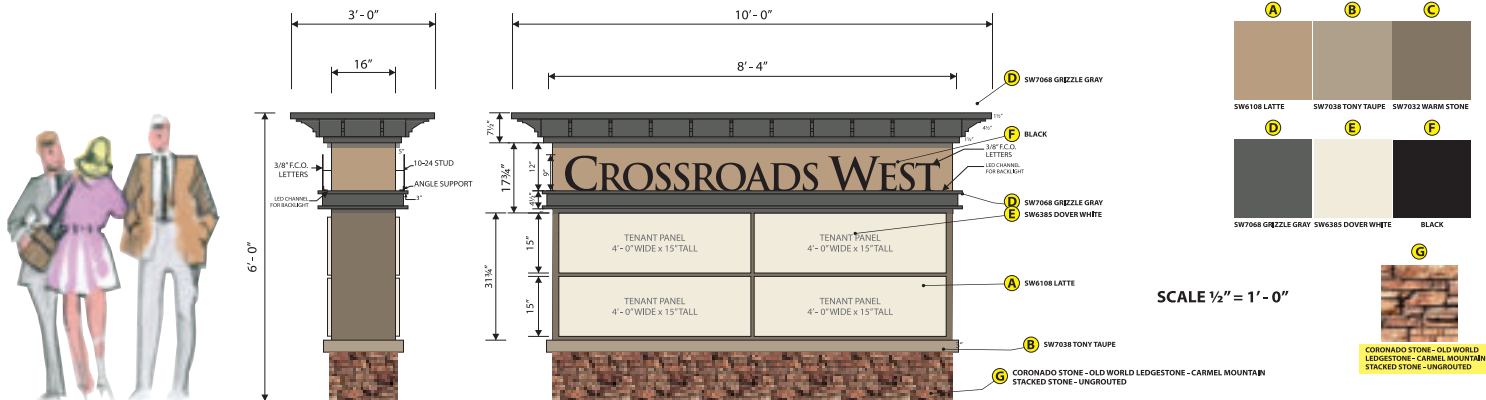
SALESPERSON SIGNOFF _____ DATE _____



Appendix C: MU-1 Master Sign Program

**BROWMAN DEVELOPMENT
SUNWEST VILLAGE
LODI, CA**

6' MONUMENT SIGN



**WESTERN
SIGN COMPANY**
SINCE 1959

6221 ENTERPRISE DRIVE, DIAMOND SPRINGS, CA 95619

Phone 916.933-3765 • 530.622-1420 Fax 530.622-9367
E-Mail design@westernsign.com Website www.westernsign.com

SALESPERSON Keith Willis
ILLUSTRATION Perry Wilson
LOCATION Riverbank, CA
DATE 12/21/17
REVISED

CUSTOMER APPROVAL _____ DATE _____

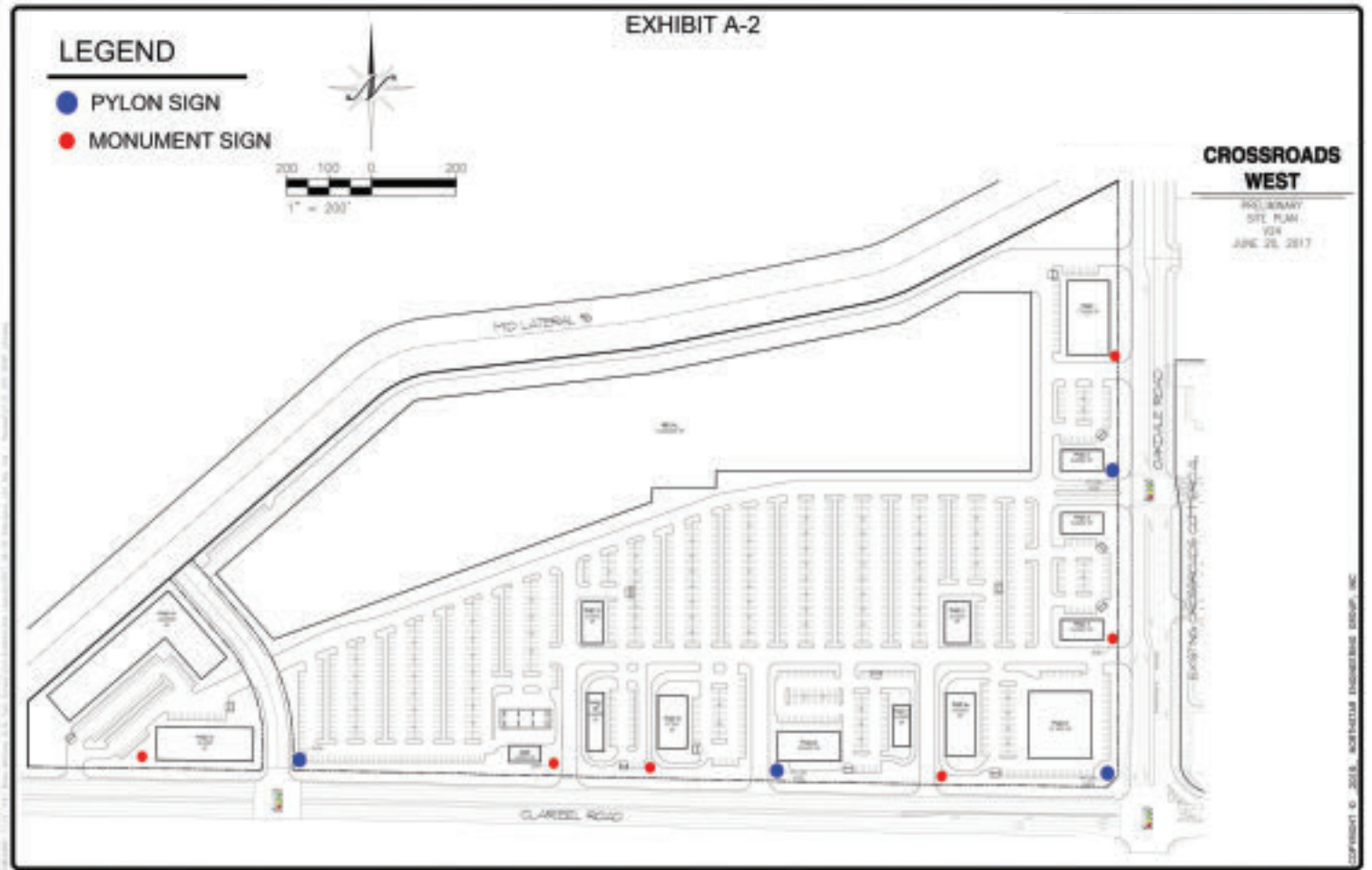
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LANDLORD APPROVAL _____ DATE _____

SALESPERSON SIGNOFF _____ DATE _____



Appendix C: MU-1 Master Sign Program



APPENDIX D

Chapter 157: Water Efficient Landscape and Irrigation



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Appendix D: Chapter 157 Water Efficient Landscape and Irrigation

Riverbank, California Code of Ordinances

CHAPTER 157: WATER EFFICIENT LANDSCAPE AND IRRIGATION

Section

- 157.01 Purpose and intent
- 157.02 Applicability
- 157.03 Water conservation definition
- 157.04 Process
- 157.05 Development standards

§ 157.01 PURPOSE AND INTENT.

The purpose and intent of this chapter is to establish landscaping regulations that are intended to:

(A) Enhance the aesthetic appearance of development in all areas of the city by providing development standards relating to quality, quantity and functional aspects of landscaping and landscape screening.

(B) Increase compatibility between residential and abutting commercial and industrial uses.

(C) Reduce the heat and glare generated by development.

(D) Establish a water conservation plan to reduce water consumption in the landscape environment using conservation principles important to the State of California and the City of Riverbank.

(E) Protect public health, safety, and welfare by minimizing the impact of all forms of physical and visual pollution, controlling soil erosion, screening incompatible land uses, preserving the integrity of neighborhoods, and enhancing pedestrian and vehicular traffic and safety.

(F) Comply with the California Department of Water Resources Title 23, Division 2, Chapter 2.7 entitled Model Water Efficient Landscape Ordinance and the California Green Building Standards of 2013 and/or these standards as amended from time to time.

(Ord. 2014-005, passed 6-10-14)

§ 157.02 APPLICABILITY.

All new development in the city shall comply with the provisions of this chapter and the requirements of the State Model Water Efficient Landscape Ordinance referenced above, which is incorporated herein by this reference. All required landscaping shall be installed by the developer and approved by the Development Services Department prior to the issuance of a certificate of occupancy for any building, unless other arrangements are agreed to by the Community Development Director.

(A) *Exceptions.* The following shall be exempt from the provisions of this section:

(1) Yards and landscape areas for single family homes when not installed by the builder or developer prior to or as a condition of home sales.

(2) Interior remodels, tenant improvements and demolitions.

(3) Changes of use to any existing building.

(4) Replacement or repair of existing plant material or irrigation systems in conjunction with routine maintenance of existing projects.

(5) Modifications or additions to existing structures which do not result in an increase of more than 10% of the floor area, or



Appendix D: Chapter 157 Water Efficient Landscape and Irrigation

1,000 square feet, whichever is greater.

(B) *Statutory authority in case of conflicting provisions.* Nothing in this section shall be deemed to affect, annul or abrogate any other laws or ordinances pertaining or applicable to the properties and areas affected by this chapter.

(Ord. 2014-005, passed 6-10-14)

§ 157.03 WATER CONSERVATION DEFINITION.

For the purpose of this chapter, the following definition shall apply unless the context clearly indicates or requires a different meaning.

WATER CONSERVATION. This definition shall mean a combination of landscape features and techniques that in the aggregate reduce the demand for and consumption of water, including but not limited to appropriate low water using plants, nonliving groundcover, a low percentage of turf coverage, permeable paving and water conserving irrigation techniques and systems, and other water saving techniques and measures approved by the Community Development Services Department.

(Ord. 2014-005, passed 6-10-14)

§ 157.04 PROCESS.

The Community Development Director shall establish a format for landscape plans and any other procedural guidelines for submittal as deemed necessary.

(A) *Plans required.* Plans for the development of required landscaping shall be submitted to the Development Services Department for review and approval prior to the issuance of any building permit or as may be required for a site plan permit. Said plans shall be prepared by a licensed landscape architect, landscape contractor, architect or certified irrigation designer.

(B) *Plan review and approval.* The Community Development Services Department shall review each project and proposed landscape plan for compliance with the landscape and water conservation requirements.

(C) *Alternative means of compliance.* The Community Development Director may allow alternative means of complying with the requirements in this section provided the alternative achieves results comparable to those achieved through strict application of the provisions of this section.

(Ord. 2014-005, passed 6-10-14)

§ 157.05 DEVELOPMENT STANDARDS.

(A) *Required.* In the following designated districts, not less than the stipulated percent of gross site area shall be landscaped in accordance with this section:

<i>Landscape Area Requirements</i>	
<i>Zone District</i>	<i>Required Landscaping (Gross % of site)</i>
R-1/PD	30
R-2	30 (a)
R-3	30 (a)
C-1/PD	10 (c)
C-2	10 (c)



Appendix D: Chapter 157 Water Efficient Landscape and Irrigation

C-M	7.5 (c)
M-1/PD	7.5 (b)(o)
M-2	7.5 (b) 5 (c)
CX-1	10 (c)

(1) In multiple-family developments of 12 or more dwelling units, 10% of the total building site shall be set aside and landscaped for the purposes of common recreational open space. Such 10% may be included in the general landscaping requirements.

(2) This requirement may be waived by the Community Development Director for remodeling, alterations, or renovations to existing buildings and developments on parcels or building sites where an existing building occupies a substantial portion of the site and there exists limited space to landscape because of the front yard or corner street side yard setback areas.

(3) In commercial and industrial districts, where a lot larger than 10,000 square feet is to be developed in phases, the Community Development Director may determine that only the developed portion of the site need be landscaped. Provision shall be made, however, to insure that the landscape requirement can still be met upon full development of the site. This exception shall not apply to any setback along a public street which shall be landscaped upon the initial development of the site. Open areas (areas not landscaped) shall be continuously maintained free of weeds, litter and debris, and shall not become a source of nuisance to adjoining property.

(B) *Determination of landscaped areas.* In determining landscaped areas, setbacks, when landscaped, private patios and all other areas not occupied by buildings, parking lots, vehicle storage areas, and driveways shall be included. Areas occupied by clubhouses, recreation buildings, pools, saunas, inter walkways, and similar amenities may be included as landscaped areas. In industrial zoning districts, areas planted along a public right-of-way shall qualify as one and one-half (1.5) times the area toward the overall required landscaping area.

(C) *Landscape materials and placement.* All landscape areas shall demonstrate a recognizable pattern or theme for the overall development. To accomplish this, new landscaping and landscape areas shall conform to the following:

(1) Plant materials shall be selected for maintenance efficiency, drought tolerance and adaptability and relationship to Riverbank's environment and climate. Trees and shrubs in reasonable numbers shall be used in the landscape design; ground cover alone shall not be acceptable. No one species of plant shall exceed 20% of the plant material. Landscaped areas shall incorporate a minimum of two of the following plantings: (1) grasses and ground covers, (2) shrubs, and (3) trees.

(2) In all commercial (C) and industrial (M) districts, plant materials shall be sized and spaced to achieve immediate effect and shall normally not be less than 24-inch box for parking lot shade trees, 15 gallon container for trees, five gallon container for shrubs, and a one gallon container for mass planting. Non-turf areas, such as shrub beds, shall be top dressed with a bark chip mulch or approved alternative.

(3) Turf shall be limited to 25% of the total landscaped area in all commercial (C) and industrial (M) districts. In residential districts turf shall not exceed 50% of the total landscaped area.

(D) *Landscape irrigation.* Provisions shall be made for a permanent "in place" irrigation system to all landscaped areas required herein, including street tree wells. All new irrigation systems shall use Xeriscaping principles including such techniques and materials as low precipitation sprinkler heads, bubblers, drip irrigation systems, timing devices and moisture sensors. All irrigation systems must be designed to comply with the "State Model Water Efficient Landscape Ordinance" and shall minimize overspray onto impervious surfaces, such as building, sidewalks, parking areas, and the like, through the use of such techniques as low-trajectory spray nozzles or underground low volume applicators. All irrigation system controllers shall be set in compliance with the day and hour watering requirements of the city and shall be designed to minimize water use by installing automatic systems such as multi-start controllers and soil moisture sensors.

(E) *Site preparation and installation.*

(1) Prior to the planting of any materials, the compacted soils surrounding a building site will be returned to a friable condition. Friable condition shall mean returning the soil to an easily crumbled or loosely compacted condition down to a minimum depth per planting material requirements, whereby the root structure of newly planted material will be allowed to spread unimpeded. The soil must be returned to a friable condition to a minimum depth as required for the planting material.

(2) Trees should be adequate in trunk diameter to support the top area of the tree. Trees, shrubs, and vines should have body and fullness that is typical of the species.



Appendix D: Chapter 157 Water Efficient Landscape and Irrigation

(3) All ground cover should be healthy, densely foliated, and well rooted cuttings, or one-gallon container plants.

(4) The spacing of trees and shrubs should be appropriate to the species used. The plant materials should be spaced so that they do not interfere with the adequate lighting of the premises or restrict access to emergency apparatus such as fire hydrants or fire alarm boxes. Proper spacing should also insure unobstructed access for vehicles and pedestrians in addition to providing clear vision of the intersections from approaching vehicles. Plant material should conform to the following spacing standards:

(a) A minimum of 25 feet from the property corner at a street intersection to the center of the first tree or large shrub.

(b) A minimum of 15 feet between center of trees and large shrubs to light standards.

(c) A minimum of 15 feet between center of trees or large shrubs and fire hydrants.

(d) A minimum of 15 feet from the intersection of a driveway with a street right-of-way to the center of any tree having a diameter larger than 18 inches at maturity or large shrub.

(F) *Maintenance*. Required planting areas shall be permanently maintained. As used in this section, "maintained" includes: watering, weeding, pruning, insect control, and replacement of plant materials and irrigation equipment as needed to preserve the health and appearance of plant materials.

(G) *Parking lot landscaping*. Parking lots and parking structures shall have interior and perimeter landscaping areas as prescribed by the following:

(1) Parking lots or parking structures adjoining street property lines or public streets shall have a perimeter landscape buffer with a minimum width as follows:

(a) If abutting an expressway: 20 feet.

(b) If abutting an arterial: 15 feet.

(c) If abutting a collector: 15 feet.

(d) If abutting a local collector: 10 feet.

(e) If abutting a local street: five feet.

(2) Vehicle overhang may encroach a maximum of two feet into landscape areas which are a minimum of ten feet wide.

(3) An average three foot high (minimum two and one-half foot and maximum three and one-half foot) continuous screen shall be installed between all parking areas and public streets. A screen shall consist of one or any combination of the following:

(a) *Walls*. A wall shall consist of concrete, concrete block, stone, brick, tile or similar type of solid masonry material.

(b) *Berms*. A berm shall be constructed of earthen materials and it shall be landscaped.

(c) *Solid fences*. A solid fence shall be constructed of wood, or other materials.

(d) *Plant materials*. Vegetation, consisting of trees or shrubs.

(4) Interior landscaped areas shall be a minimum of five feet in width, exclusive of curbs.

(5) The end of each row of parking stalls shall be separated from aisle ways by a landscaped planter or sidewalk.

(6) In all parking lots with a capacity of five parking spaces or more, a minimum of one shade tree for every five spaces shall be provided in landscape islands within the parking lot. Tree spacing shall be such that every designated parking space is within 30 feet of the trunk of a tree.

(7) All planting areas within or abutting a parking lot shall be protected with raised concrete curbs.

(H) *Landscaping in rights-of-way*. All land area within the public right-of-way adjoining all sides of any parcel or building site that is not otherwise covered with a building, structure, paving, or similar impervious surface shall be landscaped and maintained in conjunction with the landscaping installed on the adjoining property as regulated in this chapter. Landscaping within the right-of-way shall not be used when determining the required percentage of landscaping area as required in this chapter.

(1) *Design*. The design of the landscaping of the public right-of-way shall be included in the landscape plan and shall meet the requirements set forth in this chapter. Adequate space shall be provided in the landscape area to allow free, unrestricted growth and



Appendix D: Chapter 157 Water Efficient Landscape and Irrigation

development of the landscaping and street trees.

(2) *Street trees.* Street trees shall be planted in accordance with the Theme Street List, as it may be amended from time to time, or as otherwise set forth in the Chapter 96: Trees, §§ 96.03 through 96.06 relating to street trees and in accordance with the street tree planting standards as established by the City Engineer.

(I) *Driveway and corner visibility.* All landscaping material shall be maintained in accordance with the provisions of § 96.11.

(J) *Landscaping along walls.* All solid walls over three feet in height that are adjacent to public streets or right-of-ways shall comply with one of the following:

(1) Be fully landscaped with vines and/or other plant materials to prevent the placement of graffiti. All landscaping shall include the installation of a permanent irrigation system.

(2) If not landscaped, shall be constructed of split-face concrete, brick or some other type of material that will discourage the placement of graffiti.

(K) *Landscape screening of Residential (R) properties.* Where a commercial or industrial site adjoins a Residential (R) District, screening which is at least 75% opaque shall be provided. Where fences are required, such fencing shall be landscaped as appropriate.

(L) *Model homes.* To promote landscape water conservation through education, all single family residential developments with more than two model homes to be constructed by a developer, shall provide for landscaping the models entirely with water saving landscaping and irrigation in accordance with the following requirements:

(1) *Plant materials.* Each "water saving" model home to be landscaped shall contain exclusively low water use plant materials as identified on a suggested planting list available from the Community Development Department or approved by the Community Development Director.

(2) *Irrigation system.* Each "water saving" model home shall contain exclusively an irrigation system that provides a high efficiency in water application according to site conditions. (Drip or trickle may not be used in turf areas).

(3) *Signs.* Each development with "water saving" model homes shall provide the following information to potential buyers:

(a) *Front yard sign.* A four square foot sign shall be located in the front yard of each "water saving" model such that it is clearly visible to buyers. The sign shall indicate that the model features a water saving landscape and irrigation design.

(b) *Interior display.* A drawing, or combination of drawings, shall be displayed inside each "water saving" model or the sales office which provides a schematic of the landscape. These drawings shall include a key identifying the common name of the plants used in the "water saving" model yards. A brochure with the same information may be distributed with the sales information to potential buyers to satisfy this requirement.

(4) *Literature.* Additional literature describing water conserving landscaping and irrigation shall also be made available to the potential buyer and displayed.

(Ord. 2014-005, passed 6-10-14)



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APPENDIX E

Chapter 153: Single Family Residential District R-1 Zone

Duplex Residential District R-2 Zone

Multi-Family Residential District R-3 Zone



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Appendix E: Chapter 153 Single Family Residential District R-1 Zone

SINGLE-FAMILY RESIDENTIAL DISTRICT R-1 ZONE

§ 153.031 USES PERMITTED.

(A) One single-family dwelling or one manufactured home provided that the manufactured home meets the following restrictions:

(1) Manufactured homes may be installed only if no more than ten years has elapsed between the date of manufacture and the date of application for a permit to install the manufactured home.

(2) Manufactured homes must be on a foundation system approved by resolution of the City Council.

(3) All manufactured homes shall be subject to review and approval by the Community Development Director with respect to siding material, roofing materials and roof overhang to ensure, to the greatest extent feasible, compatibility with surrounding structures. A pictorial representation of the proposed manufactured home shall be submitted with the application.

(4) No other dwelling units (i.e. single family, duplex, multiple family, apartments or dwelling groups) shall be allowed on the same lot.

(Ord. 87-11, passed 7-27-87; Am. Ord. 2002-001, passed 1-28-02)

(B) Accessory buildings are normally incidental to a single-family dwelling provided that such buildings are constructed concurrent with, or subsequent to the construction of the single-family dwelling. **NORMALLY INCIDENTAL** shall be deemed to mean that, in addition to a two car garage (either attached or detached), the size of the accessory building does not exceed 20% of the living area of the dwelling.

(1) A detached accessory building, other than a covered patio as defined herein, may occupy not more than 50% of a required rear yard, subject to the following requirements within an "R" district:

(a) No detached accessory building shall be closer than six feet to the main building, inclusive of roof covering;

(b) No detached accessory building shall be allowed within the required yard areas, inclusive of roof covering;

(c) Seven and one-half feet shall be the maximum height for a "shed" located at the setback line. A shed height may be increased one and one-half feet for every one foot back from the setback line to a maximum height of 15 feet;

(d) A detached gazebo shall not be located closer than six feet to the main building inclusive of roof covering; shall not exceed a maximum height of 15 feet.

(Ord. 88-14, passed 11-28-88; Am. Ord. 2004-001, passed 6-14-04)

(C) Family day care centers limited to six paying guests.

(D) Family day care centers for seven to 12 paying guests when the following criteria are met:

(1) At least one off-street parking space shall be provided for each employee.

(2) There are no other day care centers for more than six paying guests within 300 feet of the exterior boundaries of the subject site.

(3) In addition to the required employee parking, there shall be provided at least two off-street parking spaces for loading and unloading of guests. These spaces shall be so located that vehicles head in and head out without using the public street for maneuvering, loading or unloading.

(E) Home occupations as defined in § 153.003 and regulated in §§ 153.265 through 153.267 of this chapter.

(F) Public parks and public buildings.

(G) Temporary real estate office and sign (not to exceed 100 square feet in area) may be located on any new subdivision for a period of not more than two years from the date of recording of the map of the subdivision upon which the office and sign are located. This time limit shall automatically be extended under the conditions in § 153.285(A)(1)(g) of this chapter.



Appendix E: Chapter 153 Single Family Residential District R-1 Zone

(H) Temporary construction buildings to house tools and equipment or containing supervisory office in connection with construction projects during active construction on the same property.

(I) Residential care homes in which less than seven persons not of the immediate family are provided with food, shelter and care for compensation, but not including hospitals, clinics or similar institutions devoted primarily to diagnosis and treatment of disease or injury.

(J) Signs as allowed in §§ 153.280 through 153.285.

(K) Garage sales, as defined in § 153.003.

(L) Temporary uses such as a circus, carnival, fair or festival provided that they meet the following requirements:

(1) The use shall be temporary in nature and shall not last more than four days.

(2) The organizers of the event shall notify the Riverbank Police Services Division of their intentions at least 30 days prior to the scheduled beginning of the event.

(3) A business license shall be obtained as required by § 110.18 of the city code.

(4) Licenses will only be issued to local businesses or shopping centers when conducted on the same property as the business/shopping center or to local nonprofit organizations.

(M) Temporary mobile home for ill or aged family members as regulated by § 153.219 of the city code.

(N) Second unit meeting § 153.325.

(O) One duplex on a corner lot, provided that the front door and garage of each unit faces a different street and that garages are recessed five feet from living quarters subject to an architectural and site plan review application approved by the Community Development Director.

(67 Code, § 10-3-2) (Ord. 87-11, passed 7-27-87; Am. Ord. 2003-014, passed 11-10-03; Am. Ord. 2008-008, passed 9-8-08)

§ 153.032 USES PERMITTED WITH A USE PERMIT.

(A) Churches and other religious institutions (excluding open-air or tent).

(B) Schools, public utility, quasi-public buildings.

(C) Public or private automobile parking lots when adjacent to a C or M district or to a PD district when used for commercial or industrial purposes.

(D) Mobile home park which meets the "Mobile Home Park Standards" as adopted by resolution of the City Council.

(E) Family day care centers for seven to 12 paying guests when the requirements of § 153.031(D) of this chapter are not met.



Appendix E: Chapter 153 Duplex Residential District R-2 Zone

DUPLEX RESIDENTIAL DISTRICT R-2 ZONE

§ 153.046 USES PERMITTED.

(A) A single-family dwelling or one duplex, or two dwelling units, except that on parcels with no direct frontage on a city maintained street, only one single-family dwelling is permitted.

(B) In lieu of all the residential uses listed in division (A) above, one manufactured home per lot provided that the manufactured home meets the following restrictions:

(1) Manufactured homes may be installed only if no more than ten years has elapsed between the date of manufacture and the date of application for a permit to install the manufactured home.

(2) Manufactured homes must be on a foundation system approved by resolution of the City Council.

(3) All manufactured homes shall be subject to review and approval by the Community Development Director with respect to siding material, roofing materials and roof overhang to ensure, to the greatest extent feasible, compatibility with surrounding structures. A pictorial representation of the proposed manufactured home shall be submitted with the application.

(4) No other dwelling units (such as, single-family or duplex) shall be allowed on the same lot.

(Ord. 87-11, passed 7-27-87; Am. Ord. 2002-001, passed 1-28-02)

(C) Accessory buildings normally incidental to dwellings provided that such buildings are constructed concurrent with, or subsequent to the construction of the dwellings. **NORMALLY INCIDENTAL** shall be deemed to mean that, in addition to a garage(s) (either attached or detached) as required by this title, the size of the accessory building does not exceed 20% of the living area of the dwellings.

(Ord. 88-14, passed 11-28-88)

(D) Family day care centers limited to six paying guests.

(E) Family day care centers for seven to 12 paying guests when the following criteria are met:

(1) At least one off-street parking space shall be provided for each employee.

(2) There are no other day care centers for more than six paying guests within 300 feet of the exterior boundaries of the subject site.

(3) In addition to the required employee parking, there shall be provided at least two off-street parking spaces for loading and unloading of guests. These spaces shall be located so that vehicles head in and head out without using the public street for maneuvering, loading, or unloading.

(F) Home occupations as defined in § 153.003 and regulated in §§ 153.265 through 153.267.

(G) Public parks and public buildings.

(H) Temporary real estate office and sign (not to exceed 100 square feet in area) may be located on any new subdivision for a period of not more than two years from the date of recording of the map of the subdivision upon which the office and sign are located. This time limit shall automatically be extended under the conditions described in § 153.285(A)(1)(g).

(I) Temporary construction building to house tools and equipment or containing supervisory offices in connection with construction projects during active construction on the same property.

(J) Residential care homes in which less than seven persons not of the immediate family are provided with food, shelter and care for compensation, but not including hospitals, clinics, or similar institutions devoted primarily to diagnosis and treatment of disease or injury.

(K) Signs as allowed in §§ 153.280 through 153.285.

(L) Temporary uses such as a circus, carnival, fair or festival, provided that they meet the following requirements:

(1) The use shall be temporary in nature and shall not last more than four days.

(2) The organizers of the event shall notify the Riverbank Police Services Division of their intentions at least 30 days prior to the scheduled beginning of the event.



Appendix E: Chapter 153 Duplex Residential District R-2 Zone

(3) A business license shall be obtained as required by § 110.18 of the city code.

(4) Licenses will only be issued to local businesses or shopping centers when conducted on the same property as the business/shopping center or to local nonprofit organizations.

(M) Temporary mobile home for ill or aged family members as required by § 153.219.

(N) Garage sales as defined in § 153.003.

(Ord. 87-11, passed 7-27-87)

(67 Code, § 10-4-2)

§ 153.047 USES PERMITTED WITH A USE PERMIT.

(A) Churches and other religious institutions (excluding open-air or tent).

(B) Schools, public utilities, and quasi-public buildings.

(C) Public or private automobile parking lots when adjacent to any C or M district or to any PD district which is used for commercial or industrial purposes.

(D) Mobile home parks which meet the "Mobile Home Park Standards" as adopted by resolution of the City Council.

(E) Family day care centers for seven to 12 paying guests when the requirements of § 153.046(E) are not met.

(F) Duplexes parcels with no direct frontage on a city maintained street.

(G) Accessory buildings that do not meet the requirements of § 153.046(C).

(67 Code, § 10-4-3) (Ord. 92-06, passed 7-13-92; Am. Ord. 97-03, passed 1-27-97; Am. Ord. 2003-014, passed 11-10-03)



Appendix E: Chapter 153 Multiple Family Residential District R-3 Zone

MULTIPLE FAMILY RESIDENTIAL DISTRICT R-3 ZONE

§ 153.061 USES PERMITTED.

(A) Single-family, duplex and multiple family dwellings, apartment houses, dwelling groups, except that on parcels with no direct frontage on a city maintained street, only one single-family dwelling is permitted.

(B) In lieu of all the residential uses listed in division (A) above, one manufactured home per lot provided that the manufactured home meets the following restrictions:

(1) Manufactured homes may be installed only if no more than ten years has elapsed between the date of manufacture and the date of application for a permit to install the manufactured home.

(2) Manufactured homes must be on a foundation system approved by resolution of the City Council.

(3) All manufactured homes shall be subject to review and approval by the Community Development Director with respect to siding material, roofing materials and roof overhang to ensure, to the greatest extent feasible, compatibility with surrounding structures. A pictorial representation of the proposed manufactured home shall be submitted with the application.

(4) No other dwelling units (such as, single-family, duplex, multiple family, apartments or dwelling groups) shall be allowed on the same lot.

(Ord. 87-11, passed 7-27-87; Am. Ord. 2002-001, passed 1-28-02)

(C) Accessory buildings normally incidental to permitted uses provided that such buildings are constructed concurrent with, or subsequent to the construction of the permitted use. **NORMALLY INCIDENTAL** shall be deemed to mean that, in addition to a garage(s) (either attached or detached) as required by this chapter, the size of the accessory building does not exceed 20% of the area of the main building.

(Ord. 88-14, passed 11-28-88)

(D) Family day care centers limited to six paying guests.

(E) Family day care centers for seven to 12 paying guests when the following criteria are met:

(1) At least one off-street parking space shall be provided for each employee.

(2) There are no other day care centers for more than six paying guests within 300 feet of the exterior boundaries of the subject site.

(3) In addition to the required employee parking, there shall be provided at least two off-street parking spaces for loading and unloading of guests. These spaces shall be located so that vehicles head in and head out without using the public street for maneuvering, loading, or unloading.

(F) Home occupations as defined in § 153.003 and regulated in §§ 153.265 through 153.267.

(G) Public parks and public buildings.

(H) Temporary real estate office and sign (not to exceed 100 square feet in area) may be located on any new subdivision for a period of not more than two years from the date of recording of the map of the subdivision upon which the office and sign are located. This time limit shall automatically be extended under the conditions described in § 153.285(A)(1)(g).

(I) Temporary construction building to house tools and equipment or containing supervisory offices in connection with construction projects during active construction on the same property.

(J) Residential care homes in which less than seven persons not of the immediate family are provided with food, shelter and care for compensation, but not including hospitals, clinics, or similar institutions devoted primarily to diagnosis and treatment of disease or injury.

(K) Signs as allowed in §§ 153.280 through 153.285.

(L) Temporary uses such as a circus, carnival, fair or festival, provided that they meet the following requirements:

(1) The use shall be temporary in nature and shall not last more than four days.

(2) The organizers of the event shall notify the Riverbank Police Services Division of their intentions at least 30 days prior to the scheduled beginning of the event.



Appendix E: Chapter 153 Multiple Family Residential District R-3 Zone

(3) A business license shall be obtained as required by § 110.18 of the city code.

(4) Licenses will only be issued to local businesses or shopping centers when conducted on the same property as the business/shopping center or to local nonprofit organizations.

(M) Temporary mobile home for ill or aged family members as required by § 153.219 of the city code.

(N) Garage sales as defined in § 153.003.

(Ord. 87-11, passed 7-27-87)

(O) Second unit meeting § 153.325.

(P) Emergency shelters as defined in § 153.003.

(Q) Transitional housing as defined in § 153.003.

(R) Supportive housing as defined in § 153.003.

(67 Code, § 10-5-2) (Am. Ord. 2003-014, passed 11-10-03; Am. Ord. 2015-002, passed 2-24-15)

§ 153.062 USES PERMITTED WITH A USE PERMIT.

(A) Churches and other religious institutions (excluding open-air or tent).

(B) Schools, public utilities and quasi-public buildings.

(C) Public or private automobile parking lots when adjacent to any C or M district or to any PD district which is used for commercial or industrial purposes.

(D) Hotels, rooming or boarding houses.

(E) Professional offices.

(F) Labor camp.

(G) Institutions for treatment of alcoholics.

(H) Mobile home parks which meet the "Mobile Home Park Standards" as adopted by resolution of the City Council.

(I) Family day care centers for seven to 12 paying guests when the requirements of § 153.061(E) are not met.

(J) Duplex and multiple family dwellings, apartment houses and dwelling groups on parcels with no direct frontage on a city maintained street.

(K) Accessory buildings that do not meet the requirements of § 153.061(C).

(L) Residential care homes for more than seven persons.

(67 Code, § 10-5-3) (Ord. 92-06, passed 7-13-92; Am. Ord. 97-03, passed 1-27-97; Am. Ord. 2003-014, passed 11-10-03)



APPENDIX F

Chapter 153.36 Retail Sale of Alcoholic Beverages



Appendix F: Chapter 153.36 Retail Sale of Alcoholic Beverages

RETAIL SALE OF ALCOHOLIC BEVERAGES

§ 153.360 PURPOSE.

The general purposes of the alcoholic beverage sales regulations are to protect and promote the public health, safety, comfort, convenience, prosperity, and general welfare by requiring that alcoholic beverage sales commercial activities achieve the following objectives:

- (A) To protect residential, commercial, industrial, and civic areas and minimize the adverse impacts of nonconforming and incompatible uses;
- (B) To provide opportunities for alcoholic beverage sale activities to operate in a mutually beneficial relationship to each other and to other commercial and civic services;
- (C) To provide mechanisms to address problems often associated with the public consumption of alcoholic beverages, such as litter, loitering, graffiti, unruly behavior and escalated noise levels;
- (D) To provide that alcoholic beverage sale commercial activities are not the source of undue public nuisances in the community;
- (E) To provide for properly maintained alcoholic beverage sale establishments so that negative impacts generated by these activities are not harmful to the surrounding environment in any way;
- (F) To monitor that deemed approved activities do not substantially change in mode or character of operation.

(Ord. 2005-009, passed 6-13-05)

§ 153.361 USE PERMIT REQUIRED.

Use permits shall be required for the sale of alcoholic beverages, including beer and wine, for on or off-site consumption. In determining whether to issue a use permit, the city shall apply appropriate conditions to the use permit that insure compliance with the intent of this chapter. Upon issuance of a use permit, an alcoholic beverage sale activity shall be considered deemed approved and subject to the conditions herein.

(Ord. 2005-009, passed 6-13-05)

§ 153.362 APPLICABILITY OF DEEMED APPROVED ALCOHOLIC BEVERAGE SALE REGULATIONS.

(A) *To which property applicable.* The deemed approved alcoholic beverage sale regulations shall apply, to the extent permissible under other laws, to all legal nonconforming alcoholic beverage sale commercial activities within the city.

(B) *Duplicated regulation.* Whenever any provision of the deemed approved alcoholic beverage sale regulations and any other provision of law, whether set forth in this code, or in any other law, ordinance, or resolution of any kind, imposes overlapping or contradictory regulations, or contain restrictions covering any of the same subject matter, that provision which is more restrictive or imposes higher standards shall control, except as otherwise expressly provided in the deemed approved alcoholic beverage sale regulations.

(C) *Relationship to the zoning regulations.* The nonconforming use provisions of the zoning regulations including, but not limited to, §§ 153.195, 153.196, 153.197, 153.198, 153.199, 153.200, 153.201, 153.202 and 153.203, shall apply to the deemed approved alcoholic beverage sale regulations.

(Ord. 2005-009, passed 6-13-05)

§ 153.363 DEFINITIONS.

As used in this chapter, the following terms shall have the meanings set forth.

ALCOHOLIC BEVERAGE. Alcohol, spirits, liquor, wine, beer, and every liquid or solid containing alcohol, spirits, wine, or beer, which contains one-half of one percent or more of alcohol by volume and which is fit for beverage purposes either alone or when diluted, mixed, or combined with other substances, and sales of which requires a State Department of Alcoholic Beverage Control license.



Appendix F: Chapter 153.36 Retail Sale of Alcoholic Beverages

ALCOHOLIC BEVERAGE SALES COMMERCIAL ACTIVITY. The retail sale, for on- or off- premises consumption, of liquor, beer, wine, or other alcoholic beverages, excluding full-service restaurants that comply with the below-listed definition of full service restaurant.

CONDITION OF APPROVAL. A requirement, which must be carried out by the activity in order to retain its deemed approved status.

DEEMED APPROVED ACTIVITY. Any legal nonconforming alcoholic beverage sales commercial activity, as defined in this section, in existence immediately prior to the effective date of the deemed approved alcoholic beverage sale regulations shall be considered a **DEEMED APPROVED ACTIVITY** as long as it complies with the deemed approved performance standards as set forth in § 153.364, and shall no longer be considered a legal nonconforming activity.

DEEMED APPROVED STATUS. The status conferred upon a deemed approved activity. **DEEMED APPROVED STATUS** replaces legal nonconforming status.

FULL-SERVICE RESTAURANT. A place which is regularly and in a bona fide manner used and kept open for the serving of at least lunch and dinner to guests for compensation and which has suitable kitchen facilities connected therewith, containing conveniences for cooking an assortment of foods which may be required for such meals. The sale or service of sandwiches (whether prepared in a kitchen or made elsewhere and heated up on the premises) or snack foods shall not constitute a **FULL-SERVICE RESTAURANT**. To be considered a **FULL SERVICE RESTAURANT** under the deemed approved program, the establishment must meet the following criteria:

(1) A **FULL SERVICE RESTAURANT** shall serve meals to guests at all times the establishment is open for business. An establishment shall not be considered a **FULL-SERVICE RESTAURANT** if it served alcohol without meal service being provided with the exception that alcohol sales to restaurant patrons may continue for up to two hours after meal service has ceased to allow guests to comfortably complete their meals.

(2) There shall be a real offer or holding out to sell meals. Premises shall make an offer or holding out of sales of meals to the public by maintaining and displaying a printed menu and/or a menu board. A two-thirds majority of the items offered on the menu shall be available at any given time the establishment is open. The mere offering of meals without actual sales shall not be deemed sufficient.

(3) The offer of meals is not adequate to meet the above criteria. A **FULL SERVICE RESTAURANT** shall make actual and substantial sales of meals to guests for compensation. Substantial sales shall mean that no less than 60% of total revenue shall be generated from food service and no more than 40% of revenue from the sales of alcohol.

(4) "Meals" means the usual assortment of foods commonly ordered at various times of the day for the cuisine served. The service of snack foods and/or appetizers alone shall not be deemed compliance with this requirement. Meals shall be prepared on the premises. Heating of food prepared elsewhere shall not constitute a meal for the purposes of this policy.

(5) Premises shall be equipped for meal service and maintained in good faith. Premises must possess and maintain appliances for the cooking of a variety of foods such as stoves, ovens, broilers, or other devices, as well as pots, pans, or containers that can be used for cooking. Premises shall possess the necessary utensils, table service, and condiment dispensers with which to serve meals to the public.

(6) A **FULL SERVICE RESTAURANT** shall comply with all local health department standards.

(7) A **FULL SERVICE RESTAURANT** may have a separate lounge or bar area provided that the restaurant and bar/lounge area operate as a single entity. The physical layout, entry location(s), spatial connection between the areas, and operational characteristics, among other factors, shall be used to determine compliance. Any bar/lounge area cannot remain open when the dining area is closed. However, the dining area may be open while the bar/lounge area is closed.

(8) To the extent that ABC regulations do not conflict with the above criteria, a **FULL SERVICE RESTAURANT** shall comply with all State Department of Alcoholic Beverage Control regulations related to "Bona fide public eating place, meals."

ILLEGAL ACTIVITY. An activity which has been finally determined to be in noncompliance with the deemed approved performance standards of this chapter. Such an activity shall lose its deemed approved status and shall no longer be considered a deemed approved activity.



Appendix F: Chapter 153.36 Retail Sale of Alcoholic Beverages

LEGAL NONCONFORMING ALCOHOLIC BEVERAGE SALES COMMERCIAL ACTIVITY or **LEGAL NONCONFORMING ACTIVITY**. An alcoholic beverage sales commercial activity which was a nonconforming use pursuant to the nonconforming use regulations, and for which a valid state of California Alcoholic Beverage Control license had been issued and used in the exercise of the rights and privileges conferred by the license, at a time immediately prior to the effective date of the deemed approved alcoholic beverage sale regulations. Such an activity shall be considered a deemed approved activity, and shall no longer be considered a **LEGAL NONCONFORMING ACTIVITY**, except such activity shall be subject to those zoning regulations relating to nonconforming uses, as of the effective date of the alcoholic beverage sale regulations.

PERFORMANCE STANDARDS. Regulations prescribed in the performance standards of this chapter.

PREMISES. The actual space within a building devoted to alcoholic beverage sales.

(Ord. 2005-009, passed 6-13-05)

§ 153.364 PERFORMANCE STANDARDS AND DEEMED APPROVED ACTIVITIES.

An activity shall retain its deemed approved status only if it conforms with all of the following deemed approved performance standards:

- (A) That it does not result in adverse effects to the health, peace, or safety of persons residing or working in the surrounding area;
- (B) That it does not result in jeopardizing or endangering the public health or safety of persons residing or working in the surrounding area;
- (C) That it does not result in repeated nuisance activities within the premises or in close proximity of the premises, including but not limited to disturbance of the peace, illegal drug activity, public drunkenness, drinking in public, harassment of passersby, gambling, prostitution, sale of stolen goods, public urination, theft, assaults, batteries, acts of vandalism, excessive littering, loitering, graffiti, illegal parking, excessive loud noises, especially in the late night or early morning hours, traffic violations, curfew violations, lewd conduct, or police detentions and arrests;
- (D) That it does not result in violations to any applicable provision of any other city, state, or federal regulation, ordinance or statute;
- (E) That its upkeep and operating characteristics are compatible with and will not adversely affect the livability or appropriate development of abutting properties and the surrounding neighborhood.

(Ord. 2005-009, passed 6-13-05)

§ 153.365 AUTOMATIC DEEMED APPROVED STATUS.

All alcoholic beverage sales commercial activities that were legal activities immediately prior to the effective date of the alcoholic beverage sale regulations shall automatically become deemed approved activities as of the effective date of the regulations and shall no longer be considered legal nonconforming activities. Each such deemed approved activity shall retain its deemed approved status as long as it complies with the deemed approved performance standards.

(Ord. 2005-009, passed 6-13-05)

§ 153.366 NOTIFICATION TO OWNERS OF DEEMED APPROVED ACTIVITIES.

The Community Development Director shall notify the owner of each deemed approved activity, and also the property owner if not the same, of the activity's deemed approved status. Such notice shall be sent via certified return receipt mail and shall include a copy of the performance standards of this chapter with the requirement that these be posted in a conspicuous and unobstructed place visible from the entrance of the establishment for public review, notification that the activity is required to comply with all these same performance standards and that a review fee is required, and the amount of such fee provided in the master fee schedule, and that the activity is required to comply with all other aspects of the deemed approved alcoholic beverage sale regulations. Should the notice be returned, the notice shall be sent via regular U.S. Mail.

(Ord. 2005-009, passed 6-13-05)



Appendix F: Chapter 153.36 Retail Sale of Alcoholic Beverages

§ 153.367 PROCEDURE FOR CONSIDERATION OF VIOLATIONS TO PERFORMANCE STANDARDS.

(A) Upon receiving a complaint from the public, Police Department, or any other interested party that a deemed approved activity is in violation of the performance, and once it is determined by the city that violations appear to be occurring, then the deemed approved status of the deemed approved activity in question shall be reviewed by the Planning Commission at a public hearing. Notification of the public hearing shall be in accordance with adopted city standards.

(B) The purpose of the public hearing is to receive testimony on whether the operating methods of the deemed approved activity are causing undue negative impacts in the surrounding area. At the public hearing, the Planning Commission shall determine whether the deemed approved activity conforms to the deemed approved performance standards and to any other applicable criteria, and may continue the deemed approved status for the activity in question or require such changes or impose such reasonable conditions of approval as are in the judgment of the Planning Commission necessary to ensure conformity to said criteria and such conditions shall be based on the evidence before the Planning Commission. The decision of the Planning Commission shall be based upon information compiled by staff and testimony from the business owner and all other interested parties. New conditions of approval shall be made a part of the deemed approved status and the deemed approved activity shall be required to comply with these conditions. The determination of the Planning Commission shall become final ten calendar days after the date of decision unless appealed to the City Council.

(Ord. 2005-009, passed 6-13-05)

§ 153.368 APPEAL ON THE REVOCATION OF A DEEMED APPROVED STATUS TO THE CITY COUNCIL.

(A) Within ten calendar days after the date of a decision by the Planning Commission to revoke a deemed approved status, an appeal from said decision may be taken to the City Council by any interested party. In the event the last date of appeal falls on a weekend or holiday when city offices are closed, the next date such offices are open for business shall be the last date of appeal. Such appeal shall be made on a form prescribed by the Planning Commission and shall be filed with the City Clerk. The appeal shall state specifically wherein it is claimed there was an error or abuse of discretion by the Commission or wherein its decision is not supported by the evidence in the record. Upon receipt of the appeal and an appeal fee in accordance with this section, the Council shall set the date for consideration thereof. The City Clerk shall notify the Secretary of the City Planning Commission of the receipt of said appeal and of the date set for consideration thereof; and said Secretary shall, not less than ten days prior thereto, give written notice to: the owner of the deemed approved activity; the property owner; adverse party or parties, or to the attorney, spokesperson, or representative of such party or parties; other interested groups and neighborhood associations who have requested notification; and to similar groups and individuals as the Secretary deems appropriate, of the time, date, and place of the hearing on the appeal. In considering the appeal, the Council shall determine whether the deemed approved activity conforms to the applicable deemed approved performance standards, and may approve or disapprove the revocation or require such changes therein or impose such reasonable conditions of approval as are in its judgment necessary to ensure conformity to said standards.

(B) The decision of the City Council shall be made by resolution and shall be final. The City Council shall vote on the appeal within 30 days after its first hearing of the appeal. If the Council is unable to decide the appeal at that meeting, it shall appear for a vote on each regular meeting of the Council thereafter until decided.

(Ord. 2005-009, passed 6-13-05)

§ 153.369 NOTIFICATION OF PUBLIC HEARING.

The Community Development Director shall notify the owner of each deemed approved activity, and also the property owner if not the same, of the time and place of the public hearing. Such notice shall be sent via certified return receipt mail and shall include notification that the deemed approved status of the deemed approved activity will be considered before the Officer. The public hearing shall be noticed by posting notices within 300 feet of the subject property; notice shall also be given by mail or delivery to all persons shown on the last available equalized assessment roll as owning real property in the city within 300 feet of the subject property. All such notices shall be given not less than ten days prior to the date set for the hearing, if such is to be held. Fees for notification shall be in accordance with city regulations and paid for by the deemed approved activity in question.

(A) *Notice on site.* A city-provided notice of 8 1/2 x 11 inches in dimension shall also be posted on the premises of the subject activity, placed in the window of the activity. (If a window facing the street is not present, then the placard will be required to be posted onto the exterior of the building.) All notices shall advertise the time, date, purpose, and location of the public hearing for each particular site. All notices shall be given not less than ten days prior to the date set for the hearing.

(B) *Notice by mail.* Notice by mail is deemed given on the date the notice is placed into the U.S. Mail system.

(Ord. 2005-009, passed 6-13-05)



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APPENDIX G

City of Riverbank Street Tree List





Riverbank Street Tree List

Botanical Name	Common Name	Water Use	Type	Height (Ft.)	Width (Ft.)	Power Lines	USDA Zone	Use
<i>Acer platanoides</i>	Norway Maple	Moderate	D	40-50	30-50	N	3-7	S
<i>Acer rubrum</i>	Red Maple	Moderate	D	40-70	30-50	N	3-9	S
<i>Afrocarpus gracilior</i> (<i>Podocarpus gracilior</i>)	African Fern Pine	Moderate	E	40-60	20-43	N	9-11	S
<i>Arbutus</i> "Marina"	Marina Arbutus	Low	E	40-50	25-40	N	8-9	A
<i>Carpinus betulus</i> 'Fastigiata'	European Hornbeam	Moderate	D	30-40	20-30	N	4-8	S
<i>Celtis occidentalis</i>	Common Hackberry	Low	D	40-60	40-60	N	2-9	S
<i>Cercis canadensis</i>	Eastern Redbud	Moderate	D	20-30	25-35	Y	4-8	S
<i>Chilopsis linearis</i>	Desert Willow	Very Low	D	12-20	10-15	Y	6-9	A
<i>Chionanthus retusus</i>	Chinese Fringe Tree	Moderate	D	10-20	10-20	Y	5-9	A
<i>Crataegus</i> spp. (Native and non-native species)	Hawthorn	Moderate	D	20-35	20-35	Y	3-8	S
<i>Fraxinus americana</i>	White Ash	Moderate	D	60-80	60-80	N	3-9	S
<i>Fraxinus uhdei</i>	Evergreen Ash	Moderate	E	60-80	40-60	N	9-11	S
<i>Ginkgo biloba</i> (MALE only)	Maiden Hair Tree	Moderate	D	35-50	35-50	N	4-9	S
<i>Koelreuteria paniculata</i>	Golden Rain Tree	Moderate	D	30-40	30-40	N	5-9	S
<i>Lagerstroemia</i> spp., hybrids and cvs.	Crape Myrtle	Low	D	10-30	10-30	Y	6-9	A
<i>Laurus</i> "Saratoga"	Saratoga Laurel	Low	E	20-30	15-25	Y	5-9	S
<i>Laurus nobilis</i>	Sweet Bay	Low	E	10-25	10-20	Y	8-11	A
<i>Liriodendron tulipifera</i>	Tulip Tree	Moderate	D	60-90	30-50	N	4-9	S
<i>Magnolia grandiflora</i>	Southern Magnolia	Moderate	E	60-80	30-50	N	7-9	S
<i>Magnolia x soulangeana</i>	Saucer Magnolia	Moderate	D	20-25	20-25	Y	4-9	A
<i>Melaleuca linariifolia</i>	Flax Leaf Paper Bark	Low	E	20-30	10-20	Y	9-11	A
<i>Parkinsonia</i> 'Desert Museum'	Desert Museum Palo Verde	Very Low	D	15-25	15-25	Y	6-9	A
<i>Parkinsonia florida</i> (<i>Cercidium florida</i>)	Blue Palo Verde	Very Low	D	25-35	20-30	N	9-11	A
<i>Pistacia chinensis</i>	Chinese Pistache	Low	D	30-35	20-30	N	6-9	S
<i>Podocarpus macrophyllus</i>	Yew Pine	Moderate	E	15-20	6-10	Y	7-11	A
<i>Prunus caroliniana</i>	Carolina Laurel Cherry	Low	E	15-20	15-20	Y	7-10	A
<i>Prunus cerasifera</i> 'Krauter Vesuvius'	Cherry Plum	Low	D	15-20	15-20	Y	5-8	A
<i>Quercus agrifolia</i>	Coast Live Oak	Very Low	E	20-60	20-60	N	7-10	S
<i>Quercus coccinea</i>	Scarlet Oak	Moderate	D	50-70	40-50	N	4-9	S
<i>Quercus douglasii</i>	Blue Oak	Very Low	D	20-30	15-25	N	6-9	S
<i>Quercus ilex</i>	Holly Oak	Low	E	30-60	30-60	N	7-9	S
<i>Quercus lobata</i>	Valley Oak	Low	D	30-75	30-50	N	5-10	S
<i>Quercus palustris</i>	Pin Oak	Moderate	D	50-70	40-60	N	4-8	S
<i>Quercus robur</i>	English Oak	Moderate	D	40-70	40-70	N	5-8	S
<i>Quercus rubra</i>	Red Oak	Moderate	D	50-75	50-75	N	4-8	S
<i>Quercus suber</i>	Cork Oak	Low	E	40-70	40-70	N	8-10	S
<i>Quercus virginiana</i>	Southern Live Oak	Moderate	E	40-80	60-100	N	8-10	S
<i>Raphiolepis</i> "Majestic Beauty"	Majestic Beauty	Low	E	15-25	8-10	Y	7-11	A
<i>Styphnolobium japonicum</i> (<i>Sophora japonica</i>)	Japanese Pagoda Tree	Low	D	50-75	50-75	N	4-8	A
<i>Thuja plicata</i>	Western Red Cedar	Moderate	E	50-70	15-25	N	5-7	A
<i>Ulmus parvifolia</i>	Chinese Evergreen Elm	Moderate	D	40-50	25-40	N	4-9	S
<i>Zelkova serrata</i>	Saw Leaf Zelkova	Moderate	D	50-80	50-80	N	5-8	S

Small Trees 15' - 25' Height - Minimum Recommended Planter Width 4'

Medium Trees 25' - 35' Height - Minimum Recommended Planter Width 6'

Medium to Large Trees 36' - 50' Height - Minimum Recommended Planter Width 8'

Very Large Trees > 50' - Minimum Recommended Planter Width 10'

Type:

- D = Deciduous
- E = Evergreen

Use:

- A = Accent Tree
- S = Street Tree

