

DRAWINGS HAVE BEEN REVISED PER CONTRACTOR'S CONSTRUCTION RECORDS  
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# AS-BUILT DICK'S DRIVE-IN 24050 PACIFIC HWY S. KENT, WA 98032

## CITY OF KENT GENERAL SEWER NOTES

- ALL SANITARY SEWER IMPROVEMENTS SHALL BE CONSTRUCTED IN ACCORDANCE WITH APPROVED PLANS. ANY DEVIATION FROM THESE PLANS WILL REQUIRE PRIOR APPROVAL FROM THE OWNER, THE CITY ENGINEER, AND OTHER APPROPRIATE PUBLIC AGENCIES.
- ALL MANHOLES SHALL MEET THE REQUIREMENTS OF STANDARD DETAIL 4-1(B) OF THE CITY OF KENT CONSTRUCTION STANDARDS. WITH ECCENTRIC CONES AND 3/4 INCH DIAMETER SAFETY STEPS OR LADERS. ALL MANHOLE COVERS SHALL MEET THE REQUIREMENTS OF STANDARD DETAIL 4-1(A) OF THE CITY OF KENT CONSTRUCTION STANDARDS. ALL MANHOLE COVERS SHALL MEET THE REQUIREMENTS OF STANDARD DETAILS 4-6(A) & 4-6(B). ALL SANITARY SEWER MANHOLES SHALL BE CHANNELLED TO MEET THE REQUIREMENTS OF WSDOT STANDARD PLAN B-23. ALL MANHOLE COVERS SHALL BE ADJUSTED TO FINISH GRADE AFTER THE COMPLETION OF FINAL PAVING AND LANDSCAPING.
- ALL SANITARY SEWER STUBS SHALL CONFORM TO ONE OF THE FOLLOWING SPECIFICATIONS:  
a. DUCTILE IRON - CLASS 50 (ONLY WHERE SPECIFIED ON THE PLANS).  
b. PVC CONFORMING TO ASTM D-3034, SDR 35, OR CONFORMING TO ASTM D-3035, SDR 21 WITH FITTINGS AND GASKETS CONFORMING TO THE CITY OF KENT CONSTRUCTION STANDARDS FOR THE DEPTH OF COVER.
- ALL DUCTILE IRON PIPE SHALL HAVE CLASS C DESIGN PIPE BEDDING AND ALL PVC PIPE SHALL HAVE CLASS F DESIGN PIPE BEDDING MEETING THE REQUIREMENTS OF WSDOT STANDARD PLAN B-18C.
- ALL TRENCH BACKFILL SHALL BE COMPACTED TO AT LEAST 95 PERCENT OF THE MAXIMUM DRY DENSITY PER ASTM D-1557 (MODIFIED PROCTOR), FOR THE FIRST THREE FEET ABOVE THE PIPE. THE REMAINING BACKFILL MAY BE AT 90 PERCENT OF MAXIMUM DRY DENSITY. NOT AN AREA OF FUTURE PAVEMENT OR STRUCTURAL FLOOR. OTHERWISE, THE REMAINING BACKFILL SHALL BE COMPACTED TO AT LEAST 95 PERCENT OF THE MAXIMUM DRY DENSITY TRENCH BACKFILL AND COMPACTION SHALL BE COMPLETED PRIOR TO TESTING LINES FOR LEAKAGE.
- ALL SIDE SEWER STUBS SHALL HAVE A MINIMUM 6-INCH DIAMETER AND A MINIMUM SLOPE OF 1.0 PERCENT. PROPERTY/LOT CORNERS AND/OR BUILDING LOCATIONS AND FINISH GRADES SHALL BE ESTABLISHED IN THE FIELD PRIOR TO ANY SEWER CONSTRUCTION. SIDE SEWER TEE CONNECTIONS SHALL BE REFERENCED FROM THE NEAREST DOWNSTREAM MANHOLE. ALL SIDE SEWERS SHALL BE TESTED AT THE TIME OF MAIN LINE TESTING. THE CONTRACTOR SHALL PROVIDE THE CITY WITH ACCURATE AS-BUILT DRAWINGS OF ALL SIDE SEWER LOCATIONS. SEE CITY OF KENT DEVELOPMENT ASSISTANCE BROCHURE (HEREAFTER DAB) #E-6, AS-BUILT DRAWINGS, FOR ADDITIONAL INFORMATION REGARDING AS-BUILT DRAWINGS.
- WATER MAINS SHALL NOT EXIST ABOVE THE SANITARY SEWER, AND LESS THAN 1.5 FEET OF VERTICAL CLEARANCE EXISTS BETWEEN THE TOP OF THE SEWER MAIN AND BOTTOM OF WATER MAIN. THE SEWER MAIN SHALL BE CONSTRUCTED, OR RECONSTRUCTED, WITH DUCTILE IRON PIPE (CLASS 50) CENTERED ON THE WATER MAIN FOR A MINIMUM HORIZONTAL DISTANCE OF ONE CONTINUOUS 18-FEET NOMINAL PIPE LENGTH WITH NO JOINTS.
- SANITARY SEWER MAIN LINES 8-INCH DIAMETER AND LARGER SHALL NOT BE CONSTRUCTED ANY CLOSER THAN 10 FEET TO ANY BUILDING OR 7.5 FEET TO ANY CARPORT.
- NEW CONNECTIONS TO EXISTING FACILITIES SHALL BE SEALED OFF UNTIL UPSTREAM CONSTRUCTION IS FINISHED, TESTED, CLEANED AND ACCEPTED; ALL CONSTRUCTION DUST AND WATER SHALL BE REMOVED PRIOR TO OPENING THE SEAL. ALL TV INSPECTION AND PRESSURE TESTING SHALL BE IN THE PRESENCE OF A CITY OF KENT INSPECTOR.
- FOR PUBLIC SANITARY SEWERS, AN EASEMENT MUST BE PREPARED BY A PROFESSIONAL LAND SURVEYOR LICENSED IN THE STATE OF WASHINGTON, AND CONVEYED TO THE CITY OF KENT ON FORMS APPROVED BY AND/OR PROVIDED BY THE CITY. EASEMENTS SHOULD BE 15 FEET WIDE (OR 25 FEET, AS SHOWN), 7.5 OR 12.5 FEET ON EACH SIDE OF THE PIPE.
- PRIOR TO ACCEPTANCE OF CONSTRUCTION AND/OR ISSUANCE OF CERTIFICATE OF OCCUPANCY PERMITS, SURVEYOR-CERTIFIED AS-BUILT DRAWINGS PREPARED ON MYLAR MUST BE PREPARED OF ALL PUBLIC SANITARY SEWER FACILITIES AND APPROVED BY THE CITY. SEE DAB #E-6, AS-BUILT DRAWINGS, FOR ADDITIONAL INFORMATION REGARDING AS-BUILT DRAWINGS.

## CITY OF KENT GENERAL CONSTRUCTION NOTES

- ALL WORK AND MATERIALS SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE CITY OF KENT CONSTRUCTION STANDARDS. THESE STANDARDS INCORPORATE BY REFERENCE THE LATEST EDITION AND PUBLISHED AMENDMENTS OF THE STANDARD SPECIFICATIONS FOR ROADS, BRIDGES, AND MUNICIPAL CONSTRUCTION, PREPARED JOINTLY BY WSDOT & APWA. THE CITY HAS ALSO ADOPTED SPECIAL PROVISIONS, WHICH MODIFY THESE STANDARD SPECIFICATIONS, AND THE CITY OF KENT SPECIAL PROVISIONS WILL GOVERN WHERE ANY CONFLICTS OCCUR.
- PRIOR TO ANY CONSTRUCTION ACTIVITY, THE CONTRACTOR SHALL ATTEND A PRE-CONSTRUCTION CONFERENCE WITH THE CITY OF KENT CONSTRUCTION AND INSPECTION DIVISION. THE CONTRACTOR SHALL SCHEDULE A PRE-CONSTRUCTION CONFERENCE AT 253-856-5500. PRIOR TO SCHEDULING, THE CONTRACTOR MUST SUBMIT AND RECEIVE APPROVAL FOR TRAFFIC CONTROL PLANS (TCP), TEMPORARY EROSION/SEDIMENTATION CONTROL PLANS (TECP), CONSTRUCTION AND PERFORMANCE BONDS, AND PROOF OF INSURANCE COVERAGE. SEE DEVELOPMENT ASSISTANCE BROCHURE (DAB) #11, INSURANCE REQUIREMENTS FOR CITY OF KENT PERMITS FOR INSURANCE INFORMATION.
- APPROVED PLANS MUST BE ON THE JOB SITE WHENEVER CONSTRUCTION IS IN PROGRESS.
- ALL SITE WORK IMPROVEMENT SHALL BE CONSTRUCTED IN ACCORDANCE WITH APPROVED PLANS. ANY DEVIATION FROM APPROVED PLANS WILL REQUIRE PRIOR APPROVAL FROM THE OWNER, THE CITY ENGINEER, AND OTHER APPROPRIATE PUBLIC AGENCIES.
- IT SHALL BE THE SOLE RESPONSIBILITY OF THE CONTRACTOR TO OBTAIN STREET USE PERMITS AND ANY OTHER REQUIRED PERMITS TO CONDUCT CONSTRUCTION. THE CONTRACTOR SHALL NOT BE ALLOWED WITHOUT PRIOR CITY OF KENT APPROVAL.
- IT IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR TO INDEPENDENTLY VERIFY ALL KNOWN UTILITY LOCATIONS, AND TO DISCOVER AND AVOID ANY PREVIOUSLY UNKNOWN UTILITIES THAT MAY BE AFFECTED. THE CONTRACTOR SHALL CONTACT THE UNDERGROUND UTILITIES LOCATION SERVICE (1-800-424-5555) AT LEAST 48 HOURS PRIOR TO CONSTRUCTION. THE OWNER, HIS REPRESENTATIVE, AND THE CITY ENGINEER SHALL BE CONTACTED IMMEDIATELY WHEN CONFLICTS ARE FOUND TO EXIST.
- THE CONTRACTOR SHALL KEEP ON-SITE AND OFF-SITE STREETS CLEAN AT ALL TIMES BY CLEANING WITH A COMBINATION SWEEPING AND VACUUM TRUCK. WASHING OF STREETS WILL NOT BE ALLOWED WITHOUT PRIOR CITY OF KENT APPROVAL.
- CUTS OF EXISTING PUBLIC ROADWAYS SHALL BE BACKFILLED AND COMPACTED IN ACCORDANCE WITH THE CITY OF KENT CONSTRUCTION STANDARDS. CUTS OF EXISTING ROADWAYS SHALL BE ALONG THE CENTERLINE OF THE ROADWAY. WHERE CUT LINES, IF THE PAVEMENT IS RE-OPENED TO TRAFFIC, A TEMPORARY CONCRETE MIX PATCH MUST BE PLACED IMMEDIATELY AFTER TRENCH BACKFILL AND COMPACTION. THIS TEMPORARY PATCH SHALL BE REPLACED WITH AT LEAST 3 INCHES OF COMPACTED ASPHALT CONCRETE PAVEMENT CLASS B, 6 INCHES OF CRUSHED SURFACING TOP COURSE, OR AS REQUIRED DEPENDENT UPON A SOILS ENGINEER'S RECOMMENDATION AND TEST. IN NO CASE SHALL THE REPLACEMENT PAVEMENT SECTION BE LESS THAN THE EXISTING PAVEMENT SECTION.
- THE CONTRACTOR SHALL FURNISH, INSTALL AND OPERATE ALL EQUIPMENT NECESSARY TO KEEP EXCAVATIONS ABOVE THE FOUNDATION LEVEL FREE FROM WATER DURING CONSTRUCTION, AND SHALL Dewater AND DISPOSE OF THE WATER SO AS NOT TO CAUSE INJURY TO PUBLIC OR PRIVATE PROPERTY OR NUISANCE TO THE PUBLIC. THE CONTRACTOR SHALL AT ALL TIMES HAVE ADEQUATE PUMPING EQUIPMENT AND GOOD WORKING CONDITIONS FOR ALL EMERGENCIES, INCLUDING POWER OUTAGES, AND SHALL HAVE AVAILABLE AT ALL TIMES COMPETENT WORKERS FOR THE OPERATION OF THE PUMPING EQUIPMENT.

## CITY OF KENT STORM DRAINAGE GENERAL NOTES

- ALL STORM DRAINAGE IMPROVEMENTS SHALL BE CONSTRUCTED IN ACCORDANCE WITH THESE APPROVED ANY DEVIATION FROM THESE PLANS WILL REQUIRE PRIOR APPROVAL FROM THE OWNER, THE CITY ENGINEER, AND OTHER APPROPRIATE PUBLIC AGENCIES.
- IT IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR TO INDEPENDENTLY VERIFY ALL KNOWN UTILITY LOCATIONS, AND TO DISCOVER AND AVOID ANY PREVIOUSLY UNKNOWN UTILITIES THAT MAY BE AFFECTED. THE CONTRACTOR SHALL CONTACT THE UNDERGROUND UTILITIES LOCATION SERVICE (1-800-424-5555) AT LEAST 48 HOURS PRIOR TO CONSTRUCTION. THE OWNER, HIS REPRESENTATIVE, AND THE CITY ENGINEER SHALL BE CONTACTED IMMEDIATELY WHEN CONFLICTS ARE FOUND TO EXIST.
- ALL STORM DRAINAGE STRUCTURES, AS DESIGNATED IN THE PLANS, SHALL BE ONE OF THE FOLLOWING:
  - YARD DRAIN - ASSOCIATED SAND AND GRAVEL CB-10, OR EQUAL.
  - INLET - WSDOT STANDARD PLAN B-4: ASSOCIATED SAND & GRAVEL CB-17, OR EQUAL.
  - TYPE I-C CATCH BASIN - CITY OF KENT STANDARD DETAIL 5-7: ASSOCIATED SAND AND GRAVEL CB-15, OR EQUAL.
  - TYPE I-D CATCH BASIN - ASSOCIATED SAND AND GRAVEL CB-6, OR EQUAL.
  - TYPE II, 48 INCH DIAMETER CATCH BASIN - CITY OF KENT STANDARD DETAIL 5-8(A) (WITH MINIMUM 24 INCHES DEEP DUMP BELOW PIPE INVERT): ASSOCIATED SAND AND GRAVEL CB-19, OR EQUAL.
  - TYPE II, 54 INCH DIAMETER CATCH BASIN - CITY OF KENT STANDARD DETAIL 5-8(A) (WITH MINIMUM 24 INCHES DEEP DUMP BELOW PIPE INVERT): ASSOCIATED SAND AND GRAVEL 54 INCHES PRE-CAST CATCH BASIN, OR EQUAL.
  - TYPE II, 72 INCH DIAMETER CATCH BASIN - CITY OF KENT STANDARD DETAIL 5-8(B) (WITH MINIMUM 24 INCHES DEEP DUMP BELOW PIPE INVERT): ASSOCIATED SAND AND GRAVEL 72 INCH PRE-CAST CATCH BASIN, OR EQUAL.
  - SPECIAI CATCH BASINS OR DRAINAGE STRUCTURES SHALL BE AS DETAILED IN THESE PLANS.
- ALL UNIT COVERS SHALL BE PEEL CITY OF KENT CONSTRUCTION STANDARDS AND ALL STRUCTURES DEEPER THAN FIVE FEET (TRIM TO INVERT) SHALL BE TYPE II AND EQUIPPED WITH 3/4 INCH DIAMETER SAFETY MANHOLE STEPS OR A MAN HOLE LADDER, PER CITY OF KENT CONSTRUCTION STANDARDS.
- ALL STORM SEWER MANHOLES AND TEE CONNECTIONS SHALL BE PEEL CITY OF KENT STANDARD DETAIL 5-8(A) AND 70% OF THE CITY OF KENT CONSTRUCTION STANDARDS, WITH ECCENTRIC CONES AND 3/4 INCH DIAMETER SAFETY STEPS OR LADDER. ALL STORM SEWER MANHOLE COVERS SHALL MEET THE REQUIREMENTS OF STANDARD DETAIL 4-5 OF THE CITY OF KENT CONSTRUCTION STANDARDS AND SHALL READ "STORM". ALL MANHOLES OUTSIDE OF PUBLIC RIGHT-OF-WAY AND NOT LOCATED IN PAVED AREAS SHALL HAVE SOLID, LOCKING COVERS MEETING THE REQUIREMENTS OF STANDARD DETAILS 4-6(A) & 4-6(B) OF THE CITY OF KENT CONSTRUCTION STANDARDS. ALL STORM SEWER MANHOLES SHALL HAVE A MINIMUM 0.10 FOOT TO A MAXIMUM 1.0 FOOT DROP BETWEEN INVERTS INTO AND INVERTS OUT OF THE MANHOLE. ALL MANHOLE COVERS SHALL BE ADJUSTED TO FINISH GRADE AFTER FINAL PAVING AND LANDSCAPING.
- ALL FRAMES, GRATES AND/OR SOLID COVERS SHALL BE CAST IRON OR DUCTILE IRON AND SHALL MEET CITY OF KENT CONSTRUCTION STANDARDS.
  - STANDARD FRAMES AND GRATE - CITY OF KENT STANDARD DETAIL 5-1(A) AND 5-1(B): OLYMPIC FOUNDRY CO. NO. 4426, 2ND AVE NE SEATTLE, WA 98105
  - SELF-LOCKING VANEY GRATE - CITY OF KENT STANDARD DETAIL 5-1(D): OLYMPIC FOUNDRY CO. NO. SM505 SL OR EQUAL.
  - SOLID, LOCKING, RECTANGULAR COVER - CITY OF KENT STANDARD DETAIL 5-1(C): OLYMPIC FOUNDRY CO. NO. SM505 D/T OR EQUAL.
  - SOLID, LOCKING, ROUND COVER - CITY OF KENT STANDARD DETAILS 4-5, 4-6(A) & 4-6(B): OLYMPIC FOUNDRY CO. NO. MH30 D/T, OR MH36 W/T, RESPECTIVELY, OR EQUAL.
- NOTES: (1) ALL GRATES SHALL BE MARKED "DRAIN". (3) ALL GRATES SHALL BE DEPRESSED 0.04 FEET FROM SURROUNDING CURB OR PAVEMENT TO INSURE ADEQUATE DRAINAGE. (4) THE PAVEMENT ADJACENT TO ALL GRATES AND COVERS WILL BE PAINTED USING THE STANDARD POLLUTION PREVENTION STENCIL:  
(FISH SYMBOL)  
DRAINS TO STREAM
- STENCILS ARE AVAILABLE AT THE CITY OF KENT ENGINEERING OFFICE, LOCATED AT 400 WEST GOWE, KENT, WASHINGTON.  
NOTES:
  - OLYMPIC FOUNDRY SELF-LOCK VANEY GRATE SM505SL IS AN ACCEPTABLE ALTERNATIVE SUBJECT TO APPROVAL BY THE CITY ENGINEER.
  - USE WITH TWO LOCKING BOLTS 5/8 INCH DIAMETER 11 UNC-2A STAINLESS TYPE 304 STEEL SOCKET HEAD (ALLEN HEAD) CAP SCREWS 2 INCHES LONG, AS SPECIFIED.
  - III. MATERIALS USED FOR VALVE BOXES:  
a. DUCTILE IRON ASTM A53, GRADE B9 55-06.
  - ALL OUTLET DRAIN PIPE MAY BE CONSTRUCTED OF ONE OF THE FOLLOWING MATERIALS UNLESS OTHERWISE SPECIFIED IN THE PLANS. ALL PIPE JOINTS MUST HAVE GASKETS AND SHALL BE WATERTIGHT; ALL COUPLING BANDS SHALL BE MADE OF THE SAME MATERIAL AS THE PIPE. ALL PIPES SHALL HAVE THE MINIMUM COVER SPECIFIED BELOW AND SHALL BE ADEQUATELY PROTECTED DURING CONSTRUCTION. (REFER TO THE MANUFACTURER'S RECOMMENDATIONS FOR MINIMUM COVER FOR HEAVY EQUIPMENT LOADINGS).
    - DUCTILE IRON - 4 INCH THROUGH 14 INCH DIAMETER PIPE SHALL BE CLASS 50, AND 16 INCH THROUGH 24 INCH DIAMETER PIPE SHALL BE CLASS 52, ALL IN ACCORDANCE WITH USA STANDARD A-21.1 (AWWA C-51). ALL PIPE JOINTS SHALL BE PUSH-ON, MECHANICAL, OR FLANGED. MINIMUM COVER IS 6 INCHES.
    - CONCRETE - 4 INCH THROUGH 12 INCH DIAMETER PIPE MAY BE NON-REINFORCED, BELL AND SPIGOT WITH RUBBER GASKET JOINTS, CONFORM TO ASTM C-4 (CLASS I). ALL CONCRETE PIPES 18 INCH THROUGH 36 INCH DIAMETER SHALL BE CLASS 50, AND 48 INCH DIAMETER BELL AND SPIGOT WITH RUBBER GASKET JOINTS, CONFORMING TO C-76. MINIMUM COVER IS 2 FEET.
    - PVC SDR 21 4 INCH THROUGH 18 INCH DIAMETER PIPE SHALL BE IN ACCORDANCE WITH ASTM D3035, AND SHALL HAVE AT LEAST 24 INCHES OF COVER. SDR PIPES 4 INCH THROUGH 18 INCH DIAMETER PIPE SHALL BE IN ACCORDANCE WITH ASTM D3034, AND SHALL HAVE AT LEAST 36 INCHES MINIMUM COVER. ALL JOINTS SHALL BE PUSH-ON WITH RUBBER GASKETS. PVC STORM PIPE REQUIRES KOR-N-SEAL CONCRETE BOOTS.
    - HELICAL CORROUGATED ALUMINUM PIPE (HCP) 6 INCH THROUGH 18 INCH DIAMETER PIPE SHALL BE 16 GAUGE MINI M 1 WITH 2-1/2 INCH X 1/2 INCH CORRUGATIONS. REFER TO DETAILS IN THESE PLANS FOR ANY PIPE LARGER THAN 36 INCH DIAMETER. MINIMUM COVER IS 2 FEET.
    - HELICAL CORROUGATED STEEL PIPE (HSP) 6 INCH THROUGH 36 INCH PIPES SHALL BE AT LEAST 16 GAUGE WITH 2-1/2 INCH X 1/2 INCH CORRUGATIONS. ALL STEEL PIPES SHALL BE GALVANIZED AND PROTECTED BY COAT TREATMENT. REFER TO DETAILS IN THESE PLANS FOR ANY PIPES LARGER THAN 36 INCHES IN DIAMETER. MINIMUM COVER IS 2 FEET.
  - ALL PIPE BEDDING SHALL CONFORM TO CLASS F DESIGN FOR FLEXIBLE PIPE (I.E. PVC, HCAP, HCR, OR ADS). ALL RIGID PIPE (I.E. DUCTILE IRON OR CONCRETE) BEDDING SHALL CONFORM TO CLASS B FOR PIPE COVER GREATER THAN 36 INCHES OR APWA TYPE "B" FOR PIPE COVER LESS THAN 36 INCHES. BEDDING MATERIAL SHALL BE 5/8 INCHES MINUS CRUSHED ROCK FOR ALL PIPE EXCEPT FOR DUCTILE IRON. BEDDING MATERIAL FOR DUCTILE IRON PIPE SHALL MEET THE REQUIREMENTS OF THE CITY OF KENT SPECIAL PROVISIONS.
  - ALL TRENCH BACKFILL IN AREAS OF FUTURE PAVEMENT OR STRUCTURAL LOADING SHALL BE COMPACTED TO AT LEAST 95 PERCENT OF THE MAXIMUM DRY DENSITY PER ASTM D-1557 (MODIFIED PROCTOR). ALL OTHER AREAS SHALL BE COMPACTED TO AT LEAST 90 PERCENT OF MAXIMUM DRY DENSITY.
  - CONSTRUCTION OF DEWATERING (GROUNDWATER INTERCEPTION) SYSTEMS SHALL BE IN ACCORDANCE WITH APPROVED PLANS.
  - PRIOR TO ACCEPTANCE OF CONSTRUCTION AND/OR ISSUANCE OF CERTIFICATE OF OCCUPANCY PERMITS, SURVEYOR-CERTIFIED AS-BUILT DRAWINGS PREPARED ON MYLAR MUST BE PREPARED OF ALL STORMWATER MANAGEMENT FACILITIES AND APPROVED BY THE CITY. SEE CITY OF KENT DEVELOPMENT ASSISTANCE BROCHURE #E-6, AS-BUILT DRAWINGS, FOR ADDITIONAL INFORMATION REGARDING AS-BUILT DRAWINGS.
  - ALL RETENTION/DETENTION FACILITIES WILL BE INSTALLED AND IN OPERATION PRIOR TO, OR IN CONJUNCTION WITH, ALL CONSTRUCTION ACTIVITY UNLESS THAT ACTIVITY EXCEEDS THE CAPACITY AND IN TENT OF THE EROSION/SEDIMENTATION CONTROL FACILITY, OR UNLESS OTHERWISE APPROVED BY THE CITY.
  - WHEN ALUMINUM PIPE IS USED WHERE IT WILL BE IN CONTACT WITH CONCRETE, ALL ALUMINUM SURFACES IN CONTACT WITH THE CONCRETE SHALL BE PAINTED WITH TWO COATS OF ZINC CHROMATE PAINT IN ACCORDANCE WITH SECTION 7-02.3(1) B.2. OF THE WSDOT/APWA STANDARD SPECIFICATIONS FOR ROADS, BRIDGES, AND MUNICIPAL CONSTRUCTION.
  - BIO-FILTRATION SWALES AND/OR FILTER STRIPS SHALL BE CONSTRUCTED, BEDDED OR SEEDED AND IN OPERATION PRIOR TO, OR SODDED IN CONJUNCTION WITH, ASPHALT PAVING. THE VEGETATION IN THE BIO-SWALE MUST BE WELL ESTABLISHED BEFORE PAVING BEGINS.

## CITY OF KENT GENERAL WATER NOTES

- ALL WATER SYSTEM IMPROVEMENTS SHALL BE CONSTRUCTED IN ACCORDANCE WITH APPROVED PLANS. ANY DEVIATION FROM THE PLANS WILL REQUIRE PRIOR APPROVAL FROM THE OWNER, THE CITY ENGINEER, AND OTHER APPROPRIATE PUBLIC AGENCIES.
- ALL NEW CONNECTIONS TO THE EXISTING WATER SYSTEM SHALL BE IN STRICT CONFORMANCE WITH SECTION 7-11 OF THE WSDOT/APWA STANDARD SPECIFICATIONS FOR ROADS, BRIDGES, AND MUNICIPAL CONSTRUCTION. THE CONNECTION SHALL BE MADE BETWEEN THE NEW WATER MAIN AND THE EXISTING WATER MAINS UNTIL THE NEW PIPES HAVE BEEN FLUSHED, DISINFECTED, AND TESTED. TEMPORARY PLUGS AND LOCKING SHALL BE INSTALLED AT THE POINTS OF CONNECTION TO THE EXISTING SYSTEM. THE CONTRACTOR SHALL CONTACT THE CITY OF KENT WATER DEPARTMENT AT LEAST 5 WORKING DAYS PRIOR TO THE PROPOSED CONNECTION TO THE EXISTING WATER MAIN. THE CONTRACTOR SHALL EXPOSE THE EXISTING WATER MAIN AND PROVIDE ALL NECESSARY FITTINGS FOR THE CONNECTIONS. A REPRESENTATIVE OF THE CITY OF KENT ENGINEERING DEPARTMENT SHALL BE PRESENT WHEN THE ACTUAL WATER MAIN CONNECTION IS MADE.
- PRESSURE AND PURITY TESTING SHALL BE DONE IN THE PRESENCE OF AND UNDER THE SUPERVISION OF THE CITY OF KENT DIRECTOR OF PUBLIC WORKS AND/OR HIS REPRESENTATIVE. THE CONTRACTOR SHALL PROVIDE PLUGS AND/OR TEMPORARY BLOW-OFF ASSEMBLIES FOR TESTING. THE CONTRACTOR ACCORDING TO THE PLANS.
- ALL MATERIALS USED FOR THE CONSTRUCTION OF WATER MAINS SHALL BE NEW AND UNDAMAGED AND SHALL BE INSPECTED AND APPROVED BY THE CITY OF KENT PRIOR TO INSTALLATION. THE SUPPLIERS SHALL PROVIDE THE CITY OF KENT WITH A CERTIFICATE OF MATERIALS WHEN REQUESTED. ALL PIPES, FITTINGS, VALVES, HYDRANTS, JOINTS AND RELATED APPURTENANCES SHALL CONFORM TO THE LATEST STANDARDS ISSUED BY THE WSDOT, APWA, AWWA AND THE CITY OF KENT.
- ALL WATER MAIN PIPE SHALL BE CEMENT-LINED DUCTILE IRON PIPE CONFORMING TO ANSI-A-21-11-1977 OR LATER REVISION, THICKNESS CLASS 52. CEMENT MORTAR LINING AND SEA COATING SHALL CONFORM TO ANSI A-21-4-11-1974 OR LATER REVISION. PIPE JOINTS SHALL BE PUSH-ON, MECHANICAL OR FLANGED WITH RUBBER GASKETS.
- ALL FITTINGS SHALL BE CEMENT-LINED DUCTILE IRON PIPE CONFORMING TO ANSI A-21-11-1977 OR LATER REVISION, UNLESS OTHERWISE SPECIFIED. ALL FITTING JOINTS SHALL BE DRILLED IN ACCORDANCE WITH ANSI B-16.1 AND SHALL BE THE 125-POUND CONFIGURATION UNLESS OTHERWISE SPECIFIED.
- CEMENT CONCRETE THRUST BLOCKING SHALL BE PLACED AT ALL TEES, CROSSES, BENDS, AND VALVES UNLESS APPROVED. RESTRAINED JOINT PIPES ARE INSTALLED, THRUST BLOCKING SHALL BE CEMENT CONCRETE 3000 PORED IN PLACE. THRUST BLOCKING SHALL BE CEMENT CONCRETE CLASS 5000 PORED IN PLACE. THRUST BLOCKING SHALL BE PLACED AGAINST SOLID UNDISTURBED EARTH AT THE SIDES AND BOTTOM OF THE TRENCH EXCAVATION AND SHALL BE SECURELY WRAPPED WITH 4-MIL POLYETHYLENE SHEETING.
- PIPE BEDDING MATERIAL SHALL BE PLACED TO A MINIMUM DEPTH OF 4 INCHES UNDER THE PIPE AND TO A DEPTH OF AT LEAST 6 INCHES OVER THE TOP OF THE PIPE. ALL TRENCH BACKFILL IN AREAS OF FUTURE PAVEMENT OR STRUCTURAL LOADING SHALL BE COMPACTED TO AT LEAST 95 PERCENT OF THE MAXIMUM DRY DENSITY PER ASTM D-1557 (MODIFIED PROCTOR). ALL OTHER AREAS SHALL BE COMPACTED TO 90 PERCENT OF MAXIMUM DRY DENSITY.
- ALL WATER MAINS 10 INCHES AND SMALLER IN DIAMETER SHALL HAVE A MINIMUM COVER OF 36 INCHES BELOW FINISH GRADE. ALL WATER MAINS 12 INCHES AND LARGER IN DIAMETER SHALL HAVE A MINIMUM COVER OF 48 INCHES BELOW FINISH GRADE. WHERE UTILITY CONFLICTS OCCUR, WATER MAINS SHALL BE LOWERED TO CLEAR. A MINIMUM OF 1 FOOT VERTICAL SEPARATION SHALL BE MAINTAINED BETWEEN ALL UTILITIES.
- UNLESS OTHERWISE SPECIFIED ON THE PLANS, ALL VALVES SHALL BE GATE VALVES CONFORMING TO THE LATEST REVISIONS TO AWWA STANDARD SPECIFICATIONS FOR GATE VALVES FOR ORDINARY WATER WORKS, SERVICE NO. C-500. THEY SHALL BE IRON-BODY, BRONZE-HEAD, AND STEEL-SEAT VALVES WITH BRONZE NEEDLE SEAT AND AN O-RING STUFFING BOX. ALL VALVES SHALL BE FLANGED, MECHANICAL, OR FLANGED AND SUITABLE FOR INSTALLATION WITH THE TYPE AND CLASS OF PIPE BEING USED. ALL VALVES SHALL OPEN COUNTERCLOCKWISE, AND UNLESS OTHERWISE SPECIFIED, SHALL BE NON-RISING STEM TYPE EQUIPPED TO FIT WITH THE CITY OF KENT'S EXISTING VALVE WRENCHES. ALL WATER VALVES SHALL BE FURNISHED WITH AN APPROVED VALVE BOX AND COVER. A CONCRETE VALVE MARKER POST MEETING THE REQUIREMENTS OF STANDARD DETAIL 3-3 SHALL BE PLACED AS DIRECTED BY THE ENGINEER AND SET TO LEAVE 18 INCHES EXPOSED ABOVE GRADE WHEN THE WATER VALVE BOX IS NOT WITHIN A PIPE SECTION.
- WHERE A WATER MAIN CROSSES ABOVE THE SANITARY SEWER, AND LESS THAN 1.5 FEET OF VERTICAL CLEARANCE EXISTS BETWEEN THE TOP OF THE SEWER MAIN AND BOTTOM OF WATER MAIN, THE SANITARY SEWER MAIN SHALL BE CONSTRUCTED, OR RECONSTRUCTED, WITH DUCTILE IRON PIPE (CLASS 50) CENTERED ON THE WATER MAIN FOR A MINIMUM HORIZONTAL DISTANCE OF ONE CONTINUOUS 18 FEET NOMINAL PIPE LENGTH WITH NO JOINTS.
- ALL FIRE HYDRANTS SHALL BE COMPRESSION TYPE, BREAK-AWAY (TRAFFIC MODEL) HYDRANTS CONFORMING TO AWWA C502 AND MEETING THE REQUIREMENTS OF STANDARD DETAIL 3-1 OF THE CITY OF KENT CONSTRUCTION STANDARDS. THE HYDRANT LEADS SHALL BE A MINIMUM OF 6 INCHES IN DIAMETER. AUXILIARY VALVES SHALL BE INSTALLED IN THE HYDRANT LEADS LOCATED AT THE CONNECTION TO THE CITY MAIN. ALL HYDRANTS SHALL BE EQUIPPED WITH A DRAIN. A GRAVEL POCKET OR DRY WELL SHALL BE PROVIDED UNLESS THE NATURAL SOILS WILL PROVIDE ADEQUATE DRAINAGE. HYDRANT DRAINS SHALL NOT BE CONNECTED TO STORM DRAINS. HYDRANT DRAINS SHALL BE PAINTED WITH 2 COATS OF OIL-BASED FIRE HYDRANT PAINT. HYDRANT DRAINS SHALL BE PAINTED WITH 2 COATS OF OIL-BASED FIRE HYDRANT PAINT. HYDRANT GUARD POSTS MEETING THE REQUIREMENTS OF STANDARD DETAIL 3-11 SHALL BE FURNISHED AND INSTALLED WITH FIRE HYDRANTS WHEN DIRECTED BY THE CITY ENGINEER. IOWA FIRE HYDRANTS ARE NOT ALLOWED.
- PRIOR TO ACCEPTANCE OF CONSTRUCTION AND/OR ISSUANCE OF CERTIFICATE OF OCCUPANCY PERMITS, SURVEYOR-CERTIFIED AS-BUILT DRAWINGS PREPARED ON MYLAR MUST BE PREPARED OF ALL PUBLIC WATER MAIN FACILITIES AND APPROVED BY THE CITY. SEE CITY OF KENT DEVELOPMENT ASSISTANCE BROCHURE #E-6, AS-BUILT DRAWINGS, FOR ADDITIONAL INFORMATION REGARDING AS-BUILT DRAWINGS.
- PARCELS 2, 3, 4, TOGETHER WITH TRACT A, CITY OF KENT BINDING SITE PLAN NO. BSP-2006-04 (MIDWAY RETAIL CENTER BINDING SITE PLAN), RECORDED UNDER RECORDING NO. 20070926000281, AT VOLUME 242 OF PLATS, PAGES 85 THROUGH 92, INCLUSIVE, IN KING COUNTY, WASHINGTON.

## NEW IMPERVIOUS AREA

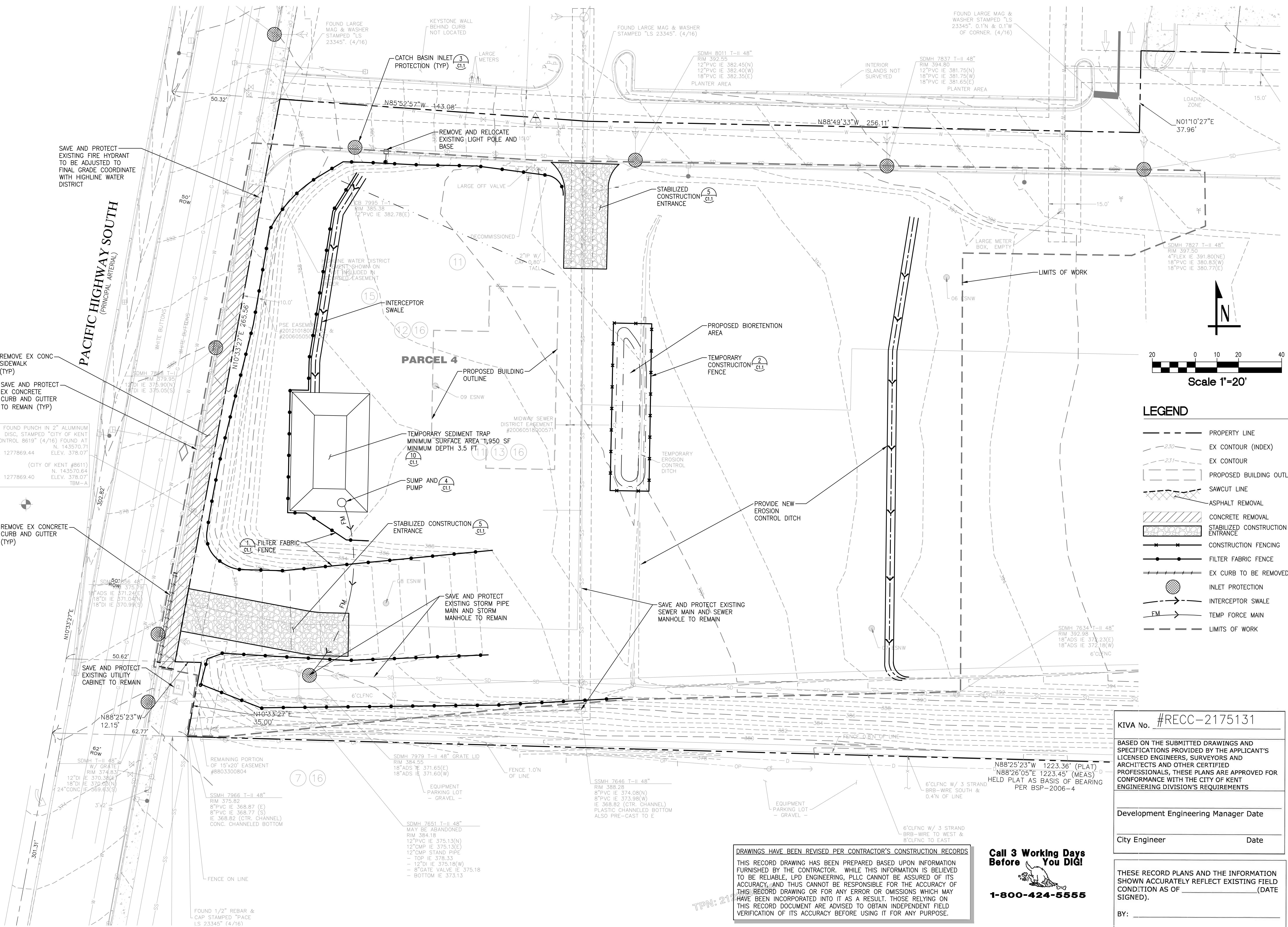
NEW IMPERVIOUS AREA = 64,578 SF (1.483 AC)  
NOTE IMPERVIOUS AREA PROVIDED FOR PERMITTING USE ONLY

City Engineer

Date

BY: \_\_\_\_\_  
THESE RECORD PLANS AND THE INFORMATION SHOWN ACC

# AS-BUILT



E:\CADD-2015\DIKS DRIVE-IN\logo\RED LOGO.jpg

**Dick's Drive-in**  
Kent  
24050 Pacific Hwy S.  
Kent, WA 98032

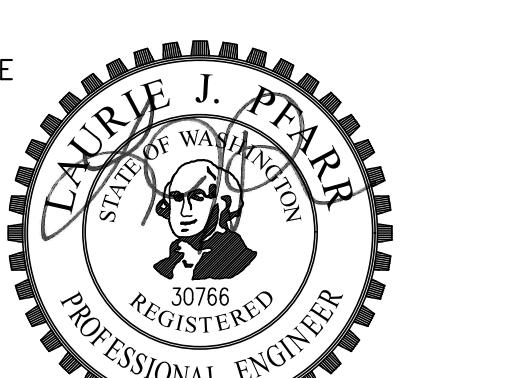
**smith co.**

1725 westlake avenue n  
suite 210  
seattle, washington 98109  
206.838.5485 p  
206.713.4076 m  
206.464.0700 f  
ksmith@smithco.org  
http://www.smithco.org/

Consultant

**LPD** 1932 First Ave  
Suite 201  
Seattle, WA 98101  
p. 206.725.1211  
f. 206.973.5344  
engineering pllc [www.lpdengineering.com](http://www.lpdengineering.com)

Consultant Registration      Architect Registration



Issues and Revisions			
No.	Date	Issues	By
1	12.28.17	PERMIT	LJP
2	02.27.18	PERMIT RESUBMITTAL	LJP
3	03.16.18	PERMIT RESUBMITTAL	LJP
4	08.08.18	SIDEWALK DELETION	LJP
5	06/13/19	RECORD DRAWINGS	LJP

Project Name	DICK'S DRIVE-IN	
Project Number		
Description	TESC AND DEMOLITION	
Computer File		
Scale	AS NOTED	
Template	4.3 (20101)	
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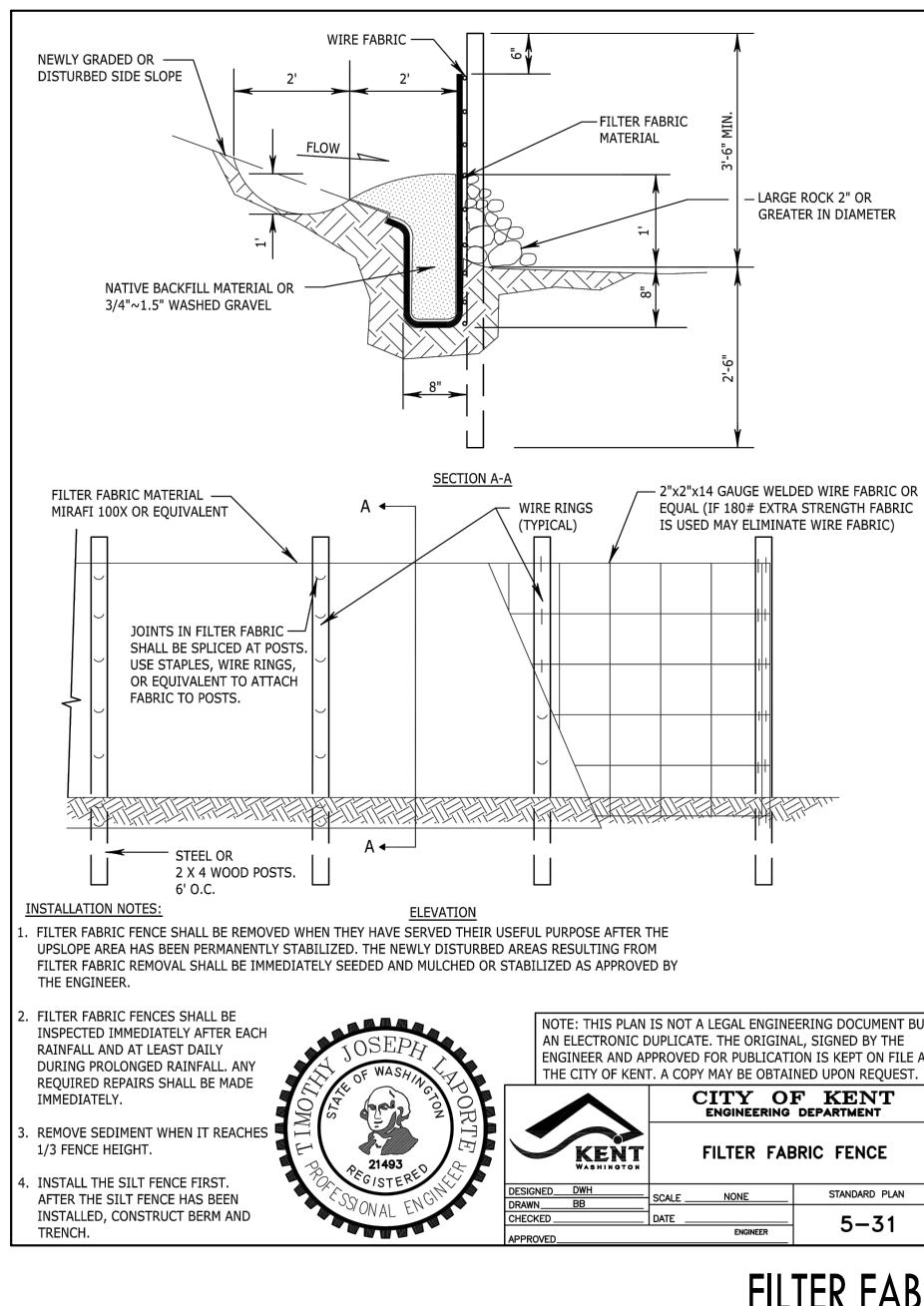
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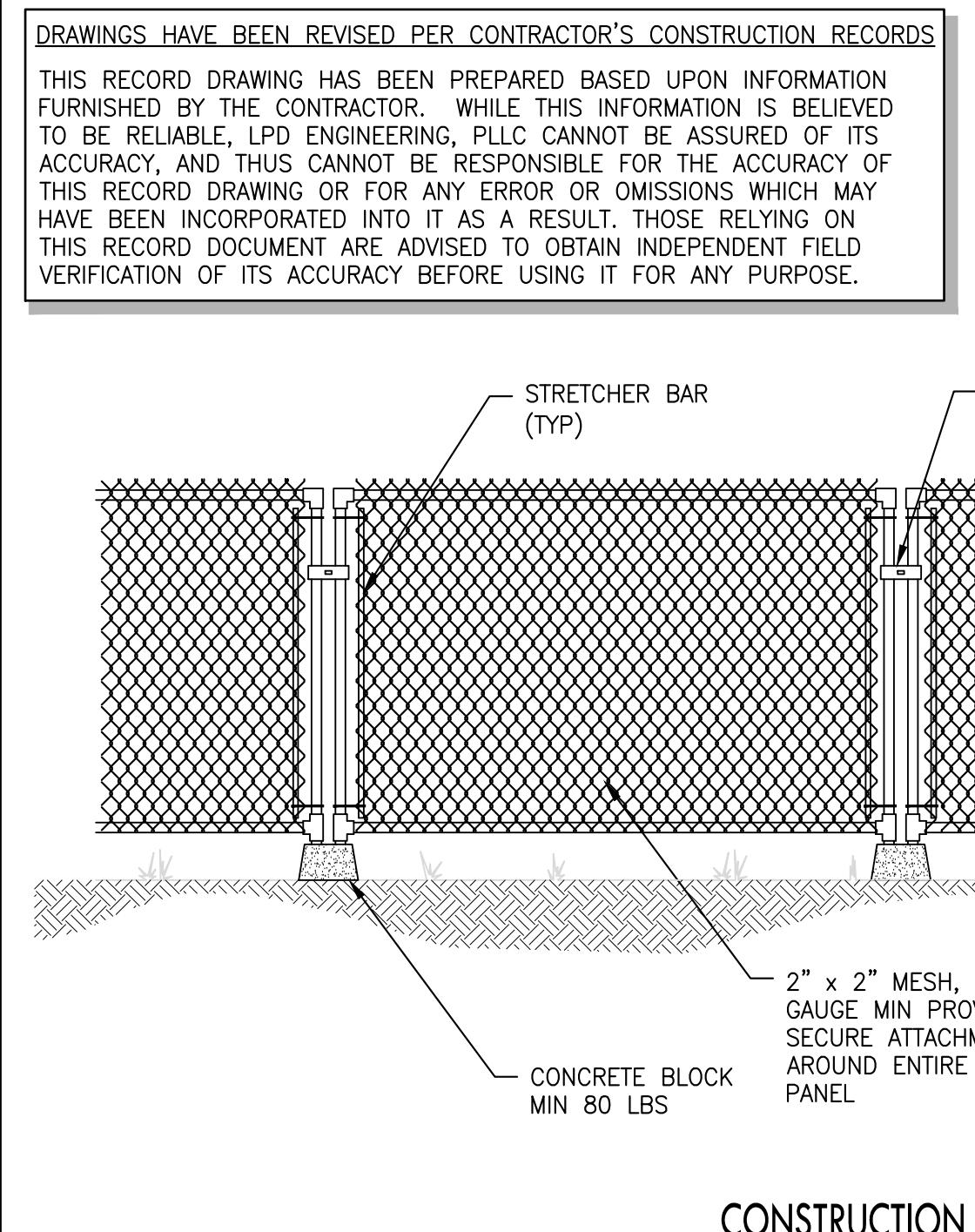
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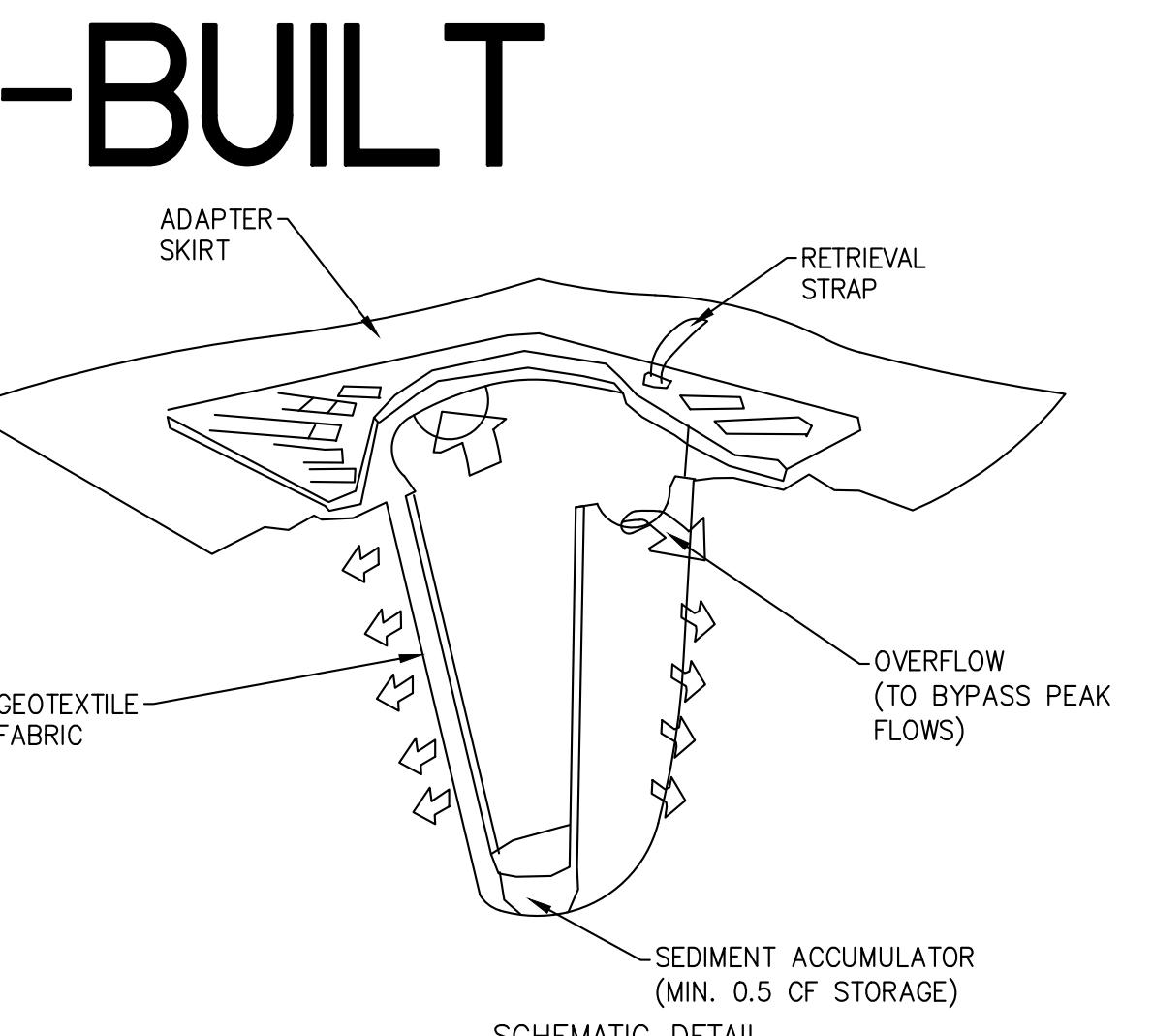


1 FILTER FABRIC FENCE

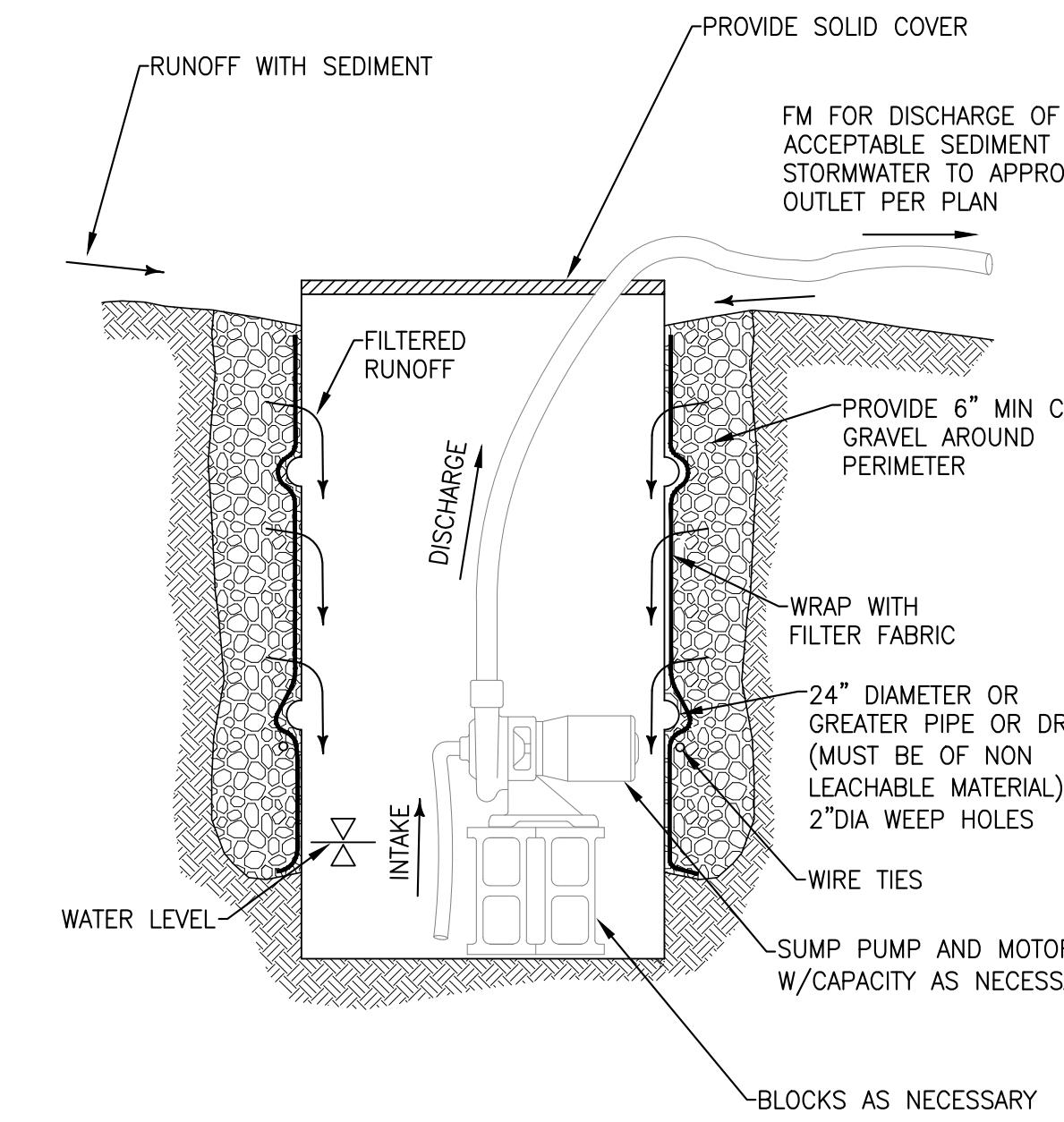


2 CONSTRUCTION FENCING

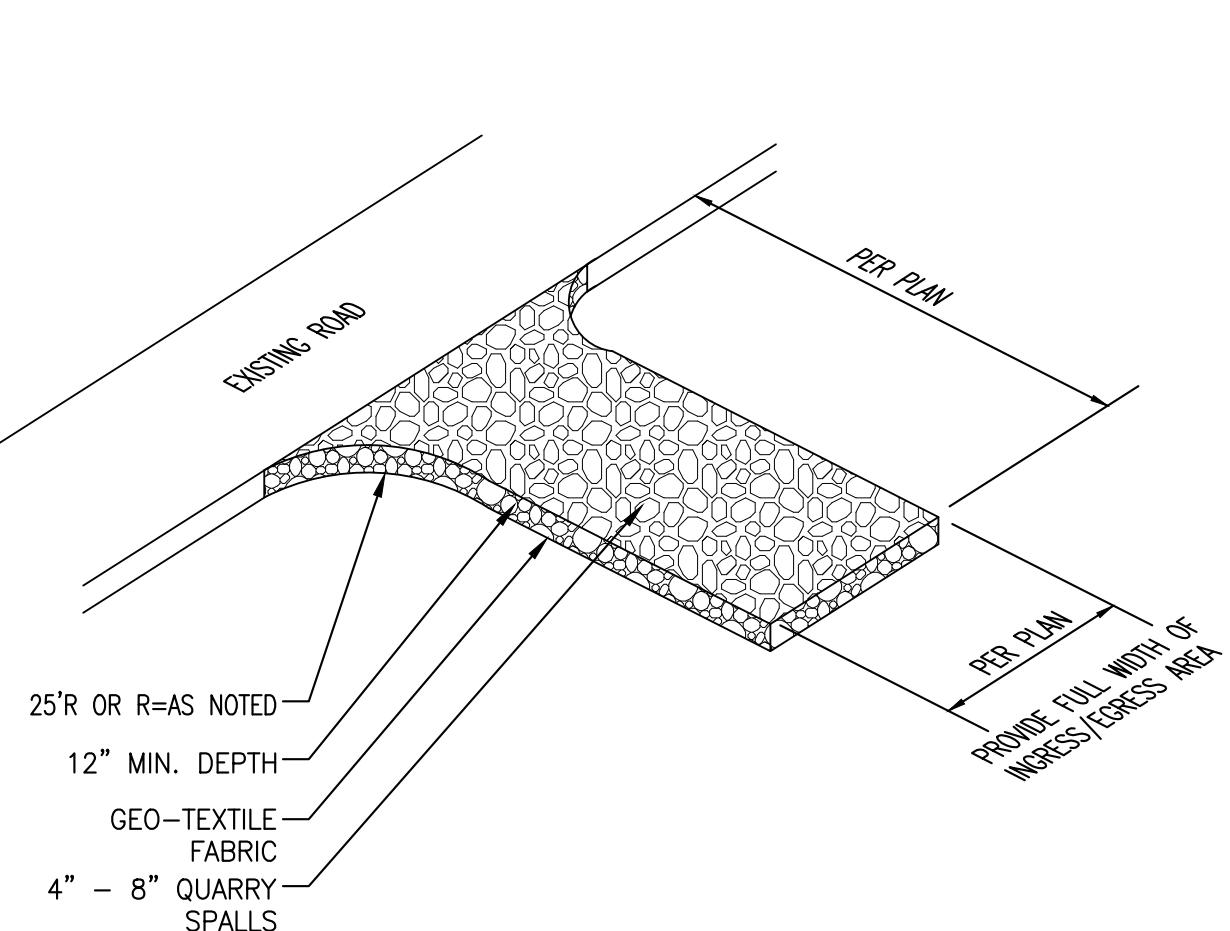
# AS-BUILT



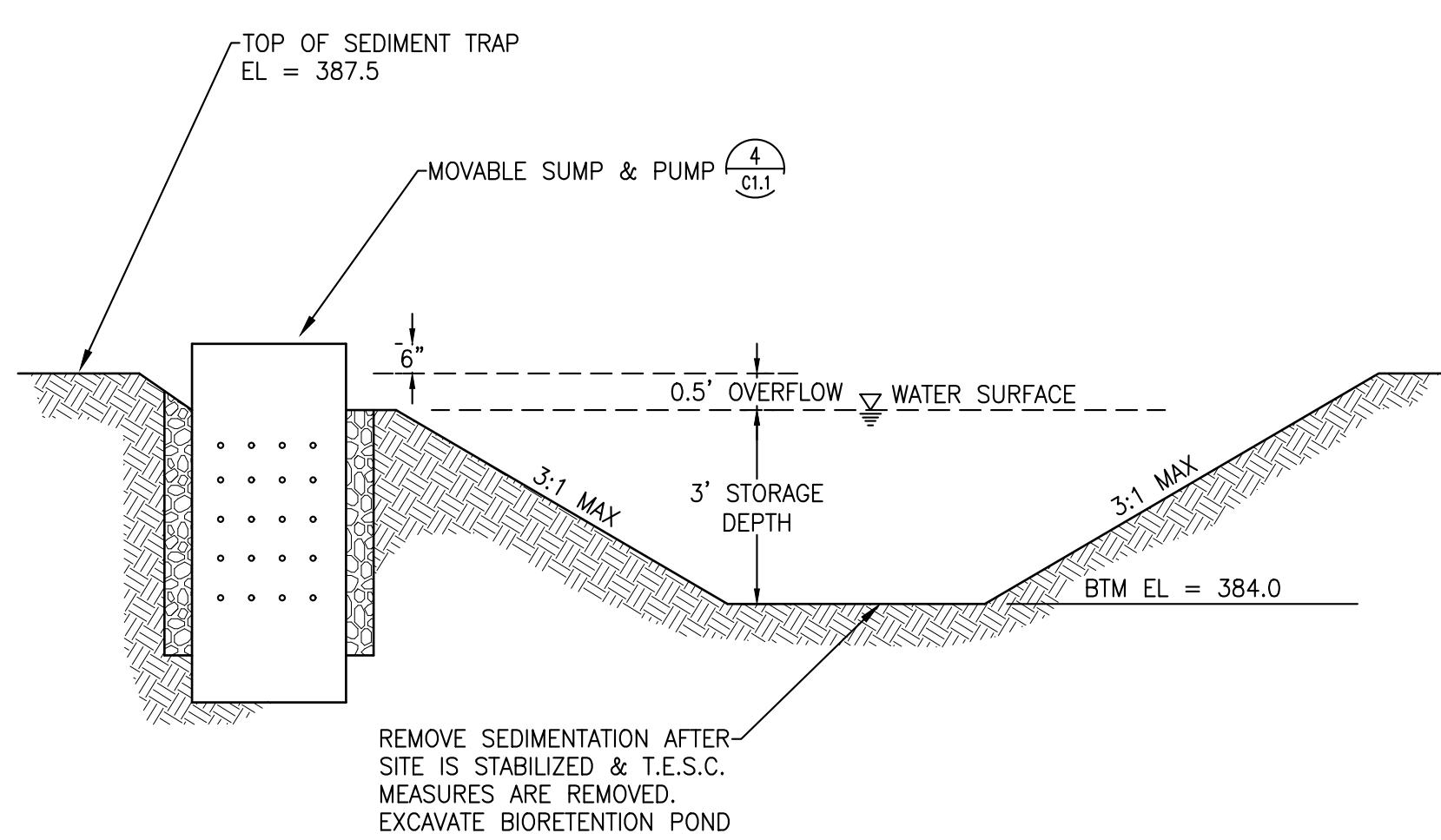
3 INLET PROTECTION



4 MOVEABLE SUMP & PUMP



5 CONSTRUCTION ENTRANCE



10 TEMPORARY SEDIMENT TRAP

## BIORETENTION AREA PROTECTION NOTES

1. INSTALL TEMPORARY CONSTRUCTION FENCE AROUND FOOTPRINT OF BIORETENTION AREA.
2. LIMIT CONSTRUCTION VEHICLE TRAFFIC WITHIN FOOTPRINT OF BIORETENTION AREAS TO MAXIMUM EXTENT FEASIBLE DURING CONSTRUCTION.
3. DO NOT EXCAVATE BIORETENTION AREAS TO SUB-GRADE UNTIL SITE HAS BEEN STABILIZED AND JUST PRIOR TO INSTALLATION OF BIORETENTION SOIL AND PLANTING.
4. GEOTECHNICAL ENGINEER TO REVIEW BIORETENTION SUBGRADE PRIOR TO INSTALLATION OF SOIL AND PLANTING.
5. DO NOT ALLOW CONSTRUCTION STORMWATER RUNOFF TO ENTER EXCAVATED BIORETENTION AREAS.

11 BIORETENTION AREA PROTECTION NOTES

## CONSTRUCTION SEQUENCE

1. SCHEDULE A PRE-CONSTRUCTION MEETING WITH THE CITY OF KENT CONSTRUCTION AND INSPECTION DEPARTMENT AT 253-856-5500.
2. REVIEW ESC NOTES.
3. CALL 'ONE-CALL' @1-800-424-5555, AND PRIVATE UTILITY LOCATE FOR UTILITY LOCATES.
4. INSTALL ESC MEASURES (ALL TEMPORARY SEDIMENTATION AND EROSION CONTROL MEASURES MUST BE IN PLACE PRIOR TO ANY CONSTRUCTION OR SITE CLEARING. EROSION AND SEDIMENTATION CONTROL PRACTICES AND/OR DEVICES SHALL BE MAINTAINED UNTIL PERMANENT VEGETATION IS ESTABLISHED.)
5. CONSTRUCT BUILDING.
6. INSTALL UTILITIES AND OTHER SITE IMPROVEMENTS.
7. INSTALL CURBS AND SIDEWALKS.
8. STABILIZE AND REVEGETATE ENTIRE SITE.
9. ESTABLISH LANDSCAPING AND PERMANENT VEGETATION. EROSION CONTROL FEATURES CAN BE REMOVED UPON FINAL SITE STABILIZATION.

KIVA No. #RECC-2175131

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Development Engineering Manager Date

City Engineer Date

THESE RECORD PLANS AND THE INFORMATION SHOWN ACCURATELY REFLECT EXISTING FIELD CONDITION AS OF \_\_\_\_\_ (DATE SIGNED).

BY: \_\_\_\_\_

Project Name	DICK'S DRIVE-IN
Project Number	
Description	TESC AND DEMOLITION DETAILS AND NOTES
Computer File	
Scale	AS NOTED
Template	4.3 (20101)

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C1.1

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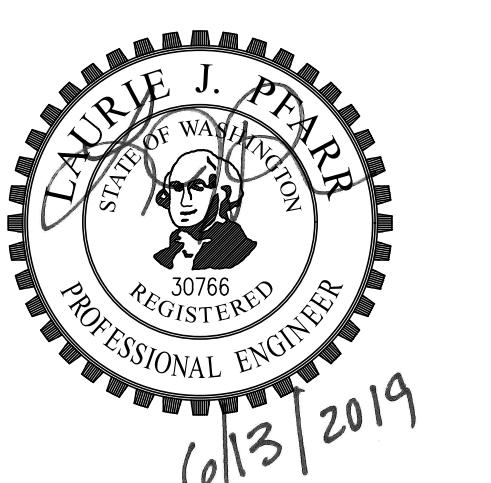
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Consultant

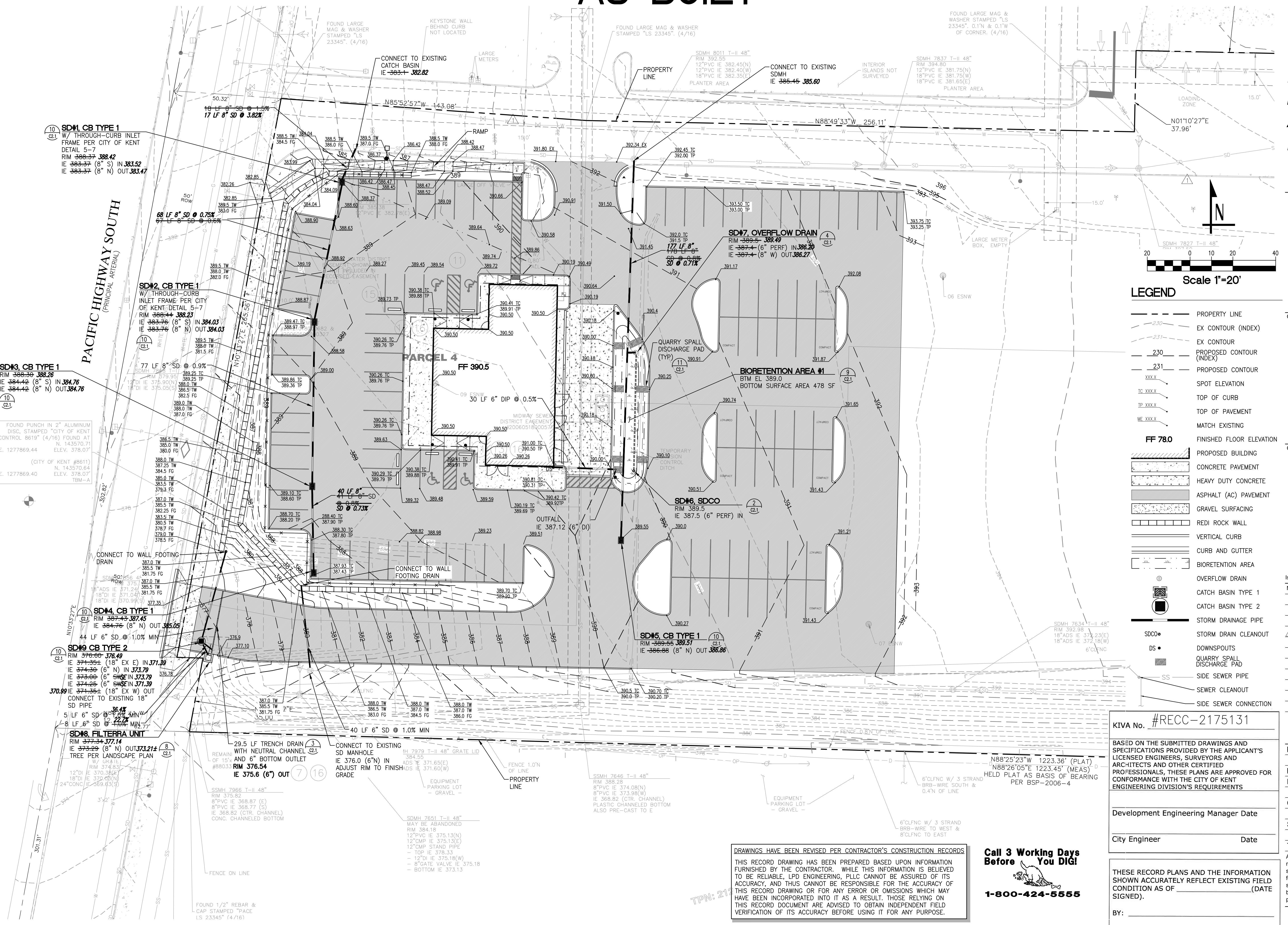
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f. 206.973.5344  
engineering pllc www.lpdengineering.com

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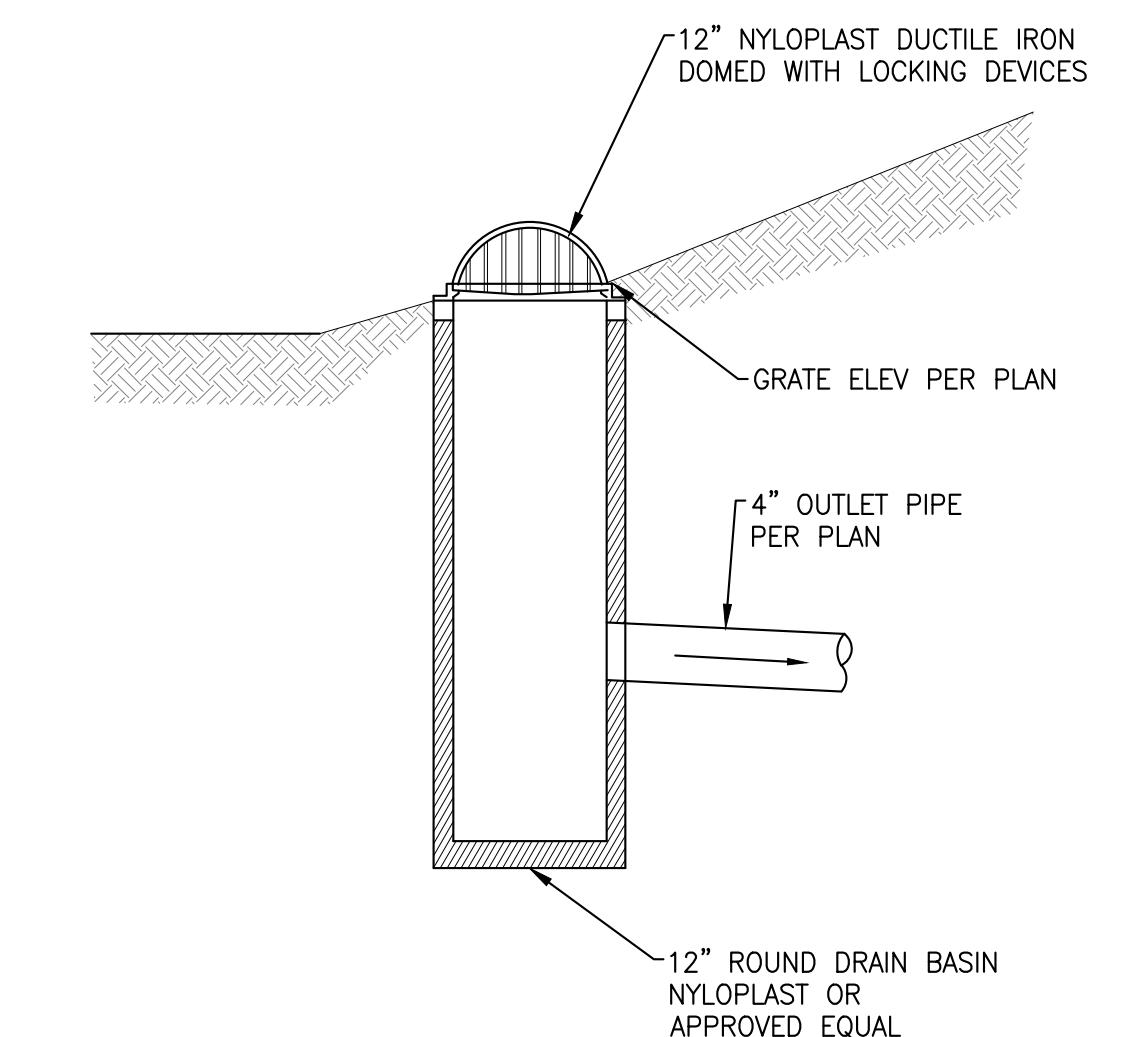
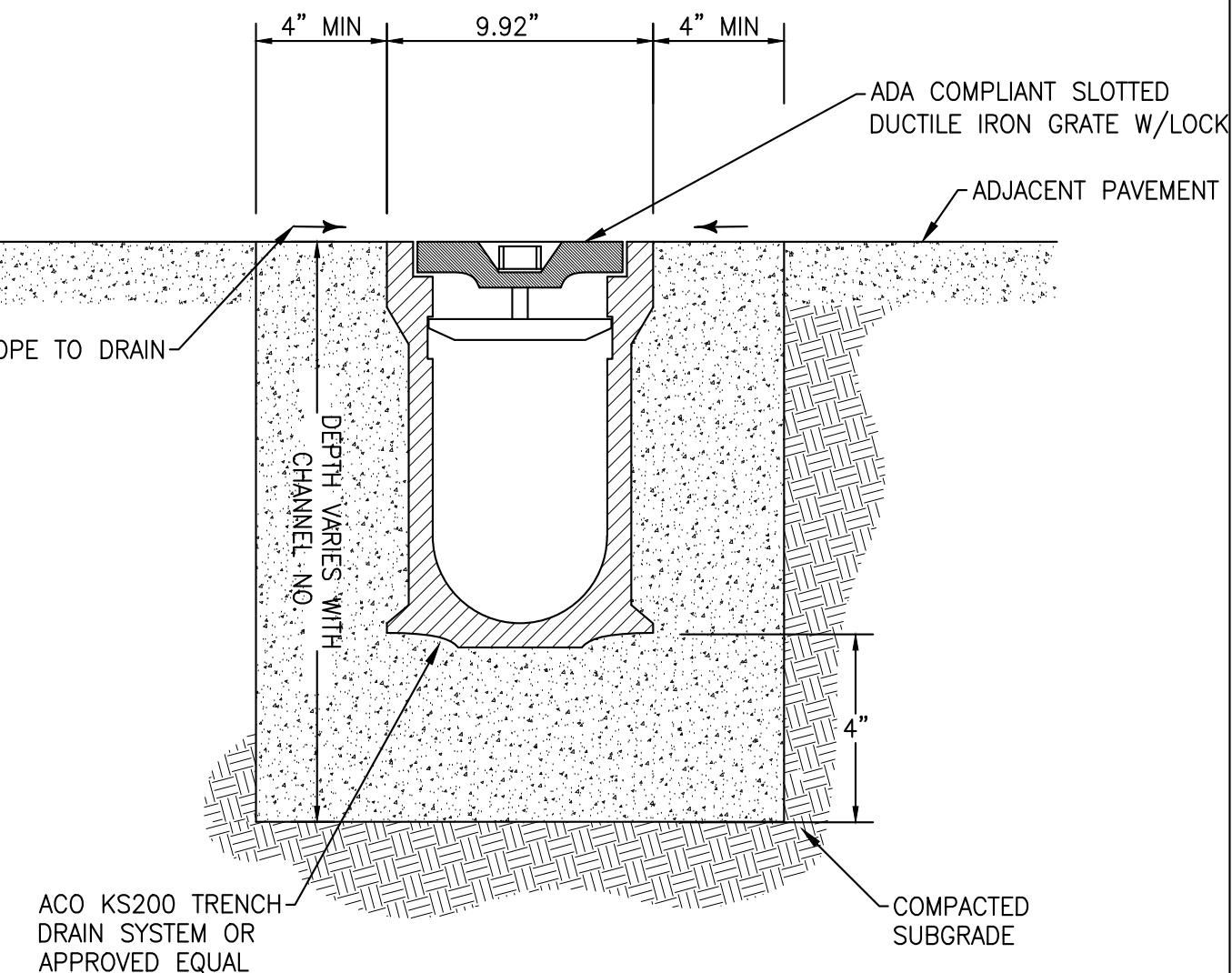
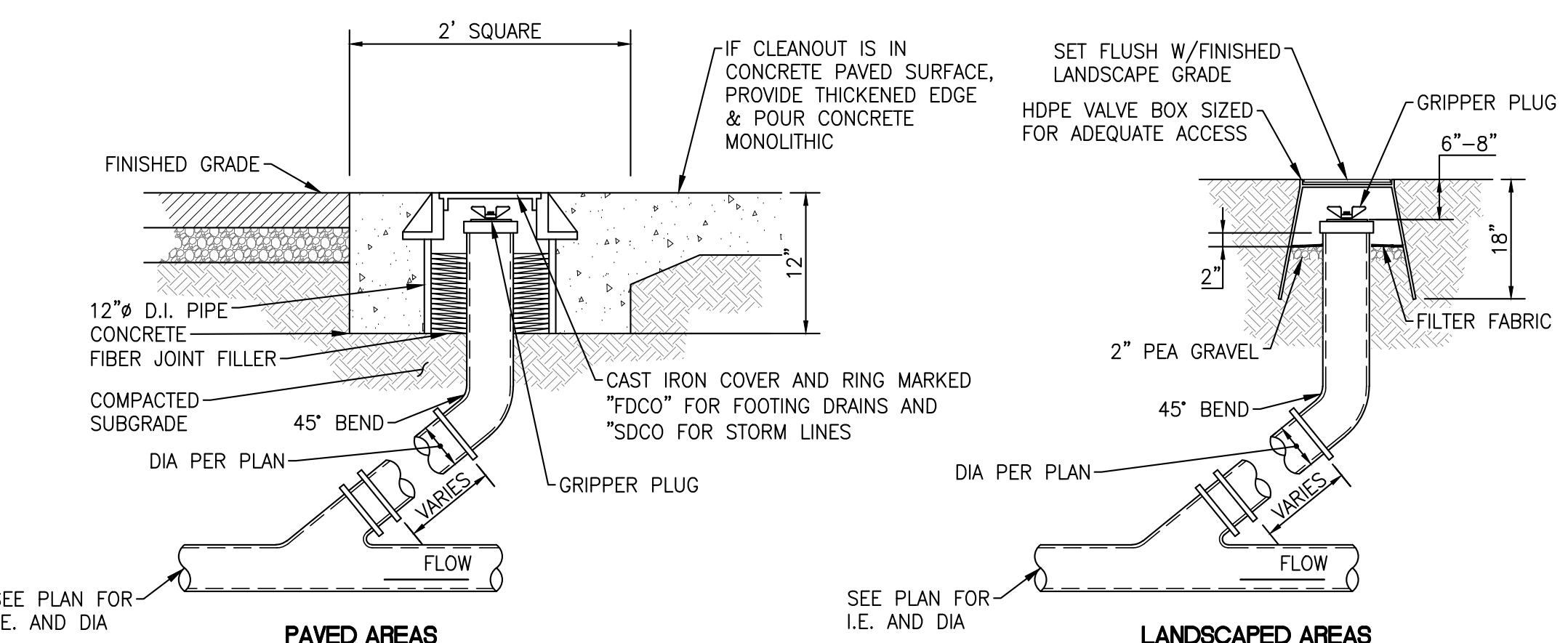


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Project Name	DICK'S DRIVE-IN	
Project Number		
Description	GRADING AND DRAINAGE	
Computer File		
Scale	AS NOTED	
Template	4.3 (20101)	
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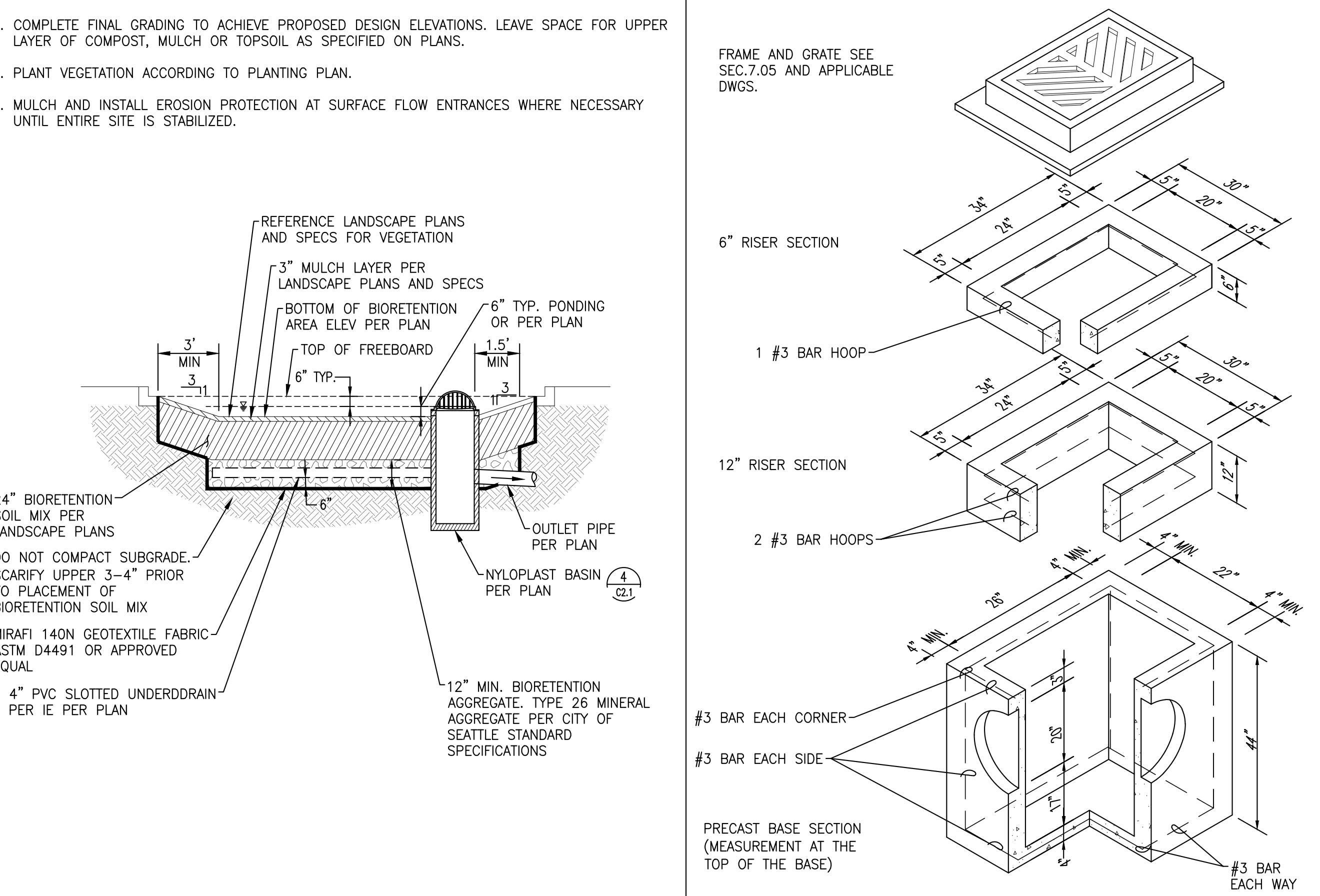


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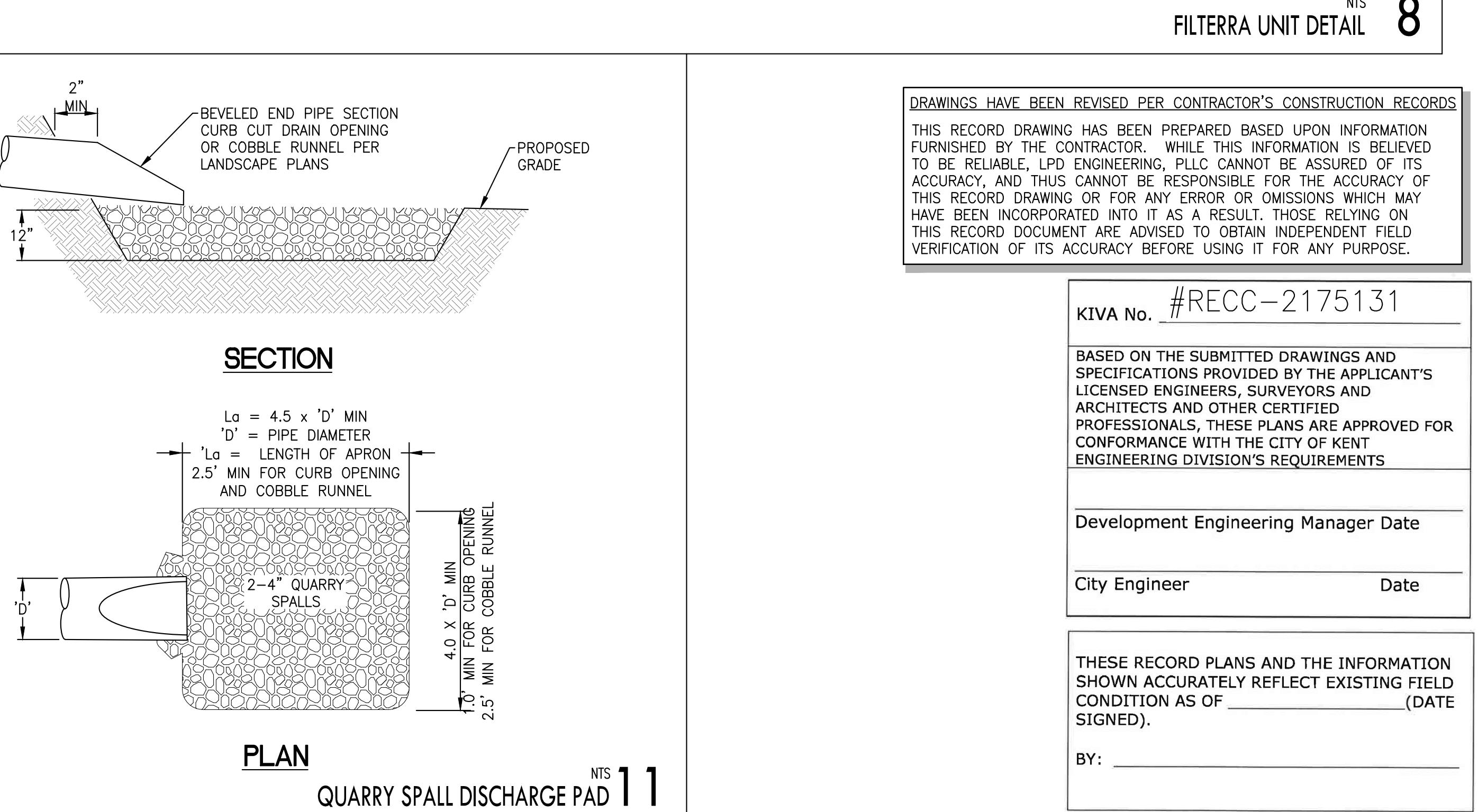
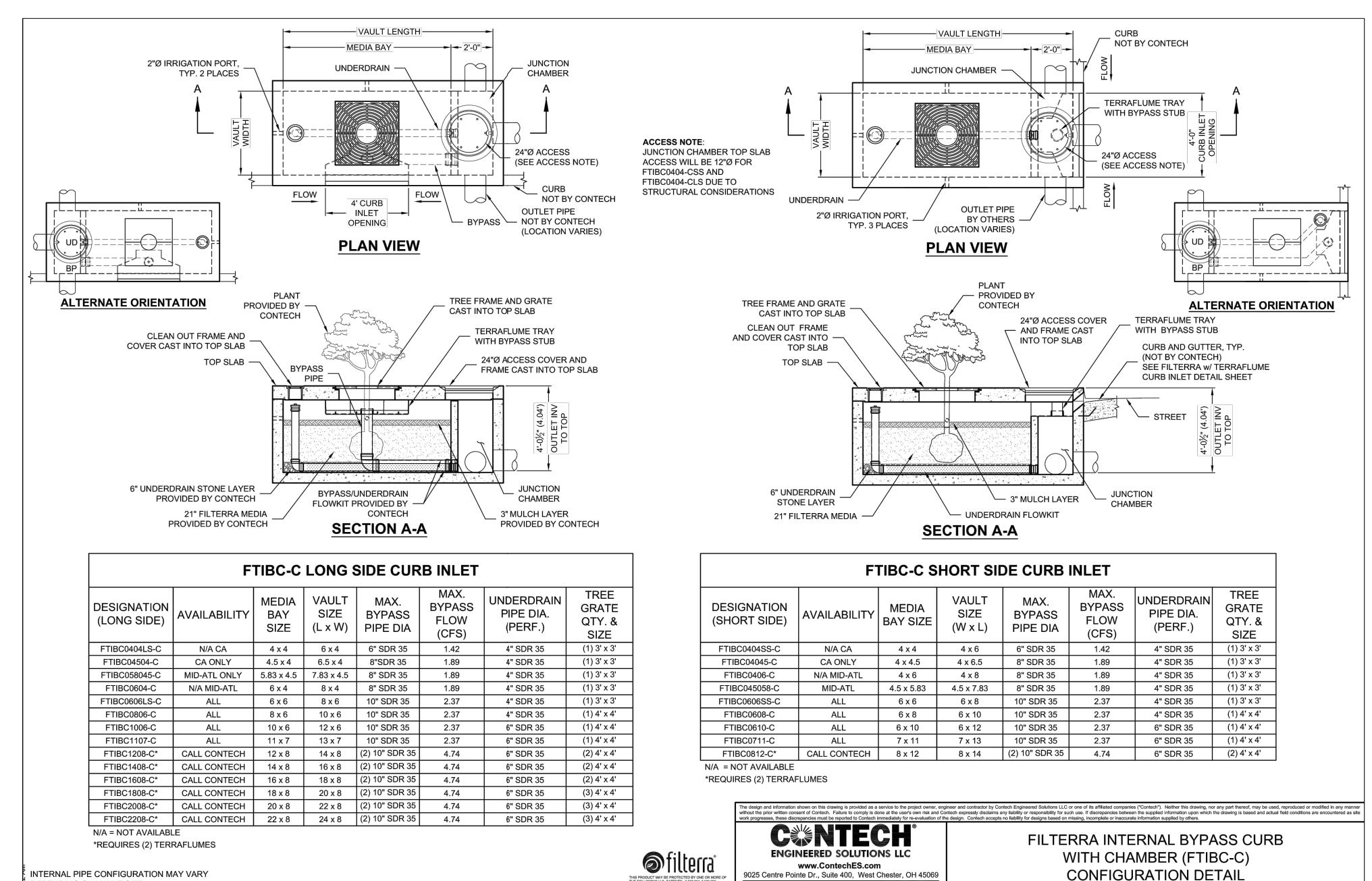


**CONSTRUCTION SEQUENCE FOR BIORETENTION AREA**

1. INSTALL TEMPORARY SEDIMENT CONTROL BMPs AS SHOWN ON PLAN.
2. COMPLETE SITE GRADING. IF APPLICABLE, CONSTRUCT CURB CUTS OR OTHER INFLOW ENTRANCE BUT PROVIDE PROTECTION SO THAT DRAINAGE IS PROHIBITED FROM ENTERING BIORETENTION CONSTRUCTION AREA.
3. STABILIZE GRADING WITHIN THE LIMIT OF DISTURBANCE EXCEPT WITHIN THE BIORETENTION AREA. BIORETENTION AREAS THAT ARE USED AS TEMPORARY SEDIMENT TRAPS SHOULD BE EXCAVATED 6 INCHES BELOW THE BOTTOM OF THE SEDIMENT TRAP.
4. EXCAVATE BIORETENTION AREA TO PROPOSED DEPTH AND SCARIFY THE TOP 3"-4" OF EXISTING SOIL SURFACES. DO NOT COMPACT NATIVE SUBGRADE.
5. INSTALL GEOTEXTILE FABRIC PER DETAIL.
6. INSTALL PVC SLOTTED UNDERDRAIN PIPE PER PLAN.
7. BACKFILL BIORETENTION AREA WITH BIORETENTION SOIL MIX AND WASHED ROCK AS SHOWN ON PLANS AND SPECIFICATIONS. OVERFILLING IS RECOMMENDED TO ACCOUNT FOR SETTLEMENT. LIGHT HAND TAMPING IS ACCEPTABLE IF NECESSARY.
8. BIORETENTION SOIL MIX SHALL BE PER THE LANDSCAPE PLANS AND SHALL MEET THE DEFAULT BIORETENTION SOIL MIX (BSM) FOUND IN BMP T7.30 OF THE STORMWATER MANAGEMENT MANUAL FOR WESTERN WASHINGTON. IF USING A CUSTOM MIX, IT SHALL BE TESTED AND COMPLY WITH THE DESIGN CRITERIA FOR CUSTOM BIORETENTION SOIL MIXES.
9. PRESOAK THE PLANTING SOIL PRIOR TO PLANTING VEGETATION TO AID IN SETTLEMENT.
10. COMPLETE FINAL GRADING TO ACHIEVE PROPOSED DESIGN ELEVATIONS. LEAVE SPACE FOR UPPER LAYER OF COMPOST, MULCH OR TOPSOIL AS SPECIFIED ON PLANS.
11. PLANT VEGETATION ACCORDING TO PLANTING PLAN.
12. MULCH AND INSTALL EROSION PROTECTION AT SURFACE FLOW ENTRANCES WHERE NECESSARY UNTIL ENTIRE SITE IS STABILIZED.



**NOTES:**



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**Dick's Drive-in**  
Kent  
24050 Pacific Hwy S.  
Kent, WA 98032

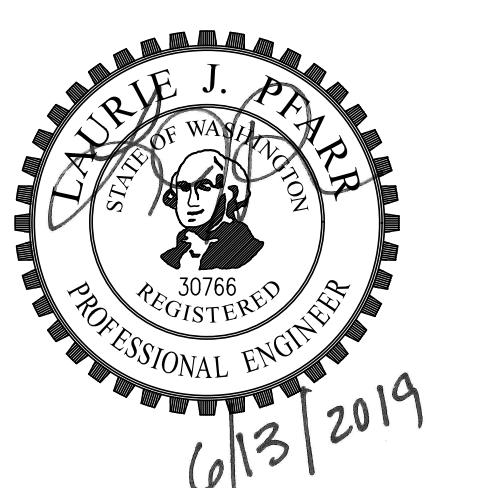
**smith co.**

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Consultant

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engineering pllc www.lpdengineering.com

Consultant Registration Architect Registration



Issues and Revisions	
No.	Date
1	12.28.17
2	02.27.18
3	03.16.18
4	08.08.18
5	06/13.19
	PERMIT
	PERMIT RESUBMITTAL
	PERMIT RESUBMITTAL
	SIDEWALK DELETION
	RECORD DRAWINGS

Project Name	DICK'S DRIVE-IN
Project Number	
Description	GRADING AND DRAINAGE DETAILS
Computer File	
Scale	AS NOTED
Template	4.3 (20101)
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**C2.1**

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*Dick's Drive-in*  
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24050 Pacific Hwy S.  
Kent, WA 98032

smith co.

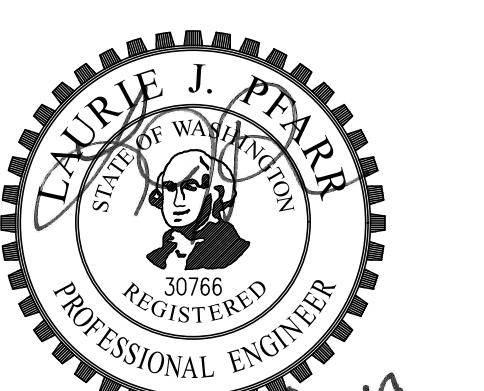
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The logo for LPD engineering PLLC features a large, bold, black 'LPD' monogram where the letters are outlined in white. Below the monogram, the word 'engineering' is written in a smaller, lowercase, sans-serif font, followed by 'pllc' in a slightly larger, lowercase, sans-serif font. To the right of the monogram, the company's address is listed: '1932 First Ave', 'Suite 201', 'Seattle, WA 98101', 'p. 206.725.1211', and 'f. 206.973.5344'. At the bottom right, the website 'www.lpdengineering.com' is displayed in a small, lowercase, sans-serif font.

---

Consultant Registration      Architect Registration



Issues and Revisions				
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#RECC-2175131

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ENGINEERING DIVISION'S REQUIREMENTS

---

Development Engineering Manager Date

---

City Engineer \_\_\_\_\_ Date \_\_\_\_\_

---

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THESE RECORD PLANS AND THE INFORMATION  
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CONDITION AS OF \_\_\_\_\_ (DATE  
SIGNED).

BY: \_\_\_\_\_

---

# C3.0

**PACIFIC HIGHWAY SOUTH (PRINCIPAL ARTERIAL)**

**LEGEND**

**Scale 1"=20'**

**DRAWINGS HAVE BEEN REVISED PER CONTRACTOR'S CONSTRUCTION RECORDS**

**Call 3 Working Days Before You DIG!**

**1-800-424-5555**

**KIVA No. #RECC-2175131**

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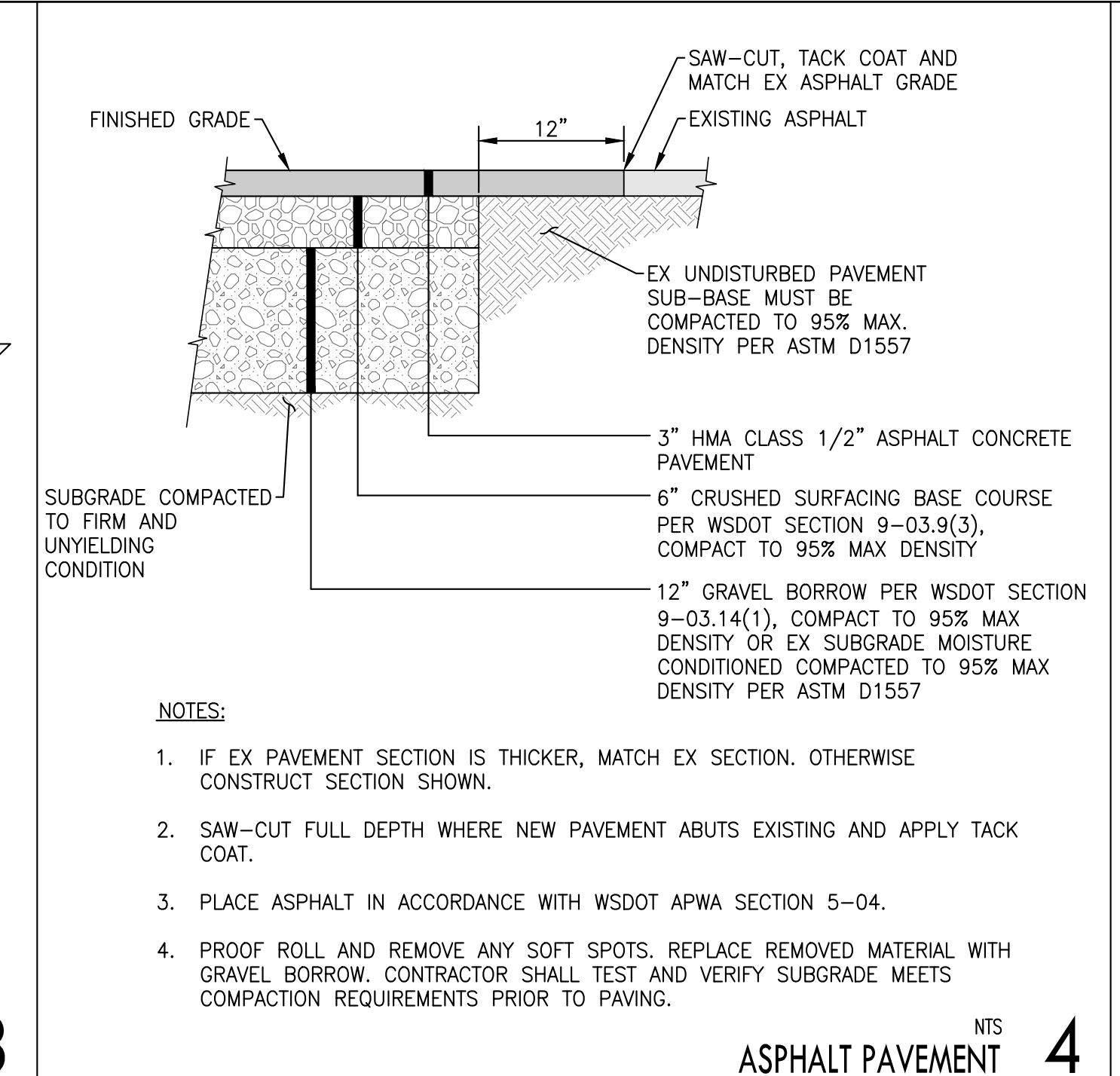
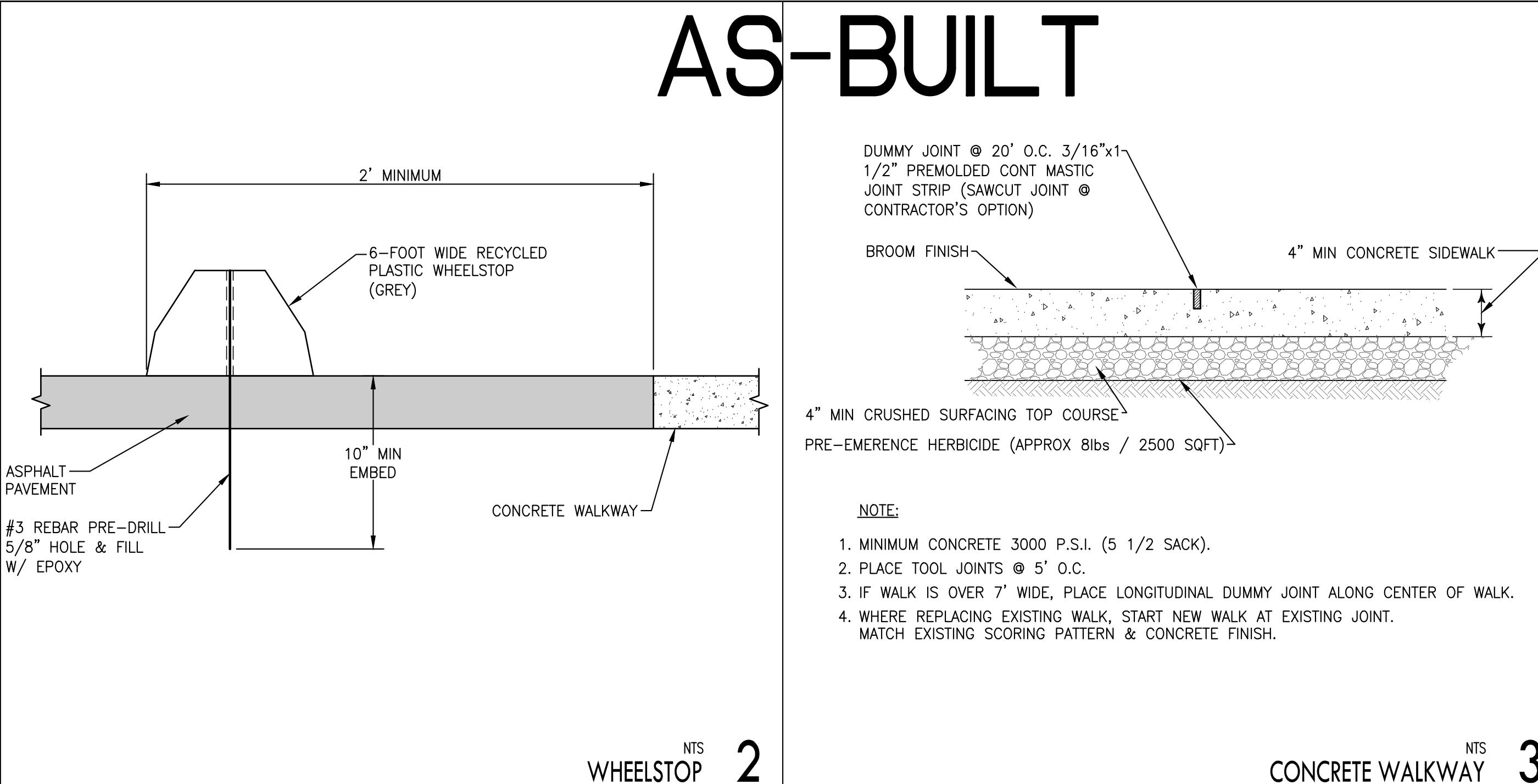
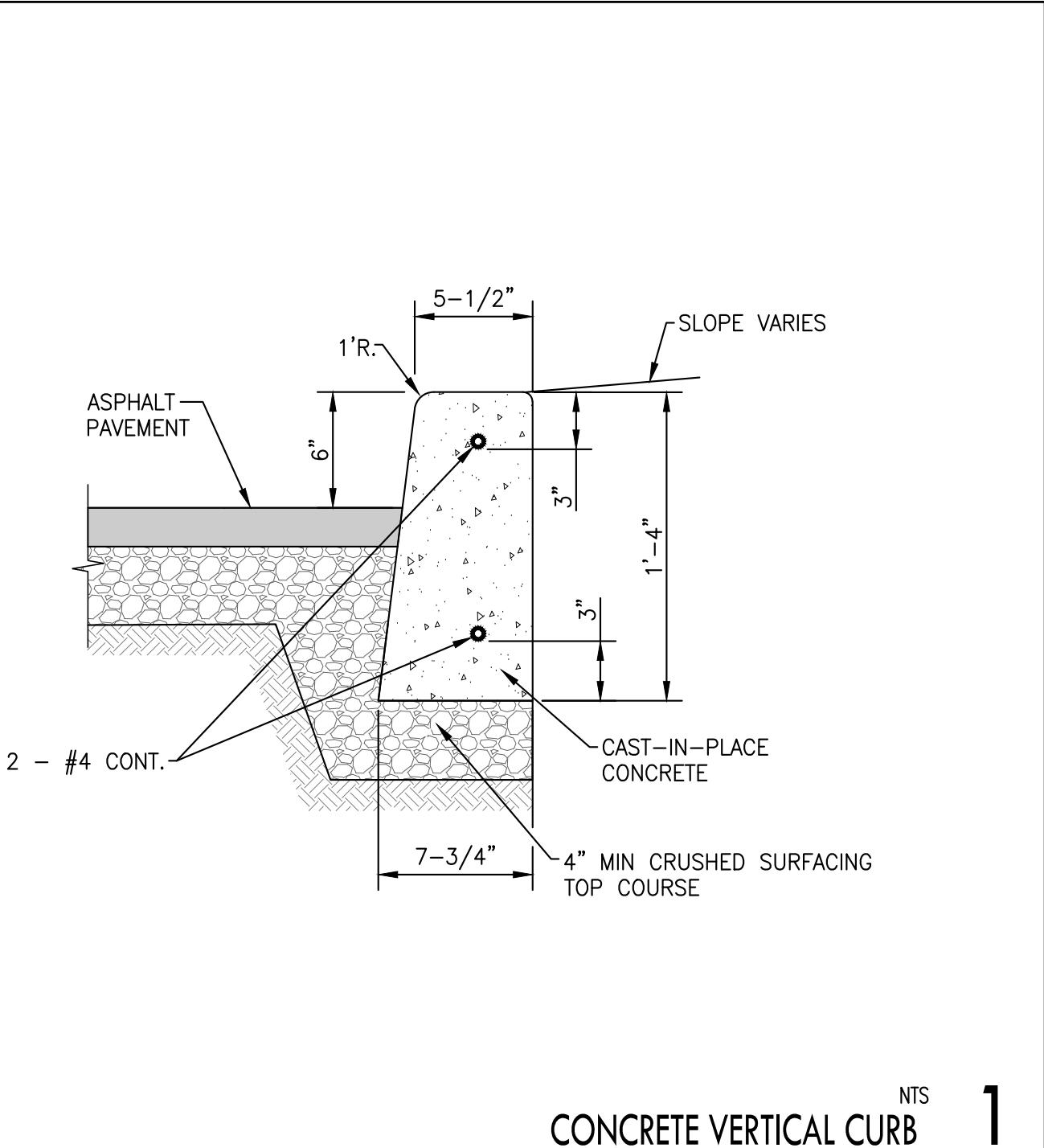
**Development Engineering Manager Date**

**City Engineer Date**

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#### Consultant

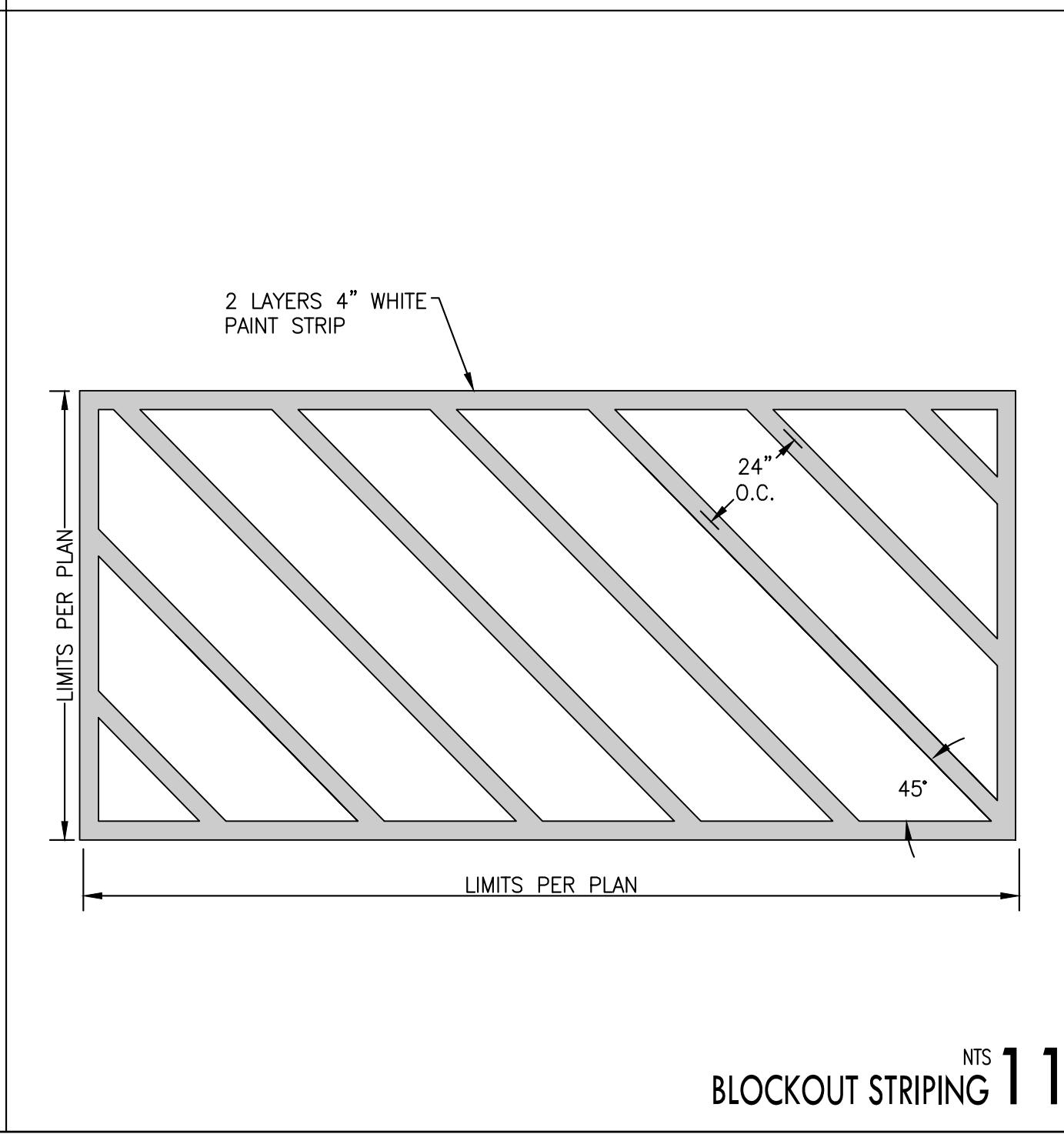
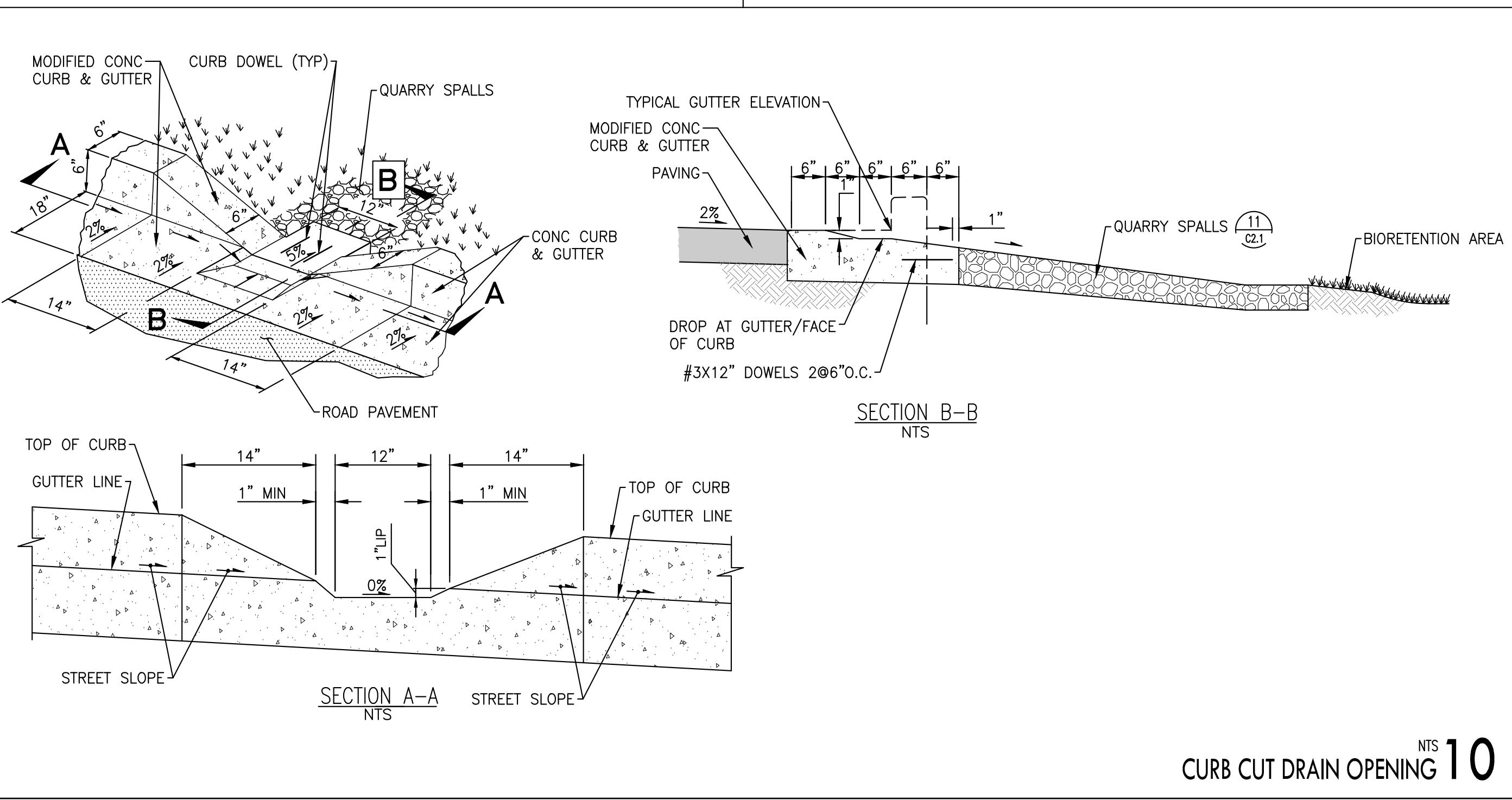
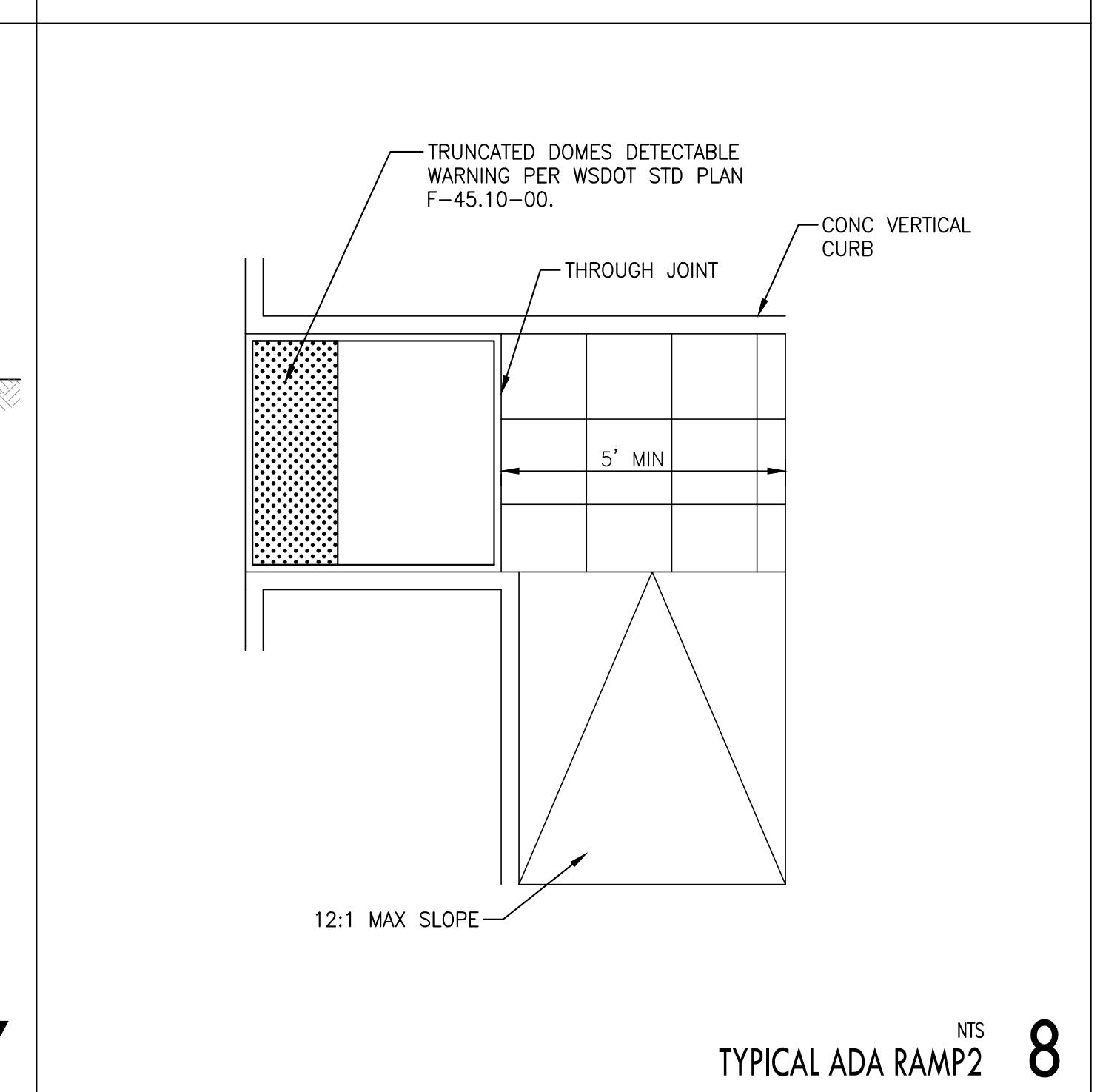
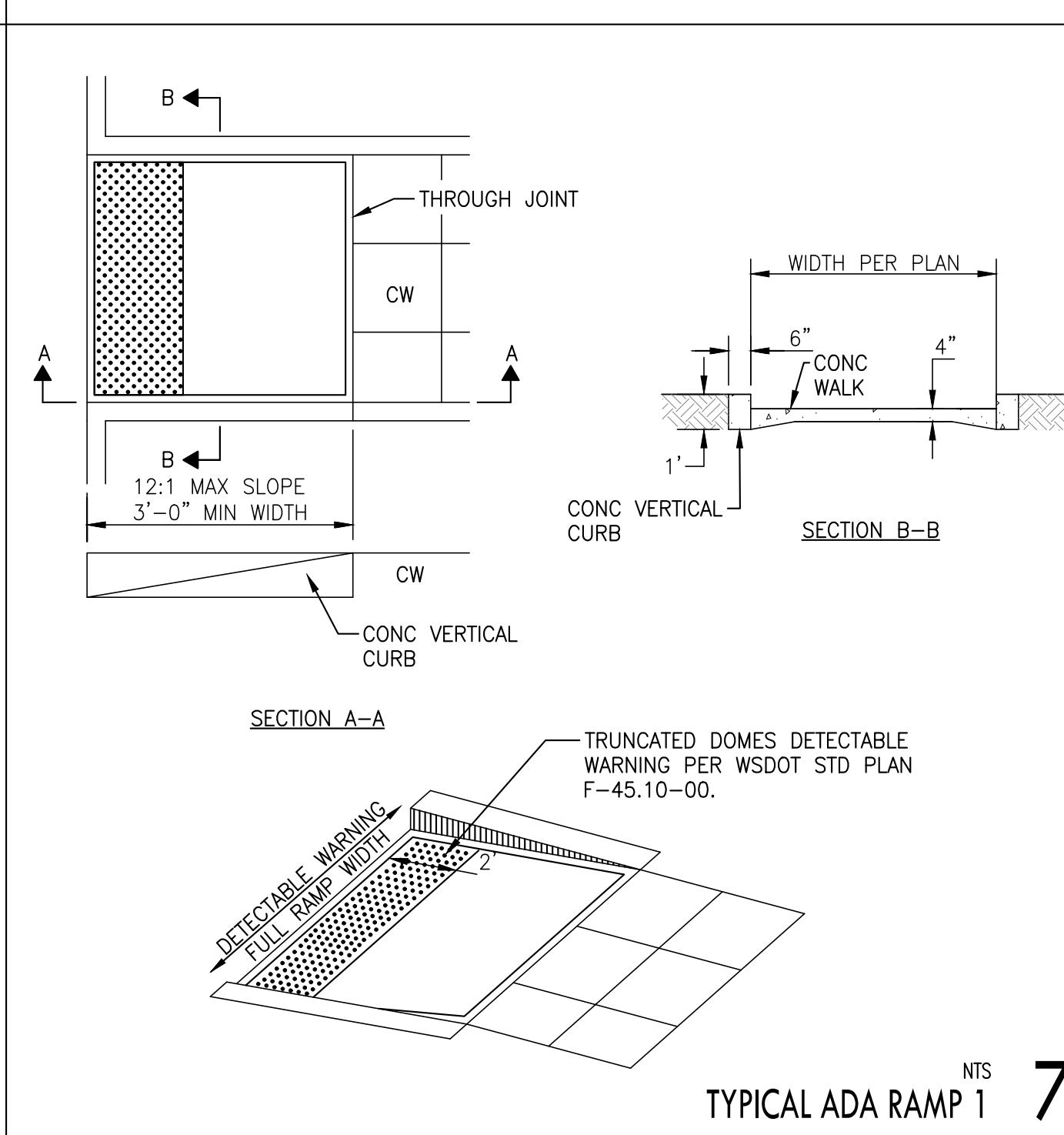
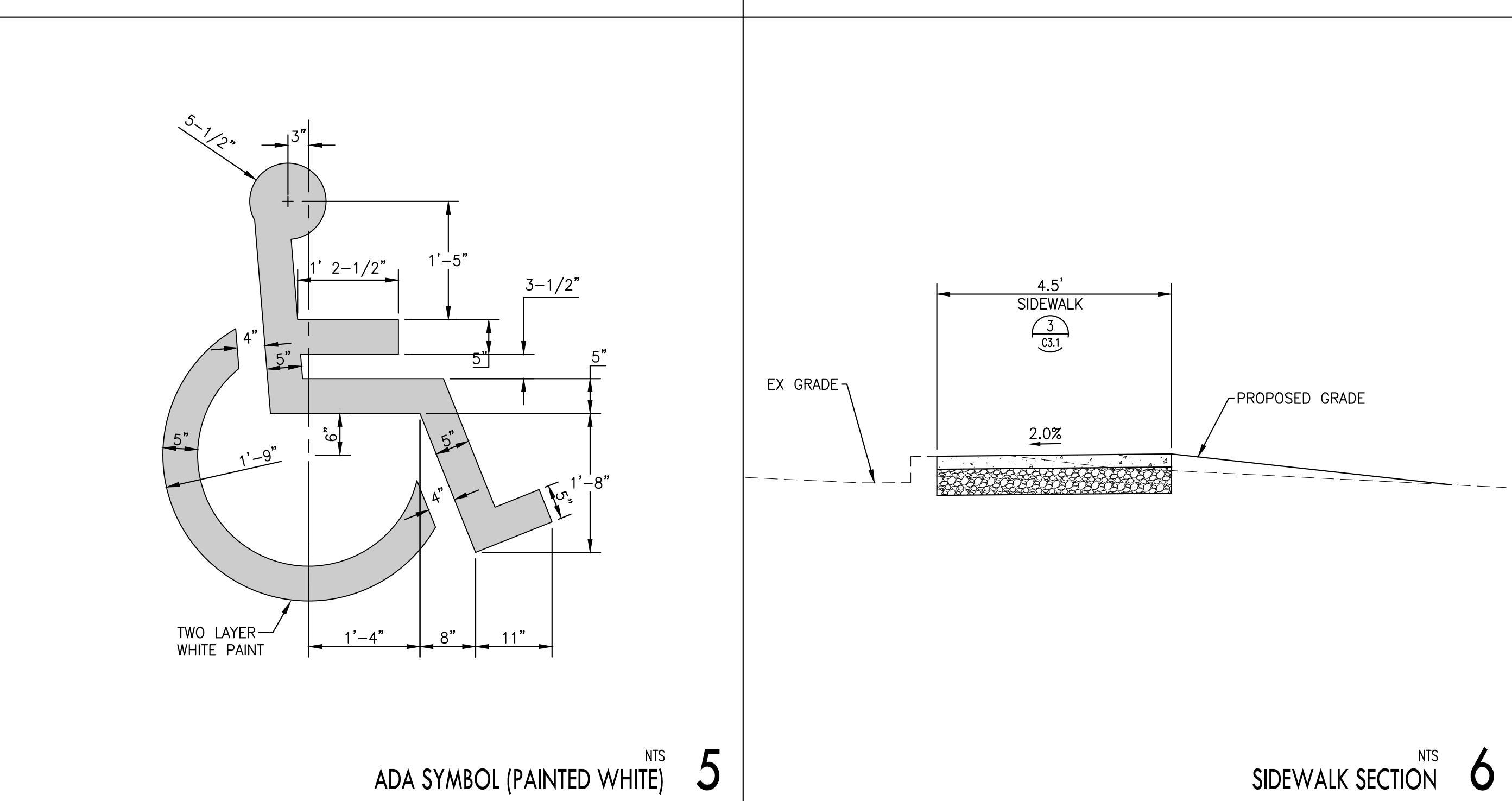
The logo for LPD engineering PLLC. It features the letters 'L', 'P', and 'D' in a large, bold, white serif font, each contained within a separate black rectangular box. Below the boxes, the words 'engineering' and 'pllc' are written in a smaller, bold, white sans-serif font.

---

Consultant Registration      Architect Registration

A circular professional engineer registration seal. The outer ring contains the text "LAURIE J. PFARR" at the top and "PROFESSIONAL ENGINEER" at the bottom. The inner circle features a profile of a man with a large, prominent nose, wearing a top hat and a coat. The text "STATE OF WASHINGTON" is written around the top of the inner circle, and "30766" is at the bottom. The word "REGISTERED" is written vertically along the left side of the inner circle.

Issues and Revisions				
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Development Engineering Manager Date

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City Engineer Date

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RV:

## C3. 1

# AS-BUILT

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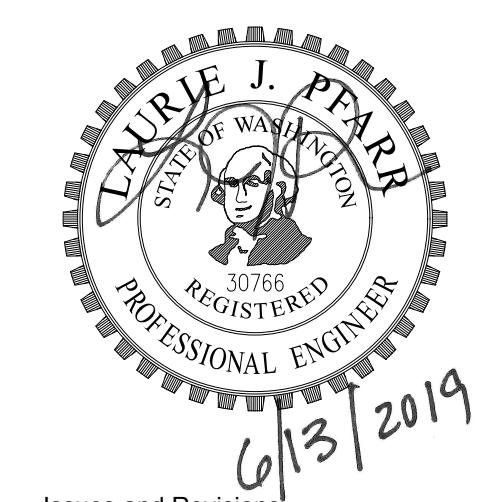
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Development Engineering Manager Date

City Engineer Date

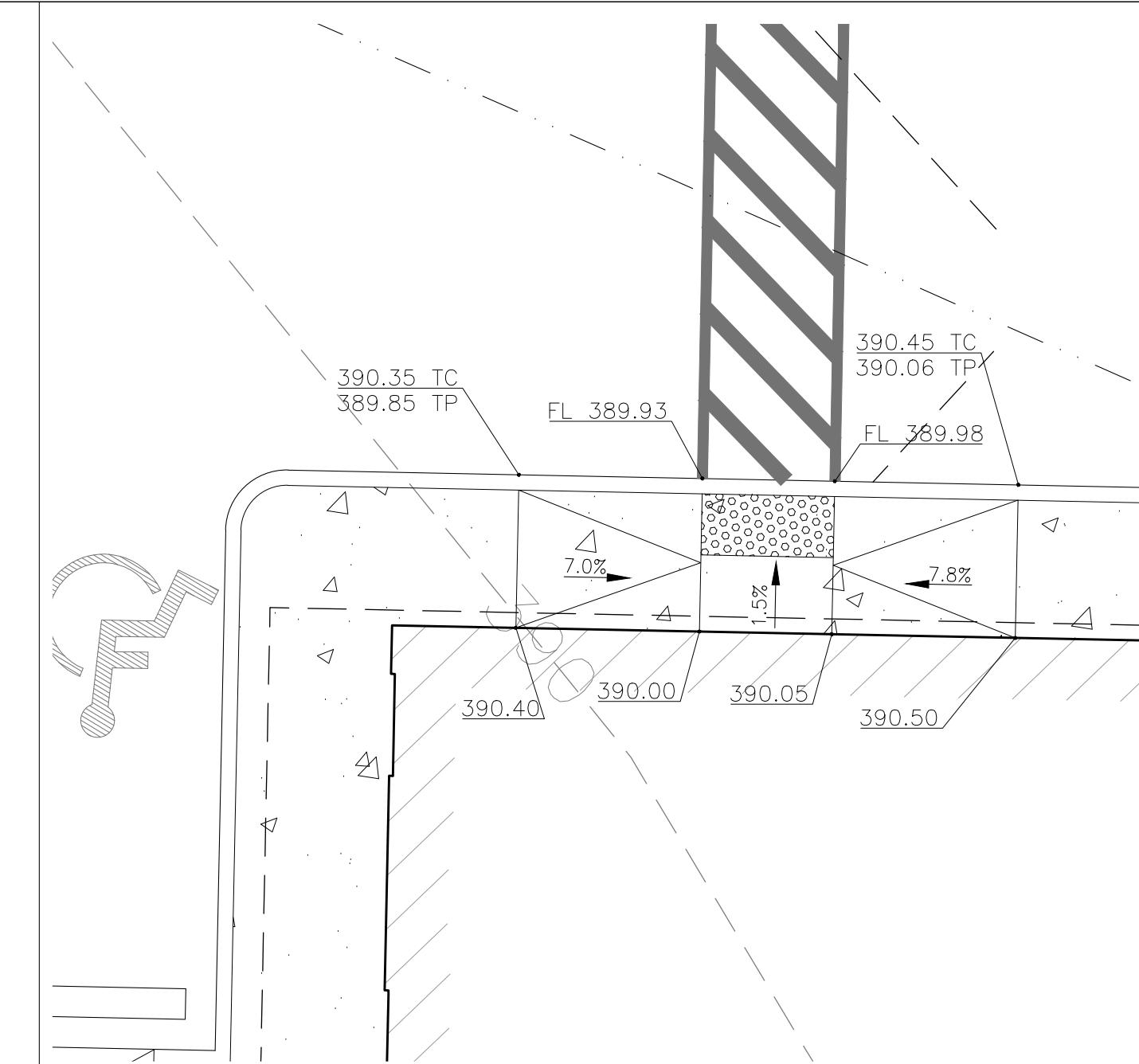
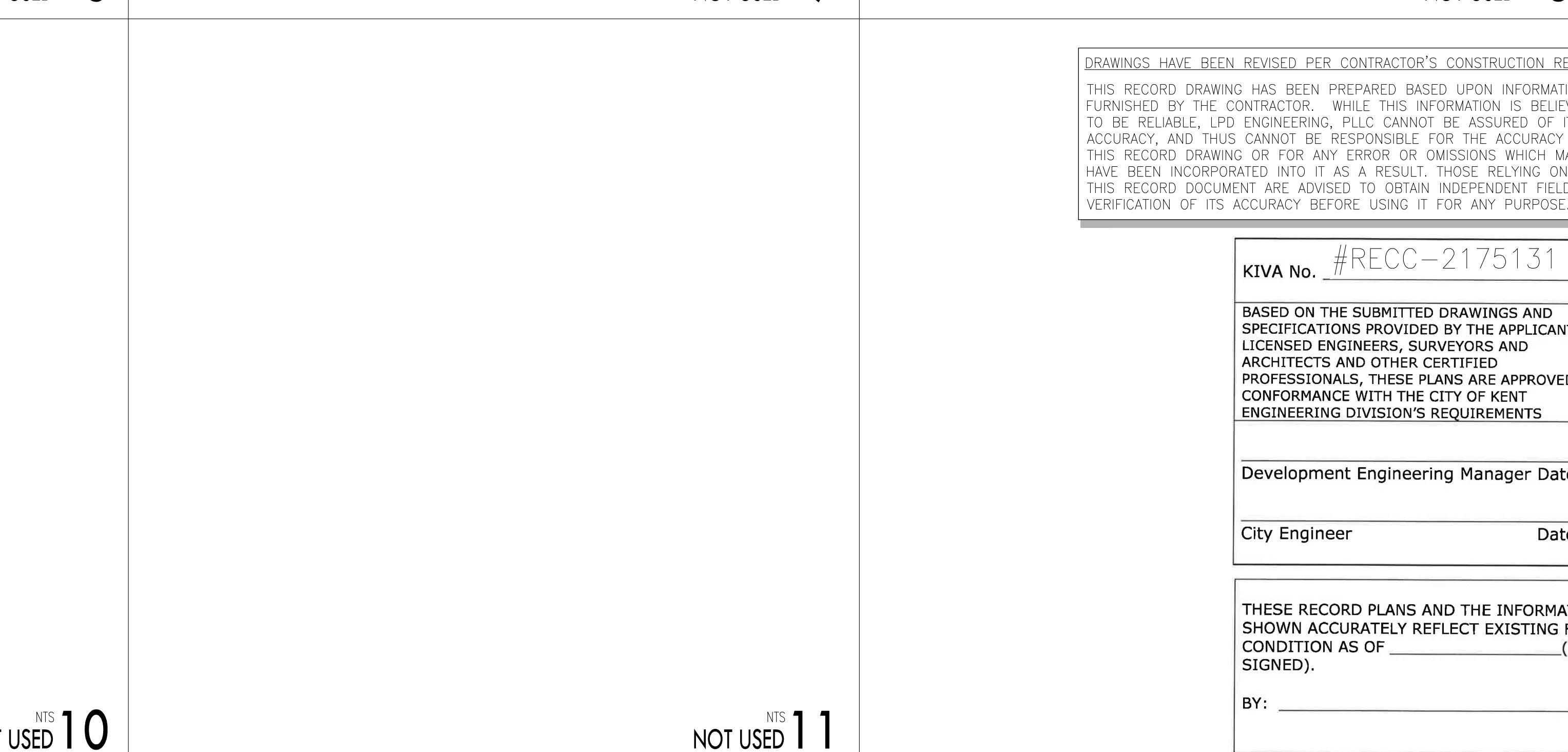
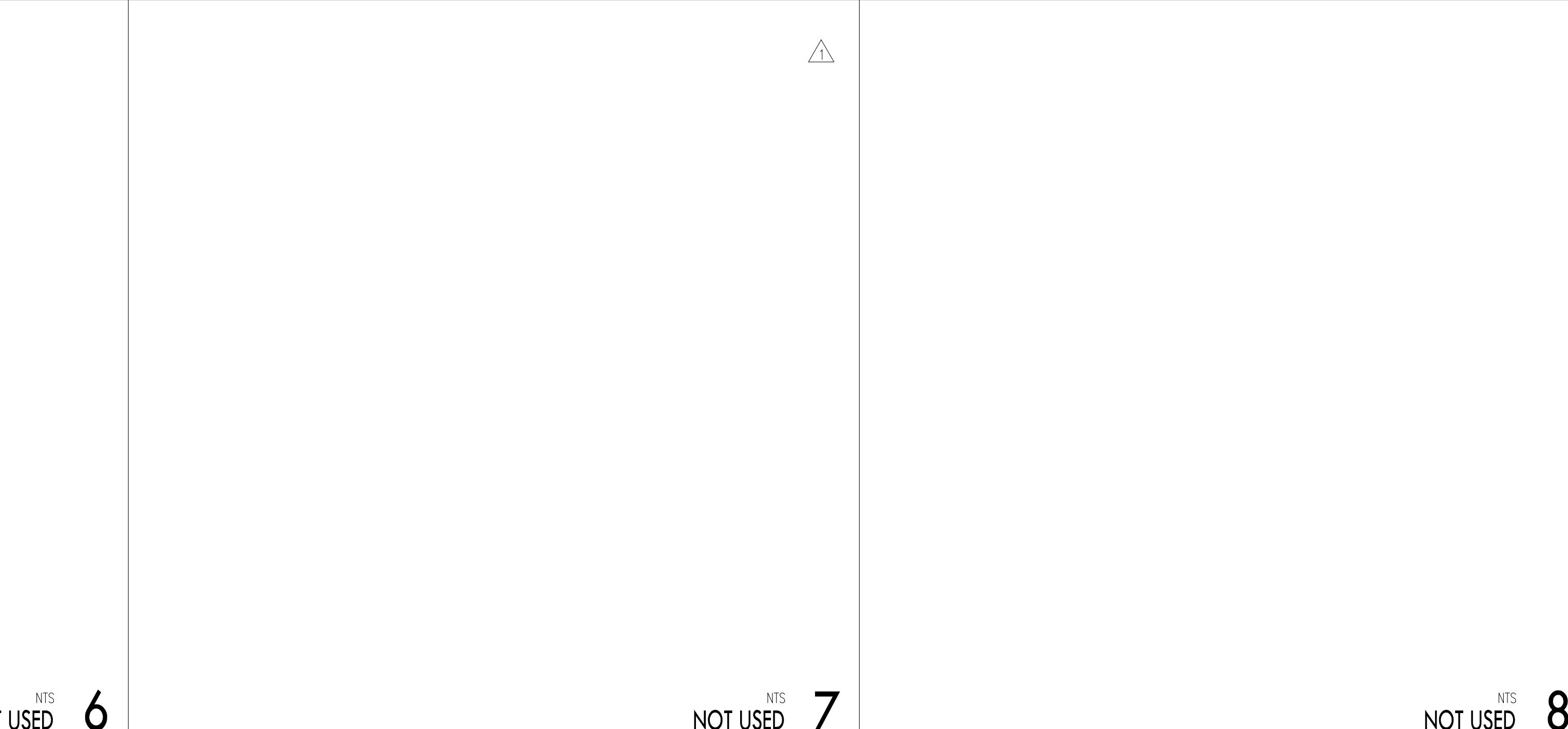
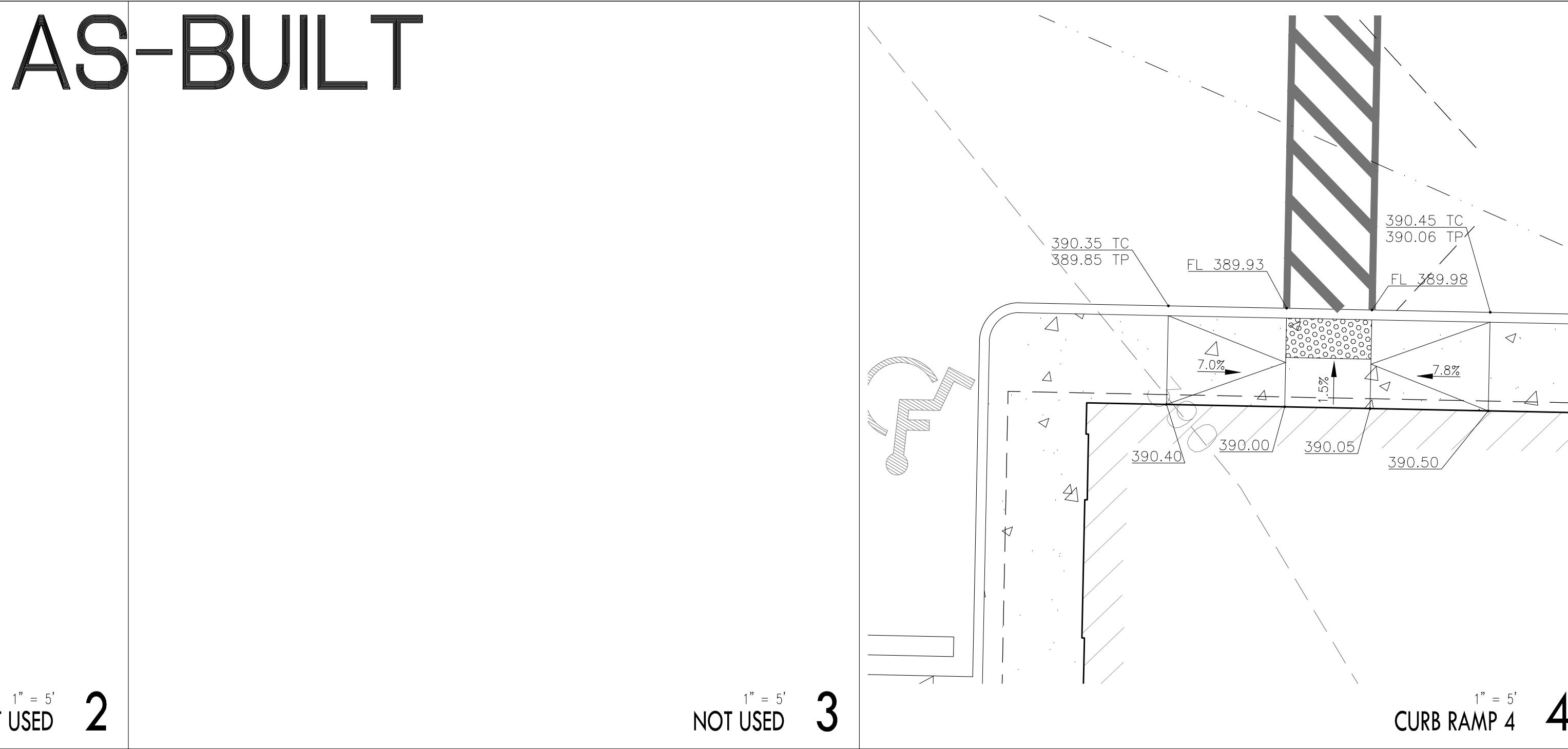
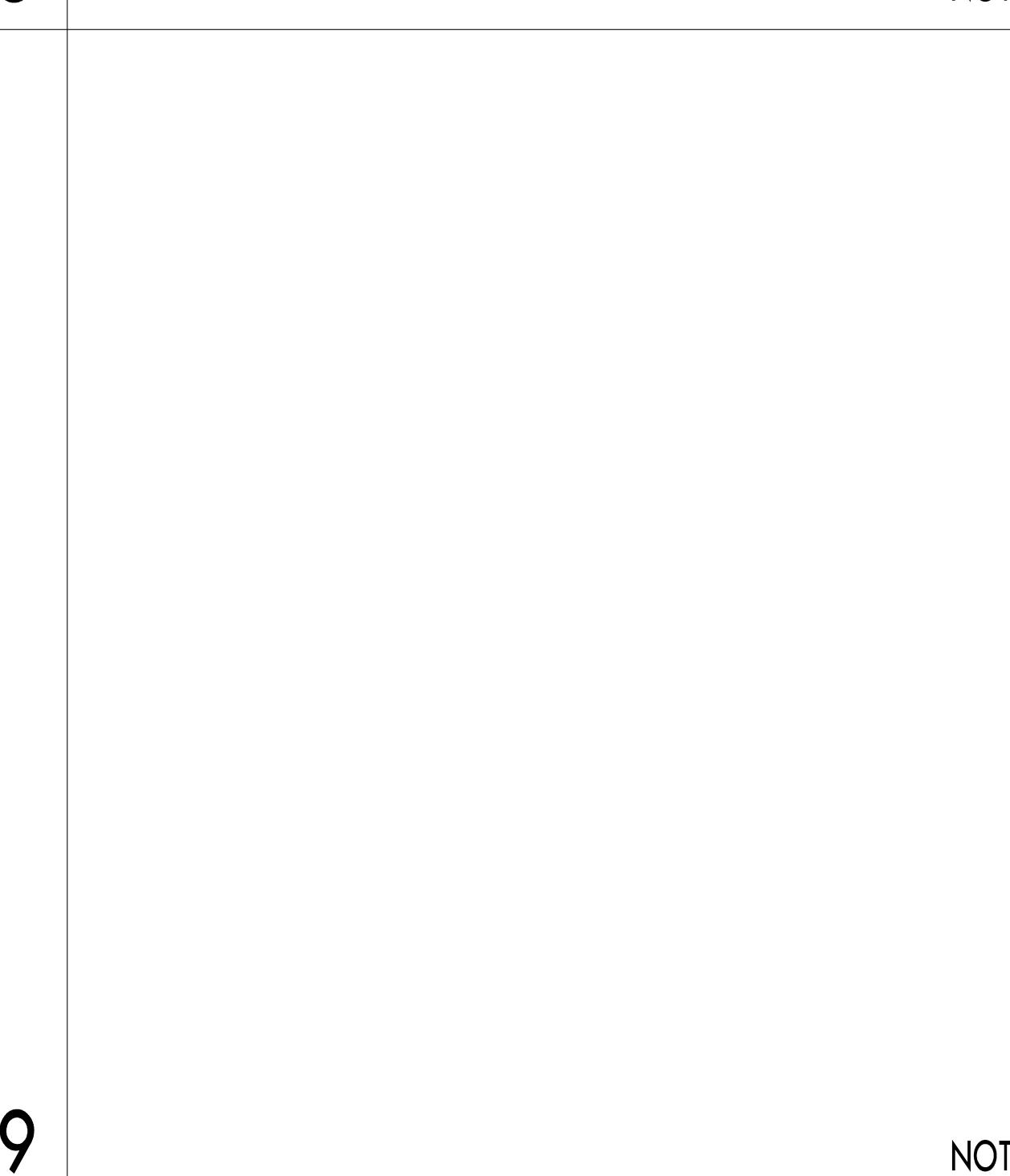
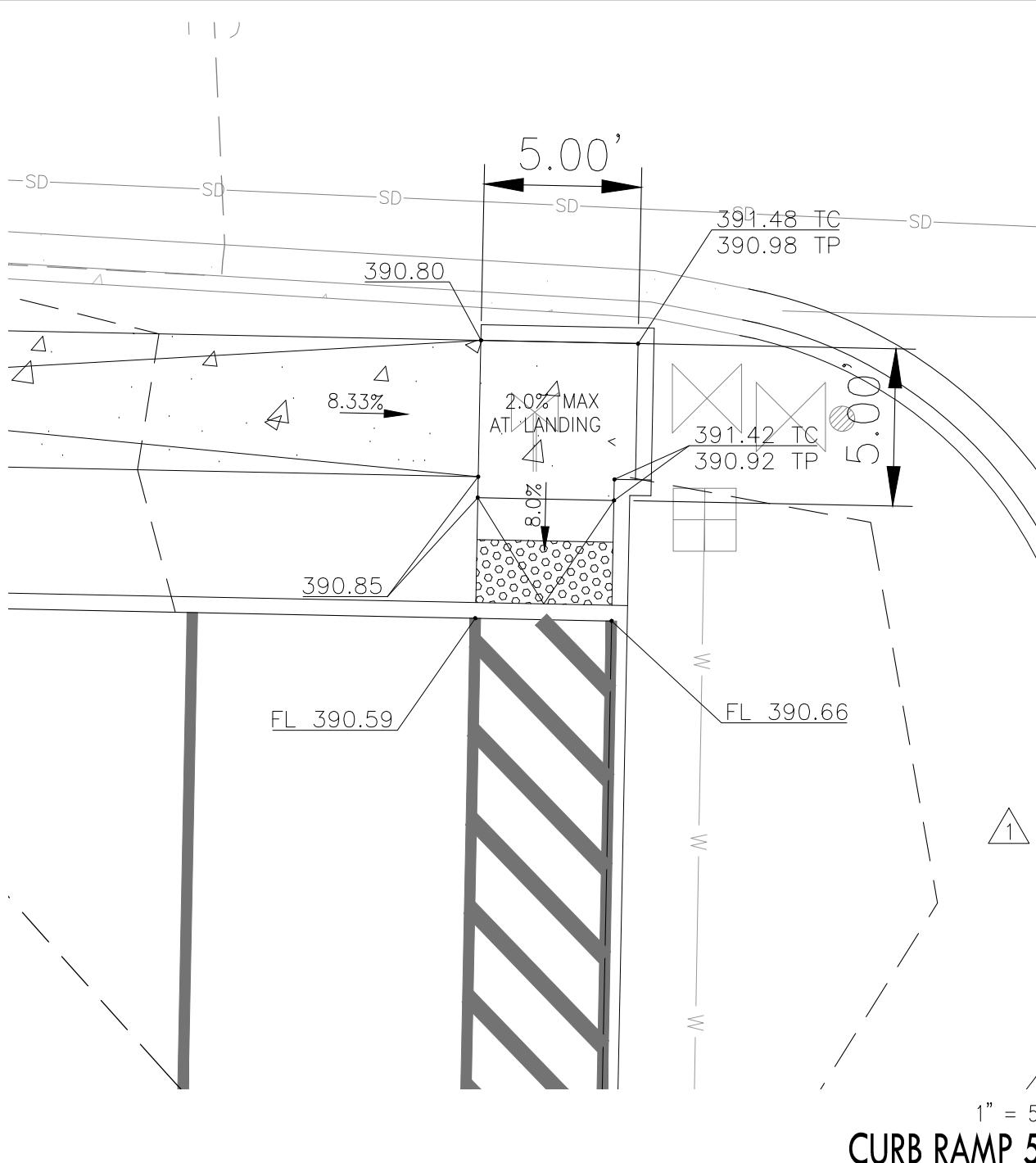
THESE RECORD PLANS AND THE INFORMATION  
SHOWN ACCURATELY REFLECT EXISTING FIELD  
CONDITION AS OF \_\_\_\_\_ (DATE  
SIGNED).

BY: \_\_\_\_\_

Project Name	DICK'S DRIVE-IN
Project Number	
Description	UTILITIES AND PAVING DETAILS
Computer File	
Scale	AS NOTED
Template	4.3 (120101)

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C3.2



# AS-BUILT

E:\CADD-2015\DIRKS DRIVE-IN\logo\RED LOGO.jpg

*Dick's Drive-in*  
Kent  
24050 Pacific Hwy S.  
Kent, WA 98032

Smith Co.

1725 westlake avenue n  
suite 210  
seattle, washington 98109

206.838.5485 p  
206.713.4076 m  
206.464.0700 f

[ksmith@smithco.org](mailto:ksmith@smithco.org)  
<http://www.smithco.org/>

## Consultant

The logo for LPD engineering PLLC. It features a large, bold, black 'L' and 'P' on the left, and a large, bold, black 'D' on the right, all contained within black rectangular boxes. Below the 'L' and 'P' boxes, the word 'engineering' is written in a smaller, black, sans-serif font. To the right of the 'D' box, the word 'pllc' is written in a smaller, black, sans-serif font. To the right of the logo, the company's address, phone number, and website are listed.

1932 First Ave  
Suite 201  
Seattle, WA 98101  
p. 206.725.1211  
f. 206.973.5344  
[www.lpdengineering.com](http://www.lpdengineering.com)

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Consultant Registration      Architect Registration

---

Consultant Registration      Architect Registration

Issues and Revisions				
No.	Date	Issues	By	Check
1	12.28.17	PERMIT	LJP	
2	02.27.18	PERMIT RESUBMITTAL	LJP	
3	03.16.18	PERMIT RESUBMITTAL	LJP	
4	08.08.18	SIDEWALK DELETION	LJP	
5	06/13/19	RECORD DRAWINGS	LJP	

KTM-A-N #RFC00-2175131

BASED ON THE SUBMITTED DRAWINGS AND SPECIFICATIONS PROVIDED BY THE APPLICANT'S LICENSED ENGINEERS, SURVEYORS AND ARCHITECTS AND OTHER CERTIFIED PROFESSIONALS, THESE PLANS ARE APPROVED FOR CONFORMANCE WITH THE CITY OF KENT ENGINEERING DIVISION'S REQUIREMENTS

---

Development Engineering Manager Date

**City Engineer** **Date**

THESE RECORD PLANS AND THE INFORMATION SHOWN ACCURATELY REFLECT EXISTING FIELD CONDITION AS OF \_\_\_\_\_ (DATE SIGNED).

BY:

Project Name DICK'S DRIVE-IN

Project Number

## REDI ROCK WALL SECTIONS

### Computer File

---

Scale AS NOTED

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## 22.2

**C3,3**

# FRONTAGE SECTION - ONE WALL 2

# AS-BUILT

E:\CADD-2015\DIKS DRIVE-IN\logo\RED LOGO.jpg

Dick's Drive-in  
Kent  
24050 Pacific Hwy S.  
Kent, WA 98032

smith co.

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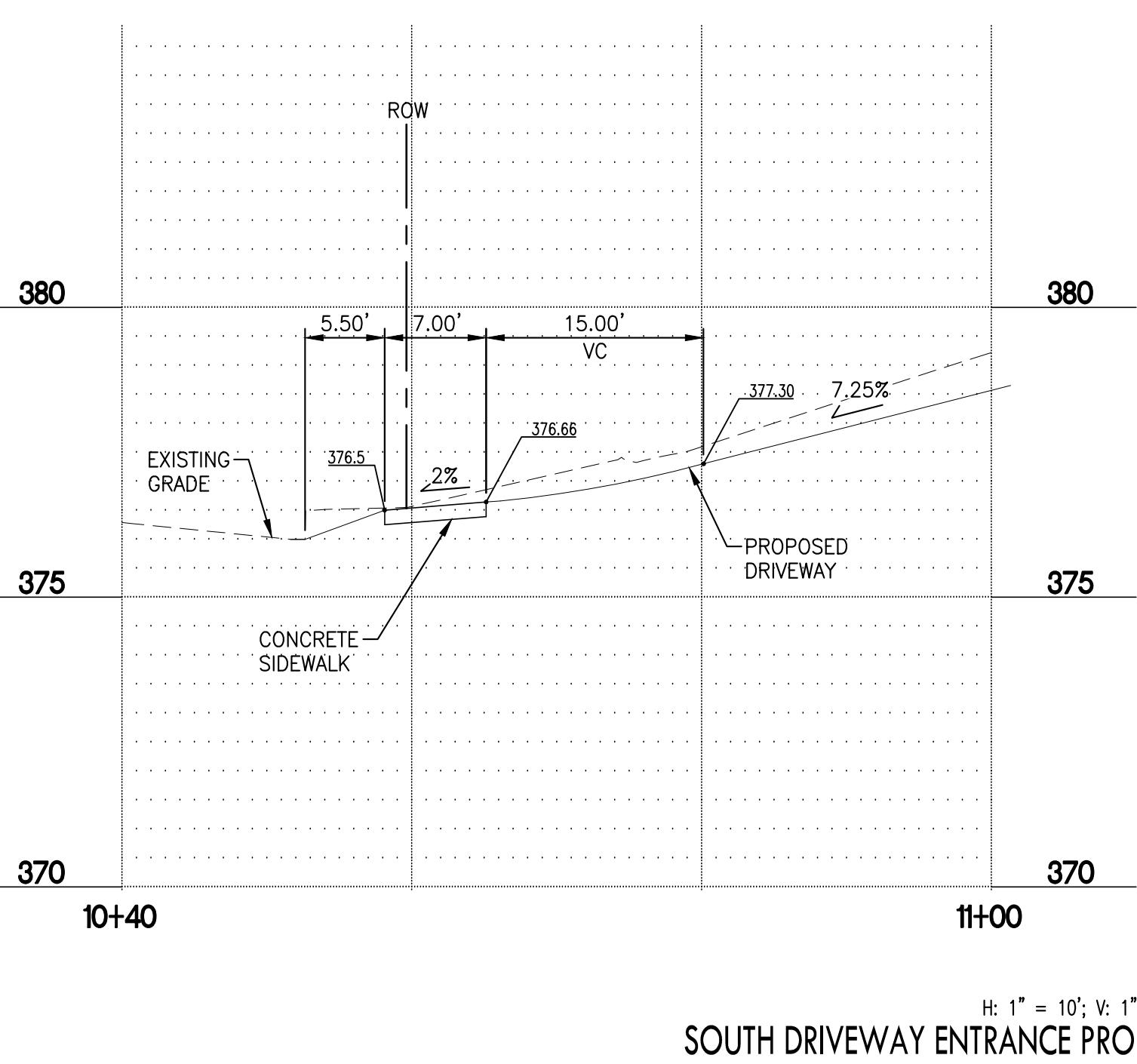
Consultant

LPD 1932 First Ave  
Suite 201  
Seattle, WA 98101  
p. 206.725.1211  
f. 206.973.5344  
engineering pllc www.lpdengineering.com

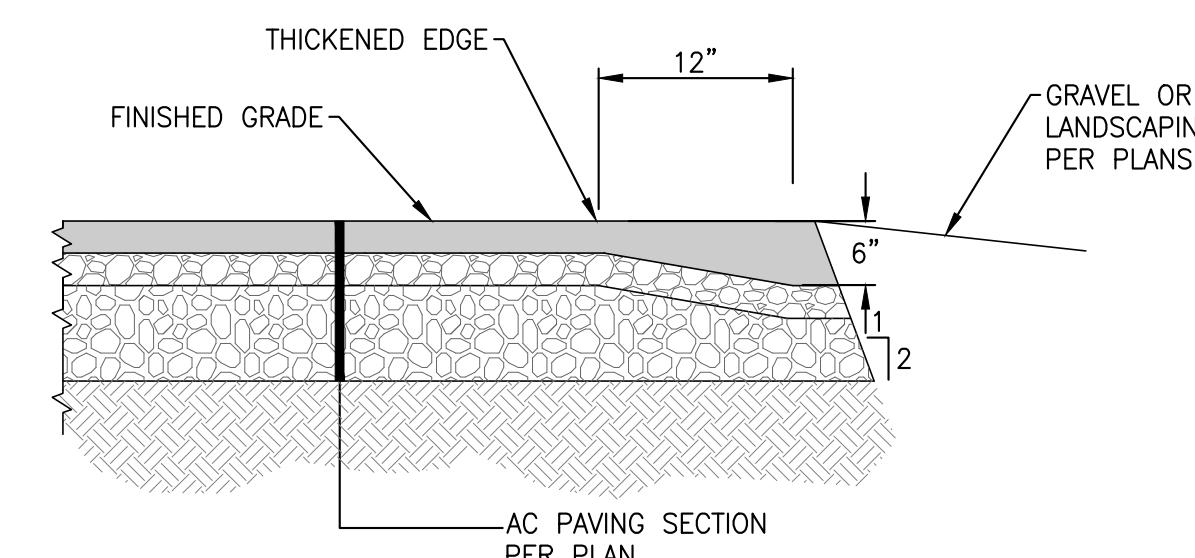
Consultant Registration Architect Registration



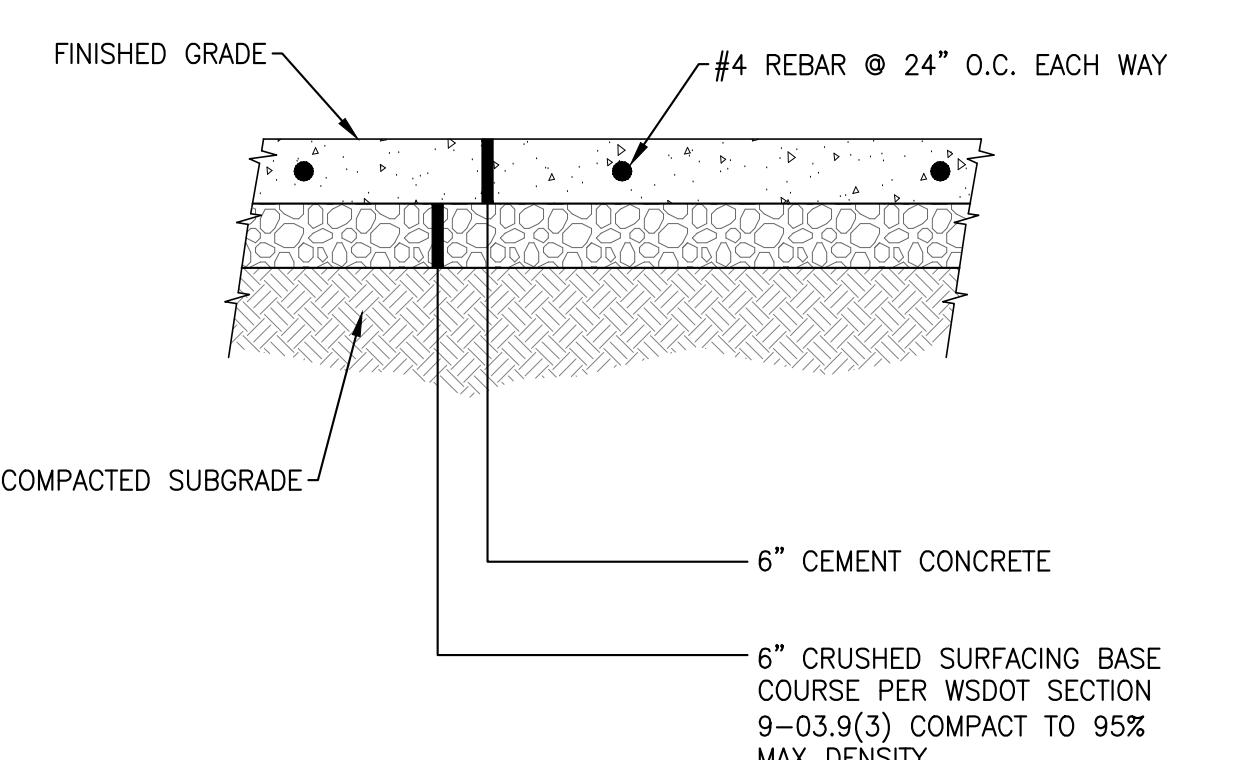
Issues and Revisions	
No.	Date
1	12.28.17
2	02.27.18
3	03.16.18
4	08.08.18
5	06/13.19
	Issues By Check
1	PERMIT LJP
2	PERMIT RESUBMITTAL LJP
3	PERMIT RESUBMITTAL LJP
4	SIDEWALK DELETION LJP
5	RECORD DRAWINGS LJP



NOT USED 1

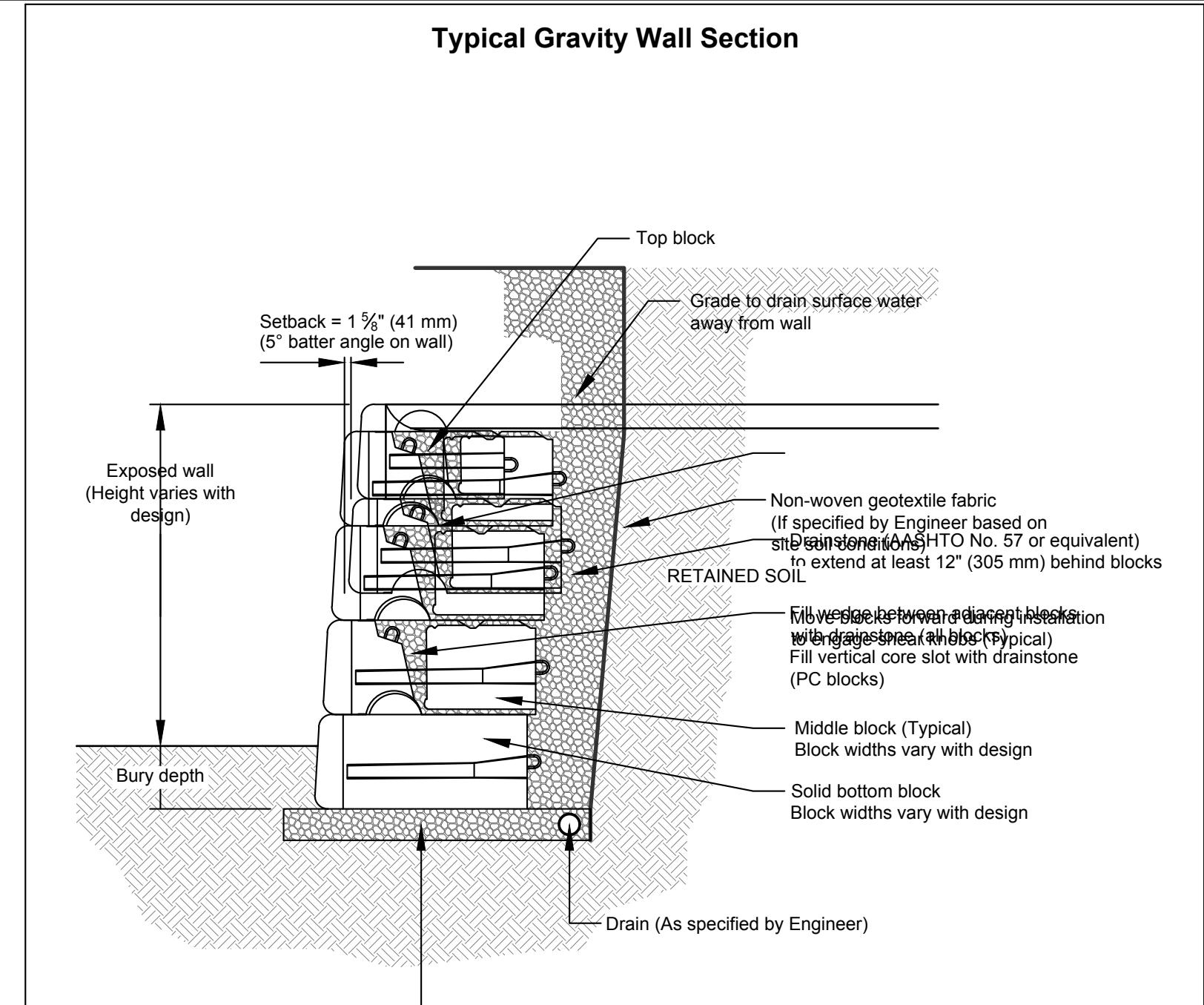


NOT USED 3  
THICKENED ASPHALT EDGE



NOT USED 4

HEAVY DUTY CONCRETE 5

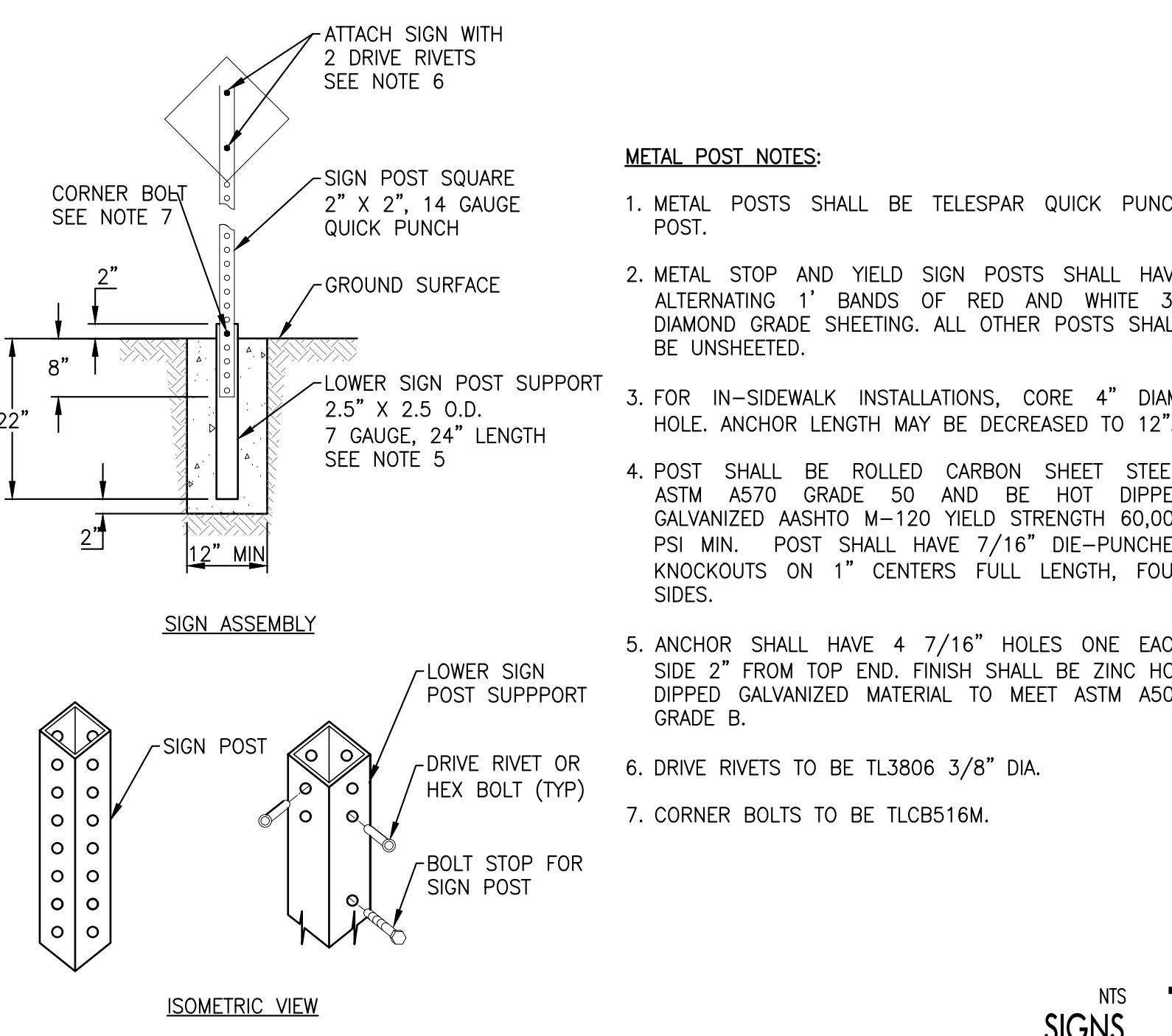


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DRAWN BY:	APPROVED BY:	DATE:	TITLE:	FILE:
JRJ	JRJ	17MAR2016	Typical Gravity Wall Detail	REDI-ROCK® 05481 US 31 SOUTH, CHARLEVOIX, MI 49720 (866) 222-8400 ext 3010 • engineering@redi-rock.com www.redi-rock.com
			1 of 1	1 Typical Gravity Wall Detail 031716.dwg

NOT USED 6  
TYPICAL REDI-ROCK GRAVITY WALL

KIVA No. #RECC-2175131	
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Development Engineering Manager Date	
City Engineer Date	
Computer File	
Scale AS NOTED	
Template 4.3 (20101)	
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DRAWINGS HAVE BEEN REVISED PER CONTRACTOR'S CONSTRUCTION RECORDS	
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BY: _____	



ADA PARKING SIGN 8

C3.4