

DRAWINGS HAVE BEEN REVISED PER CONTRACTOR'S CONSTRUCTION RECORDS

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## CITY OF KENT GENERAL SEWER NOTES

- ALL SANITARY SEWER IMPROVEMENTS SHALL BE CONSTRUCTED IN ACCORDANCE WITH APPROVED PLANS. ANY DEVIATION FROM THESE PLANS WILL REQUIRE PRIOR APPROVAL FROM THE OWNER, THE CITY ENGINEER, AND OTHER APPROPRIATE PUBLIC AGENCIES.
- ALL MANHOLES SHALL MEET THE REQUIREMENTS OF STANDARD DETAIL 4-1(B) OF THE CITY OF KENT CONSTRUCTION STANDARDS, WITH ECCENTRIC CONES AND 3/4" INCH DIAMETER SAFETY STEPS OR APPROVED LADDERS. ALL "SHALLOW MANHOLES" SHALL MEET THE REQUIREMENTS OF STANDARD DETAIL 4-1(A) OF THE CITY OF KENT CONSTRUCTION STANDARDS. ALL MANHOLE COVERS SHALL MEET THE REQUIREMENTS OF STANDARD DETAIL 4-5, AND SHALL READ "SEWER". ALL MANHOLES OUTSIDE OF PUBLIC RIGHT-OF-WAY, AND/OR NOT LOCATED IN PAVED AREAS, SHALL HAVE SOLID, LOCKING COVERS MEETING THE REQUIREMENTS OF STANDARD DETAILS 4-6(A) & 4-6(B). ALL SANITARY SEWER MANHOLES SHALL BE CHANNELLED TO MEET THE REQUIREMENTS OF WSDOT STANDARD PLAN B-23. ALL MANHOLE COVERS SHALL BE ADJUSTED TO FINISH GRADE AFTER THE COMPLETION OF FINAL PAVING AND LANDSCAPING.
- ALL SANITARY SEWER PIPE SHALL CONFORM TO ONE OF THE FOLLOWING SPECIFICATIONS:
  - DUCTILE IRON - CLASS 50 (ONLY WHERE SPECIFIED ON THE PLANS).
  - PVC - CONFORMING TO ASTM D-3034, SDR 35, OR CONFORMING TO ASTM D-3035, SDR 21 WITH FITTINGS AND GASKETS CONFORMING TO THE CITY OF KENT CONSTRUCTION STANDARDS FOR THE DEPTH OF COVER.
- ALL DUCTILE IRON PIPE SHALL HAVE CLASS C DESIGN PIPE BEDDING AND ALL PVC PIPE SHALL HAVE CLASS F DESIGN PIPE BEDDING MEETING THE REQUIREMENTS OF WSDOT STANDARD PLAN B-180.
- ALL TRENCH BACKFILL SHALL BE COMPACTED TO AT LEAST 95 PERCENT OF THE MAXIMUM DRY DENSITY PER ASTM D-1557 (MODIFIED PROCTOR). FOR THE FIRST THREE FEET ABOVE THE PIPE, THE REMAINING BACKFILL MAY BE TO 90 PERCENT OF MAXIMUM DRY DENSITY IF NOT IN AN AREA OF FUTURE PAVEMENT OR STRUCTURAL FILL; OTHERWISE THE REMAINING BACKFILL SHALL ALSO BE COMPACTED TO AT LEAST 95 PERCENT OF THE MAXIMUM DRY DENSITY. TRENCH BACKFILL AND COMPACTION SHALL BE COMPLETED PRIOR TO TESTING LINES FOR LEAKAGE.
- ALL SIDE SEWER STUBS SHALL HAVE A MINIMUM 6-INCH DIAMETER AND A MINIMUM SLOPE OF 1 PERCENT. PROPERTY LOT CORNERS AND/OR BUILDING LOCATIONS AND FINISH GRADES SHALL BE ESTABLISHED IN THE FIELD PRIOR TO ANY SEWER CONSTRUCTION. SIDE SEWER TIE CONNECTIONS SHALL BE REFERENCED FROM THE NEAREST DOWNSTREAM MANHOLE. ALL SIDE SEWERS SHALL BE TESTED AT THE TIME OF MAIN LINE TESTING. THE CONTRACTOR SHALL PROVIDE THE CITY WITH ACCURATE AS-BUILT DRAINAGE SEWER LOCATIONS. SEE CITY OF KENT DEVELOPMENT ASSISTANCE BROCHURE (HEREINAFTER DAB) #E-6, AS-BUILT DRAWINGS, FOR ADDITIONAL INFORMATION REGARDING AS-BUILT DRAWINGS.
- WHERE A WATER MAIN CROSSES ABOVE THE SANITARY SEWER, AND LESS THAN 1.5 FEET OF VERTICAL CLEARANCE EXISTS BETWEEN THE TOP OF THE SEWER MAIN AND BOTTOM OF WATER MAIN, THE SEWER MAIN SHALL BE DEMOLISHED, OR RECONSTRUCTED, WITH DUCTILE IRON PIPE (CLASS 50) CENTERED ON THE WATER MAIN FOR A MINIMUM HORIZONTAL DISTANCE OF ONE CONTINUOUS 18-FOOT NOMINAL PIPE LENGTH WITH NO JOINTS.
- SANITARY SEWER MAIN LINES 8-INCH DIAMETER AND LARGER SHALL NOT BE CONSTRUCTED ANY CLOSER THAN 10 FEET TO ANY BUILDING OR 7.5 FEET TO ANY CARPORT.
- NEW CONNECTIONS TO EXISTING FACILITIES SHALL BE SEALED OFF UNTIL UPSTREAM CONSTRUCTION IS FINISHED, TESTED, CLEANED AND ACCEPTED. ALL CONSTRUCTION DEBRIS AND WATER SHALL BE REMOVED PRIOR TO OPENING THE SEAL. ALL TV INSPECTION AND PRESSURE TESTING SHALL BE IN THE PRESENCE OF A CITY OF KENT INSPECTOR.
- FOR PUBLIC SANITARY SEWERS, AN EASEMENT MUST BE PREPARED BY A PROFESSIONAL LAND SURVEYOR LICENSED IN THE STATE OF WASHINGTON, AND CONVEYED TO THE CITY OF KENT ON FORMS APPROVED BY AND/OR PROVIDED BY THE CITY. EASEMENTS SHOULD BE 15 FEET WIDE (OR 25 FEET, AS SHOWN), 7.5 OR 12.5 FEET ON EACH SIDE OF THE PIPE.
- PRIOR TO ACCEPTANCE OF CONSTRUCTION AND/OR ISSUANCE OF CERTIFICATE OF OCCUPANCY PERMITS, SURVEYOR-CERTIFIED AS-BUILT DRAWINGS PREPARED ON MYLAR MUST BE PREPARED OF ALL PUBLIC SANITARY SEWER FACILITIES AND APPROVED BY THE CITY. SEE CITY OF KENT DEVELOPMENT ASSISTANCE BROCHURE (HEREINAFTER DAB) #E-6, AS-BUILT DRAWINGS, FOR ADDITIONAL INFORMATION REGARDING AS-BUILT DRAWINGS.

## CITY OF KENT GENERAL CONSTRUCTION NOTES

- ALL WORK AND MATERIALS SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE CITY OF KENT CONSTRUCTION STANDARDS. THESE STANDARDS INCORPORATE BY REFERENCE THE LATEST EDITION AND PUBLISHED AMENDMENTS OF THE STANDARD SPECIFICATIONS FOR ROADS, BRIDGES, AND MUNICIPAL CONSTRUCTION, PREPARED JOINTLY BY WSDOT & APWA. THE CITY HAS ALSO ADOPTED SPECIAL PROVISIONS, WHICH MODIFY THESE STANDARD SPECIFICATIONS, AND THE CITY OF KENT SPECIAL PROVISIONS WILL GOVERN WHERE ANY CONFLICTS OCCUR.
- PRIOR TO ANY CONSTRUCTION ACTIVITY, THE CONTRACTOR SHALL ATTEND A PRE-CONSTRUCTION CONFERENCE WITH CITY OF KENT CONSTRUCTION AND INSPECTION PERSONNEL. THE CONTRACTOR SHALL SCHEDULE THE PRE-CONSTRUCTION CONFERENCE AT 253-856-5500. PRIOR TO SCHEDULING, THE CONTRACTOR MUST SUBMIT AND RECEIVE APPROVAL FOR PROJECT CONTROL PLANS (TOP), TEMPORARY EROSION/SPILL A SOILS ENGINEER'S RECOMMENDATION AND TEST. IN NO CASE SHALL THE REPLACEMENT PAVEMENT SECTION BE LESS THAN THE EXISTING PAVEMENT SECTION.
- THE CONTRACTOR SHALL FURNISH, INSTALL AND OPERATE ALL EQUIPMENT NECESSARY TO KEEP EXCAVATIONS ABOVE THE FOUNDATION LEVEL FREE FROM WATER DURING CONSTRUCTION, AND SHALL Dewater AND DISPOSE OF THE WATER SO AS NOT TO CAUSE INJURY TO PUBLIC OR PRIVATE PROPERTY OR NUISANCE TO THE PUBLIC. THE CONTRACTOR SHALL AT ALL TIMES HAVE AVAILABLE SUFFICIENT PUMPING EQUIPMENT IN GOOD WORKING CONDITION FOR ALL EMERGENCIES, INCLUDING POWER OUTAGES, AND SHALL HAVE AVAILABLE AT ALL TIMES COMPETENT WORKERS FOR THE OPERATION OF THE PUMPING EQUIPMENT.

## CITY OF KENT STORM DRAINAGE GENERAL NOTES

- ALL STORM DRAINAGE IMPROVEMENTS SHALL BE CONSTRUCTED IN ACCORDANCE WITH THESE APPROVED ANY DEVIATION FROM THESE PLANS WILL REQUIRE PRIOR APPROVAL FROM THE OWNER, THE CITY ENGINEER, AND OTHER APPROPRIATE PUBLIC AGENCIES.
- IT IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR TO INDEPENDENTLY VERIFY ALL KNOWN UTILITY LOCATIONS, AND TO DISCOVER AND AVOID ANY PREVIOUSLY UNKNOWN UTILITIES THAT MAY BE AFFECTED. THE CONTRACTOR SHALL CONTACT THE UNDERGROUND UTILITIES LOCATION SER VICE (1-800-424-5555) AT LEAST 48 HOURS PRIOR TO CONSTRUCTION. THE OWNER OR HIS REPRESENTATIVE, AND THE CITY ENGINEER SHALL BE CONTACTED IMMEDIATELY WHEN A CONFLICT IS FOUND TO EXIST.
- ALL STORM DRAINAGE STRUCTURES, AS DESIGNATED IN THE PLANS, SHALL BE ONE OF THE FOLLOWING:
  - YARD DRAIN - ASSOCIATED SAND AND GRAVEL CB-10, OR EQUAL.
  - INLET - WSDOT STANDARD PLAN B-4; ASSOCIATED SAND & GRAVEL CB-17, OR EQUAL.
  - TYPE I-C CATCH BASIN - CITY OF KENT STANDARD DETAIL 5-6(A) (WITH MINIMUM 24 INCHES DEEP SUMP BELOW PIPE INVERT); ASSOCIATED SAND AND GRAVEL 54 INCHES PRE-CAST CATCH BASIN, OR EQUAL.
  - TYPE I-D CATCH BASIN - ASSOCIATED SAND AND GRAVEL CB-6, OR EQUAL.
  - TYPE II, 48 INCH DIAMETER CATCH BASIN - CITY OF KENT STANDARD DETAIL 5-8(A) (WITH MINIMUM 24 INCHES DEEP SUMP BELOW INVERT); ASSOCIATED SAND AND GRAVEL CB-19, OR EQUAL.
  - TYPE II, 54 INCH DIAMETER CATCH BASIN - CITY OF KENT STANDARD DETAIL 5-8(A) (WITH MINIMUM 24 INCHES DEEP SUMP BELOW PIPE INVERT); ASSOCIATED SAND AND GRAVEL 54 INCHES PRE-CAST CATCH BASIN, OR EQUAL.
  - TYPE II, 72 INCH DIAMETER CATCH BASIN - CITY OF KENT STANDARD DETAIL 5-8(B) (WITH MINIMUM 24 INCHES DEEP SUMP BELOW PIPE INVERT); ASSOCIATED SAND AND GRAVEL 72 INCH PRE-CAST CATCH BASIN, OR EQUAL.
  - SPECIAL CATCH BASINS OR DRAINAGE STRUCTURES SHALL BE AS DETAILED IN THESE PLANS.
- NOTE: ALL UNIT COVERS SHALL BE PER CITY OF KENT CONSTRUCTION STANDARDS AND ALL STRUCTURES DEEPER THAN FIVE FEET (RIM TO INVERT) SHALL BE TYPE II AND EQUIPPED WITH 3/4 INCH DIAMETER SAFETY MANHOLE STEPS OR A MAN HOLE LADDER, PER CITY OF KENT CONSTRUCTION STANDARDS.
- ALL STORM SEWER MANHOLES AND TYPE II CATCH BASINS SHALL BE PER CITY OF KENT STANDARD DETAIL 5-8(A) AND/OR 5-8(B) OF THE KENT CONSTRUCTION STANDARDS, WITH ECCENTRIC CONES AND 3/4 INCH DIAMETER SAFETY STEPS OR LADDER. ALL STORM SEWER MANHOLE COVERS SHALL MEET THE REQUIREMENTS OF STANDARD DETAIL 4-5 OF THE CITY OF KENT CONSTRUCTION STANDARDS AND SHALL READ "STORM". ALL MANHOLES OUTSIDE OF PUBLIC RIGHT-OF-WAY AND NOT LOCATED IN PAVED AREAS SHALL HAVE SOLID, LOCKING COVERS MEETING THE REQUIREMENTS OF STANDARD DETAILS 4-6(A) & 4-6(B) OF THE CITY OF KENT CONSTRUCTION STANDARDS. ALL STORM SEWER MANHOLES SHALL HAVE A MINIMUM 0.10 FOOT TO A MAXIMUM 1.0 FOOT DROP BETWEEN INVERTS INTO AND INVERTS OUT OF THE MANHOLE. ALL MANHOLE COVERS SHALL BE ADJUSTED TO FINISH GRADE AFTER FINAL PAVING AND LANDSCAPING.
- ALL FRAMES, GRATES AND/OR SOLID COVERS SHALL BE CAST IRON OR DUCTILE IRON AND SHALL MEET CITY OF KENT CONSTRUCTION STANDARDS:
  - STANDARD FRAME AND GRATE - CITY OF KENT STANDARD DETAIL 5-1(A) AND 5-1(B): OLYMPIC FOUNDRY CO. NO. SM60 D/T OR EQUAL.
  - SELF-LOCKING VANED GRATE - CITY OF KENT STANDARD DETAIL 5-1(D): OLYMPIC FOUNDRY CO. NO. SM50-SL OR EQUAL.
  - SOLID, LOCKING, RECTANGULAR COVER - CITY OF KENT STANDARD 5-1(C): OLYMPIC FOUNDRY CO. NO. SM60S D/T, OR EQUAL.
  - SOLID, LOCKING, ROUND COVER - CITY OF KENT STANDARD DETAILS 4-5, 4-6(A) & 4-6(B): OLYMPIC FOUNDRY CO. NO. MH30 D/T, OR MH36 W/T, RESPECTIVELY, OR EQUAL.
- NOTES: (1) ALL GRATES SHALL BE MARKED "DRAIN" OR "STREET" AND BE DEPRESSED 0.04 FEET FROM SURROUNDING CURB OR PAVEMENT TO INSURE ADEQUATE DRAINAGE. (2) ALL SOLID COVERS SHALL BE MARKED "DRAIN" (3) ALL GRATES SHALL BE DEPRESSED 0.04 FEET FROM SURROUNDING CURB OR PAVEMENT TO INSURE ADEQUATE DRAINAGE. (4) THE PAVEMENT ADJACENT TO ALL GRATES AND COVERS WILL BE PAINTED USING THE STANDARD POLLUTION PREVENTION STENCIL:

DUMP NO WASTE  
(FISH SYMBOL)  
DRAINS TO STREAM

STENCILS ARE AVAILABLE AT THE CITY OF KENT ENGINEERING OFFICE, LOCATED AT 400 WEST GOWE, KENT, WASHINGTON.

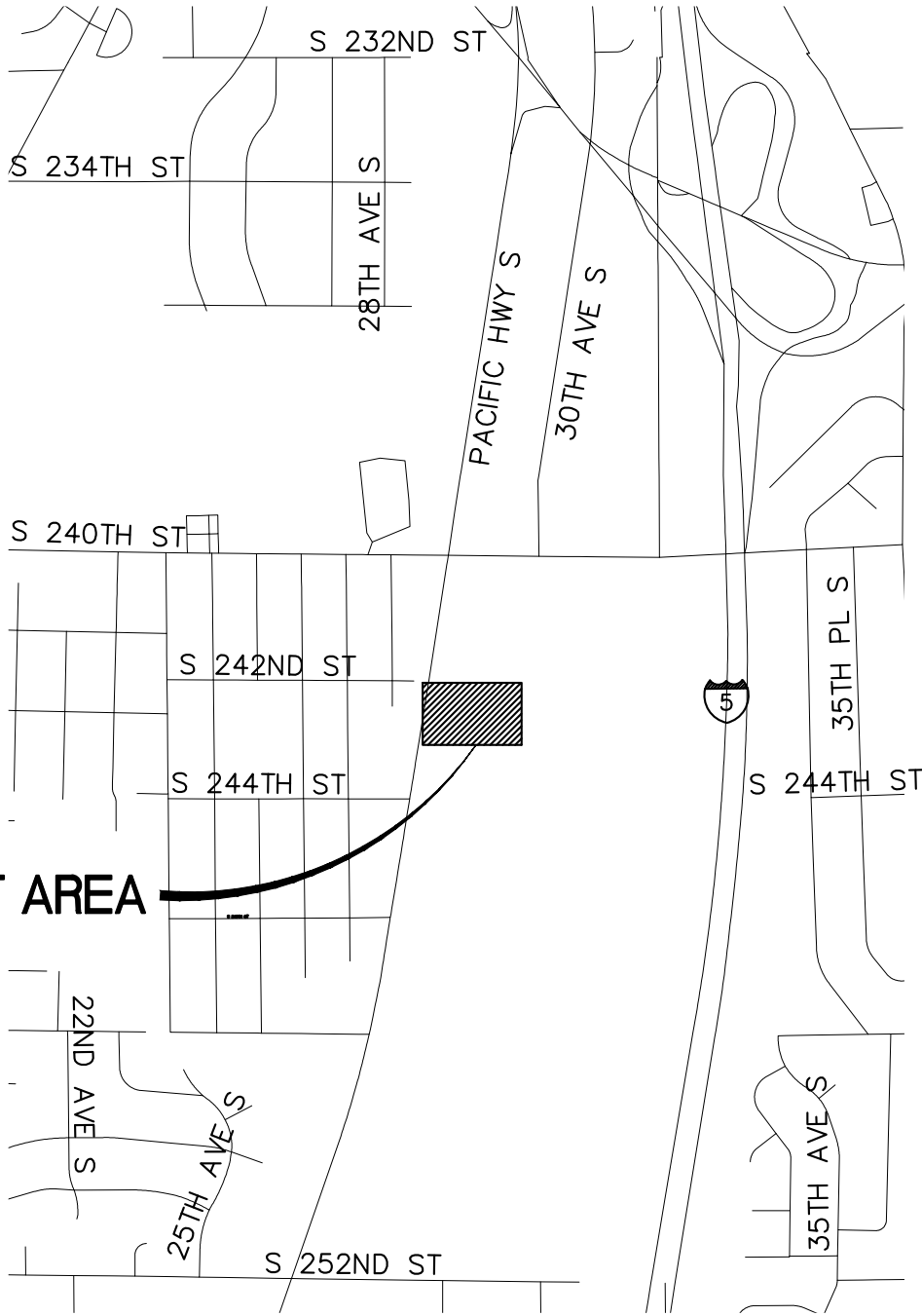
- NOTES:
- OLYMPIC FOUNDRY SELF-LOCK VANED GRATE SM50SSL IS AN ACCEPTABLE ALTERNATIVE SUBJECT TO APPROVAL BY THE CITY ENGINEER.
  - USE WITH TWO LOCKING BOLTS 5/8 INCH DIAMETER 11 UNC-2A STAINLESS TYPE 304 STEEL SOCKET HEAD (ALLEN HEAD) CAP SCREWS 2 INCHES LONG, AS SPECIFIED.
  - MATERIAL IS DUCTILE IRON ASTM A536 GRADE 80 55-06.
    - OUTFALL TO STREAM - DUMP NO POLLUTANTS.
    - IF THE CONTRACTOR MAY BE CONSTRUCTED OF ONE OF THE FOLLOWING MATERIALS UNLESS OTHERWISE SPECIFIED IN THE PLANS, ALL PIPE JOINTS MUST HAVE GASKETS AND SHALL BE WATER TIGHT; ALL COUPLING BANDS SHALL BE MADE OF THE SAME MATERIAL AS THE PIPE. ALL PIPES SHALL HAVE THE MINIMUM COVER SPECIFIED BELOW AND SHALL BE ADEQUATELY PROTECTED DURING CONSTRUCTION. (REFER TO THE MANUFACTURER'S RECOMMENDATIONS FOR MINIMUM COVER FOR HEAVY EQUIPMENT LOADINGS.)
  - DUCTILE IRON - 4 INCH THROUGH 14 INCH DIAMETER PIPE SHALL BE CLASS 50, AND 16 INCH THROUGH 24 INCH DIAMETER PIPE SHALL BE CLASS 52, ALL IN ACCORDANCE WITH USA STANDARD A-21.1 (AWWA C-51). ALL PIPE JOINTS SHALL BE PUSH-ON, MECHANICAL, OR FLANGED. MINIMUM COVER IS 6 INCHES.
  - CONCRETE - 4 INCH THROUGH 12 INCH DIAMETER PIPE MAY BE NON-REINFORCED, BELL AND SPIGOT WITH RUBBER GASKET JOINTS, CONFORMING TO ASTM C-4 (CLASS II). ALL CONCRETE PIPES 18 INCH THROUGH 36 INCH DIAMETER SHALL BE CLASS IV REINFORCED, BELL AND SPIGOT WITH RUBBER GASKET JOINTS, CONFORMING TO ASTM C-76. MINIMUM COVER IS 2 FEET.
  - PVC - SDR 21, 4 INCH THROUGH 18 INCH DIAMETER PIPE SHALL BE IN ACCORDANCE WITH ASTM D3035, AND SHALL HAVE AT LEAST 24 INCHES OF COVER. SDR PIPES 4 INCH THROUGH 18 INCH DIAMETER PIPE SHALL BE IN ACCORDANCE WITH ASTM D3034, AND SHALL HAVE AT LEAST 36 INCHES MINIMUM COVER. ALL JOINTS SHALL BE PUSH-ON WITH RUBBER GASKETS. PVC STORM PIPE REQUIRES KOR-N-SEAL CONCRETE BOOTS.
  - HELICAL CORRUGATED ALUMINUM PIPE (HCAP) 6 INCH THROUGH 18 INCH DIAMETER PIPE SHALL BE 16 GAUGE MIN 1 MUM WITH 2-2/3 INCH X 1/2 INCH CORRUGATIONS. REFER TO DETAILS IN THESE PLANS FOR ANY PIPES LARGER THAN 36 INCH DIAMETER. MINIMUM COVER IS 2.0 FEET.
  - HELICAL CORRUGATED STEEL PIPE (HCSP) - 6 INCH THROUGH 36 INCH PIPES SHALL BE AT LEAST 16 GAUGE WITH 2-2/3 INCH X 1/2 INCH CORRUGATIONS. ALL STEEL PIPES SHALL BE GALVANIZED AND PROTECTED BY ASPHALT TREATMENT 1 REFER TO DETAILS IN THESE PLANS FOR ANY PIPES LARGER THAN 36 INCHES DIAMETER. MINIMUM COVER IS 2 FEET.
  - ALL PIPE BEDDING SHALL CONFORM TO CLASS B DESIGN FOR FLEXIBLE PIPE (I.E. PVC, HCAP, HCSP, OR ADS). ALL RIGID PIPE (I.E. DUCTILE IRON OR CONCRETE) BEDDING SHALL CONFORM TO CLASS B FOR PIPE COVER GREATER THAN 36 INCHES OR APWA TYPE "B" FOR PIPE COVER LESS THAN 36 INCHES. BEDDING MATERIAL SHALL BE 5/8 INCHES MINUS CRUSHED ROCK FOR ALL PIPE EXCEPT FOR DUCTILE IRON. BEDDING MATERIAL FOR DUCTILE IRON PIPE SHALL MEET THE REQUIREMENTS OF THE CITY OF KENT SPECIAL PROVISIONS.
  - ALL TRENCH BACKFILL IN AREAS OF FUTURE PAVEMENT OR STRUCTURAL FILLING SHALL BE COMPACTED TO AT LEAST 95 PERCENT OF THE MAXIMUM DRY DENSITY PER ASTM D-1557 (MODIFIED PROCTOR). ALL OTHER AREAS SHALL BE COM PACT ED TO AT LEAST 90 PERCENT OF MAXIMUM DRY DENSITY.
  - CONSTRUCTION OF DewaterING (GROUNDWATER INTERCEPTION) SYSTEMS SHALL BE IN ACCORDANCE WITH APPROVED PLANS.
  - PRIOR TO ACCEPTANCE OF CONSTRUCTION AND/OR ISSUANCE OF CERTIFICATE OF OCCUPANCY PERMITS, SURVEYOR-CERTIFI ED AS-BUILT DRAWINGS PREPARED ON MYLAR MUST BE PREPARED OF ALL STORMWATER MANAGEMENT FACILITIES AND APPROVED BY THE CITY. SEE CITY OF KENT DEVELOPMENT ASSISTANCE BROCHURE #E-6, AS-BUILT DRAWINGS, FOR ADDITIONAL INFORMATION REGARDING AS-BUILT DRAWINGS.
  - ALL RETENTION/DETENTION FACILITIES WILL BE INSTALLED AND IN OPERATION PRIOR TO, OR IN CONJUNCTION WITH, ALL CONSTRUCTION ACTIVITY UNLESS THAT ACTIVITY EXCEEDS THE CAPACITY AND IN TENT OF THE EROSION/SEDIMENTATION CONTROL FACILITY, OR UNLESS OTHERWISE APPROVED BY THE CITY.
  - WHEN ALUMINUM PIPE IS USED WHERE IT WILL BE IN CONTACT WITH CONCRETE, ALL ALUMINUM SURFACES IN CONTACT WITH THE CONCRETE SHALL BE PAINTED WITH TWO COATS OF ZINC CHROMATE PAINT IN ACCORDANCE WITH SECTION 7-02.3X(1) B.2. OF THE WSDOT/APWA STANDARD SPECIFICATIONS FOR ROADS, BRIDGES, AND MUNICIPAL CONSTRUCTION.
  - BIO-FILTRATION SWALES AND/OR FILTER STRIPS SHALL BE CONSTRUCTED, BEDDED OR SEEDED AND IN OPERATION PRIOR TO, OR SODDED IN CONJUNCTION WITH, ASPHALT PAVING. THE VEGETATION IN THE BIO-SWALE MUST BE WELL ESTABLISHED BEFORE PAVING BEGINS.

## CITY OF KENT GENERAL WATER NOTES

- ALL WATER SYSTEM IMPROVEMENTS SHALL BE CONSTRUCTED IN ACCORDANCE WITH APPROVED PLANS. ANY DEVIATION FROM THE PLANS WILL REQUIRE PRIOR APPROVAL FROM THE OWNER, THE CITY ENGINEER, AND OTHER APPROPRIATE PUBLIC AGENCIES.
- ALL NEW CONNECTIONS TO THE EXISTING WATER SYSTEM SHALL BE IN STRICT CONFORMANCE WITH SECTION 7-11 OF THE WSDOT/APWA STANDARD SPECIFICATIONS FOR ROADS, BRIDGES, AND MUNICIPAL CONSTRUCTION. NO CONNECTION SHALL BE MADE BETWEEN THE NEW WATER MAIN AND THE EXISTING WATER MAINS UNTIL THE NEW PIPES HAVE BEEN FLUSHED, DISINFECTED, AND TESTED. TEMPORARY PLUGS AND BLOCKING SHALL BE INSTALLED AT THE POINTS OF CONNECTION TO THE EXISTING SYSTEM. THE CONTRACTOR SHALL CONTACT THE CITY OF KENT WATER DEPARTMENT AT LEAST 5 WORKING DAYS PRIOR TO THE PROPOSED CONNECTION TO THE EXISTING WATER MAIN. THE CONTRACTOR SHALL EXPOSE EXISTING WATER MAINS AND PROVIDE ALL NECESSARY FITTINGS FOR THE CONNECTIONS. A REPRESENTATIVE OF THE CITY OF KENT ENGINEERING DEPARTMENT SHALL BE PRESENT WHEN THE ACTUAL WATER MAIN CONNECTION IS MADE.
- PRESSURE AND PURITY TESTING SHALL BE DONE IN THE PRESENCE OF AND UNDER THE SUPERVISION OF THE CITY OF KENT DIRECTOR OF PUBLIC WORKS AND/OR HIS REPRESENTATIVE. THE CONTRACTOR SHALL PROVIDE PLUGS AND/OR TEMPORARY BLOW-OFF ASSEMBLIES FOR TESTING AND PURITY ACCEPTANCE PRIOR TO TIE-IN.
- ALL MATERIALS USED FOR THE CONSTRUCTION OF WATER MAINS SHALL BE NEW AND UNDAMAGED AND SHALL BE INSPECTED AND APPROVED BY THE CITY OF KENT PRIOR TO INSTALLATION. THE SUPPLIERS SHALL PROVIDE THE CITY OF KENT WITH A CERTIFICATE OF MATERIALS WHEN ALL PIPES, FITTINGS, WATER MAINS, HYDRANTS, JOINTS AND RELATED APPURTENANCES SHALL CONFORM TO THE LATEST STANDARDS ISSUED BY THE WSDOT, APWA, AWWA AND THE CITY OF KENT.
- ALL WATER MAIN PIPE SHALL BE CEMENT-LINED DUCTILE IRON PIPE CONFORMING TO ANSI-A-21-11-1977 OR LATEST REVISION, THICKNESS CLASS 52. CEMENT MORTAR LINING AND SEAL COATING SHALL BE COMPACTED TO ANSI A-21-4-11-1974 OR LATEST REVISION. PIPE JOINTS SHALL BE PUSH-ON MECHANICAL JOINT OR FLANGED WITH RUBBER GASKETS.
- ALL FITTINGS SHALL BE CAST IRON, CEMENT-LINED, CLASS 250, CONFORMING TO ANSI A-21-11-1977 OR LATEST REVISION, UNLESS OTHERWISE SPECIFIED. ALL FITTING JOINTS SHALL BE DRILLED IN ACCORDANCE WITH ANSI B-16.1 AND SHALL BE THE 125-POUND RATING UNLESS OTHERWISE SPECIFIED.
- CEMENT CONCRETE THRUST BLOCKING SHALL BE PLACED AT ALL TEES, CROSSES, BENDS, AND VALVES UNLESS APPROVED RESTRAINED JOINT PIPES ARE INSTALLED. THRUST BLOCKING SHALL BE CEMENT CONCRETE CLASS 3000 POURED IN PLACE. THRUST BLOCKING SHALL BEAR AGAINST SOLID UNDISTURBED EARTH AT THE SIDES AND BOTTOM OF THE TRENCH EXCAVATION AND SHALL BE SECURELY WRAPPED WITH 4-MIL POLYETHYLENE SHEET. THRUST JOINTS SHALL BE PLACED TO A MINIMUM DEPTH OF 4 INCHES UNDER THE PIPE AND TO A DEPTH OF AT LEAST 6 INCHES OVER THE TOP OF THE PIPE. ALL TRENCH BACKFILL IN AREAS OF FUTURE PAVEMENT OR STRUCTURAL LOADING SHALL BE COMPACTED TO AT LEAST 95 PERCENT OF THE MAXIMUM DRY DENSITY PER ASTM D-1557 (MODIFIED PROCTOR). ALL OTHER AREAS SHALL BE COMPACTED TO 90 PERCENT OF MAXIMUM DRY DENSITY.
- ALL WATER MAINS 10 INCHES AND SMALLER IN DIAMETER SHALL HAVE A MINIMUM COVER OF 36 INCHES BELOW FINISH GRADE. ALL WATER MAINS 12 INCHES AND LARGER IN DIAMETER SHALL HAVE A MINIMUM COVER OF 48 INCHES BELOW FINISH GRADE. WHERE UTILITY CONFLICTS OCCUR, WATER MAINS SHALL BE LOWERED TO CLEAR, A MINIMUM OF 1 FOOT VERTICAL SEPARATION SHALL BE MAINTAINED BETWEEN ALL UTILITIES.
- UNLESS OTHERWISE SPECIFIED ON THE PLANS, ALL VALVES SHALL BE GATE VALVES CONFORMING TO THE LATEST REVISIONS TO AWWA STANDARD SPECIFICATIONS FOR GATE VALVES FOR ORDINARY WATER WORKS SERVICE NO. C-500. THEY SHALL BE IRON-BODY, BRONZE-MOUNTED, RESILIENT SEATED VALVES WITH A BRONZE WEDGING DEVICE AND/OR AN O-RING STUFFING BOX. ALL VALVES SHALL BE FLANGED OR MECHANICAL JOINT AND SUITABLE FOR INSTALLATION WITH THE TYPE AND CLASS OF PIPE BEING USED. ALL VALVES SHALL OPEN COUNTERCLOCKWISE, AND UNLESS OTHERWISE SPECIFIED, SHALL BE NON-RISING STEM TYPE EQUIPPED TO FIT WITH THE CITY OF KENT'S EXISTING VALVE WRENCHES. ALL WATER VALVES SHALL BE FURNISHED WITH AN APPROVED VALVE BOX AND COVER. A CONCRETE VALVE MARKER POST MEETING THE REQUIREMENTS OF STANDARD DETAIL 3-3 SHALL BE PLACED AS DIRECTED BY THE ENGINEER AND SET TO LEAVE 18 INCHES EXPOSED ABOVE GRADE WHEN THE WATER VALVE BOX IS NOT WITHIN A PAVED SECTION.
- WHERE A WATER MAIN CROSSES ABOVE THE SANITARY SEWER, AND LESS THAN 1.5 FEET OF VERTICAL CLEARANCE EXISTS BETWEEN THE TOP OF THE SEWER MAIN AND BOTTOM OF WATER MAIN, THE SANITARY SEWER MAIN SHALL BE CONSTRUCTED, OR RECONSTRUCTED, WITH DUCTILE IRON PIPE (CLASS 50) CENTERED ON THE WATER MAIN FOR A MINIMUM HORIZONTAL DISTANCE OF ONE CONTINUOUS 18 FEET NOMINAL PIPE LENGTH WITH NO JOINTS.
- ALL FIRE HYDRANTS SHALL BE COMPRESSION TYPE, BREAK-AWAY (TRAFFIC MODEL) HYDRANTS CONFORMING TO AWWA C502 AND MEETING THE REQUIREMENTS OF STANDARD DETAIL 3-1 OF THE CITY OF KENT CONSTRUCTION STANDARDS. THE HYDRANT LEADS SHALL BE A MINIMUM OF 6 INCHES IN DIAMETER. AUXILIARY VALVES SHALL BE INSTALLED IN THE HYDRANT LEADS LOCATED IN THE CONNECTION TO THE CITY MAIN. ALL HYDRANTS SHALL BE EQUIPPED WITH A DRAIN, A GRISEL POCKET OR DRY WELL. SHALL BE PROVIDED UNLESS THE NATURAL SOILS WILL PROVIDE ADEQUATE DRAINAGE. HYDRANT DRAINS SHALL NOT BE CONNECTED TO STORM DRAINS OR LOCATED WITHIN 10 FEET OF OR CONNECTED TO SANITARY SEWERS. FIRE HYDRANTS SHALL BE PAINTED WITH 2 COATS OF WHITE PAINT, SIMILAR TO WATERLOUS HYDRANT ENAMEL V1814-W (SHINING WHITE) OR EQUAL. CONCRETE FIRE HYDRANT GUARD POSTS MEETING THE REQUIREMENTS OF STANDARD DETAIL 3-11 SHALL BE FURNISHED AND INSTALLED WITH FIRE HYDRANTS WHEN DIRECTED BY THE CITY ENGINEER. IOWA FIRE HYDRANTS ARE NOT ALLOWED.
- PRIOR TO ACCEPTANCE OF CONSTRUCTION AND/OR ISSUANCE OF CERTIFICATE OF OCCUPANCY PERMITS, SURVEYOR-CERTIFIED AS-BUILT DRAWINGS PREPARED ON MYLAR MUST BE PREPARED OF ALL PUBLIC WATER MAIN FACILITIES AND APPROVED BY THE CITY. SEE CITY OF KENT DEVELOPMENT ASSISTANCE BROCHURE #E-6, AS-BUILT DRAWINGS, FOR ADDITIONAL INFORMATION REGARDING AS-BUILT DRAWINGS.

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C0.0	COVER SHEET
C1.0	TESC AND DEMOLITION
C1.1	OTHER AND DEMOLITION DETAILS
C2.0	GRADING AND DRAINAGE
C2.1	GRADING AND DRAINAGE DETAILS
C3.0	UTILITIES AND PAVING
C3.1	UTILITIES AND PAVING DETAILS
C3.2	UTILITIES AND PAVING DETAILS
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## SITE/PROJECT AREA



## VICINITY MAP

NTS

E:\CADD-2015\DICKS DRIVE-IN\Logo\RED LOGO.jpg

*Dick's Drive-in*  
Kent  
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Kent, WA 98032

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seattle, washington 98109

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206.713.4076 m  
206.464.0700 f

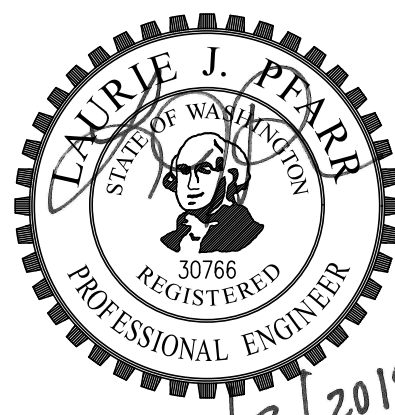
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Consultant Registration

Architect Registration



Issues and Revisions				
No.	Date	Issues	By	Check
1	12.28.17	PERMIT	LJP	
2	02.27.18	PERMIT RESUBMITTAL	LJP	
3	03.16.18	PERMIT RESUBMITTAL	LJP	
4	08.08.18	SIDEWALK DELETION	LJP	
5	06/13.19	RECORD DRAWINGS	LJP	

TBM B	ELEVATION: 378.07'
HELD EXISTING CITY OF KENT HORIZONTAL AND VERTICAL CONTROL POINT #8619. A 2" ALUMINUM DISC AT THE CENTERLINE OF SIDEWALK ON THE EAST SIDE OF PACIFIC HIGHWAY, ±90'N OF POWER POLE.	

TBM B	ELEVATION: 400.23'
SET MAG NAIL IN ASPHALT 3' SOUTH OF BUILDING FACE AND ±70'WEST OF SOUTHEAST CORNER OF "LOWES"	

## LEGAL DESCRIPTION

PARCELS 2, 3, 4, TOGETHER WITH TRACT A, CITY OF KENT BINDING SITE PLAN NO. BSP-2006-04 (MIDWAY RETAIL CENTER BINDING SITE PLAN), RECORDED UNDER RECORDING NO. 20070926000281, AT VOLUME 242 OF PLATS, PAGES 85 THROUGH 92, INCLUSIVE, IN KING COUNTY, WASHINGTON.

## NEW IMPERVIOUS AREA

NEW IMPERVIOUS AREA = 64,578 SF (1.483 AC)

NOTE IMPERVIOUS AREA PROVIDED FOR PERMITTING USE ONLY

KIVA No. #RECC-2175131

BASED ON THE SUBMITTED DRAWINGS AND SPECIFICATIONS PROVIDED BY THE APPLICANT'S LICENSED ENGINEERS, SURVEYORS AND ARCHITECTS AND OTHER CERTIFIED PROFESSIONALS, THESE PLANS ARE APPROVED FOR CONFORMANCE WITH THE CITY OF KENT ENGINEERING DIVISION'S REQUIREMENTS

Development Engineering Manager Date

City Engineer

Date

THESE RECORD PLANS AND THE INFORMATION SHOWN ACCURATELY REFLECT EXISTING FIELD CONDITION AS OF (DATE SIGNED).

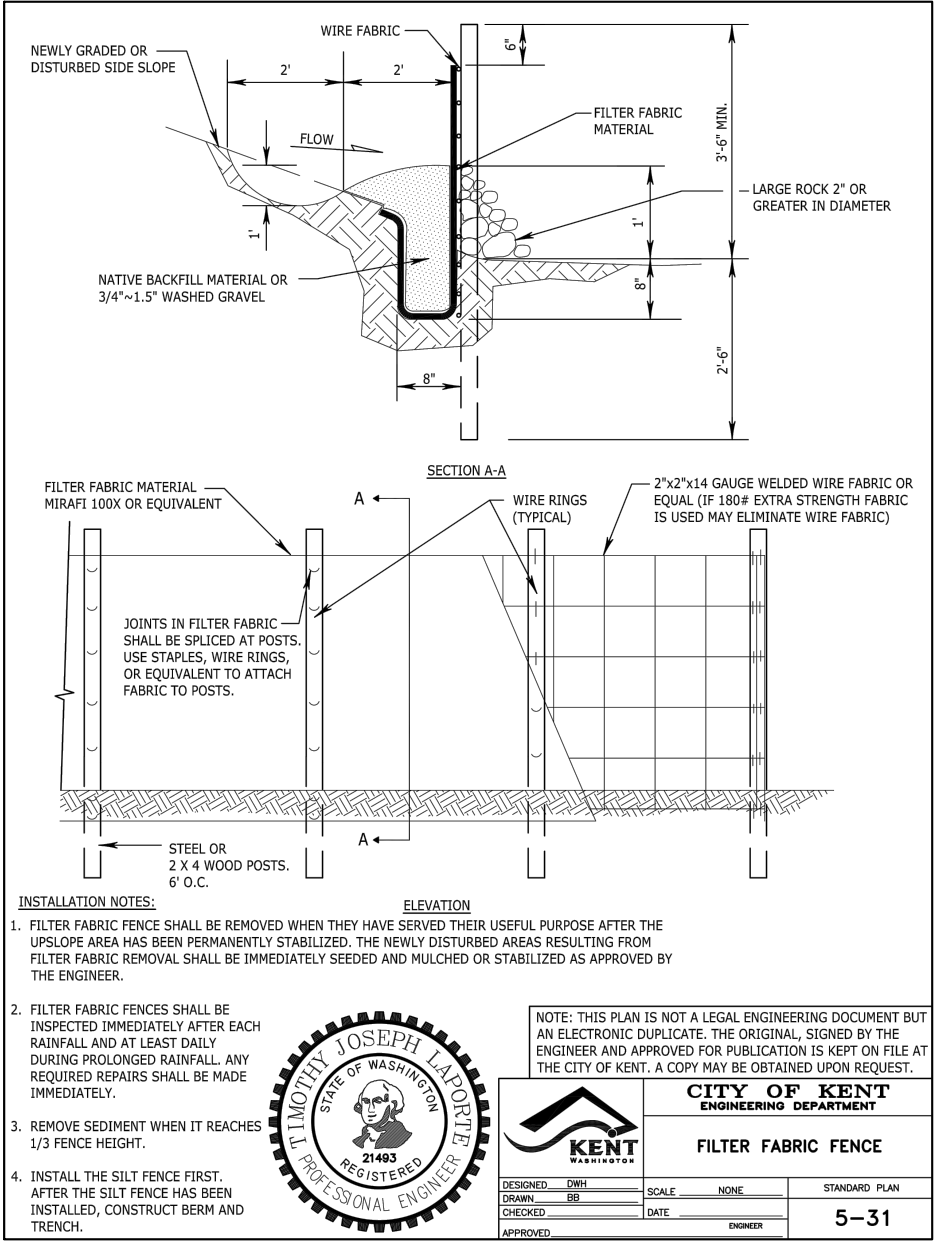
BY:

C0.0



TPN: 218

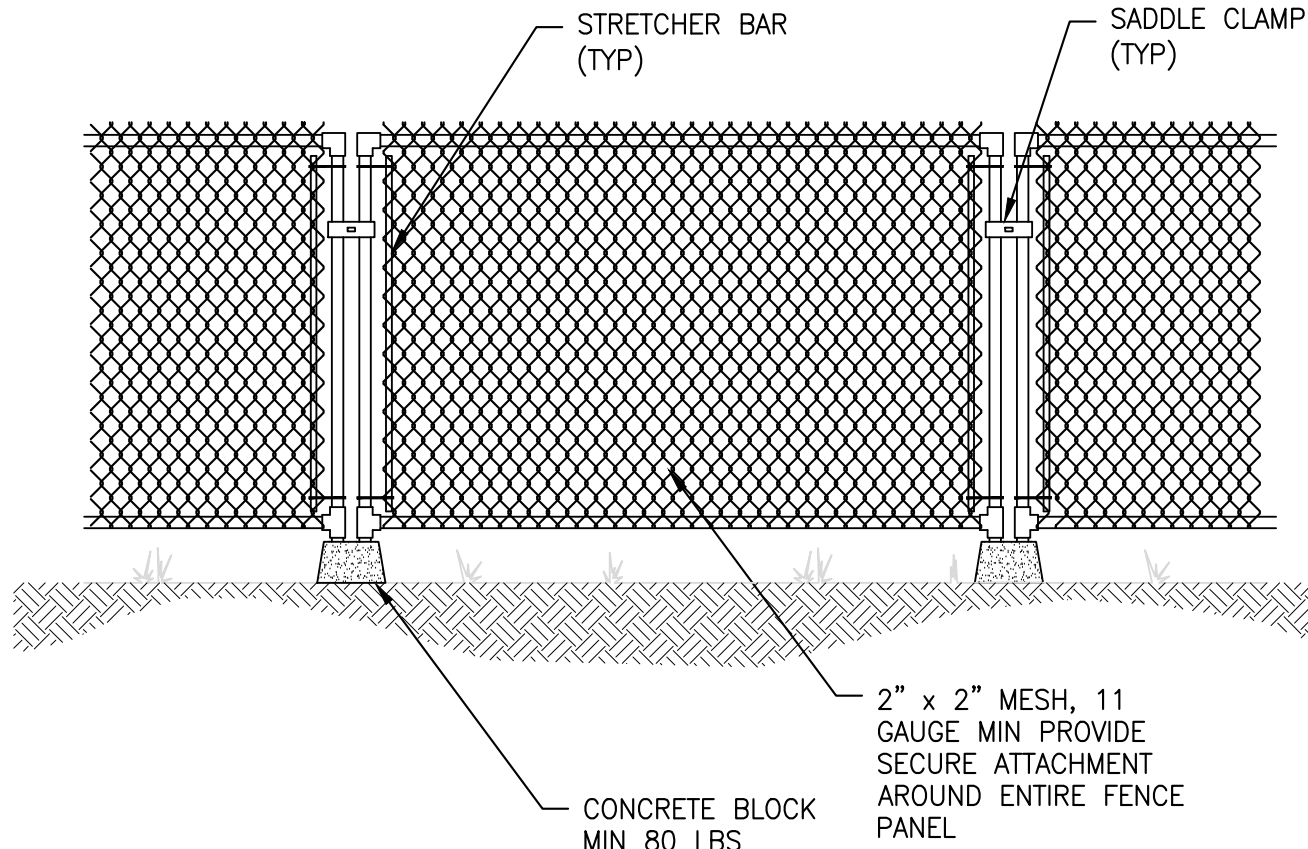




NTS  
FILTER FABRIC FENCE 1

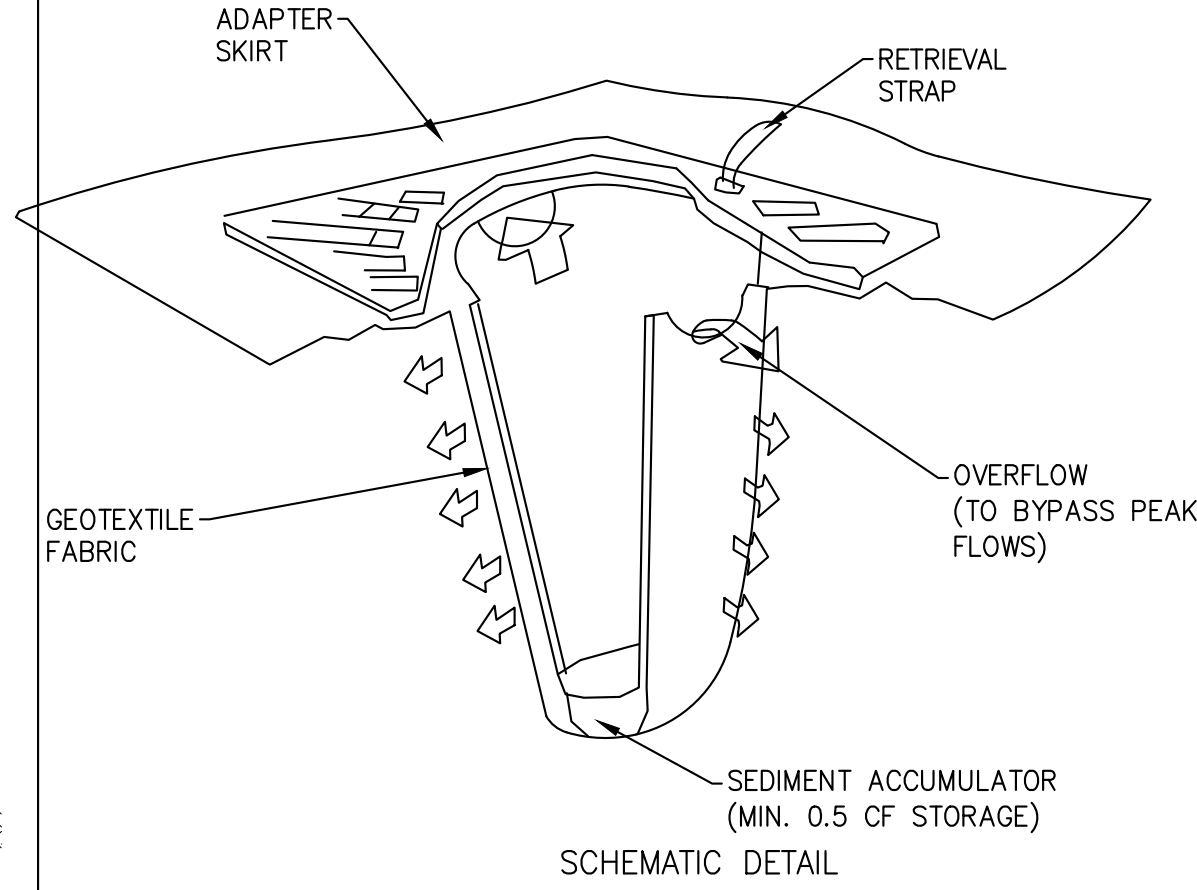
DRAWINGS HAVE BEEN REVISED PER CONTRACTOR'S CONSTRUCTION RECORDS

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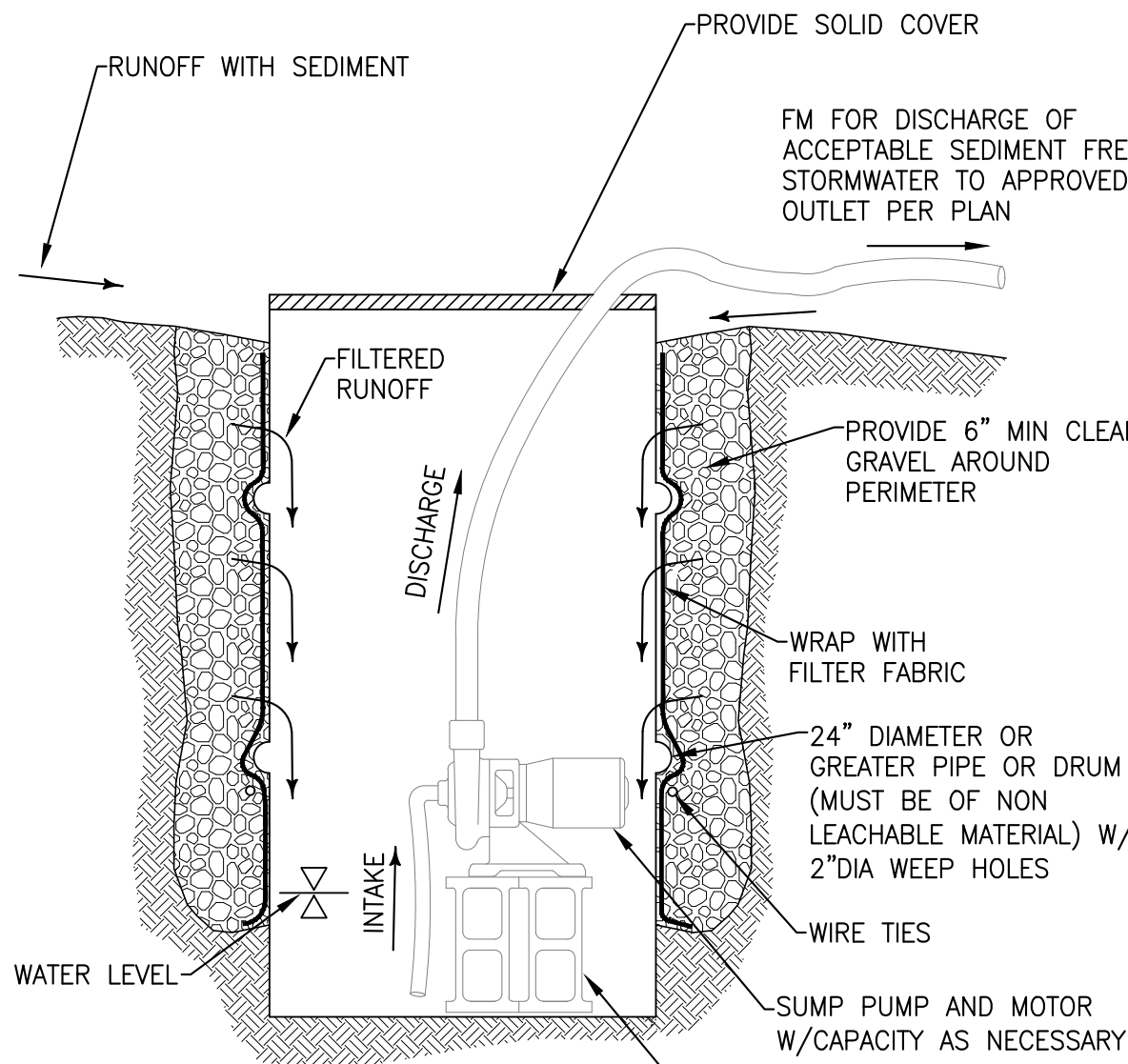


NTS  
CONSTRUCTION FENCING 2

# AS-BUILT



PROVIDE "STREAMGUARD SEDIMENT CATCH BASIN INSERT" OR APPROVED EQUAL  
MANUFACTURER INFORMATION:  
BOWHEAD ENVIRONMENTAL & SAFETY  
P.O. BOX 375  
PRESTON, WA 98050  
(800) 909-3677  
WWW.SHOPBOWHEAD.COM  
NTS  
INLET PROTECTION 3



NTS  
MOVEABLE SUMP & PUMP 4

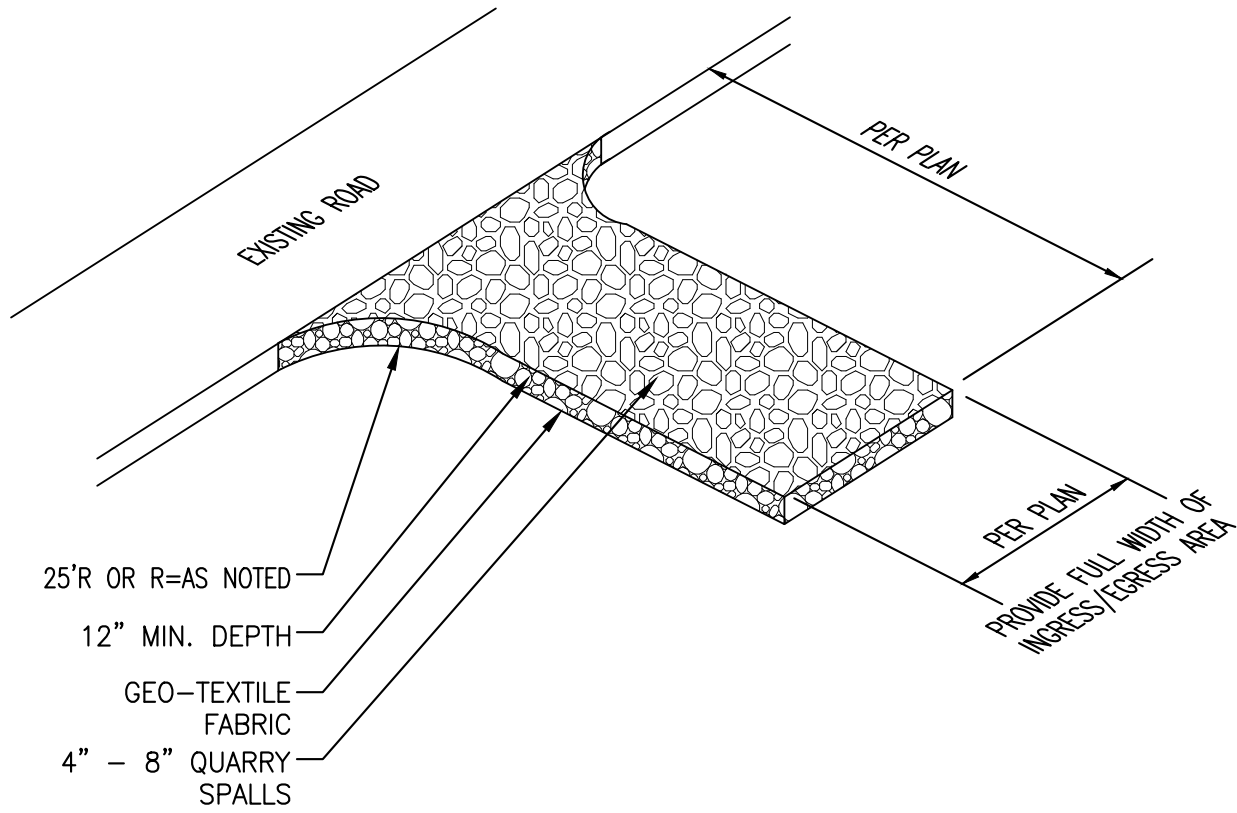
## TESC NOTES

- THE IMPLEMENTATION OF THESE EROSION & SEDIMENTATION CONTROL (ESC) PLANS AND THE CONSTRUCTION, MAINTENANCE, REPLACEMENT, AND UPGRADING OF THESE ESC FACILITIES IS THE RESPONSIBILITY OF THE CONTRACTOR UNTIL ALL CONSTRUCTION IS APPROVED.
- ALL WORK AND MATERIALS SHALL BE IN ACCORDANCE WITH THE CITY OF KENT STANDARDS AND SPECIFICATIONS.
- A COPY OF THE APPROVED EROSION CONTROL PLANS MUST BE ON THE JOB SITE WHENEVER CONSTRUCTION IS IN PROGRESS.
- MARK CLEARING LIMITS AND ENVIRONMENTALLY CRITICAL AREAS. WITHIN THE BOUNDARIES OF THE PROJECT SITE AND PRIOR TO BEGINNING LAND DISTURBING ACTIVITIES, CLEARLY MARK ALL CLEARING LIMITS, EASEMENTS, SETBACKS, ALL ENVIRONMENTALLY CRITICAL AREAS AND THEIR BUFFERS, AND ALL TREES, AND DRAINAGE COURSES THAT ARE TO BE PRESERVED WITHIN THE CONSTRUCTION AREA.
- THE ESC FACILITIES SHOWN ON THIS PLAN MUST BE CONSTRUCTED IN CONJUNCTION WITH ALL CLEARING AND GRADING ACTIVITIES IN SUCH A MANNER AS TO INSURE THAT SEDIMENT-LADEN WATER DOES NOT ENTER THE DRAINAGE SYSTEM OR VIOLATE APPLICABLE WATER STANDARDS, AND MUST BE COMPLETED PRIOR TO ALL OTHER CONSTRUCTION.
- THE ESC FACILITIES SHOWN ON THIS PLAN ARE THE MINIMUM REQUIREMENTS FOR ANTICIPATED SITE CONDITIONS. DURING THE CONSTRUCTION PERIOD, THESE ESC FACILITIES SHALL BE UPGRADED (E.G. ADDITIONAL SUMPS, RELOCATION OF DITCHES AND SILT FENCES), AS NEEDED FOR UNEXPECTED STORM EVENTS. ADDITIONALLY MORE ESC FACILITIES MAY BE REQUIRED TO ENSURE COMPLETE SILTATION CONTROL. THEREFORE, DURING THE COURSE OF CONSTRUCTION IT SHALL BE THE OBLIGATION AND RESPONSIBILITY OF THE CONTRACTOR TO ADDRESS ANY NEW CONDITIONS THAT MAY BE CREATED BY THEIR ACTIVITIES AND TO PROVIDE ADDITIONAL FACILITIES OVER AND ABOVE THE MINIMUM REQUIREMENTS AS MAY BE NEEDED.
- TO THE MAXIMUM EXTENT FEASIBLE, RETAIN TOP LAYER WITHIN THE BOUNDARIES OF THE PROJECT SITE INCLUDING THE DUFF LAYER, TOP SOIL, AND NATIVE VEGETATION IN AN UNDISTURBED STATE.
- CUT AND FILL SLOPES SHALL BE CONSTRUCTED IN A MANNER THAT WILL MINIMIZE EROSION. OFF-SITE STORMWATER RUN-ON OR GROUNDWATER SHALL BE DIVERTED AWAY FROM SLOPES AND UNDISTURBED AREAS.
- PREVENT ON-SITE EROSION BY STABILIZING ALL EXPOSED AND UNWORKED SOILS, INCLUDING STOCK PILES. AMEND ALL DISTURBED SOILS TO THE MAXIMUM EXTENT FEASIBLE AS THE PROJECT PROGRESSES. FROM OCTOBER 1 TO APRIL 30, NO SOILS SHALL REMAIN EXPOSED AND UNWORKED FOR MORE THAN TWO DAYS. FROM MAY 1 TO SEPTEMBER 30, NO SOILS SHALL REMAIN EXPOSED FOR MORE THAN SEVEN DAYS. SOILS SHALL BE STABILIZED AT THE END OF THE SHIFT BEFORE A HOLIDAY OR WEEKEND IF NEEDED BASED ON THE WEATHER FORECAST. SOIL STOCKPILES SHALL BE STABILIZED FROM EROSION, PROTECTED WITH SEDIMENT TRAPPING MEASURES, AND BE LOCATED AWAY FROM STORM DRAIN INLETS, WATERWAYS, AND DRAINAGE CHANNELS. BEFORE THE COMPLETION OF THE PROJECT, PERMANENTLY STABILIZE ALL EXPOSED SOILS THAT HAVE BEEN DISTURBED DURING CONSTRUCTION. THE FOLLOWING BMPs OR APPROVED EQUALS CAN BE USED FOR SOIL STABILIZATION:

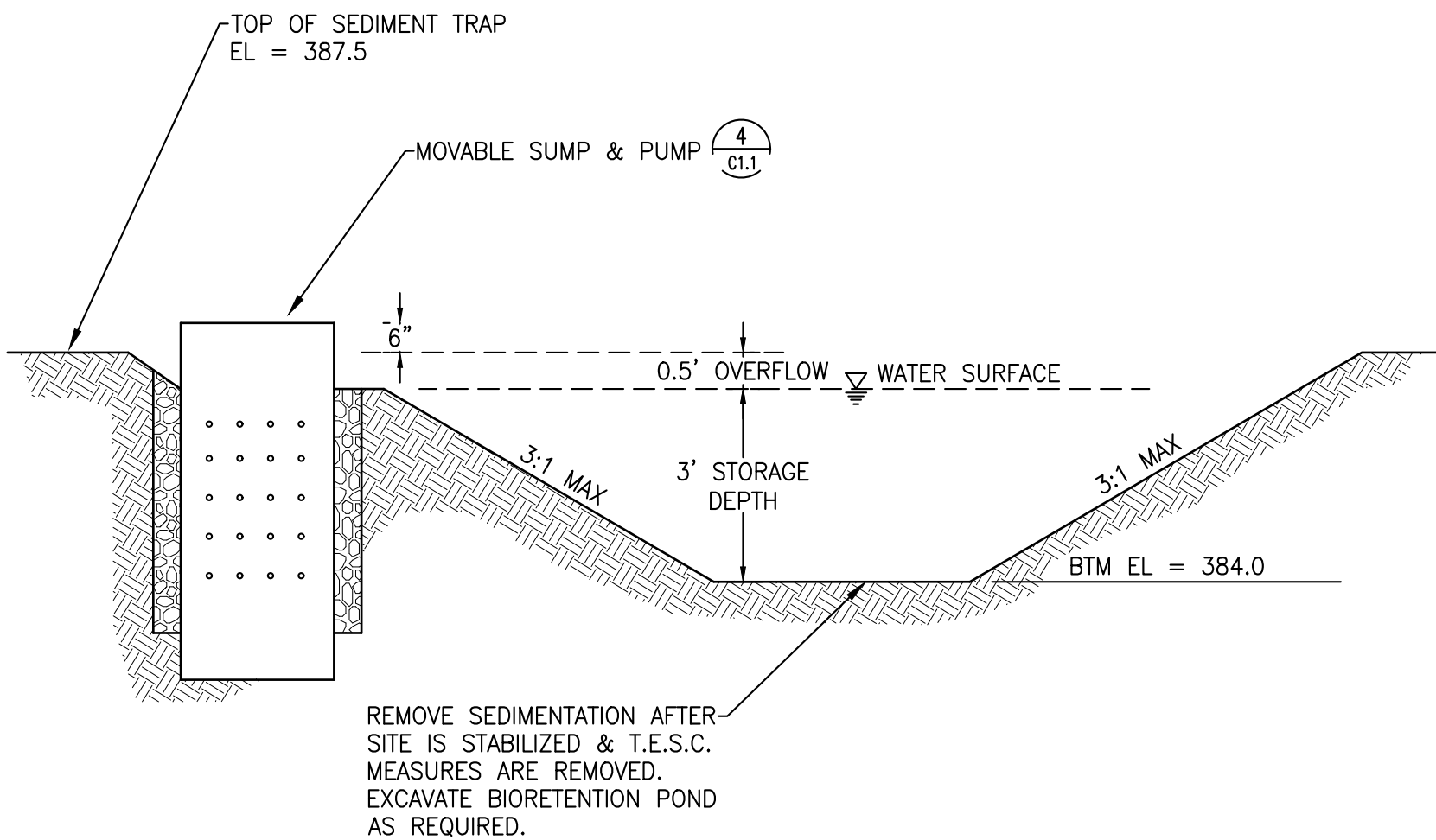
- COMPOST BLANKETS – COMPOST BLANKETS CAN BE USED AS TEMPORARY EROSION CONTROL AND THEN BE MIXED INTO THE SOIL TO HELP MEET THE POST CONSTRUCTION SOIL REQUIREMENTS.
  - SEEDING AND MULCHING
  - MATTING/ROLLED EROSION CONTROL PRODUCTS.
- PROTECT DOWNSTREAM PROPERTIES AND RECEIVING WATERS FROM THE DEVELOPMENT ACTIVITIES FROM EROSION DUE TO INCREASES IN THE VOLUME, VELOCITY, AND PEAK FLOW RATE OF DRAINAGE WATER FROM THE PROJECT SITE.
  - PREVENT EROSION AND SEDIMENT TRANSPORT FROM THE SITE BY ROUTING ALL DRAINAGE WATER FROM DISTURBED AREAS THROUGH A SEDIMENT TRAP OR OTHER APPROPRIATE SEDIMENT REMOVAL BEST MANAGEMENT PRACTICES (BMP) PRIOR TO DISCHARGING FROM THE SITE. SEDIMENT CONTROLS INTENDED TO TRAP SEDIMENT ON SITE SHALL BE CONSTRUCTED AS ONE OF THE FIRST STEPS IN GRADING AND SHALL BE FUNCTIONAL BEFORE OTHER LAND DISTURBING ACTIVITIES TAKE PLACE.
  - STABILIZED CONSTRUCTION ENTRANCES AND/OR WASH PADS SHALL BE INSTALLED AT THE BEGINNING OF CONSTRUCTION AND MAINTAINED FOR THE DURATION OF THE PROJECT. ADDITIONAL REQUIREMENTS SHALL BE ENFORCED BY THE INSPECTOR TO ENSURE THAT ALL PAVED AREAS ARE KEPT CLEAN OF SILT FROM CONSTRUCTION VEHICLES.
  - LIMIT CONSTRUCTION VEHICLE ACCESS TO ONE ROUTE. STABILIZE ALL ACCESS POINTS AND PREVENT TRACKING SEDIMENT ONTO PUBLIC ROADS. PROMPTLY REMOVE ANY SEDIMENT TRACKED OFF SITE. PROVIDE PERIODIC STREET CLEANING BY SWEEPING OR SHOVELING ANY SEDIMENT THAT MAY HAVE BEEN TRACKED OUT.
  - OFF-SITE STREETS MUST BE KEPT CLEAN AT ALL TIMES. IF DIRT IS DEPOSITED ON A PUBLIC STREET, THE STREET SHALL BE CLEANED WITH A VACUUM SWEEPER OR BROOM AND SHOVEL; STREET WASHING IS ALLOWED ONLY AS A LAST RESORT AND MUST BE APPROVED BY THE EROSION CONTROL INSPECTOR. ALL VEHICLES SHALL LEAVE THE SITE BY WAY OF THE CONSTRUCTION VEHICLE ENTRANCE AND SHALL BE CLEANED OF MUD PRIOR TO EXITING ONTO THE STREET.
  - ANY CATCH BASIN COLLECTING WATER FROM THE SITE, WHETHER THEY ARE ON OR OFF OF THE SITE, SHALL BE PROTECTED PER PROJECT DOCUMENTS. PREVENT SEDIMENT FROM ENTERING ALL STORM DRAINS, INCLUDING DITCHES, WHICH RECEIVE DRAINAGE WATER FROM THE PROJECT. STORM DRAIN INLETS PROTECTION DEVICES SHALL BE CLEANED OR REMOVED AND REPLACED AS RECOMMENDED BY THE PRODUCT MANUFACTURER, OR MORE FREQUENTLY IF REQUIRED TO PREVENT FAILURE OF THE DEVICE OR FLOODING. STORM DRAIN INLETS MADE OPERABLE DURING CONSTRUCTION SHALL BE PROTECTED SO THAT DRAINAGE WATER DOES NOT ENTER THE DRAINAGE SYSTEM WITHOUT FIRST BEING FILTERED OR TREATED TO REMOVE SEDIMENTS. STORM DRAIN INLET PROTECTION DEVICES SHALL BE REMOVED AT THE CONCLUSION OF THE PROJECT.
  - AT NO TIME SHALL MORE THAN ONE FOOT OF SEDIMENT BE ALLOWED TO ACCUMULATE WITHIN A CATCH BASIN. ALL CATCH BASINS AND CONVEYANCE LINES SHALL BE CLEANED PRIOR TO PAVING. THE CLEANING OPERATION SHALL NOT FLUSH SEDIMENT LADEN WATER INTO DOWNSTREAM SYSTEM.

- ALL TEMPORARY ON-SITE DRAINAGE SYSTEMS SHALL BE CONSTRUCTED AND STABILIZED TO PREVENT EROSION. STABILIZATION SHALL BE PROVIDED AT THE OUTLETS OF ALL DRAINAGE SYSTEMS, ADEQUATE TO PREVENT EROSION OF OUTLETS, ADJACENT STREAM BANKS, SLOPES, AND DOWNSTREAM REACHES. IF ANY PORTION OF THE EROSION/SEDIMENTATION CONTROL ELEMENTS IS DAMAGED OR NOT FUNCTIONING, OR IF THE CLEARING LIMIT BOUNDARY BECOMES NON-DEFINED, IT SHALL BE REPAIRED IMMEDIATELY.
- MEASURES SHALL BE TAKEN TO CONTROL POTENTIAL POLLUTANTS. COMPLY WITH THE REQUIREMENTS FOR EACH OF THE FOLLOWING: CONSTRUCTION RELATED ACTIVITIES: POLLUTANT DISPOSAL (INCLUDING SEDIMENT, WASTE MATERIALS, AND DEMOLITION DEBRIS); CHEMICAL STORAGE; ON-SITE FUELING; MAINTENANCE, FUELING AND REPAIR OF HEAVY EQUIPMENT AND VEHICLES; CLEANUP OF CONTAMINATED SURFACES; DISCHARGE OF WHEEL WASH WASTEWATER; FERTILIZER AND PESTICIDE APPLICATION; PH-MODIFYING SOURCES.
- WHEN DEWATERING DEVICES DISCHARGE ON SITE OR TO A PUBLIC DRAINAGE SYSTEM, DEWATERING DEVICES SHALL DISCHARGE INTO A SEDIMENT TRAP TO REMOVE SEDIMENT CONTAMINATION, OR OTHER SEDIMENT REMOVAL BMP. A SEPARATE DEWATERING PERMIT IS REQUIRED FOR: ONE ACRE OR GREATER DISTURBED AREA; CONTAMINATED SURFACE AND /OR GROUNDWATER; EXCAVATION GREATER THAN 12 FEET IN DEPTH; SIGNIFICANT VOLUME OF GROUNDWATER; OTHER SITE SPECIFIC CONDITIONS LEADING TO SIGNIFICANT DEWATERING.
- ALL TEMPORARY AND PERMANENT EROSION AND SEDIMENT CONTROL BMPs SHALL BE INSPECTED, MAINTAINED AND REPAIRED AS NEEDED TO ASSURE CONTINUED PERFORMANCE OF THEIR INTENDED FUNCTION. ALL TEMPORARY EROSION AND SEDIMENT CONTROLS SHALL BE REMOVED WITHIN FIVE (5) DAYS AFTER FINAL SITE STABILIZATION IS ACHIEVED OR AFTER THE TEMPORARY CONTROLS ARE NO LONGER NEEDED, WHICHEVER IS LATER. TRAPPED SEDIMENT SHALL BE REMOVED OR STABILIZED ON SITE. DISTURBED SOIL AREAS RESULTING FROM REMOVAL SHALL BE PERMANENTLY STABILIZED. FOR PROJECTS WITH 5,000 SQUARE FEET OR MORE OF NEW PLUS REPLACED IMPERVIOUS SURFACE OR 7,000 SQUARE FEET OR MORE OF LAND DISTURBING ACTIVITY, SITE INSPECTIONS SHALL BE CONDUCTED BY A CERTIFIED EROSION AND SEDIMENT CONTROL LEAD WHO SHALL BE IDENTIFIED IN THE CONSTRUCTION STORMWATER CONTROL PLAN AND SHALL BE PRESENT ON-SITE OR ON-CALL AT ALL TIMES.
- CONSTRUCTION SITE OPERATORS SHALL MAINTAIN, UPDATE, AND IMPLEMENT THE CONSTRUCTION STORMWATER CONTROL PLAN, AND SHALL MODIFY THE CONSTRUCTION STORMWATER CONTROL PLAN TO MAINTAIN COMPLIANCE.
- IN THE CONSTRUCTION OF UNDERGROUND UTILITY LINES, WHERE FEASIBLE, NO MORE THAN ONE HUNDRED FIFTY (150) FEET OF TRENCH SHALL BE OPENED AT ONE TIME.
- DEVELOPMENT PROJECTS SHALL BE PHASED IN ORDER TO MINIMIZE THE AMOUNT OF LAND DISTURBING ACTIVITY OCCURRING AT THE SAME TIME AND SHALL TAKE INTO ACCOUNT SEASONAL WORK LIMITATIONS.
- AFTER CONSTRUCTION BUT BEFORE THE PROJECT IS CONSIDERED COMPLETED, PERMANENTLY STABILIZE ALL EXPOSED SOILS THAT HAVE BEEN DISTURBED DURING CONSTRUCTION.

NTS  
TESC NOTES 8



NTS  
CONSTRUCTION ENTRANCE 5



NTS  
TEMPORARY SEDIMENT TRAP 10

## BIORETENTION AREA PROTECTION NOTES

- INSTALL TEMPORARY CONSTRUCTION FENCE AROUND FOOTPRINT OF BIORETENTION AREA.
- LIMIT CONSTRUCTION VEHICLE TRAFFIC WITHIN FOOTPRINT OF BIORETENTION AREAS TO MAXIMUM EXTENT FEASIBLE DURING CONSTRUCTION.
- DO NOT EXCAVATE BIORETENTION AREAS TO SUB-GRADE UNTIL SITE HAS BEEN STABILIZED AND JUST PRIOR TO INSTALLATION OF BIORETENTION SOIL AND PLANTING.
- GEOTECHNICAL ENGINEER TO REVIEW BIORETENTION SUBGRADE PRIOR TO INSTALLATION OF SOIL AND PLANTING.
- DO NOT ALLOW CONSTRUCTION STORMWATER RUNOFF TO ENTER EXCAVATED BIORETENTION AREAS.

NTS  
BIORETENTION AREA PROTECTION NOTES 11

## CONSTRUCTION SEQUENCE

- SCHEDULE A PRE-CONSTRUCTION MEETING WITH THE CITY OF KENT CONSTRUCTION AND INSPECTION DEPARTMENT AT 253-856-5500.
- REVIEW ESC NOTES.
- CALL 'ONE-CALL,' @1-800-424-5555, AND PRIVATE UTILITY LOCATE FOR UTILITY LOCATES.
- INSTALL ESC MEASURES (ALL TEMPORARY SEDIMENTATION AND EROSION CONTROL MEASURES MUST BE IN PLACE PRIOR TO ANY CONSTRUCTION OR SITE CLEARING. EROSION AND SEDIMENTATION CONTROL PRACTICES AND/OR DEVICES SHALL BE MAINTAINED UNTIL PERMANENT VEGETATION IS ESTABLISHED.)
- CONSTRUCT BUILDING.
- INSTALL UTILITIES AND OTHER SITE IMPROVEMENTS.
- INSTALL CURBS AND SIDEWALKS.
- STABILIZE AND REVEGETATE ENTIRE SITE.
- ESTABLISH LANDSCAPING AND PERMANENT VEGETATION. EROSION CONTROL FEATURES CAN BE REMOVED UPON FINAL SITE STABILIZATION.

KIVA No. #RECC-2175131

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Development Engineering Manager Date

City Engineer Date

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BY:

NTS  
CONSTRUCTION SEQUENCE 12

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**Dick's Drive-in**  
Kent  
24050 Pacific Hwy S.  
Kent, WA 98032

**smith co.**

1725 westlake avenue n  
suite 210  
seattle, washington 98109

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206.713.4076 m  
206.464.0700 f

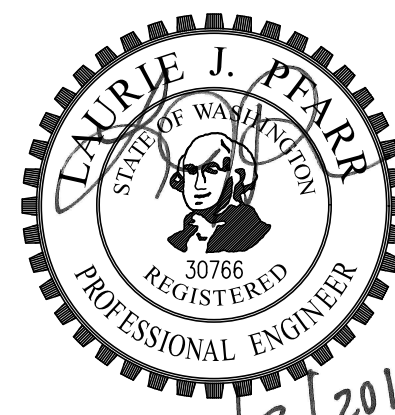
ksmith@smithco.org  
http://www.smithco.org/

Consultant

**LPD** 1932 First Ave  
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f. 206.973.5344  
engineering pllc www.lpdengineering.com

Consultant Registration

Architect Registration



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5	06/13.19	RECORD DRAWINGS	LJP	

Project Name	DICK'S DRIVE-IN
Project Number	
Description	TESC AND DEMOLITION DETAILS AND NOTES
Computer File	
Scale	AS NOTED
Template	4.3 (120101)

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C1.1



# AS-BUILT

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**Dick's Drive-in**  
Kent  
24050 Pacific Hwy S.  
Kent, WA 98032

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1725 westlake avenue n  
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Consultant

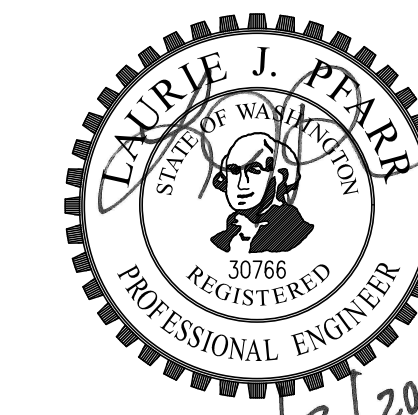
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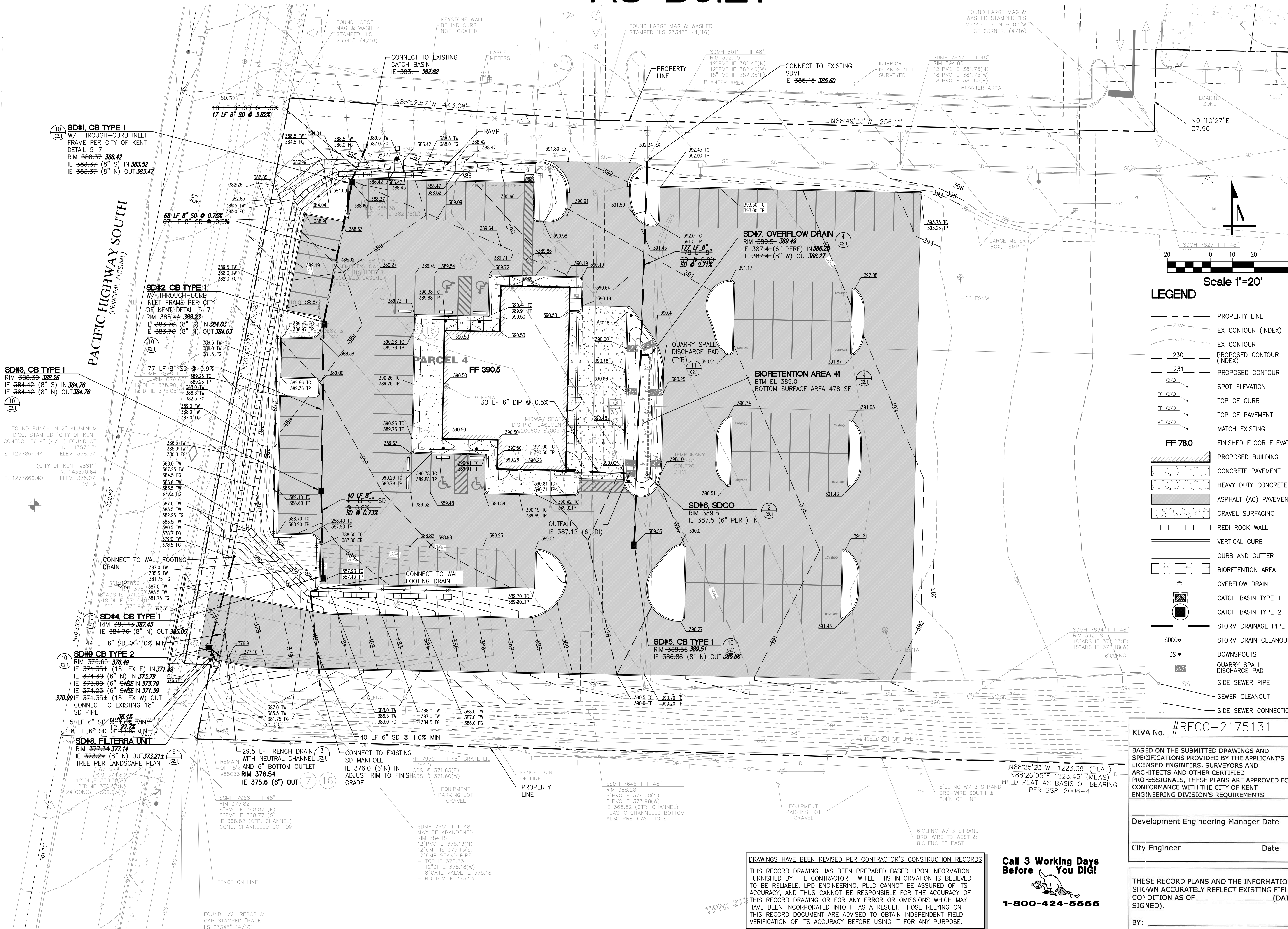


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Project Name	DICK'S DRIVE-IN
Project Number	
Description	GRADING AND DRAINAGE
Computer File	
Scale	AS NOTED
Template	4.3 (120101)

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Call 3 Working Days  
Before You Dig!

**1-800-424-5555**

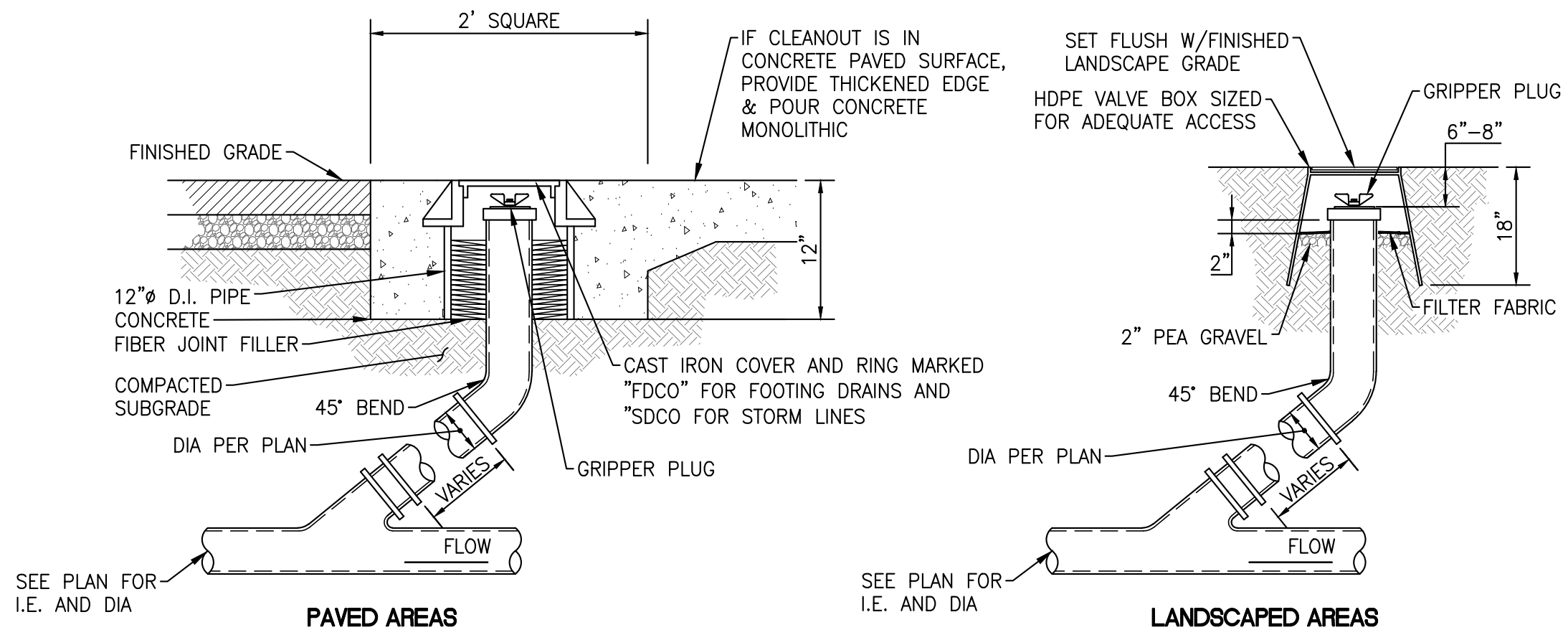
KIVA No.	#RECC-2175131
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City Engineer	Date

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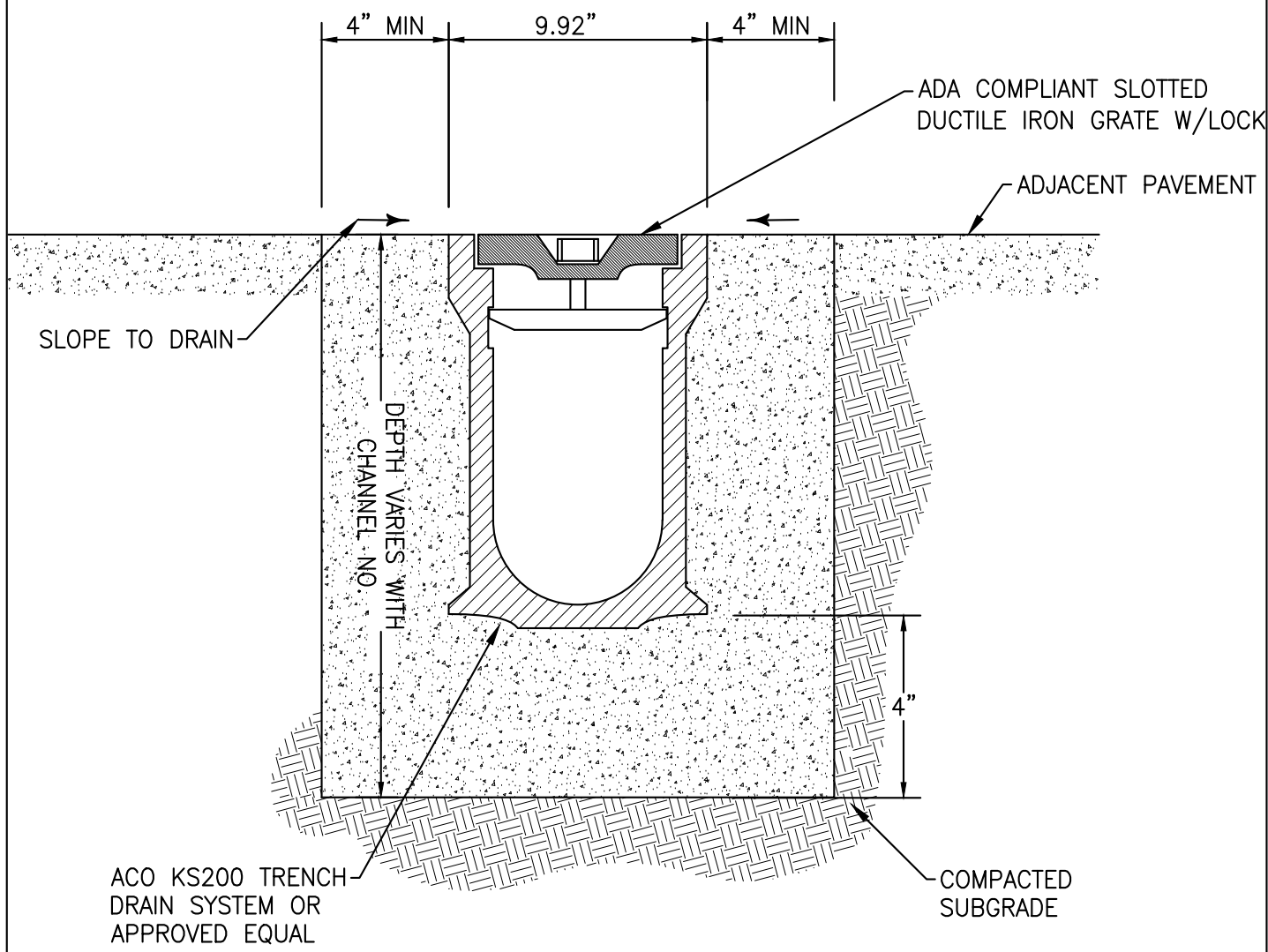
BY: \_\_\_\_\_



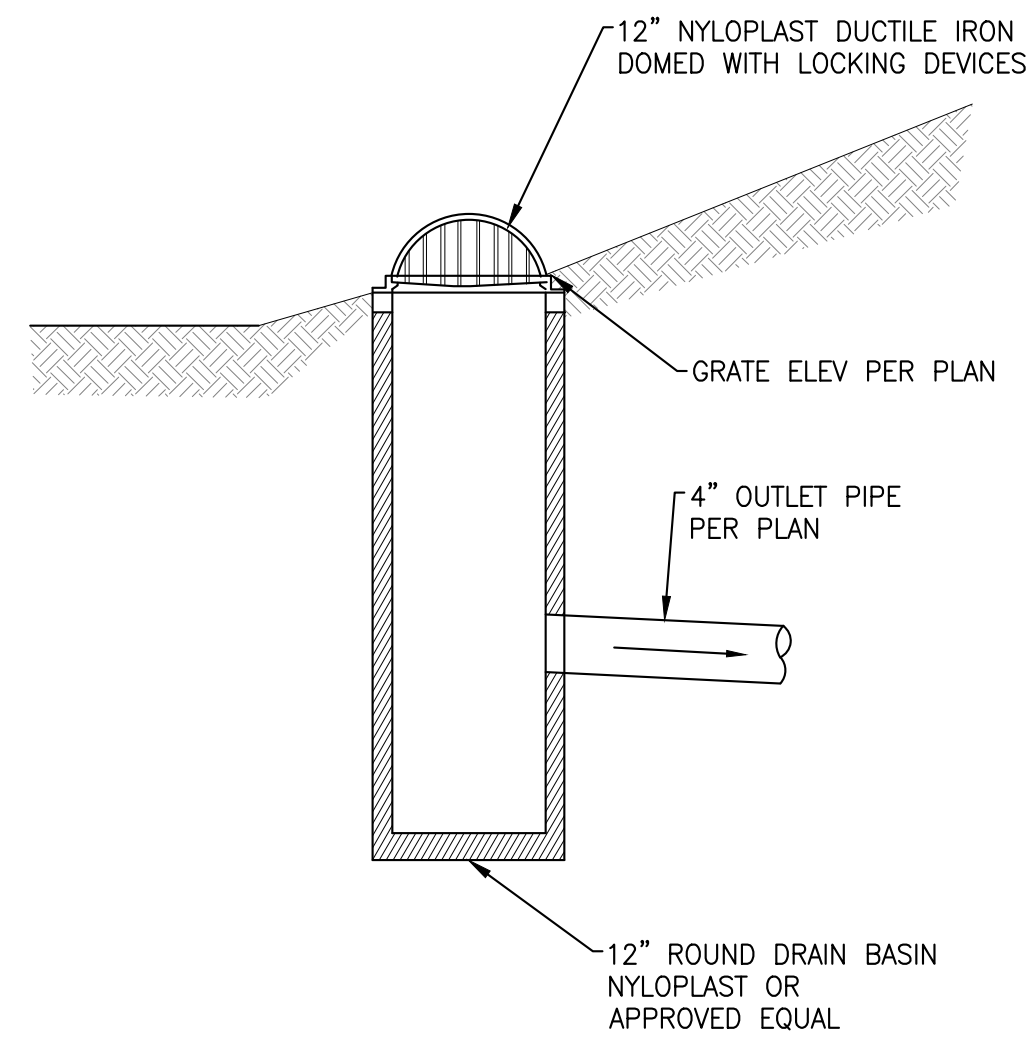
# AS-BUILT



## SEWER/STORM DRAIN CLEANOUTS<sup>NTS</sup> 2



### NTS TRENCH DRAIN 3



OVERFLOW DRAIN<sup>NTS</sup> 4

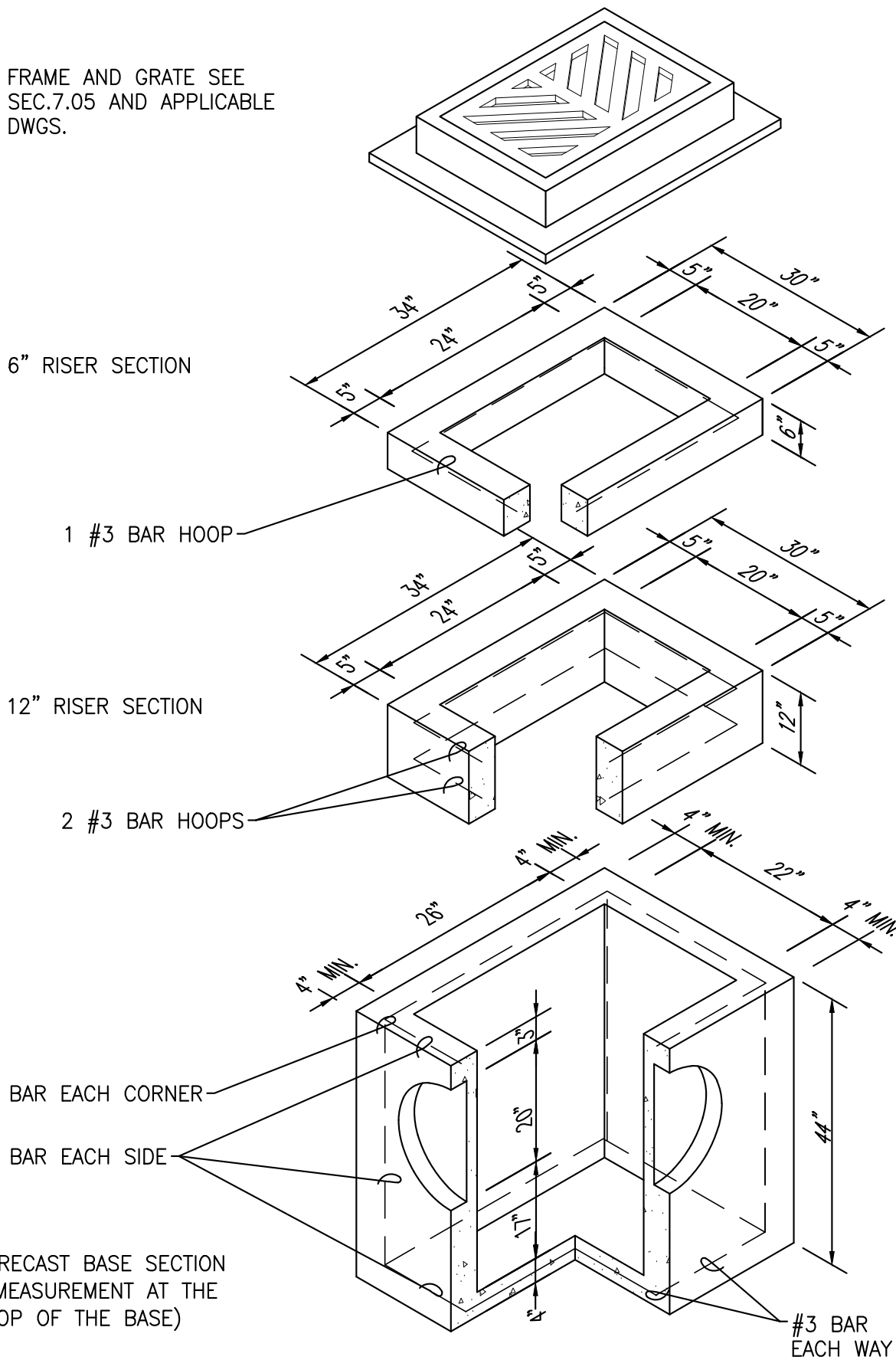
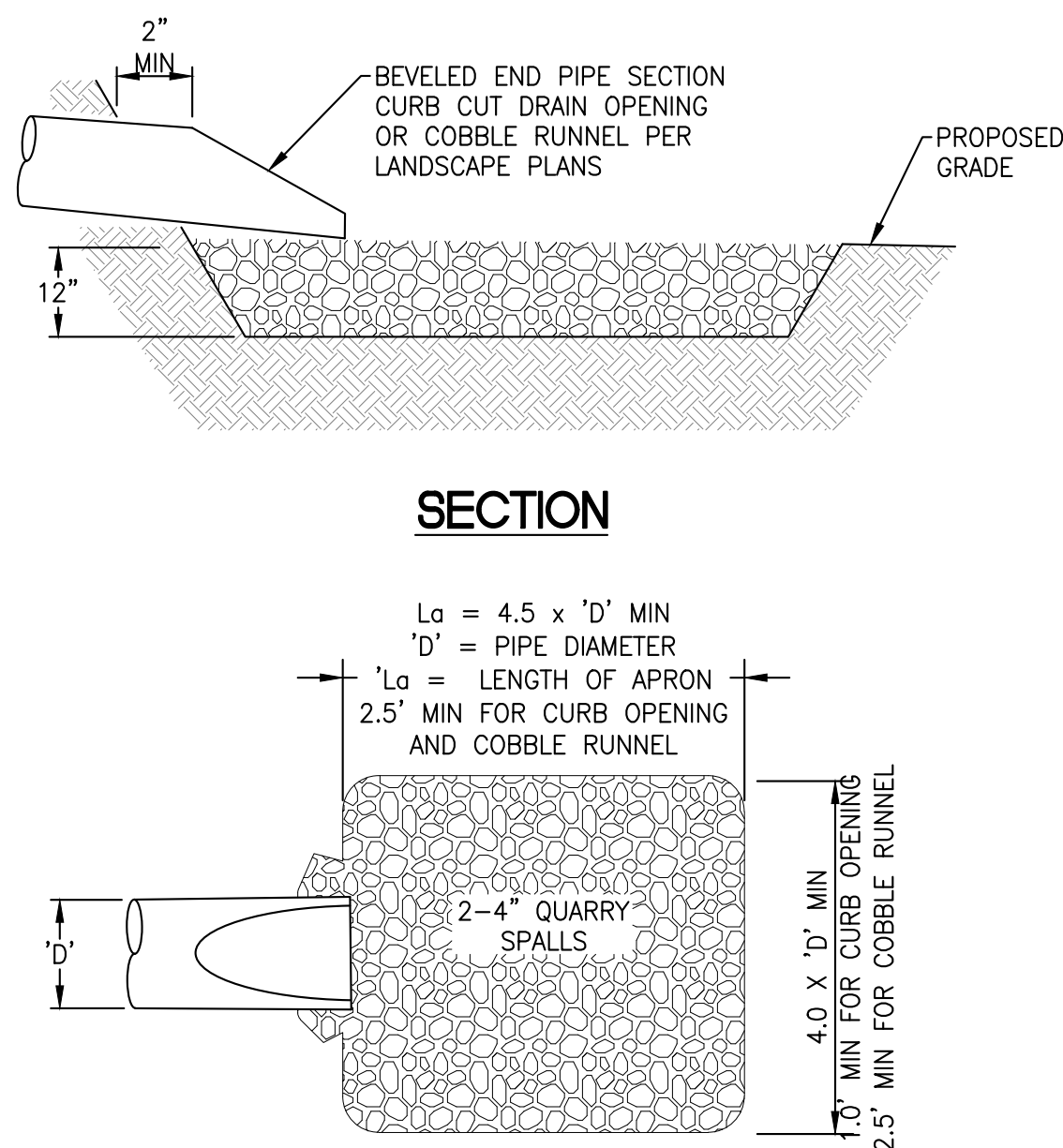
CONSTRUCTION SEQUENCE FOR BIORETENTION AREA

1. INSTALL TEMPORARY SEDIMENT CONTROL BMPs AS SHOWN ON PLAN.
2. COMPLETE SITE GRADING. IF APPLICABLE, CONSTRUCT CURB CUTS OR OTHER INFLOW ENTRANCE BUT PROVIDE PROTECTION SO THAT DRAINAGE IS PROHIBITED FROM ENTERING BIORETENTION CONSTRUCTION AREA.
3. STABILIZE GRADING WITHIN THE LIMIT OF DISTURBANCE EXCEPT WITHIN THE BIORETENTION AREA. BIORETENTION AREAS THAT ARE USED AS TEMPORARY SEDIMENT TRAPS SHOULD BE EXCAVATED 6 INCHES BELOW THE BOTTOM OF THE SEDIMENT TRAP.
4. EXCAVATE BIORETENTION AREA TO PROPOSED DEPTH AND SCARIFY THE TOP 3"-4" OF EXISTING SOIL SURFACES. DO NOT COMPACT NATIVE SUBGRADE.
5. INSTALL GEOTEXTILE FABRIC PER DETAIL.
6. INSTALL PVC SLOTTED UNDERDRAIN PIPE PER PLAN.
7. BACKFILL BIORETENTION AREAS WITH BIORETENTION SOIL MIX AND WASHED ROCK AS SHOWN ON PLANS AND SPECIFICATIONS. OVERTILLING IS RECOMMENDED TO ACCOUNT FOR SETTLEMENT. LIGHT HAND TAMPING IS ACCEPTABLE IF NECESSARY.
8. BIORETENTION SOIL MIX SHALL BE PER THE LANDSCAPE PLANS AND SHALL MEET THE DEFAULT BIORETENTION SOIL MIX (BSM) FOUND IN BMP 17.30 OF THE STORMWATER MANAGEMENT MANUAL FOR WESTERN WASHINGTON. IF USING A CUSTOM MIX, IT SHALL BE TESTED AND COMPLY WITH THE DESIGN CRITERIA FOR CUSTOM BIORETENTION SOIL MIXES.
9. PRESOAK THE PLANTING SOIL PRIOR TO PLANTING VEGETATION TO AID IN SETTLEMENT.
10. COMPLETE FINAL GRADING TO ACHIEVE PROPOSED DESIGN ELEVATIONS. LEAVE SPACE FOR UPPER LAYER OF COMPOST, MULCH OR TOPSOIL AS SPECIFIED ON PLANS.
11. PLANT VEGETATION ACCORDING TO PLANTING PLAN.
12. MULCH AND INSTALL EROSION PROTECTION AT SURFACE FLOW ENTRANCES WHERE NECESSARY UNTIL ENTIRE SITE IS STABILIZED.

NOTES:

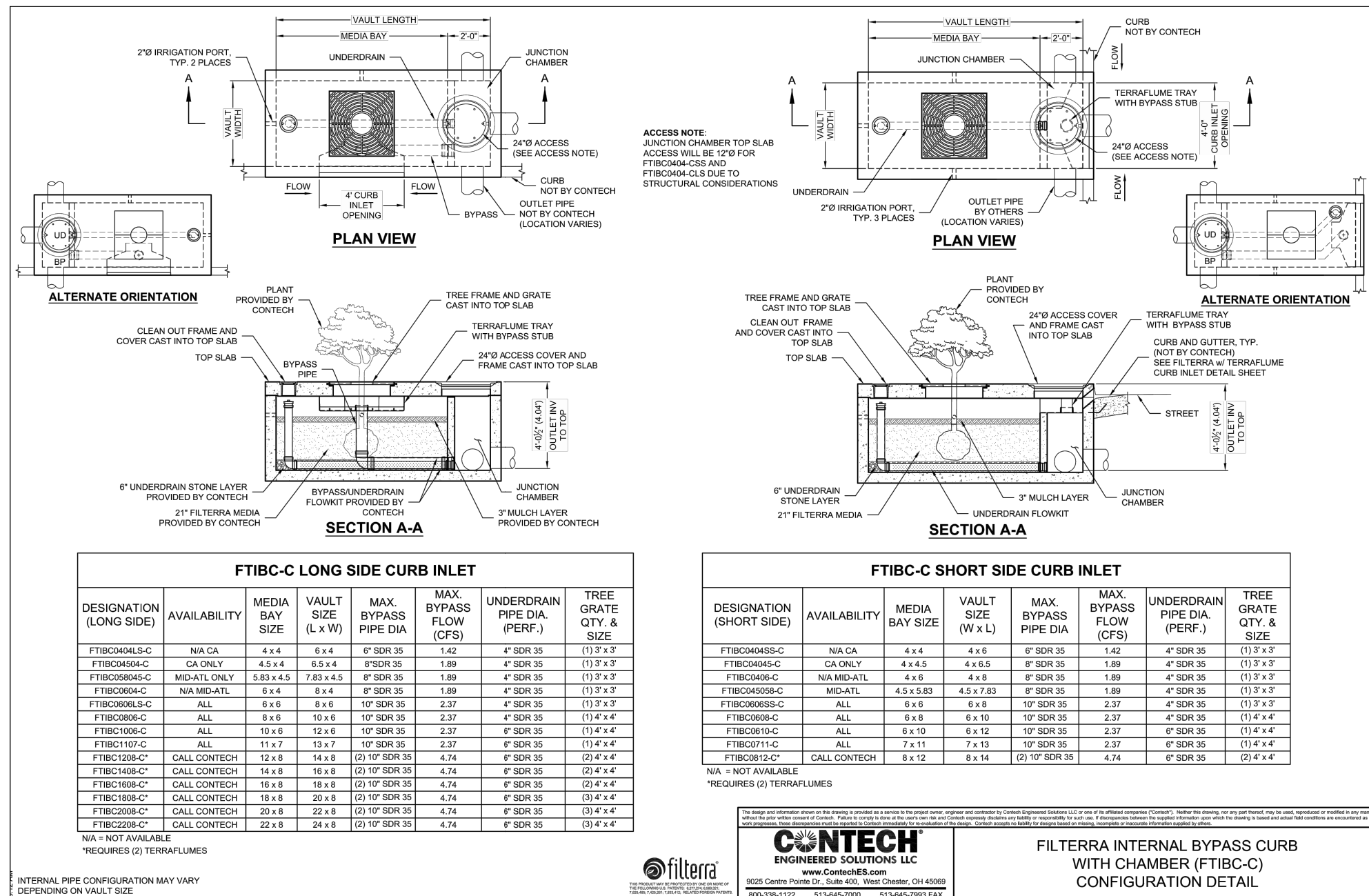
1. CATCH BASINS SHALL BE CONSTRUCTED IN ACCORDANCE WITH ASTM C478 (ASHTO M 199) & C890 UNLESS OTHERWISE SHOWN ON PLANS OR NOTED IN THE WSDOT/APWA STANDARD SPECIFICATIONS.
2. AS AN ACCEPTABLE ALTERNATIVE TO REBAR, WELDED WIRE FABRIC HAVING A MIN. AREA OF 0.12 SQUARE INCHES PER FOOT MAY BE USED. WELDED WIRE FABRIC SHALL COMPLY TO ASTM A497 (ASHTO M 221). WIRE FABRIC SHALL NOT BE PLACED IN KNOCKOUTS.
3. ALL REINFORCED CAST-IN-PLACE CONCRETE SHALL BE CLASS 4000.
4. PRECAST BASINS SHALL BE FURNISHED WITH CUTOUTS OR KNOCKOUTS. KNOCKOUTS SHALL HAVE A WALL THICKNESS OF 2" MIN. ALL PIPE SHALL BE INSTALLED IN FACTORY PROVIDED KNOCKOUTS. UNUSED KNOCKOUTS NEED NOT BE GROUTED IF WALL IS LEFT INTACT.
5. KNOCKOUT OR CUTOUT HOLE SIZE IS EQUAL TO PIPE OUTER DIA. PLUS CATCH BASIN WALL THICKNESS.
6. ROUND KNOCKOUTS MAY BE ON ALL 4 SIDES, WITH MAX. DIAM. OF 20". KNOCKOUTS MAY BE EITHER ROUND OR "D" SHAPE.
7. THE MAX. DEPTH FROM THE FINISHED GRADE TO THE PIPE INVERT IS 5'-0".
8. THE TAPER ON THE SIDES OF THE PRECAST BASE SECTION AND RISER SECTION SHALL NOT EXCEED 1/2"/FT.
9. CATCH BASIN FRAME AND GRATE SHALL BE IN ACCORDANCE WITH STANDARD SPECIFICATIONS AND MEET THE STRENGTH REQUIREMENTS OF FEDERAL SPECIFICATION RR-F-62ID. MATING SURFACES SHALL BE FINISHED TO ASSURE NON-ROCKING FIT WITH ANY COVER POSITION.
10. FRAME AND GRATE MAY BE INSTALLED WITH FLANGE DOWN OR CAST INTO RISER.
11. FOR CATCH BASINS IN PARKING LOTS REFER TO WSDOT/APWA STANDARD DWG. B1-b.
12. EDGE OF RISER OR BRICK SHALL NOT BE MORE THAN 2" FROM VERTICAL EDGE OF CATCH BASIN WALL.

FRAME AND GRATE SEE  
SEC.7.05 AND APPLICABLE  
DWGS.

TYPE 1 CATCH BASIN<sup>NTS</sup> 10

## PLAN

QUARRY SPALL DISCHARGE PAD<sup>NTS</sup> 1 1



NTS  
FILTERRA UNIT DETAIL 8

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Development Engineering Manager Date

City Engineer \_\_\_\_\_ Date \_\_\_\_\_

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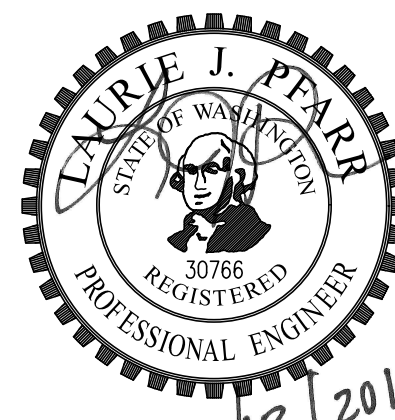
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1	12.28.17	PERMIT		LJP
2	02.27.18	PERMIT RESUBMITTAL		LJP
3	03.16.18	PERMIT RESUBMITTAL		LJP
4	08.08.18	SIDEWALK DELETION		LJP
5	06/13..19	RECORD DRAWINGS		LJP

Project Name	DICK'S DRIVE-IN
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Project Number	
Description	GRADING AND DRAINAGE DETAILS

Computer File

Scale	AS NOTED
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Template	4.3 (120101)
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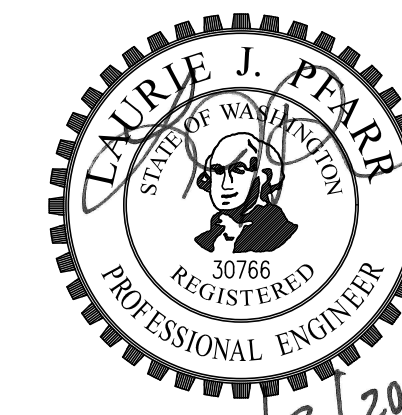
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5	06/13.19	RECORD DRAWINGS	LJP	

## LEGEND

- PROPERTY LINE
- EX CONTOUR (INDEX)
- EX CONTOUR
- PROPOSED CONTOUR (INDEX)
- PROPOSED CONTOUR
- SPOT ELEVATION
- TOP OF CURB
- TOP OF PAVEMENT
- MATCH EXISTING
- FINISHED FLOOR ELEVATION
- PROPOSED BUILDING
- CONCRETE PAVEMENT
- HEAVY DUTY CONCRETE
- ASPHALT (AC) PAVEMENT
- GRAVEL SURFACING
- REDI ROCK WALL
- VERTICAL CURB
- CURB AND GUTTER
- BIORETENTION AREA
- OVERFLOW DRAIN
- CATCH BASIN TYPE 1
- CATCH BASIN TYPE 2
- STORM DRAINAGE PIPE
- STORM DRAIN CLEANOUT
- DOWNSPOUTS
- QUARRY SPALL DISCHARGE PAD
- SS SIDE SEWER PIPE
- SEWER CLEANOUT
- SIDE SEWER CONNECTION

KIVA No. #RECC-2175131

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Development Engineering Manager Date

City Engineer

Date

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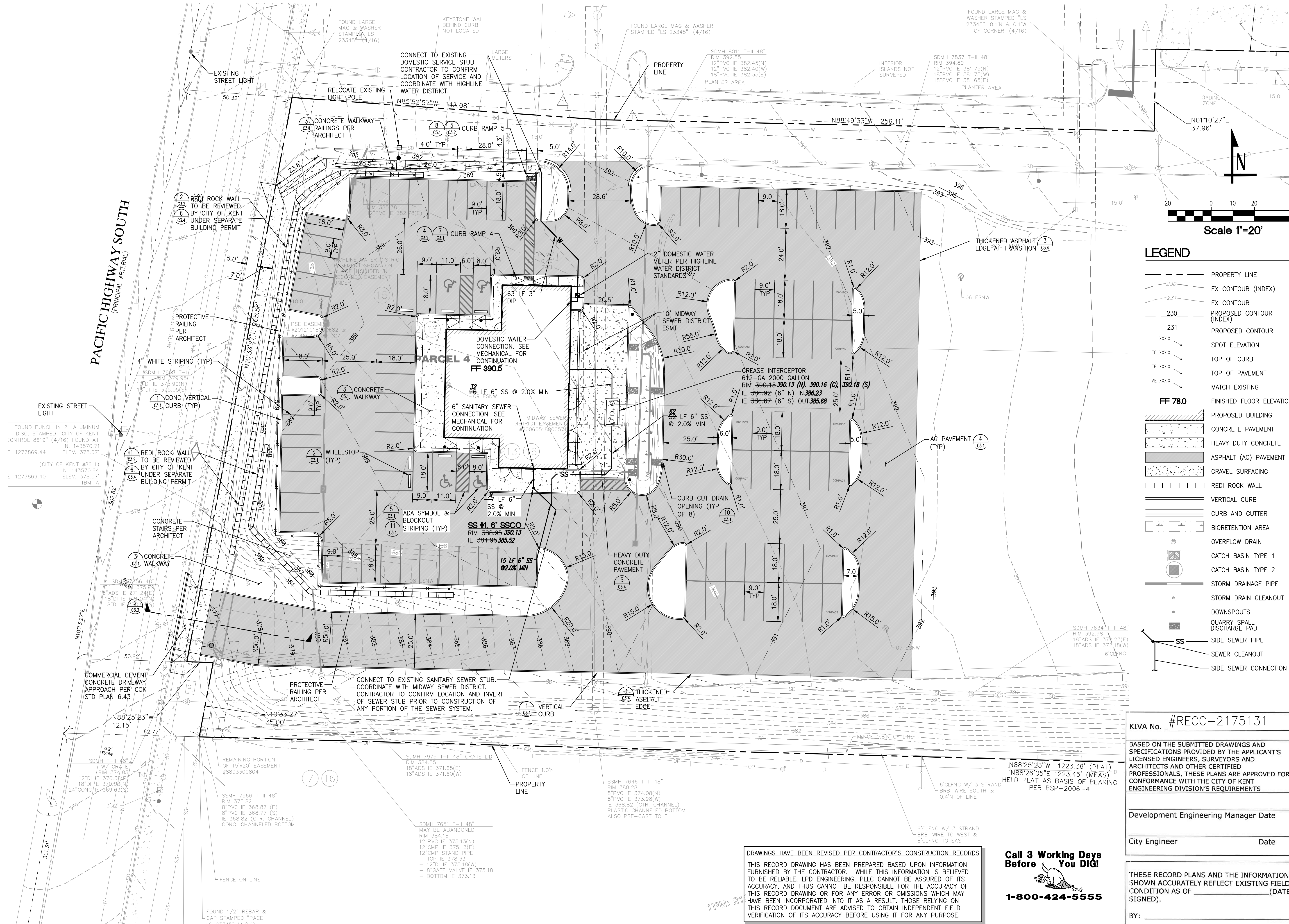
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AS-BUILT

1" = 5'  
CURB RAMP 1 1

1" = 5'  
NOT USED 2

1" = 5'  
NOT USED 3

1" = 5'  
CURB RAMP 4 4

1" = 5'  
CURB RAMP 5 5

NTS  
NOT USED 6

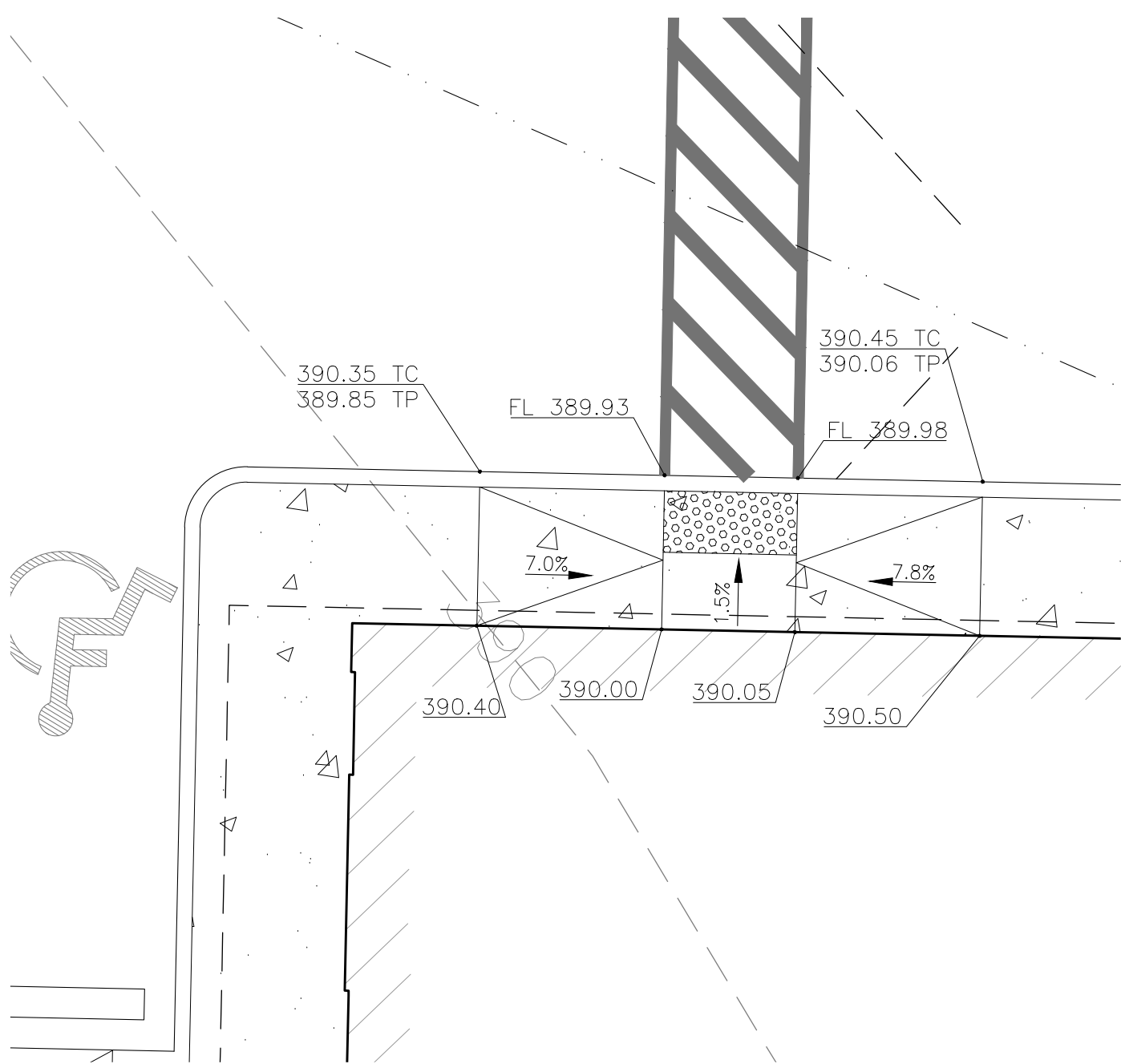
NTS  
NOT USED 7

NTS  
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NTS  
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NTS  
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NTS  
NOT USED 11



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BY: \_\_\_\_\_

Project Name DICK'S DRIVE-IN

Project Number

Description UTILITIES AND PAVING DETAILS

Computer File

Scale AS NOTED

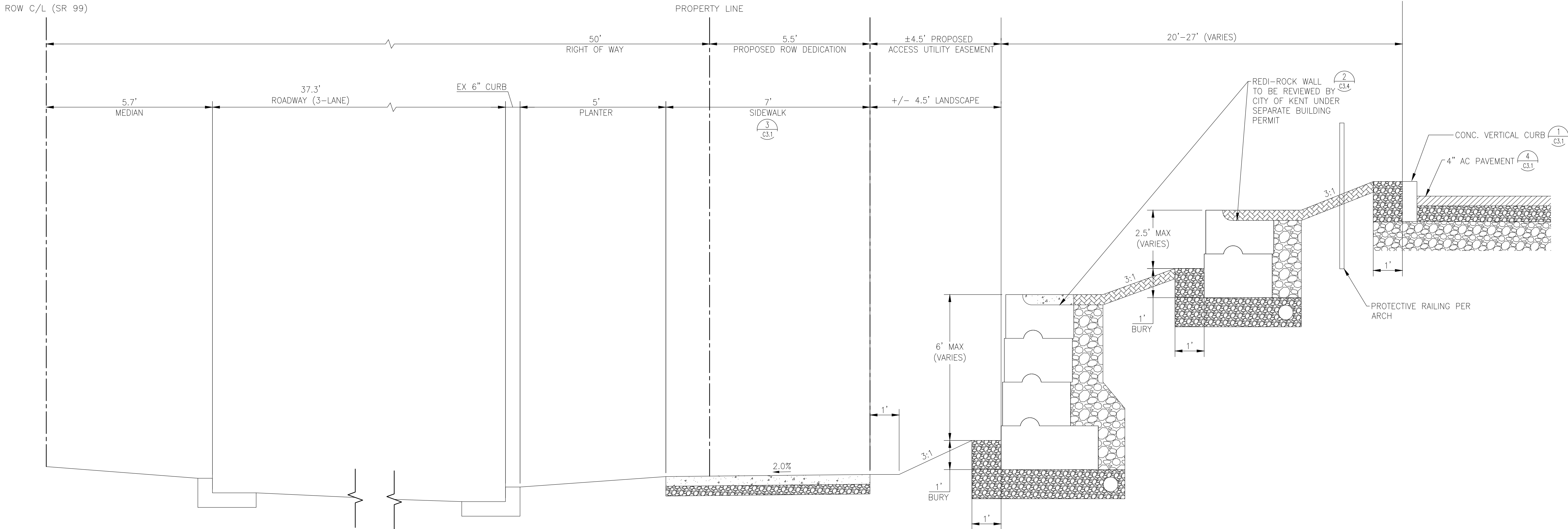
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C3.2



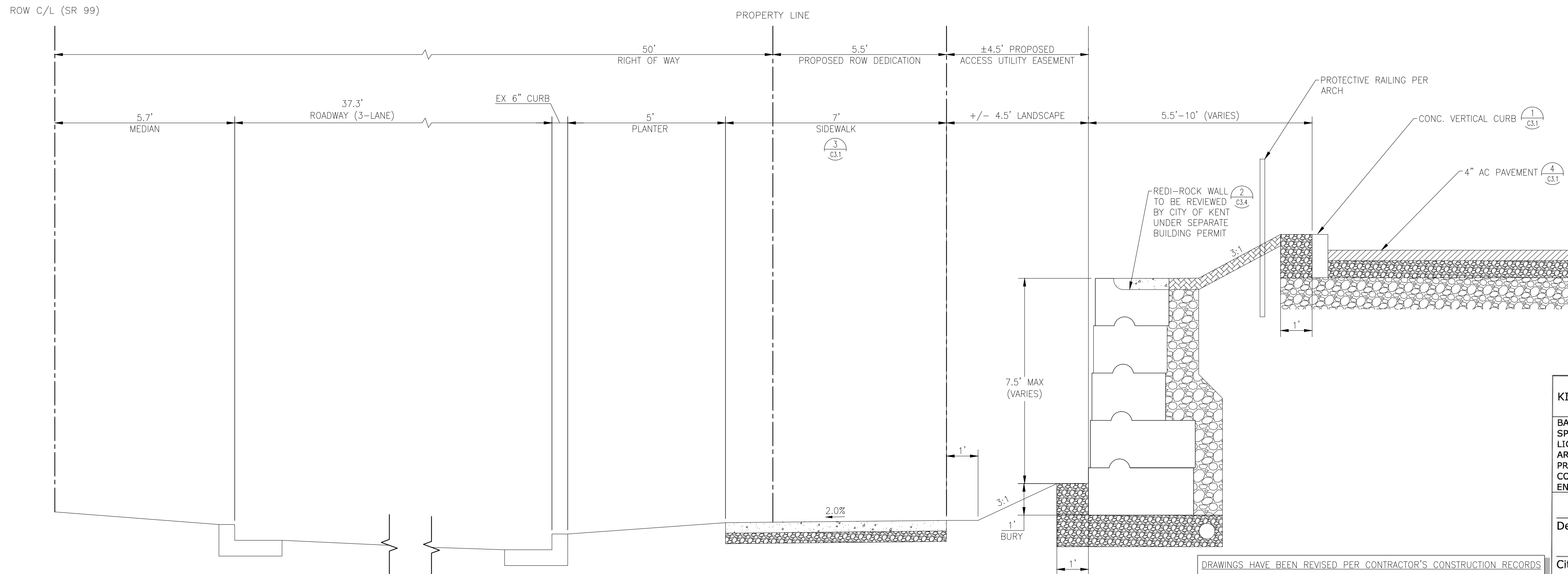
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1"=2'

FRONTAGE SECTION - TWO WALLS

1



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FRONTAGE SECTION - ONE WALL <sup>1"=2'</sup> 2

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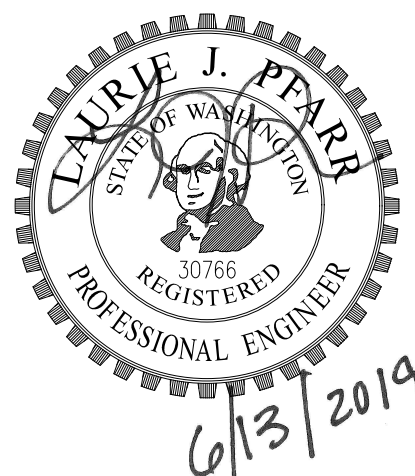
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Project Name	DICK'S DRIVE-IN
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Project Number	
Description	REDI ROCK WALL SECTIONS

Computer File

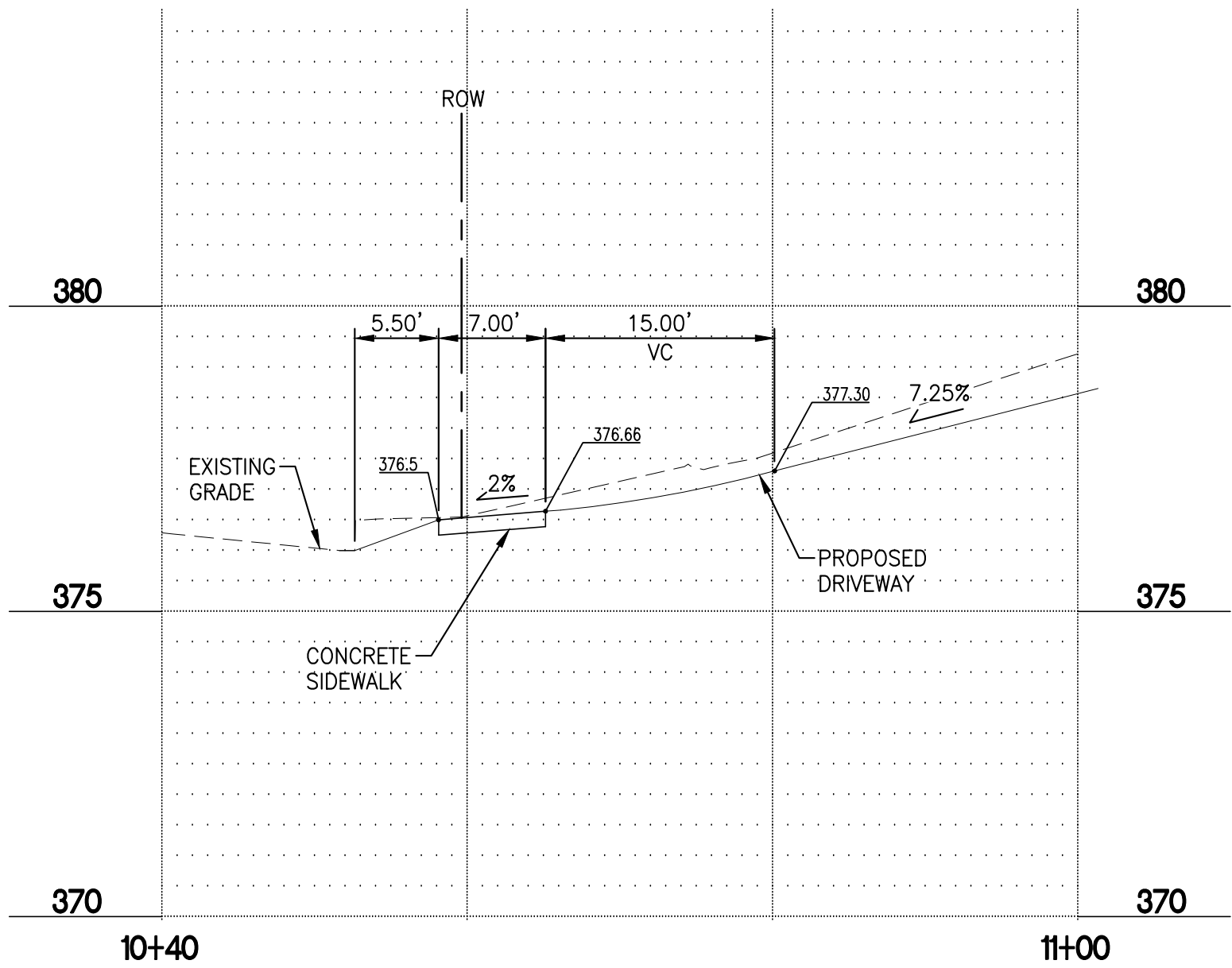
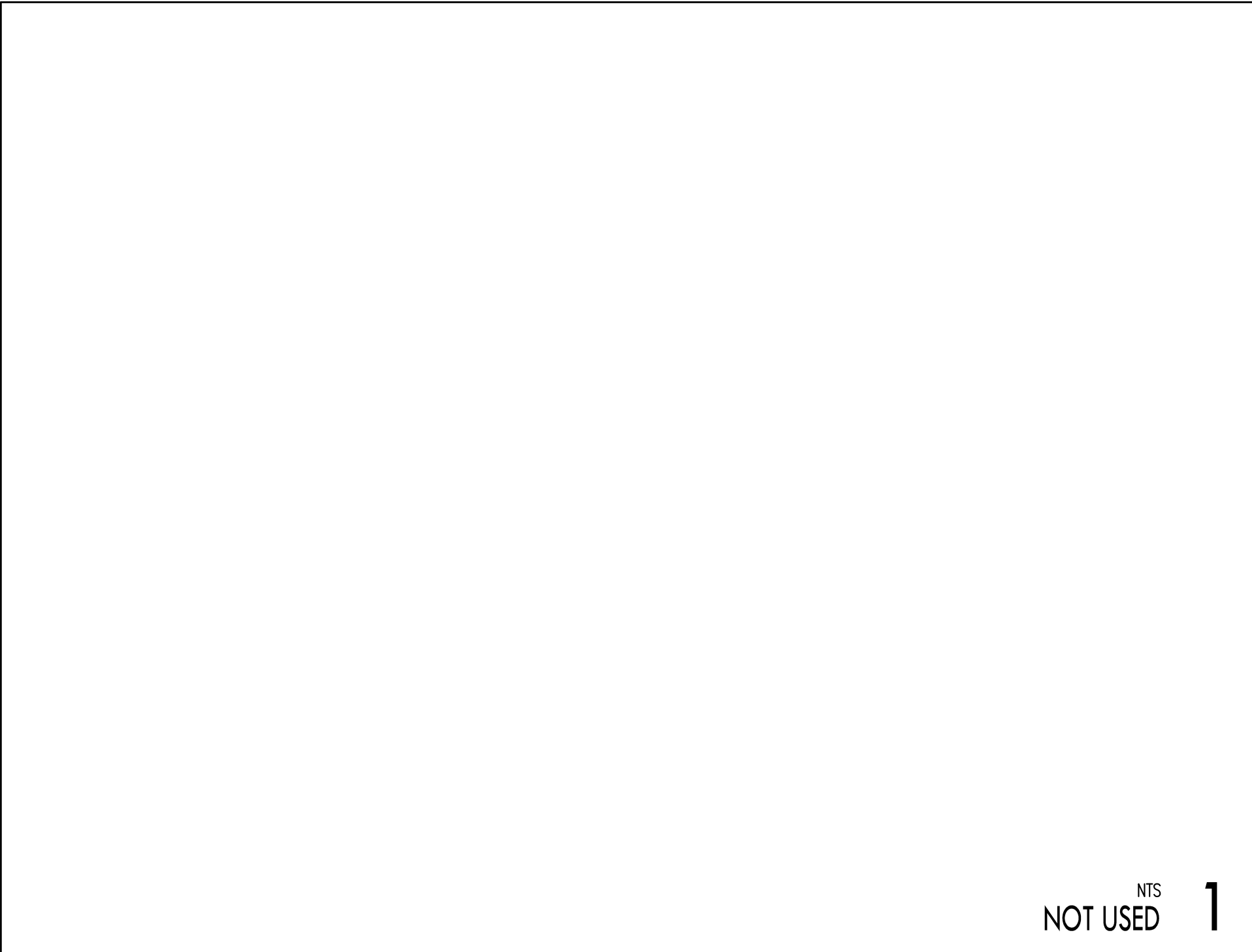
Scale	AS NOTED
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### C3.3

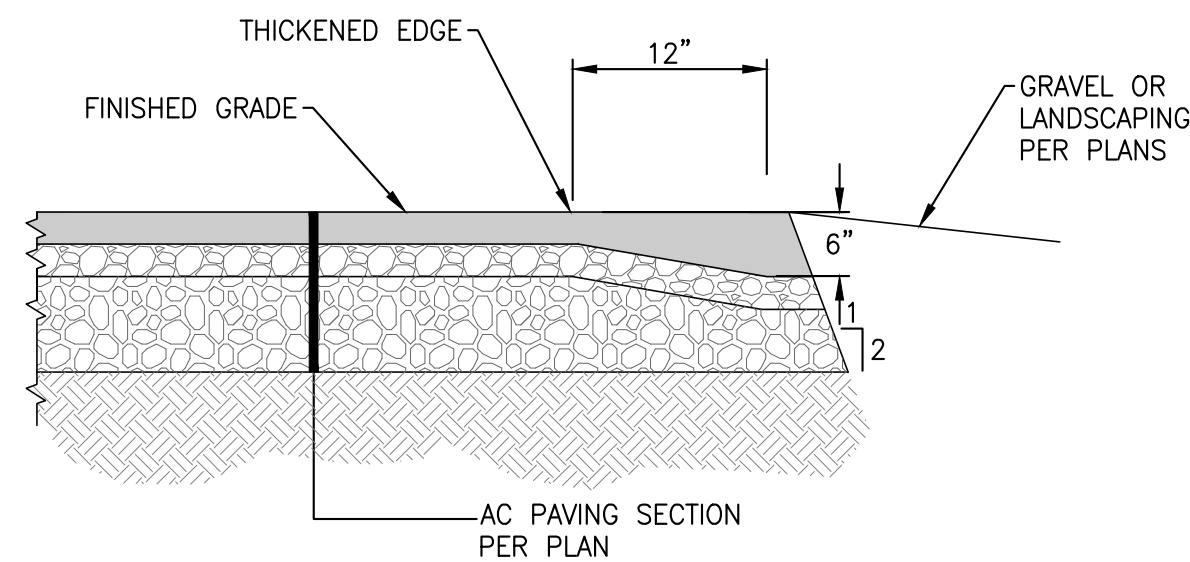




SOUTH DRIVEWAY ENTRANCE PROFILE

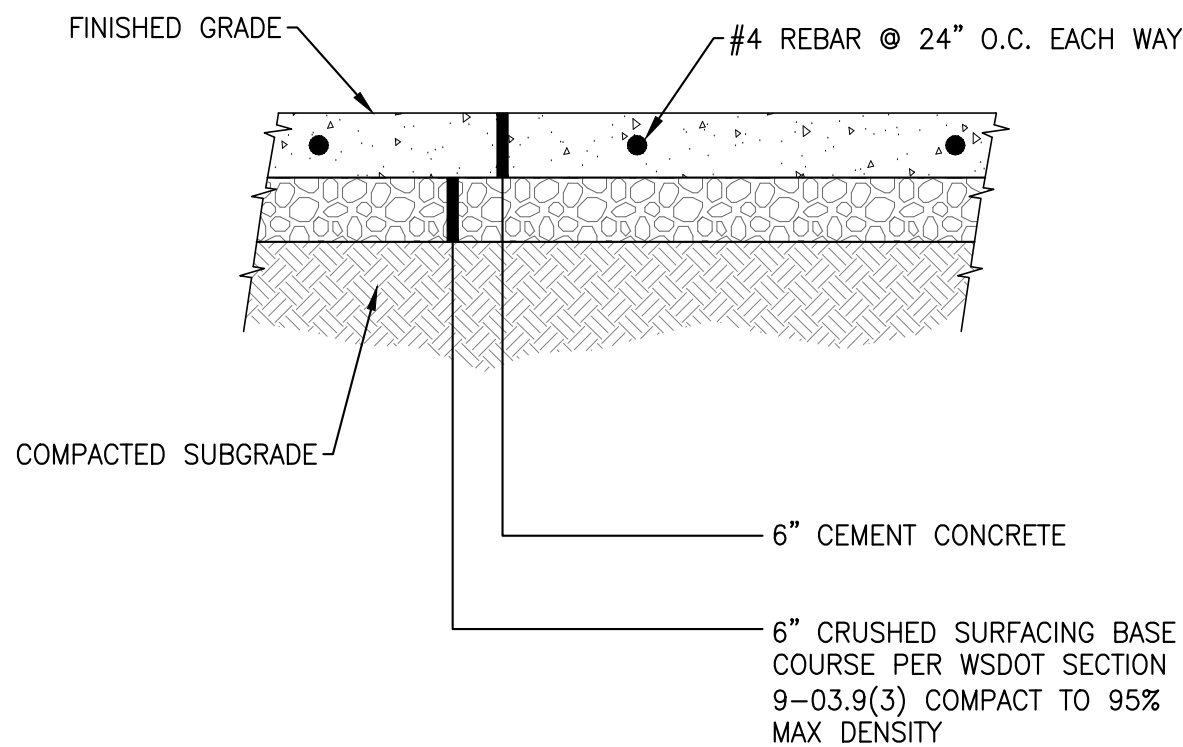
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# AS-BUILT



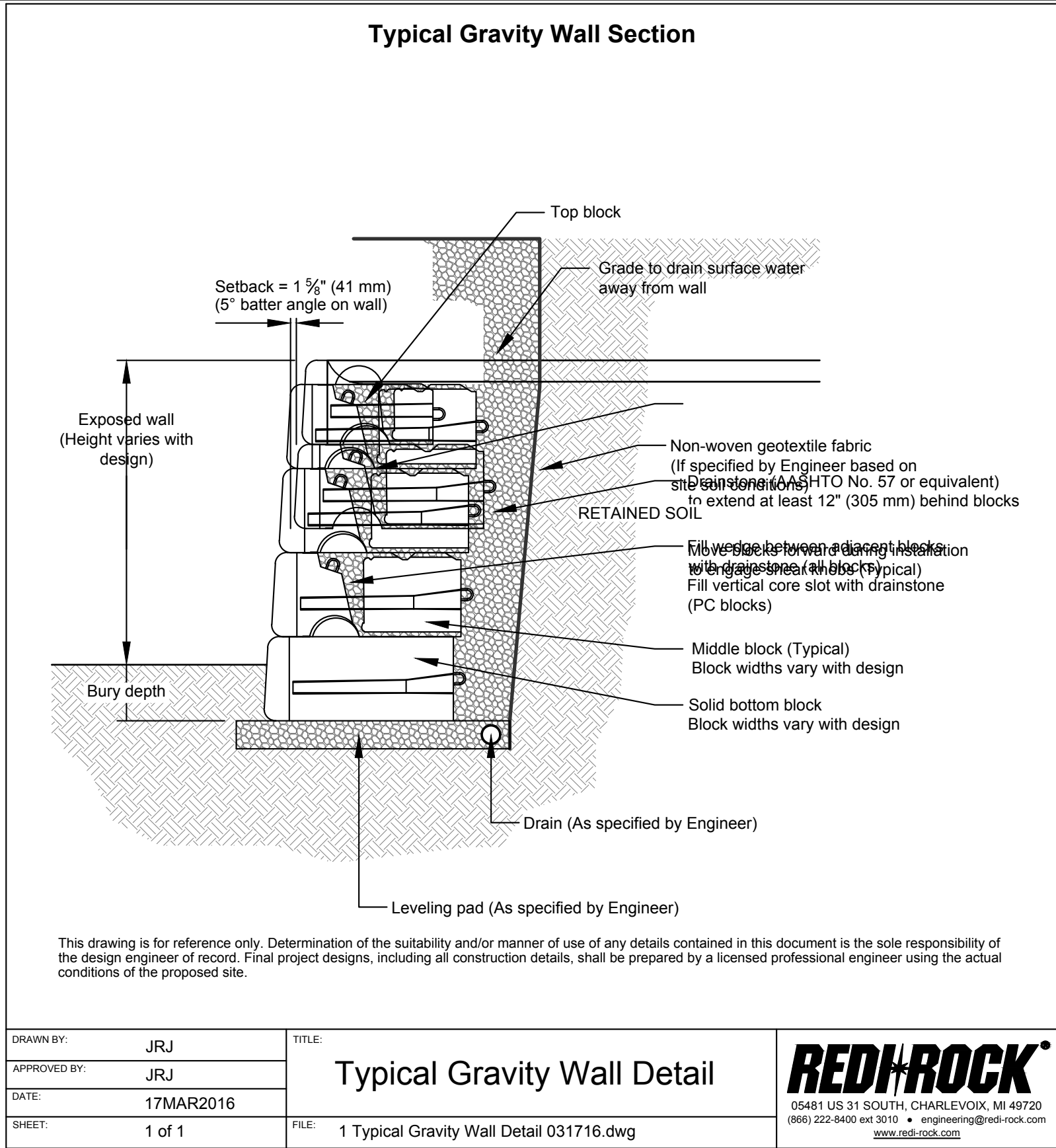
THICKENED ASPHALT EDGE

3



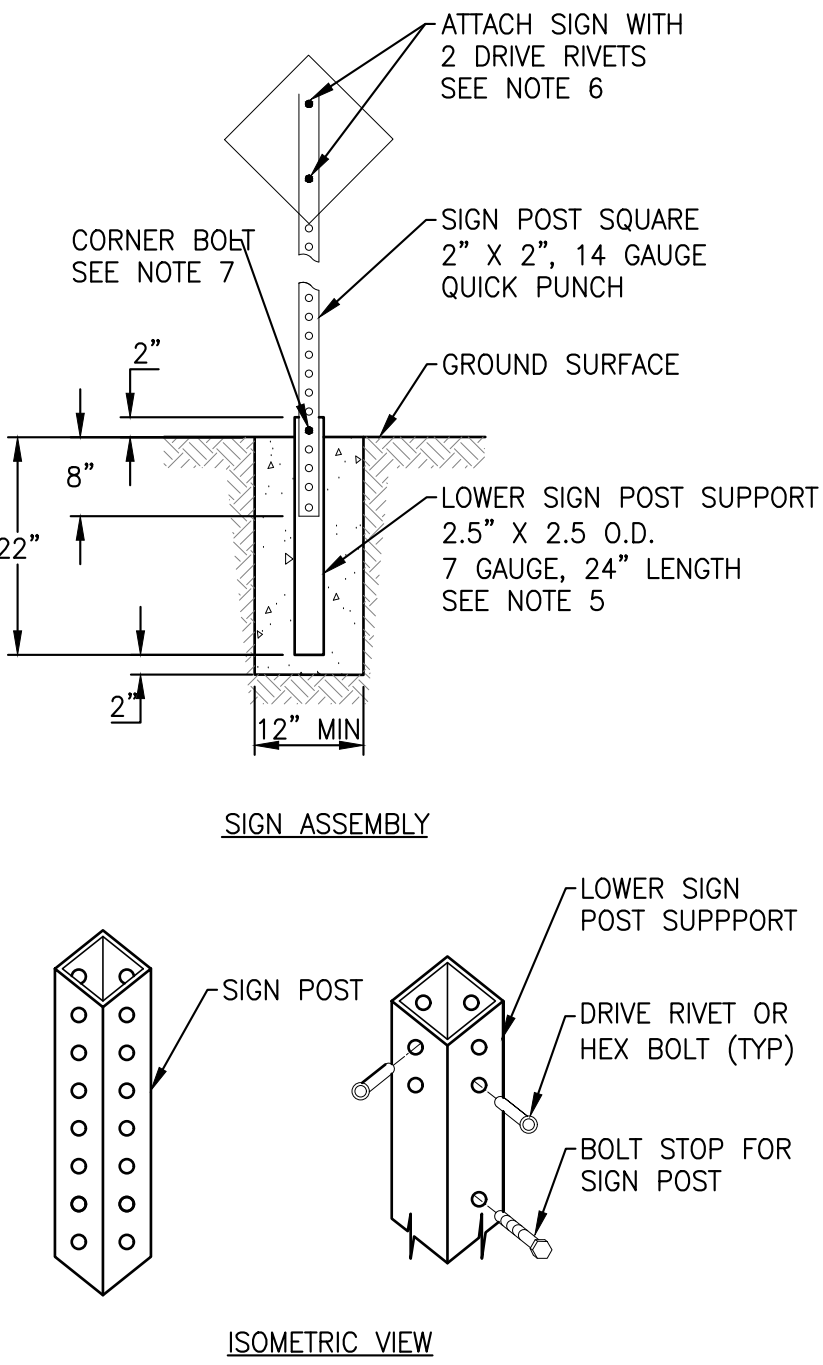
HEAVY DUTY CONCRETE

5



TYPICAL REDI-ROCK GRAVITY WALL

6



#### METAL POST NOTES:

- METAL POSTS SHALL BE TELESAR QUICK PUNCH POST.
- METAL STOP AND YIELD SIGN POSTS SHALL HAVE ALTERNATING 1" BANDS OF RED AND WHITE 3M DIAMOND GRADE SHEETING. ALL OTHER POSTS SHALL BE UNSHEETED.
- FOR IN-SIDEWALK INSTALLATIONS, CORE 4" DIAM. HOLE. ANCHOR LENGTH MAY BE DECREASED TO 12".
- POST SHALL BE ROLLED CARBON SHEET STEEL, ASTM A570 GRADE 50 AND BE HOT DIPPED GALVANIZED AASHTO M-120 YIELD STRENGTH 80,000 PSI MIN. POST SHALL HAVE 7/16" DIE-PUNCHED KNOCKOUTS ON 1" CENTERS FULL LENGTH, FOUR SIDES.
- ANCHOR SHALL HAVE 4 7/16" HOLES ONE EACH SIDE 2" FROM TOP END. FINISH SHALL BE ZINC HOT DIPPED GALVANIZED MATERIAL TO MEET ASTM A500 GRADE B.
- DRIVE RIVETS TO BE TL3806 3/8" DIA.
- CORNER BOLTS TO BE TLCS16M.



SIGN POSTS SHALL PER SIGN POST NOTES INDICATED ON DETAIL 7 THIS SHEET

EACH PARKING SPACE RESERVED FOR PERSONS WITH PHYSICAL DISABILITIES SHALL BE IDENTIFIED BY A REFLECTORIZED SIGN PERMANENTLY POSTED IMMEDIATELY ADJACENT TO AND VISIBLE FROM EACH STALL OR SPACE CONSISTING OF A PROFILE VIEW OF A WHEELCHAIR WITH OCCUPANT IN WHITE ON DARK BLUE BACKGROUND. THE SIGN SHALL NOT BE SMALLER THAN 70 SQUARE INCHES IN AREA AND SHALL BE CENTERED ON AND JUST BEYOND THE FRONT EDGE OF THE PARKING STALL. SIGNS MAY ALSO BE HUNG ON THE WALL AT THE INTERIOR END OF THE PARKING SPACE. SIGN MOUNTING HEIGHT SHALL BE 60 INCHES ABOVE THE PARKING SPACE FINISHED GRADE. ADDITIONAL SIGN SHALL ALSO BE POSTED, IN A CONSPICUOUS PLACE, AT EACH ENTRANCE TO OFF-STREET PARKING FACILITIES, OR IMMEDIATELY ADJACENT TO AND VISIBLE FROM EACH STALL OR SPACE. THE SIGN SHALL BE NOT LESS THAN 17 INCHES BY 22 INCHES IN SIZE WITH LETTERING NOT LESS THAN 1 INCH IN HEIGHT, WHICH CLEARLY AND CONSPICUOUSLY STATES THE FOLLOWING:

"UNAUTHORIZED VEHICLES PARKED IN DESIGNATED DISABLED PARKING SPACES NOT DISPLAYING DISTINGUISHING PLACARDS OR LICENSE PLATES ISSUED FOR PHYSICALLY DISABLED PERSONS MAY BE TOWED AWAY AT OWNER'S EXPENSE. TOWED VEHICLES MAY BE RECLAIMED AT \_\_\_\_\_ OR BY TELEPHONING \_\_\_\_\_"

ADA PARKING SIGN

8

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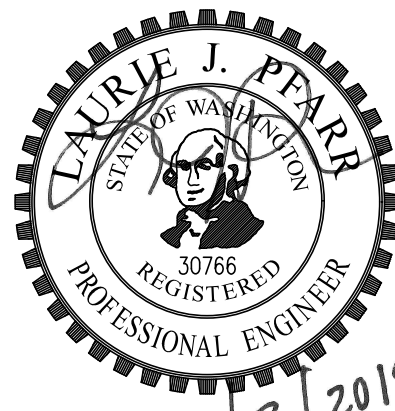
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Project Name DICK'S DRIVE-IN

Project Number  
Description DRIVEWAY PROFILES AND DETAILS

Computer File

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