

ORDINANCE NO. 1098

AN ORDINANCE OF THE CITY COUNCIL OF THE CITY OF NORCO, CALIFORNIA, APPROVING ZONE CODE AMENDMENT 2023-02: A CITY-INITIATED PROPOSAL TO AMEND NORCO MUNICIPAL CODE TITLE 18, SECTION 18.43.06 (3) "PROCEDURE FOR HEARINGS AND APPEAL THEREFROM", REVISING THE PUBLIC NOTICE REQUIREMENTS FOR CONDITIONAL USE PERMIT APPROVAL TO AUTHORIZE THE REUSE OR REDEVELOPMENT OF ANY EXISTING WAREHOUSE OR INDUSTRIAL BUILDING WITH A GROSS FLOOR AREA LARGER THAN 20,000 SQUARE FEET, IN THE M-1 ZONE AND WITHIN THE INDUSTRIAL DISTRICT OF THE GATEWAY SPECIFIC PLAN.

WHEREAS, the CITY OF NORCO initiated Zone Code Amendment 2023-02, a proposal to amend Section 18.43.06 (3), "Procedures for Hearing and Appeal Therefrom", of the Norco Municipal Code Title 18 (Zoning Code), relating to public notice requirements for approval of reuse or redevelopment of any existing warehouse or industrial building with a gross floor area larger than 20,000 square feet; and

WHEREAS, the Zone Code Amendment was duly submitted to said City's Planning Commission for decision at a public hearing for which proper notice was given; and

WHEREAS, the Zone Code Amendment was scheduled for public hearing on April 12, 2023 on or about 7 p.m. in the Council Chambers at 2820 Clark Avenue, Norco, California 92860; and

WHEREAS, at the time set, the Planning Commission held a public hearing and received both oral and written testimony pertaining to the Zone Code Amendment; and

WHEREAS, the Planning Commission adopted Resolution No. 2023-08 recommending that the City Council adopt the Zone Code Amendment; and

WHEREAS, the Zone Code Amendment was duly submitted to said City's City Council for decision at a public hearing for which proper notice was given; and

WHEREAS, the Zone Code Amendment was scheduled for public hearing on May 3, 2023 on or about 7 p.m. in the Council Chambers at 2820 Clark Avenue, Norco, California 92860; and

WHEREAS, at the time set, the City Council held a public hearing and received both oral and written testimony pertaining to the Zone Code Amendment; and

WHEREAS, the City of Norco acting as the Lead Agency has determined that this project is exempt from review under the California Environmental Quality Act (CEQA). Pursuant to CEQA Guidelines (California Code of Regulations, §§ 15000 et seq.) Section 15061 (b) (3), it can be seen with certainty that there is no possibility that the adoption and implementation of the subject amendments will have a significant effect on the environment.

NOW, THEREFORE, the City Council of the City of Norco, California, does hereby make the following FINDINGS AND DETERMINATION:

I. FINDINGS.

- A. The proposed Zone Code Amendment is necessary as it addresses the findings in Ordinance 1086 and 1087 that enacted a moratorium and an extension to that moratorium respectively, on the approval of new entitlement applications for the construction of industrial and warehouse buildings in the City.
- B. The proposed Zone Code Amendment is necessary to ensure and maintain orderly development of land uses consistent with the objectives, policies, general land uses, programs, and actions of the City of Norco General Plan. None of the amendments conflict with current General Plan policies, objectives or programs of the General Plan.
- C. The proposed Zone Code Amendment will not be detrimental to the public convenience, health, safety, or general welfare of the City because the amendments will increase compatibility of industrial and warehouse uses with surrounding properties.
- D. This project is exempt from review under the California Environmental Quality Act (CEQA) pursuant to CEQA Guidelines Section 15061(b)(3), which stipulates that CEQA only applies to projects which have the potential for causing a significant effect on the environment. Where it can be seen with certainty that there is no possibility that the activity in question may have a significant effect on the environment, the activity is not subject to CEQA. Based on the evidence presented to the City Council on May 3, 2023, on the subject project, the proposed Zone Code and Specific Plan Amendments updates permitted uses, development standards, architectural standards, and the list of permitted uses within areas of the city. Each new development within all affected areas of the said Zone Code and Specific Plan Amendments will continue to be reviewed under CEQA at the time of project submittal. Therefore, the proposed changes in regulation have no potential to have a significant effect on the environment, and are therefore not subject to CEQA, under the commonsense exemption.

II. DETERMINATION:

NOW THEREFORE, the City Council of the City of Norco, California, does hereby approve as follows:

SECTION 1:

Title 18 (Zoning) of the Norco Municipal Code, Section 18.43.06 (3) "Procedures for Hearing and Appeal Therefrom" is hereby amended to read as follows:

18.43.06 Procedure for Hearing and Appeals Therefrom

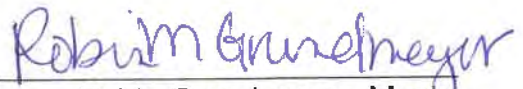
(3) The Community Development Director or designee shall cause notice of the public hearing to be published at least once in a newspaper of general circulation, published and circulated in the City, or if there is none, it shall be posted in at least five public places within the City, not less than 10 calendar days before the date of said hearing. The Community Development Director or designee shall also cause said notice to be mailed, postage prepaid, not less than 10 calendar days before the date of said hearing to owners of real property within a minimum radius of 300 feet of the exterior boundaries of the property, which is the subject of the application, or within the minimum radius that is required such that a minimum of 25 property owners will be notified. However, public notice for a hearing for conditional use permit approval to authorize the reuse or redevelopment of any existing warehouse or industrial building with a gross floor area larger than 20,000 in the Heavy Commercial/Light Manufacturing (M-1) Zone and the Gateway Specific Plan Industrial District (I District) shall be mailed to all property owners within 1,000 feet of the exterior boundaries of the property which is the subject of the application. The names and addresses of property owners for mailing the notice of public hearing shall be taken from the last equalized assessment roll, or, alternatively, from such other records as contain more recent information in the opinion of the Planning Commission.

SECTION 2. EFFECTIVE DATE: This Ordinance shall become effective 30 days after final passage thereof.


SECTION 3. SEVERABILITY: If any section, subsection, sentence, clause, or phrase of this Ordinance is for any reason held to be invalid or unconstitutional by the decision of any court of competent jurisdiction, such decision shall not affect the validity of the remaining portions of the Ordinance, and each section, subsection, sentence, clause, sentences, clauses, or phrases here of irrespective of the fact that any one or more of the sections, subsections, sentences, clauses, or phrases hereof be declared invalid or unconstitutional.

SECTION 4. POSTING: The Mayor shall sign this Ordinance and the City clerk shall attest hereto and shall cause the same within 15 days of its passage to be posted at no less than five public places within the City of Norco.

PASSED AND ADOPTED by the City Council of the City of Norco, California at a regular meeting held June 7, 2023.


Robin Grundmeyer, Mayor
City of Norco, California


ATTEST:


for Dana Roa, CMC, City Clerk
City of Norco, California

I, Dana Roa, CMC, City Clerk of the City of Norco, California, do hereby certify that the foregoing Ordinance was introduced at a regular meeting of the City Council of the City of Norco, California, duly held on May 3, 2023, and thereafter at a regular meeting of said City Council duly held on June 7, 2023, it was duly passed and adopted by the following roll call vote of the City Council:

AYES: GRUNDMEYER, BASH, ALEMAN, HANNA, NEWTON
NOES: NONE
ABSENT: NONE
ABSTAIN: NONE

IN WITNESS THEREOF, I have hereunto set my hand and affixed the official seal of the City of Norco, California, on June 7, 2023


for Dana Roa, CMC, City Clerk
City of Norco, California

ORDINANCE NO. 1097

AN ORDINANCE OF THE CITY COUNCIL OF THE CITY OF NORCO, CALIFORNIA, GATEWAY SPECIFIC PLAN 99-01, AMENDMENT NO. 10: A CITY-INITIATED PROPOSAL TO AMEND SECTION 4.10.1.4 "INDUSTRIAL DISTRICT" OF THE GATEWAY SPECIFIC PLAN, AMENDING THE CITY'S EXISTING INDUSTRIAL REGULATIONS TO PROVIDE NEW/UPDATED DEVELOPMENT, USE, AND OPERATIONAL REGULATIONS FOR INDUSTRIAL AND WAREHOUSE USES.

WHEREAS, the CITY OF NORCO initiated Gateway Specific Plan 99-01, Amendment No. 10, a proposal to amend Section 4.10.1.4 "Industrial District" of the Gateway Specific Plan, relating to industrial uses; and

WHEREAS, the Gateway Specific Plan 99-01, Amendment No. 10 was duly submitted to said City's Planning Commission for decision at a public hearing for which proper notice was given; and

WHEREAS, the Gateway Specific Plan 99-01, Amendment No. 10 was scheduled for public hearing on April 12, 2023, on or about 7 p.m. in the Council Chambers at 2820 Clark Avenue, Norco, California 92860; and

WHEREAS, at the time set, the Planning Commission held a public hearing and received both oral and written testimony pertaining to the Specific Plan Amendment; and

WHEREAS, the Planning Commission adopted Resolution No. 2023-07 recommending that the City Council adopt Gateway Specific Plan 99-01, Amendment No. 10; and

WHEREAS, Gateway Specific Plan 99-01, Amendment No. 10 was duly submitted to said City's City Council for decision at a public hearing for which proper notice was given; and

WHEREAS, the Gateway Specific Plan 99-01, Amendment No. 10 was scheduled for public hearing on May 3, 2023, on or about 7 p.m. in the Council Chambers at 2820 Clark Avenue, Norco, California 92860; and

WHEREAS, at the time set, the City Council held a public hearing and received both oral and written testimony pertaining to the Gateway Specific Plan 99-01, Amendment No. 10; and

WHEREAS, the City of Norco acting as the Lead Agency has determined that this project is exempt from review under the California Environmental Quality Act (CEQA). Pursuant to CEQA Guidelines (California Code of Regulations, §§ 15000 et seq.) Section 15061 (b) (3), it can be seen with certainty that there is no possibility that the adoption and implementation of the subject amendments will have a significant effect on the environment.

NOW, THEREFORE, the Planning Commission of the City of Norco, California, does hereby recommend to the City's City Council the following FINDINGS AND DETERMINATIONS:

I. FINDINGS.

- A. The proposed Specific Plan Amendment is necessary as it addresses the findings in Ordinance 1086 and 1087 that enacted a moratorium and an extension to that moratorium respectively, on the approval of new entitlement applications for the construction of industrial and warehouse buildings in the City.
- B. The proposed Specific Plan Amendment is necessary to ensure and maintain orderly development of land uses consistent with the objectives, policies, general land uses, programs, and actions of the City of Norco General Plan. None of the amendments conflict with current General Plan policies, objectives, or programs of the General Plan.
- C. The proposed Specific Plan Amendment will not be detrimental to the public convenience, health, safety, or general welfare of the City because the amendments will increase compatibility of industrial and warehouse uses with surrounding properties.
- D. This project is exempt from review under the California Environmental Quality Act (CEQA) pursuant to CEQA Guidelines Section 15061(b)(3), which stipulates that CEQA only applies to projects which have the potential for causing a significant effect on the environment. Where it can be seen with certainty that there is no possibility that the activity in question may have a significant effect on the environment, the activity is not subject to CEQA. Based on the evidence presented to the City Council on May 3, 2023, on the subject project, the proposed Specific Plan Amendment updates permitted uses, development standards, architectural standards, and the list of permitted uses within areas of the city. Each new development within all affected areas of the said Specific Plan Amendment will continue to be reviewed under CEQA at the time of project submittal. Therefore, the proposed changes in regulation have no potential to have a significant effect on the environment, and are therefore not subject to CEQA, under the commonsense exemption.

II. DETERMINATION:

NOW THEREFORE, the City Council of the City of Norco, California, does hereby approve as follows:

SECTION 1:

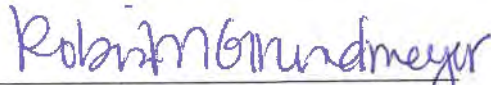
Section 4.10.1.4 "Industrial District" of the Gateway Specific Plan is hereby amended in its entirety to read as written in Attachment "A".

SECTION 2. EFFECTIVE DATE: This Ordinance shall become effective 30 days after final passage thereof.

SECTION 3. SEVERABILITY: If any section, subsection, sentence, clause, or phrase of this Ordinance is for any reason held to be invalid or unconstitutional by the decision of any court of competent jurisdiction, such decision shall not affect the validity of the remaining portions of the Ordinance, and each section, subsection, sentence, clause, sentences, clauses, or phrases here of irrespective of the fact that any one or more of the sections, subsections, sentences, clauses, or phrases hereof be declared invalid or unconstitutional.


SECTION 4. POSTING: The Mayor shall sign this Ordinance and the City clerk shall attest hereto and shall cause the same within 15 days of its passage to be posted at no less than five public places within the City of Norco.

PASSED AND ADOPTED by the City Council of the City of Norco, California at a regular meeting held June 7, 2023.



Robin Grundmeyer, Mayor
City of Norco, California


ATTEST:


for Dana Roa, CMC, City Clerk
City of Norco, California

I, Dana Roa, CMC, City Clerk of the City of Norco, California, do hereby certify that the foregoing Ordinance was introduced at a regular meeting of the City Council of the City of Norco, California, duly held on May 3, 2023, and thereafter at a regular meeting of said City Council duly held on June 7, 2023, it was duly passed and adopted by the following roll call vote of the City Council:

AYES:	GRUNDMEYER, BASH, ALEMAN, HANNA, NEWTON
NOES:	NONE
ABSENT:	NONE
ABSTAIN:	NONE

IN WITNESS THEREOF, I have hereunto set my hand and affixed the official seal of the City of Norco, California, on June 7, 2023

for  *Deputy*
Dana Roa, CMC, City Clerk
City of Norco, California

Attachment A: Gateway Specific Plan, Industrial District (4.10.1.4)

GATEWAY SPECIFIC PLAN

4.10.1.4 Industrial District Pages 102-108

20.100.001 4.10.1.4 Industrial District

a. Intent and Purpose

The purpose of the Industrial District is to provide for the orderly development of light industrial uses that also generate employment growth in manufacturing and scientific and technical services in areas of the City where the impacts to sensitive uses such as residential uses, educational uses, and public parks and open spaces uses will be minimized. Uses permitted in the District include manufacturing, research laboratories, and small warehouses. The development standards established for this District are intended to protect the public health, safety, and welfare by providing standards that control the site design, building design, and size of new and existing allowed uses.

b. Applicability

The provisions of this chapter shall apply to all existing and new development in the Industrial District as well as the redevelopment or reuse of existing warehouse buildings as follows:

1) New Development

The development standards in this chapter shall apply to all existing and new development permitted in Table 1: Allowed Uses.

2) Redevelopment and Reuse of Existing Warehouses

All existing warehouse buildings larger than 50,000 square feet in gross floor area that are proposed for redevelopment or reuse that meet any of the following conditions shall comply with the development standards in this chapter:

- a) The redevelopment or reuse of the existing warehouse building will result in 50 percent or more of the assessed value of the building being replaced, removed, or destroyed;
- b) The redevelopment or reuse of the existing warehouse building will result in an alteration resulting in a 10 percent or more decrease in gross floor area or in the number of parking spaces required;

Attachment "A"

- c) The redevelopment or reuse of the existing warehouse building will result in a change of use that requires a conditional use permit; or
- d) The existing warehouse building and the property on which it is located has been vacant or unused for at least one year.
- e) No expansion or gross floor area of an existing warehouse building larger than 50,000 square feet in gross floor area is permitted.

c. Allowed Uses

- 1) Uses allowed in the Industrial District are listed in Table 1: Allowed Uses. These uses are allowed if they comply with the applicable development standards established in this Chapter. The uses listed consist of permitted uses (P) and conditionally permitted uses (CUP), i.e., those permitted with a conditional use permit.
- 2) The requirements for a conditional use permit are established in Chapter 18.45 of the Municipal Code, Conditional Use Permits.

Table 1: Allowed Uses

Permitted Use	I District
Animal Services	
Kennel, Animal Boarding	P
Veterinary Hospitals	P
Industrial, Manufacturing, Processing and Wholesaling	
Industrial Light, General	P
Manufacturing and Processing, Heavy	CUP
Manufacturing and Processing, Light	P
Manufacturing and Processing – Previously Prepared Materials	P
Mini-Storage Warehousing (Existing)	CUP
Outdoor Storage and Display (Accessory Use Only)	P
Research, Development and Testing Facility	P
Warehousing (only allowed in buildings less than 50,000 square feet in gross floor area)	P

Gateway Specific Plan: 4.10.1.4 Industrial District

Wholesaling and Distribution (only allowed in buildings less than 50,000 square feet in gross floor area)	P
Mixed Use	
Mixed-Use Centers (Light Industrial, Office, Commercial) (Limited to 25% Commercial Uses)	P
Mixed-Use Development (Light Industrial, Office, Commercial) Uses	P
Public Services	
Public Services: Emergency Services	P
Public Services: Minor	P
Recreation, Education and Assembly	
Indoor Commercial Recreation	P
Trade School	P
Residential	
Residence for Owner, Caretaker or Manager	CUP
Retail Trade	
General Retail Business	P
Restaurant or Café/Coffee Shop	P
Services	
Emergency Shelter	P
General Services	P
Office	P
Stables/Riding Academies	CUP
Urban Agriculture	
Food Production	P
Nurseries	P

d. Limitations on Allowed Uses

All uses allowed in the Industrial District shall be subject to the following conditions and limitations:

- 1) Noise, dust, mud, odor, smoke, vibrations or other similar discharges from operations and uses conducted on the premises shall be minimized; and

- 2) All uses in the Industrial District shall be conducted completely within a fully enclosed building except:
 - a) Recreational facilities customarily conducted in the open;
 - b) Exterior storage provided that is completely surrounded by a masonry wall at least seven feet in height;
 - c) Outdoor dining areas and employee parking; and,
 - d) Surface parking lots.
- 3) No automobile dismantling businesses are allowed in this district.
- 4) Small professional offices up to 15,000 square feet in gross floor area are allowed as the principal permitted use except that, in conjunction with an allowed manufacturing, light industrial, light assembly, processing, or mixed use, the gross floor area may be greater, as determined by the Director of Community Development or designee.

e. Allowed Accessory Uses

The following uses are permitted in the Industrial District when developed and used in conjunction with one or more of the principal allowed uses.

- 1) Employee recreation facilities and play areas.
- 2) Wholesale sales and distribution, only allowed in buildings less than 50,000 square feet in gross floor area.
- 3) Surface parking lots.
- 4) Open storage incidental to a principal use provided the storage is screened from public view by a solid masonry or stucco stud wall of one color and not less than seven feet in height. Stored materials shall not be stacked above the height of the wall and shall not be located within the required parking area.

f. Development Standards

General site development standards for the Industrial District are listed in Table 2: Development Standards. Any “integrated” industrial center(s) shall be exempt from lot size and frontage requirements and shall be subject to the discretionary actions of the Planning Commission.

Table 2: Development Standards

Gateway Specific Plan: 4.10.1.4 Industrial District

Standard	M-1 Zone
Lot Requirement Standards	
Area (min.)	43,560 sq ft (1.0 acre) ¹
Width (min.)	125 ft
Depth (min.)	250 ft
Building Placement Standards	
Setback	
Front Yard (min.)	25 ft ^{2,3}
<i>When abutting Second Street between Mountain Avenue and Pacific Avenue</i>	25 ft ^{2,3}
Side Yard (min.) None required except under the following conditions:	
<i>When abutting First Street</i>	25 ft ^{2,3,4}
<i>When abutting Second Street between Mountain Avenue and Pacific Avenue</i>	25 ft ^{2,3,4}
<i>When abutting Pacific Avenue</i>	50 ft ^{2,3,4}
<i>When abutting any other dedicated street</i>	25 ft ^{2,3,4}
<i>When abutting a different district</i>	15 ft ^{2,3,4,5}
<i>When abutting a residential district</i>	50 ft, with 15 ft landscape area ^{2,3,5}
Rear Yard (min.) None required except under the following condition:	
<i>When abutting a different district or adjacent to a dedicated street</i>	15 ft ^{2,4,5}
<i>When abutting a residential district</i>	50 ft, with 15 ft landscape area ^{2,3,5}
<i>When abutting Pacific Avenue</i>	50 ft, with 10 ft landscape area ^{2,3,5}
Building Form Standards	
Building Height (max.)	25 ft ⁶
<i>When utilizing a parapet wall to screen rooftop equipment</i>	30 ft
<i>When located within 75 feet of Pacific Avenue and Second Street rights-of-way</i>	20 ft or one story
Building Floor Area (max.)	20,000 sq. ft. ⁷
Coverage (max.)	N/A
Gross FAR (max.)	N/A
Other Requirements	
Yard Area: Where two or more legally established building sites at the time of adoption of these regulations are combined into one site, the minimum yard area for the new site shall be the aggregate of yard areas that would have been required for each of the original sites under these regulations.	
Landscaping	Section 18.24.10.D and 4.10.1.4.j
Off-Street Parking and Loading	Chapter 18.38

Gateway Specific Plan: 4.10.1.4 Industrial District

Signs	Chapter 18.37
Wall, Fences, and Structures in the Setback Area	Subsection 18.24.36

End Notes

- ¹ The specified minimum lot area is not intended to prohibit two or more separate uses on a lot where the lot is in undivided ownership.
- ² All setback areas shall be landscaped in compliance with the Norco Municipal Code Section 18.24.10.D, Landscaping, Screening, and Trees, and Section 4.10.1.4.), Landscape Requirements.
- ³ No vehicle parking is allowed in required setback areas.
- ⁴ Where the property abuts any sensitive uses or an Agricultural or Residential Zone, a 50-foot building setback is required which may be used for off-street parking or screened outdoor storage.
- ⁵ A 7-foot high masonry wall is required within the setback area.
- ⁶ Building height up to 40 feet (inclusive of a parapet wall to screen rooftop equipment) is subject to approval of a conditional use permit.
- ⁷ Any building greater than 20,000 sq ft but less than 50,000 sq ft is subject to approval of a conditional use permit. Buildings greater than 50,000 sq ft in gross floor area are not allowed.

g. Site Design Standards

See Norco Municipal Code, Section 18.24.10, Site Design Standards.

h. Building Design Standards

See Norco Municipal Code, Section 18.24.12, Building Design Standards.

i. Standards for Trucks

See Norco Municipal Code, Section 18.24.14, Standards for Trucks.

j. Landscape Standards

In the Industrial District, the following landscaping requirements shall apply, in addition to the landscape standards provided in Norco Municipal Code Section 18.24.10.D, Landscaping, Screening, and Trees. In the event of a conflict, the most restrictive standard shall prevail:

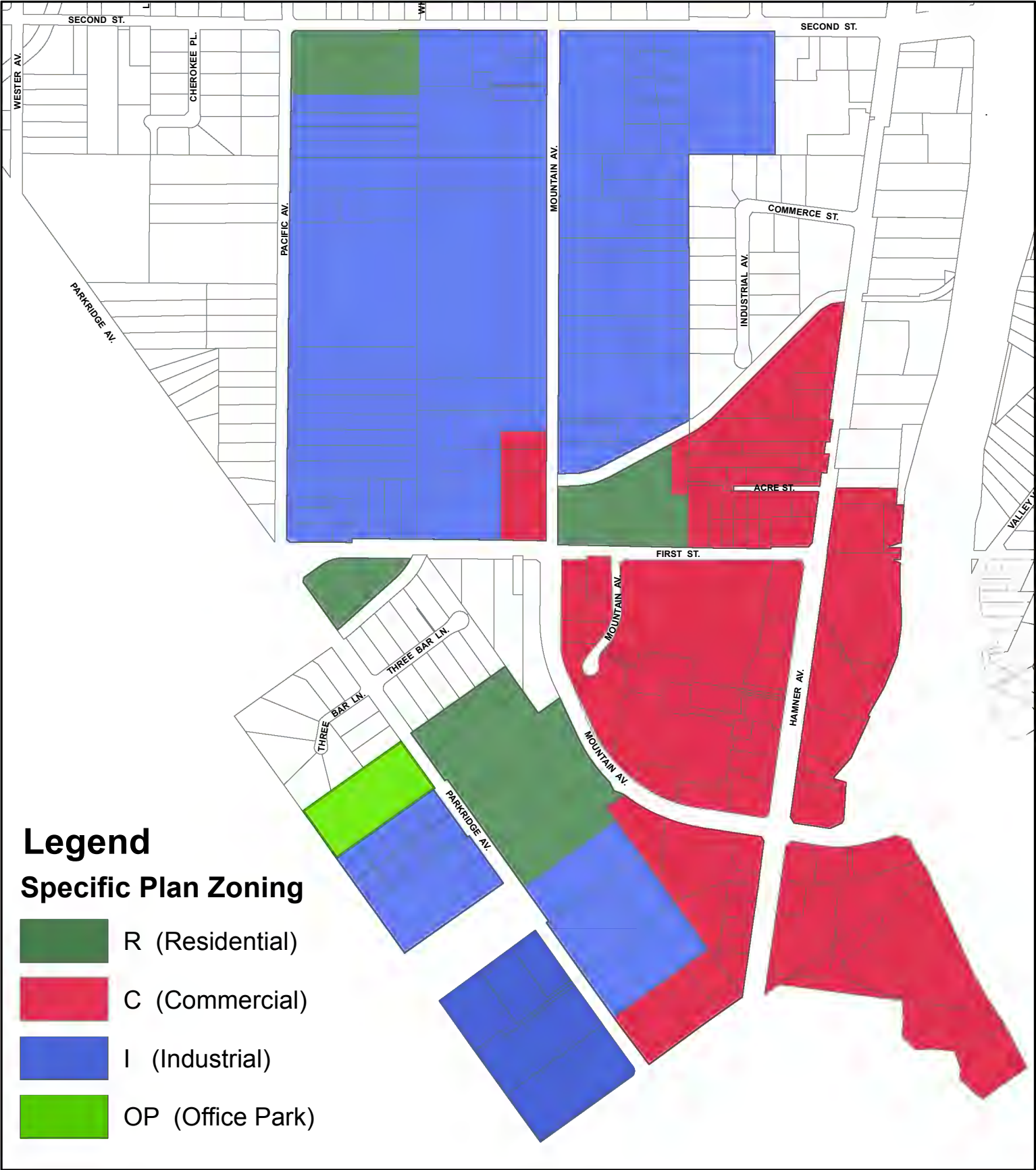
- 1) In the front yard setback, an at-grade or raised landscape planter shall be employed in front yard setbacks fronting First or Second Street, between Mountain Avenue and Pacific Avenue, where the planting areas shall consist of undulating earth berms not less than 42" in height. A permanent drought tolerant ground cover such as turf, ivy, or gazania, and trees are the basic planting materials required. At a minimum, one 15 gallon tree shall be provided for every 25 feet of lot frontage on a dedicated street. Tree groupings shall be informal. A recommended plant palette is included as Appendix B of this Plan.
- 2) Landscaping is not required in side yard setbacks unless the side yard abuts a dedicated street or abuts a different zoning district.

- 3) Landscaping for the purpose of this chapter shall consist of drought tolerant trees, shrubs, vines, annual and perennial flowers, ground cover or any combination of these landscape materials.
- 4) A minimum of 15 percent of the total of any development site shall be devoted to landscaping which includes all landscaped setback areas. One-third of the required landscaping shall be in the parking lot areas.
- 5) All landscaped areas shall be provided with permanent irrigation systems.
- 6) All landscaped areas shall be maintained in a neat, clean and healthy condition. This shall include pruning, weeding, removal of litter, fertilizing and replacement of plants when necessary.
- 7) Landscape design shall include a variety of open space areas, utilizing earth mounds of variable heights, where feasible, with a varied grouping pattern of trees, shrubs, and ground covers.
- 8) Within vehicle parking areas, standards as required in Section 4.10.1.2(e)(1) (f) shall be applicable.

k. Definitions

See Norco Municipal Code, Section 18.24.20, Definitions.

Gateway Specific Plan



Legend

Specific Plan Zoning

-  R (Residential)
-  C (Commercial)
-  I (Industrial)
-  OP (Office Park)

GATEWAY SPECIFIC PLAN

4.10.1.2 Commercial District Pages 95-98

4.10.1.2 Commercial District

The Commercial District is intended to facilitate the development of underutilized and vacant parcels of land within the Project Area in such a way that they will contribute to the economic development of the City. The Gateway Specific Plan Project Area, because of its location to major roadways, the Norco Auto Mall and the Second Street/I-15 on/off ramp, and because of the availability of significant amounts of vacant and recyclable land, will become the primary commercial area within Norco.

a. Principle Uses Permitted

See Appendix C, Permitted and Conditionally Permitted Uses.

b. Limitations on Permitted Uses

Every use permitted in the Commercial District shall be subject to the following conditions and limitations.

- 1) No operations and uses conducted on the premises shall be offensive by reason of noise, dust, mud, odor, smoke, vibrations or other similar causes; and
- 2) All uses in the Commercial District shall be conducted completely within a fully enclosed building except:
 - a. Recreational activities customarily conducted in the open;
 - b. Exterior storage with the approval of a conditional use permit;
 - c. Outdoor dining areas; and
 - d. Surface parking lots.

- 3) All uses in the Commercial District shall comply with all other applicable City, County, State and Federal laws and regulations.
- 4) No automobile dismantling businesses are allowed in this district except as required for the permitted repair of automobiles within specific zones.
- 5) Small auto service and repair uses with up to four (4) service bays are permitted within the auto related commercial designation. Small auto service and repair uses with up to two (2) service bays are permitted within the Highway Oriented Commercial designation. Projects proposing a greater number of bays shall be referred to the Planning Commission for consideration in the form of a conditional use permit.

c. Permitted Accessory Uses

The following uses are permitted in the Commercial District when developed and used in conjunction with one or more of the principal permitted uses.

- 1) Employee recreation facilities and play areas;
- 2) Retail sales and services;
- 3) Permitted wholesale sales and services ("willcall" types of business)
- 4) Surface parking lots.

d. Site Development Standards

Any "integrated" commercial center(s) shall be exempt from lot size and frontage requirements; said centers shall be subject to discretionary actions of the Planning Commission.

- 1) Lot Area

Every lot in the Commercial District shall be a minimum of 13,125 square feet and have a minimum frontage on a dedicated street of 75 feet.

2) Height

No building or structure within the Commercial District shall exceed a height of 35 feet. Buildings utilizing a parapet wall in order to hide rooftop equipment or buildings incorporating rooftop architectural features shall not exceed 50 feet at the highest point. Building heights may be increased through the provision of a conditional use permit at the discretion of the City's Planning Commission.

3) Front Yard Setback Requirements

When no front yard parking is included within the site plan a 25 foot minimum, suitably landscaped setback shall be provided. When front yard parking is included within the site plan a 60 foot minimum setback shall be provided; this setback shall include a fifteen (15) foot minimum, suitably landscaped area along the lot frontage.

4) Side Yard Setback Requirements

No side yard setback is required unless the following condition exist:

- a. Such yard abuts a dedicated street, in which case a fifteen (15) foot minimum, suitably landscaped setback shall be provided. Refer to Sub-section (d)(3) (Front Yard Setback Requirements), if parking facilities are located in the side yard setback.

5) Rear Yard Setback Requirements

No rear yard setback is required, unless such rear yard abuts a dedicated street in which case a fifteen (15) foot minimum, suitably landscaped setback is required.

e. Landscape Requirements

1) In the Commercial District, landscape requirements shall be as follows:

- a) Landscaping for the purpose of this chapter shall consist of trees, shrubs, vines, annual and perennial flowers, ground coverings or any combination thereof. Drought tolerant

species shall be used whenever feasible. A recommended plant palette is included in Appendix B of this Plan.

- b) A minimum of 15 percent of the total of any development site shall be devoted to landscaping which includes all landscaped setbacks. One-third of the required landscaping shall be within parking lot areas.
- c) All landscaped areas shall be provided with permanent watering facilities.
- d) All landscaped areas shall be maintained in a neat, clean and healthy condition. This shall include proper pruning, weeding, removal of litter, fertilizing and replacement of plants when necessary.
- e) Landscape design shall include a variety of open space areas, utilizing earth mounds of variable heights where feasible with a variegated grouping pattern of trees, shrubs and groundcover.
- f) Within vehicle parking and outdoor display areas the design guidelines shown below are recommended. However, these guidelines are not intended to prohibit flexibility in creating various shapes (circular, triangular) or varying sizes. Reference Section 4.11, Parking Requirements, for general parking stall design provisions.

GATEWAY SPECIFIC PLAN

4.10.1.3 Office Park District Pages 99-102

4.10.1.3 Office Park District

The Office Park District is intended to serve local and sub-regional office and office park needs. This district provides for the development of garden office, single occupant office and ancillary uses.

The District is intended to accommodate professional/administrative office and personnel services. Typically, uses consist of executive, management, administrative, or clerical uses including the establishment of branch offices, data processing centers and the provision of consultation establishments of a professional nature. Additional office land uses consist of activities which cater to business support and personal services. Uses typically include medical and health care clinics, travel agencies, insurance agencies, copy centers, and other like land uses.

a. Principal Uses Permitted

See Appendix C, Permitted and Conditionally Permitted Uses.

b. Limitation on Permitted Uses

Every use permitted in the Office Park District shall be subject to the following conditions and limitations:

1. All uses in the Office Park District shall be conducted completely within a fully enclosed building except:
 - a. Recreational facilities customarily conducted in the open;
 - b. Exterior storage provided that it is completely surrounded by a masonry wall at least seven feet in height;
 - c. Outdoor dining areas and employee parking; and,
 - d. Surface parking lots.
2. All uses in the Office Park District shall comply with all other applicable City, County, State and Federal laws and regulations.

c. Permitted Accessory Uses

The following uses are permitted in the Office Park District when developed and used in conjunction with one or more of the principal permitted uses.

1. Surface parking lots
2. Eating establishments; no bars or lounges are permitted

d. Site Development Standards

Any "integrated" office park(s) shall be exempt from lot size and frontage requirements; said office parks shall be subject to discretionary actions of the Planning Commission.

1. Lot area

Every lot in the Office Park District shall be a minimum of 13,125 square feet. Minimum width shall be 75 feet; minimum depth shall be 175.

2. Building Height

No building or structure within the Office Park District shall exceed a height of two stories or 35 feet. Buildings utilizing a parapet wall in order to hide rooftop equipment, or buildings incorporating rooftop architectural features shall not

exceed 50 feet at the highest point. The following exception is applicable to that area designated as Office Park located northeast of Parkridge Avenue and southeast of the Three Bar Lane residential area:

The building height limitation shall be one (1) story or twenty (20) feet.

3. **Front Yard Setback Requirements**

In the Office Park District the front yard setback shall not be less than fifteen (15) feet from the Parkridge Avenue right-of-way; this setback shall be fully landscaped.

The front yard setback shall not be less than twelve (12) feet from the right-of-way of all other streets; this area shall be fully landscaped.

4. **Side Yard and Rear Yard Setback Requirements**

There shall be no requirement, except where any rear yard is provided, there shall also be provided a side yard of twelve (12) feet on at least one side of the lot, or a recorded vehicular access easement from a public street to said rear yard.

e. **Landscape Requirements**

1. In the Office Park District, landscaping requirements shall be as follows:

- a. Landscaping for the purpose of this section shall consist of trees, shrubs, vines, annual and perennial flowers, ground coverings or any combination thereof. Drought tolerant species shall be used whenever feasible. A recommended plant palette is included in Appendix B of this Plan.
- b. A minimum of 15 percent of the total of any development site shall be devoted to landscaping which includes all landscaped setback areas. One-third of the required landscaping shall be within parking lot areas.
- c. All landscaped areas shall be provided with permanent watering facilities.
- d. All landscaped areas shall be maintained in a neat, clean and healthy condition. This shall include proper pruning, weeding, removal of litter, fertilizing and replacement of plants when necessary.
- e. Landscape design shall include a variety of open space areas, utilizing earth mounds of variable heights were feasible with a variegated grouping pattern of trees, shrubs and groundcover.

- f. Within vehicle parking areas, standards as required in Section 4.10.1.2 (e)(1)(f) shall be applicable.

GATEWAY SPECIFIC PLAN

4.10.1.4 Industrial District Pages 102-105

4.10.1.4 Industrial District

The Industrial District is intended to facilitate the economic development of the City by creating an expanded employment base. This District provides for the development of light industrial land uses which generally includes research & development facilities, light manufacturing activities, custom manufacturing, assembly, fabrication and wholesaling with related office and administrative functions.

a. Principal Uses Permitted

See Appendix C, Permitted and Conditionally Permitted Uses.

b. Limitations on Permitted Uses

Every use permitted in the Industrial District shall be subject to the following conditions and limitations:

- 1) No operations and uses conducted on the premises shall be objectionable by reason of noise, dust, mud, odor, smoke, vibrations or other similar causes; and
- 2) All uses in the Industrial District shall be conducted completely within a fully enclosed building except:
 - a) Recreational facilities customarily conducted in the open;
 - b) Exterior storage provided that is is completely surrounded by a masonry wall at least seven feet in height;
 - c) Outdoor dining areas and employee parking; and,
 - d) Surface parking lots.
- 3) No automobile dismantling businesses are allowed in this district.
- 4) Small professional offices up to 15,000 square feet in floor area may be the principal permitted use except that, in conjunction with a permitted manufacturing, light industrial, warehousing, distribution, light assembly, processing or mixed use the size may be greater.

c. Permitted Accessory Uses

The following uses are permitted in the Industrial District when developed and used in conjunction with one or more of the principal permitted uses.

- 1) Employee recreation facilities and play areas.
- 2) Wholesale sales and distribution.
- 3) Surface parking lots.
- 4) Open storage incidental to a principal use provided the storage is screened from public view by a solid masonry or stucco stud wall of one color to be not less than seven feet in height. Such storage shall not be stacked above the height of the wall and shall not be located within the required parking area.

d. Site Development Standards

Any "integrated" industrial center(s) shall be exempt from lot size and frontage requirements; said centers shall be subject to the discretionary actions of the Planning Commission.

1) Lot Area

Every lot in the Industrial District shall be a minimum of 43,560 square feet (1.0 acre). Minimum width shall be 125 feet; minimum depth shall be 250 feet.

2) Building Height

No building or structure within the Industrial District shall exceed a height of 35 feet, however that building utilizing a parapet wall in order to hide rooftop equipment shall not exceed 40 feet. For buildings located within 75 feet of Pacific Avenue right-of-way, a one (1) story or twenty (20) feet building height limitation shall apply.

3) Front Yard Setback Requirements

In the Industrial District the front yard setback shall not be less than fifteen (15) feet from the street right-of-way. These setbacks shall be fully landscaped and maintained in accordance with sub-section (e) Landscape Requirements. No parking shall be allowed in this setback area. The following exception is applicable:

- a. All front yard setbacks abutting Second Street between Mountain Avenue and Pacific Avenue, shall not be less than 25 feet. No parking shall be allowed in this setback area. These setbacks shall be fully landscaped and maintained in accordance with sub-section (e), (Landscape Requirements).

4) Side Yard Setback Requirements

No side yard setback is required unless one of the following conditions exist, in which case the stated setback requirement shall be applicable:

- a) Such side yard abuts one of the following dedicated roadways:

<u>Street</u>	<u>Setback</u>
Pacific Avenue	Fifty (50) feet minimum
Second Street, between Mountain and Pacific Avenues	Twenty (20) feet minimum (fully landscaped)
First Street	Twenty (20) feet minimum (fully landscaped)

- b) Such side yard abuts any other dedicated street in which case a ten (10) foot suitably landscaped setback is required.
- c) Where the abutting property on the side yard is in another district in which case a minimum fifteen (15) foot suitably landscaped setback is required. Landscaped setback shall include a minimum seven (7) foot high masonry wall; location of this wall shall be approved by the City Planning Department. No parking shall be allowed in this setback.

5) Rear Yard Setback Requirements

No rear yard setback is required, unless the abutting property is in a different district or adjacent to a dedicated street in which case an minimum fifteen (15) foot suitably landscaped setback is required. Landscaped setback shall include a seven (7) foot high masonry wall; location of this wall shall be approved by the City Planning Department. For property abutting Pacific Avenue, a fifty (50) foot setback is required with at least five (5) feet of landscaping adjacent to the right-of-way.

6) Yard Area

Where two or more legally established building sites at the time of adoption of these regulations are combined into one site, the minimum yard area for the new

site shall be the aggregate of yard areas that would have been required for each of the original sites under these regulations.

g. Landscape Requirements

- 1) In the Industrial District, landscaping requirements shall be as follows:
 - a) In the front yard setback, an at-grade or raised landscape planter shall be employed except in front yard setbacks fronting First or Second Street, between Mountain Avenue and Pacific Avenue, where the planting areas shall consist of undulating earth berms not less than 42" in height. A permanent drought tolerant ground cover such as turf, ivy or gazania, and trees are the basic planting materials recommended. At a minimum, one (1) 15 gallon tree shall be provided for every 25 feet of lot frontage on a dedicated street. Tree groupings shall be informal. A recommended plant palette is included as Appendix B of this Plan.
 - b) Landscaping is not required in side yard setbacks unless the side yard abuts a dedicated street or abuts a different zoning district.
 - c) Landscaping for the purpose of this chapter shall consist of trees, shrubs, vines, annual and perennial flowers, ground cover or any combination thereof. Drought tolerant species shall be used whenever feasible.
 - d) A minimum of 15 percent of the total of any development site shall be devoted to landscaping which includes all landscaped setback areas. One-third of the required landscaping shall be in the parking lot areas.
 - e) All landscaped areas shall be provided with permanent watering facilities.
 - f) All landscaped areas shall be maintained in a neat, clean and healthy condition. This shall include proper pruning, weeding, removal of litter, fertilizing and replacement of plants when necessary.
 - g) Landscape design shall include a variety of open space areas, utilizing earth mounds of variable heights where feasible with a variegated grouping pattern of trees, shrubs and ground cover.
 - h) Within vehicle parking areas, standards as required in Section 4.10.1.2 (e)(1) (f) shall be applicable.

APPENDIX C

SUMMARY OF PERMITTED (X) AND CONDITIONALLY PERMITTED (O) USES

DISTRICTS

	COMMERCIAL OFFICE	INDUSTRIAL
**** COMMERCIAL RETAIL USES ****		
Antique Shops	X	
Apparel:		
a) Boutique	X	
b) General	X	
Appliance Stores and Repair	X	
Art, Music, and Photographic Studios and Supply Stores	X	X
Auto Service Stations	O	O
Auto Service (including motorcycles, boats, trailers, campers):		
a) Sales (with ancillary repair facilities)	O	
b) Rentals	O	
c) Automobile Repair and Service	O	
d) Coin-op Washing	O	
e) Automatic Washing	O	
f) Parts and Supplies	O	
Bakeries (retail only)	X	X
Barber and Beauty Shops	X	X
Bed & Breakfast	O	O
Bicycle Shops	X	
Blueprint and Photocopy Services	X	X
Book, Gift and Stationery Shops (other than adult related material)	X	X

SUMMARY OF PERMITTED (X) AND CONDITIONALLY PERMITTED (O) USES

DISTRICTS

	COMMERCIAL OFFICE	INDUSTRIAL
Candy and Confectionaries	X	X
Catering Establishments	X	
China and Glasaware Stores	X	
Christmas Tree Sales lot	X	
Churches	X	
Cleaning and Pressing Establishments	X	
Cocktail Lounge (bar, lounge, tavern) including related entertainment	X	
Commercial Recreation		
a) Indoor uses such as bowling and billiards	O	O
b) Outdoor uses such as tennis and basketball	O	O
Convalescent Facilities and Hospital	O	O
Curtain and Drapery Shops	X	
Day Care Centers	O	O
Delicatessens and Speciality Food Stores	X	X
DRUG Stores and Pharmacies	X	
Farmers Market	X	
Floor Covering Shop	X	
Florist Shops	X	X
Furniture Stores	X	
Hardware Stores	X	O
Health and Athletic Clubs/Gyms and Weight Reducing Clinics	X	

SUMMARY OF PERMITTED (X) AND CONDITIONALLY PERMITTED (O) USES

DISTRICTS

	COMMERCIAL OFFICE	INDUSTRIAL
Hobby Shops	X	
Ice Creams Stores and Soda Fountains	X	
Janitorial Services and Supplies	X	
Jewelry Stores	X	
Laundry (Self Service)	X	
Leather Goods and Luggage Stores	X	
Liquor Stores	X	
Messenger and Wire Service	X	
Music, Dance and Martial Arts Studios	X	X
Newspaper and Magazine Stores	X	
Nurseries & Garden Supplies Stores within enclosed areas	X	X
Office, Business Machine and Computer Component Stores	X	
Paint, Glass and Wallpaper Stores	X	
Pet Shops	X	
Photocopy	X	X
Record and Tape Stores	X	

SUMMARY OF PERMITTED (X) AND CONDITIONALLY PERMITTED (O) USES

DISTRICTS

	COMMERCIAL	OFFICE	INDUSTRIAL
Restaurants (sit down) :			
a) With entertainment and/or serving of beverages	O		O
b) Incidental serving of beer and wine (without a cocktail lounge bar, entertainment, or dancing	X		O
c) Cafe, limited to 20 seats (including outdoor seating	O		
d) Fast Food: with drive-thru	O		

Shoe Stores, Sales and Repair	X		X

Speciality Retail	O		

Sporting Goods Stores:			
a) Speciality; backpacking, tennis, skiing, mountaineering, fishing etc.	X		
b) General; Encompassing a variety of sports equipment	X		

Supermarkets	X		

Swimming Pool supplies	X		

Tailor Shops	X		X

Television, Radio, VCR, Stereo and Cd Components	X		

Toy Stores	X		

Variety Department Stores, Junior Department Stores	X		

Veterinary (domestic)			
a) Non-boarding	X		X
b) Boarding	O		O

Watch and Clock Repair Shops	X		

Yardage Goods Stores	X		

SUMMARY OF PERMITTED (X) AND CONDITIONALLY PERMITTED (O) USES

DISTRICTS

	COMMERCIAL OFFICE	INDUSTRIAL
**** WHOLESALE USES ****		
Wholesale Commercial Outlets	X	
**** ENTERTAINMENT AND CULTURAL USES ****		
Arcades	O	O
.....		
Cultural/Artist Exhibit		
a) Indoor gallery and art sales	X	O
b) Outdoor art exhibit	O	
.....		
Discotheques	O	
.....		
Theatres		
a) Dinner Theatre	O	
b) Movie Theatre	O	
**** OFFICE AND ADMINISTRATIVE USES ****		
Administrative, Business, and Professional Office	X	X
.....		
Banks, Finance Service and Institutions		
- without drive-thru	X	X
- with drive-thru	O	O
.....		
Business and Office Services	X	
.....		
Design Firms	X	X
.....		
Medical/Dental Offices and Related Health Clinic	X	X
.....		
Optician and Optometrical Shops	X	X
.....		
Realtor and Real Estate Offices	X	X
.....		
Travel Agencies	X	X
.....		

SUMMARY OF PERMITTED (X) AND CONDITIONALLY PERMITTED (O) USES

DISTRICTS

	COMMERCIAL OFFICE	INDUSTRIAL
**** PUBLIC USES ****		
Transit Facilities		X
Public Utility Installations		
**** HOTEL USES ****		
Hotel/Motel	X	
Hotel Facilities	X	
Ancillary Uses:		
a) Beauty/Barber Shop	X	
b) Cafes	X	
c) Catering Services	O	
d) Cocktail Lounge	O	
e) Conference/Convention Facilities	O	
f) Florist Shops	X	
g) Gift Shops	X	
h) Newspaper/Magazine Stores	X	
i) Pharmacies	X	
j) Restaurants (sit down)	X	
k) Tourist Information	X	
l) Travel Agencies	X	

SUMMARY OF PERMITTED (X) AND CONDITIONALLY PERMITTED (O) USES

DISTRICTS

**** Manufacturing ****	COMMERCIAL OFFICE	INDUSTRIAL
Small Professional Offices involved in industrial, related activities, eg. engineering, industrial design and consultation	X	
Warehouse/Distribution		X
Light Assembly and Processing		X
Mixed Use Projects including commercial, office and light industrial uses where the light industrial uses make-up at least 75% of the total square footage.		X
Wholesaling and Warehouse		X
Research Development and Testing Laboratories		X
Manufacturing, compounding, processing, packaging, or treatment of products such as:		
a) Bakery Goods		X
b) Candy		X
c) Cosmetics		X
d) Dairy Products		X
e) Drug Products		X
f) Food Products(excluding fish and meat products, sauerkraut, vinegar, yeast and the rendering of fats and oils)		X
g) Fruit and Vegetable packaging		X
h) Perfumes		X
i) Manufacturing/maintenance of electric/ neon signs		X
j) Toiletries		X
k) Novelties		X
l) Retail Lumber		X
m) Shoes		X
n) Stone monument works		X
Ambulance Service		O
Animal hospitals and shelters		O
Delicatessens		O
Recreation bulk station and terminal		O
Recreational club		O
Stables and riding academies		O

Gateway Specific Plan



PREPARED FOR
THE CITY OF NORCO
COMMUNITY REDEVELOPMENT AGENCY

PREPARED BY
URBAN FUTURES, INC.

THE GATEWAY SPECIFIC PLAN

Prepared for
THE CITY OF NORCO
COMMUNITY REDEVELOPMENT AGENCY

Prepared by:
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in association with:
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CM Engineering Associates
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MAY 1991

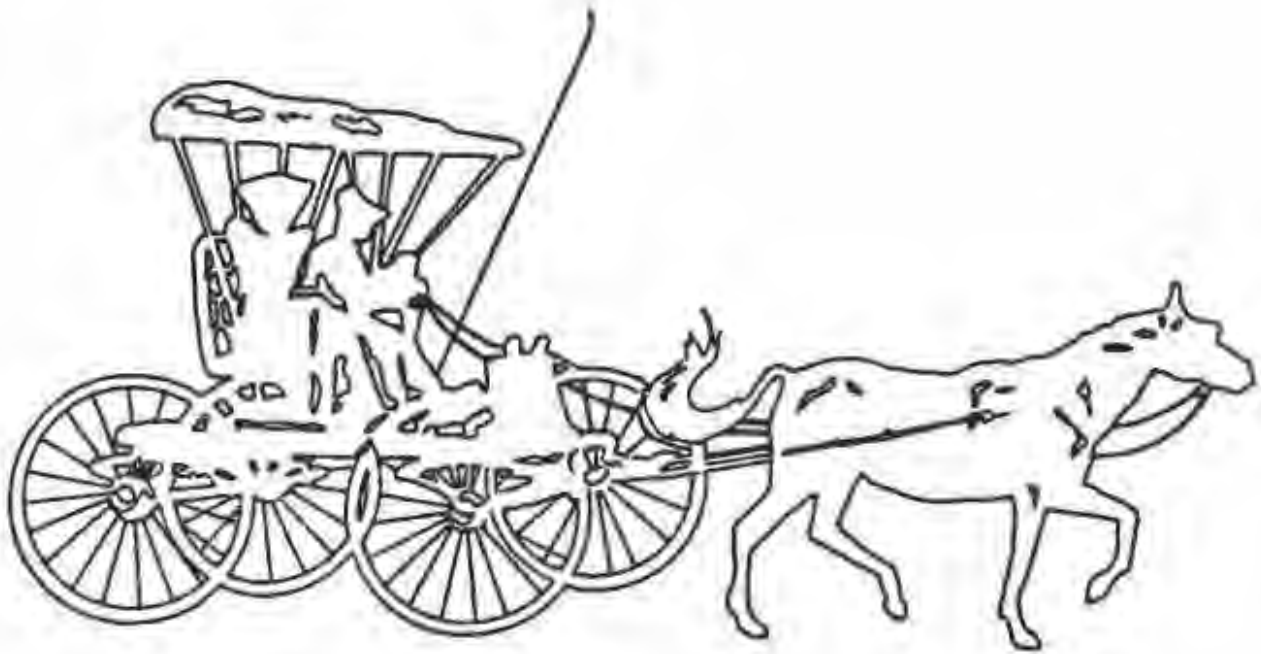


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GATEWAY SPECIFIC PLAN
CITY OF NORCO

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Terry Wright, Mayor Pro Tem
George Lambert, City Manager/Executive Director

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Robert Morgan, Commissioner
Paul Ryan, Chairperson
Douglas Crouse, Commissioner
Ronald Darwin, Vice Chairperson
Peter McMahon, Commissioner

STREETS AND TRAILS COMMISSION

James A. Lee
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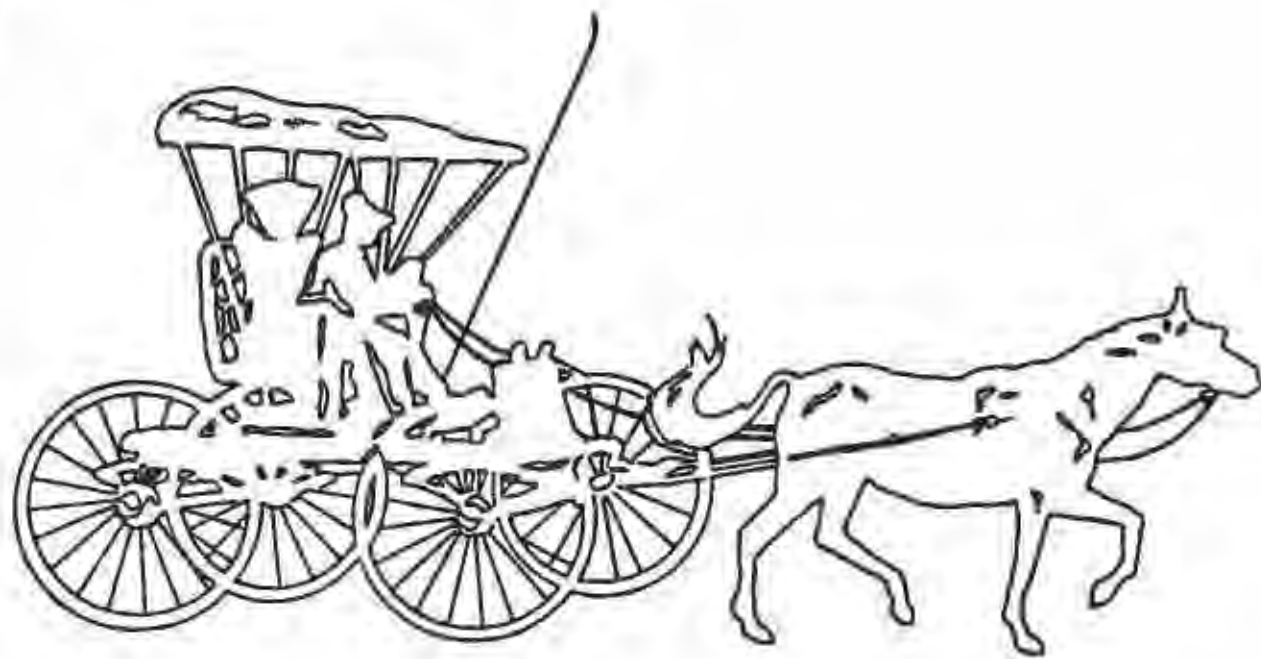
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DIGEST

GATEWAY SPECIFIC PLAN DIGEST

The City of Norco is a community committed to city living in a rural atmosphere. The City's development and design review process places heavy emphasis on an equestrian life style, quality of development, long term viability, and support of community goals in all development projects.

WHAT IS THIS PLAN ABOUT?

The purpose of this Specific Plan is to provide a balanced and unified pattern of development within the Gateway Specific Plan Project Area (the "Project Area") by taking advantage of future community and sub-regional growth opportunities. The Project Area is shown in Exhibit D1.

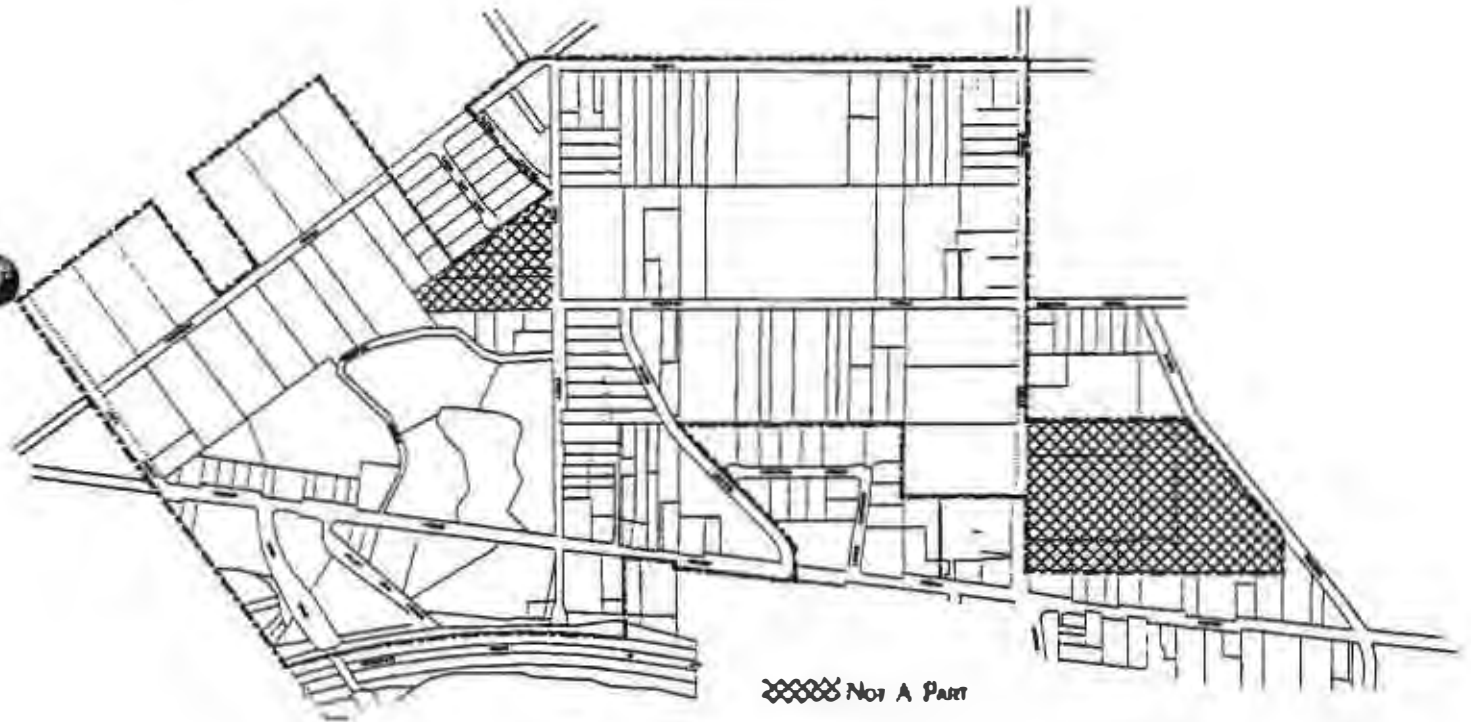


EXHIBIT D-1
Gateway Specific Plan Project Area Map



The primary purpose of the Gateway Specific Plan is to facilitate private development projects, public infrastructure and roadway improvement projects which will generate the following positive impacts within the Project Area:

- o Provide high quality development standards;
- o Help unify the Project Area's design image;
- o Reflect the community's heritage;
- o Strengthen the economic viability of the I-15 Freeway Corridor;
- o Provide a balanced mix of land uses and businesses;
- o Deal effectively with traffic and related safety issues; and
- o Provide a long range strategy to effectively mitigate existing service infrastructure/public service deficiencies.

WHY A SPECIFIC PLAN?

The Specific Plan is a tool that combines traditional zoning with detailed design and development standards tailored to specific conditions. The Specific Plan is a comprehensive document which contains all policies and development standards necessary for the design of any project within the Project Area. The Specific Plan examines the needs of all affected areas and implements the policies of the General Plan.

Development Standards unique to the Project Area are used to provide the necessary guidelines for development. The Gateway Specific Plan includes six major components:

1. Executive Summary
2. Specific Development Plan
3. Design Guidelines and Development Standards
4. Development Regulations
5. Plan Implementation
6. General Plan Consistency

The primary purpose of these components is to effectively develop within the Project Area, the following elements:

- o Community Design
- o Circulation
- o Land Use
- o Utilities Infrastructure
- o Implementation

COMMUNITY DESIGN

Architecture:

One of the more recognizable aspects of a community's design image is its architectural style. This Plan recognizes the importance of the community's heritage and highlights significant architectural elements within the Project Area.

As shown conceptually, or based upon a general idea or understanding of Project Area design principles, Exhibits D2 and D3 demonstrate that the Gateway Specific Plan calls for the architectural elements within the Project Area to be developed around a western/early Californian architectural theme. This theme will depict the life-style of the citizens of Norco and give the Project Area a unique design quality. This theme will be predominant within the Commercial and Office Park districts, however, appropriate elements will also be included within the Industrial District.



Industrial Architectural Design EXHIBIT D-2

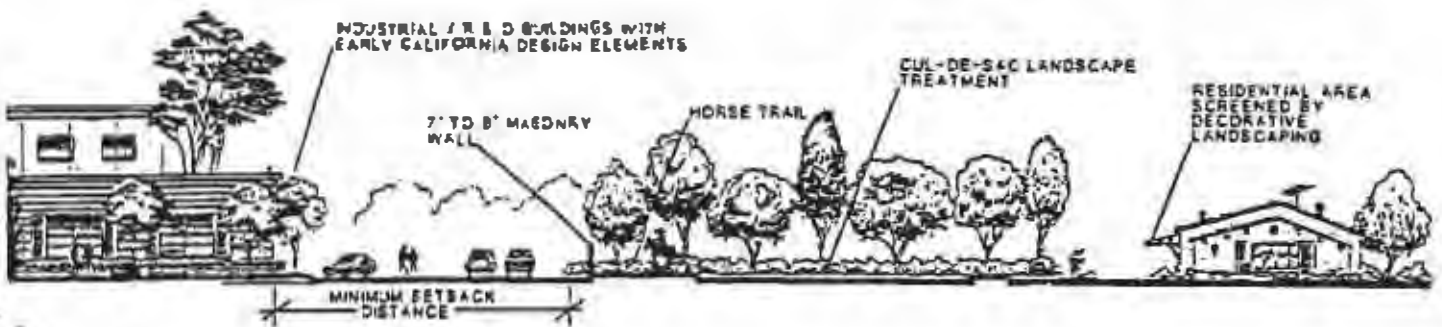


Commercial Architectural Design EXHIBIT D-3

Landscape Architecture

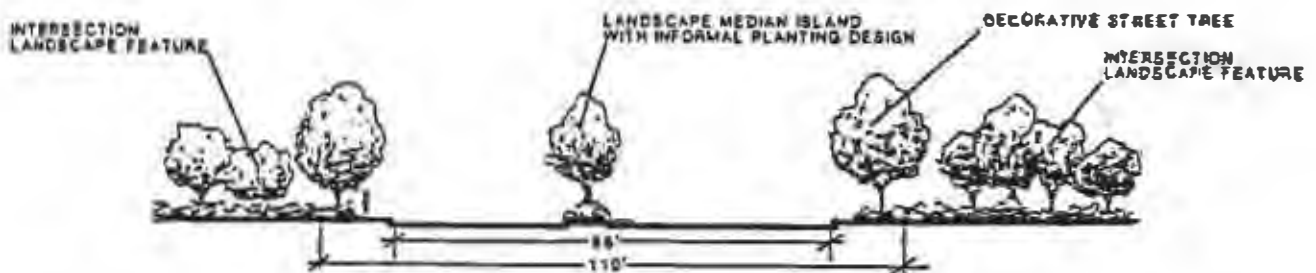
The Gateway Specific Plan Project Area will be the nucleus of the City's economic foundation. Opportunities exist to improve the visual qualities of Project Area roadways by introducing a landscape design program that would establish a system of City entries, landscaped medians, street trees and signage. The design of the streetscape will help create an aesthetically pleasing and functional Project Area. Beautification of the street system will enhance existing business opportunities and compliment new businesses to the area. Exhibits D.4 and D-5 show conceptual illustrations for rights-of-way located within the Gateway Specific Plan.

EXHIBIT D-4



Pacific Avenue Roadway Section and Landscape Buffer Detail

EXHIBIT D-5



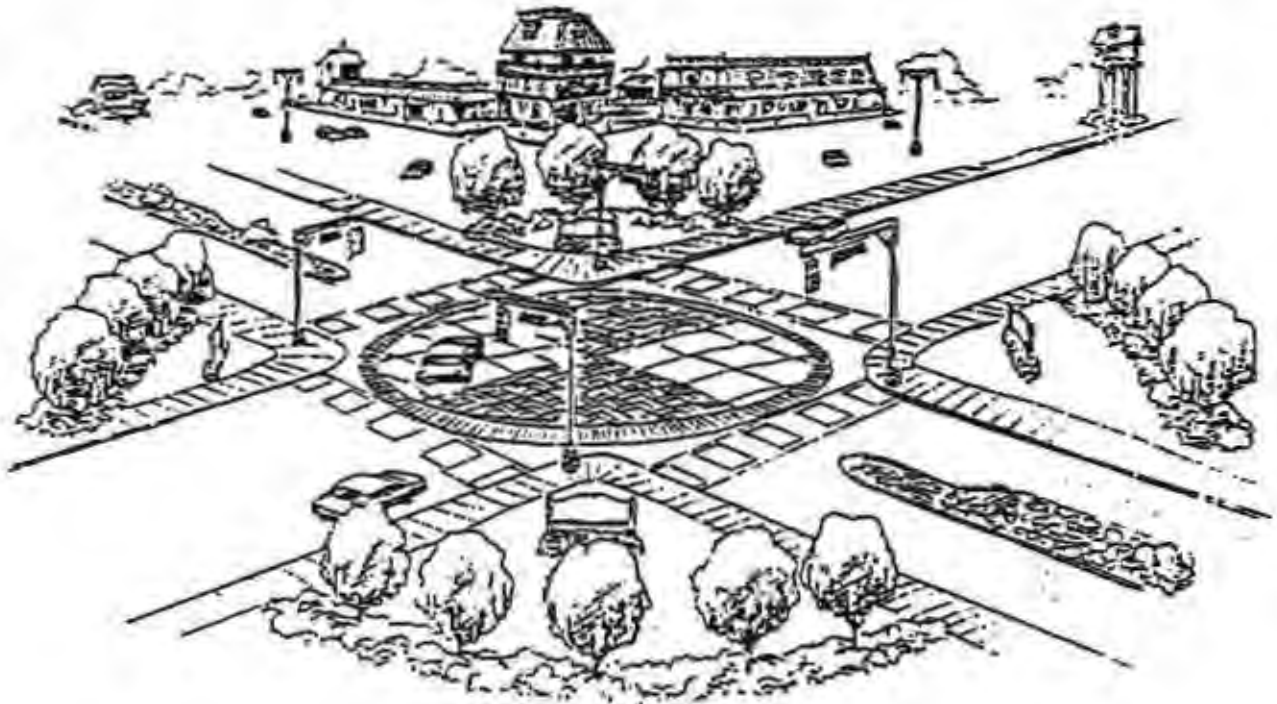
Typical 110' Right-of-Way with Median Island and Intersection Landscape Feature

CIRCULATION

A major concern of all people familiar with the Project Area is existing and future traffic/circulation problems. The traffic analysis predicts an ultimate daily traffic count of 71,010 additional two-way trips per day as a result of Plan build-out. This means that the importance of adequate traffic control measures and a well planned circulation system cannot be overemphasized.

The Gateway Specific Plan calls for the following major traffic/circulation system modifications and improvements to the Project Area:

1. Widening and realignment of Parkridge Avenue, First Street, Second Street and Mountain Avenue.
2. Cul-de-sacing of Pacific Avenue north of its intersection with First Street and Parkridge Avenue.
3. Cul-de-sacing of First Street at its eastern terminus and the cul-de-sacing of Valley View Drive.
4. Installation of landscaped medians on Hamner Avenue.
5. Yuma Drive extension to First Street and Mountain Avenue.



First Street/Mountain Avenue Intersection Concept

EXHIBIT D-6

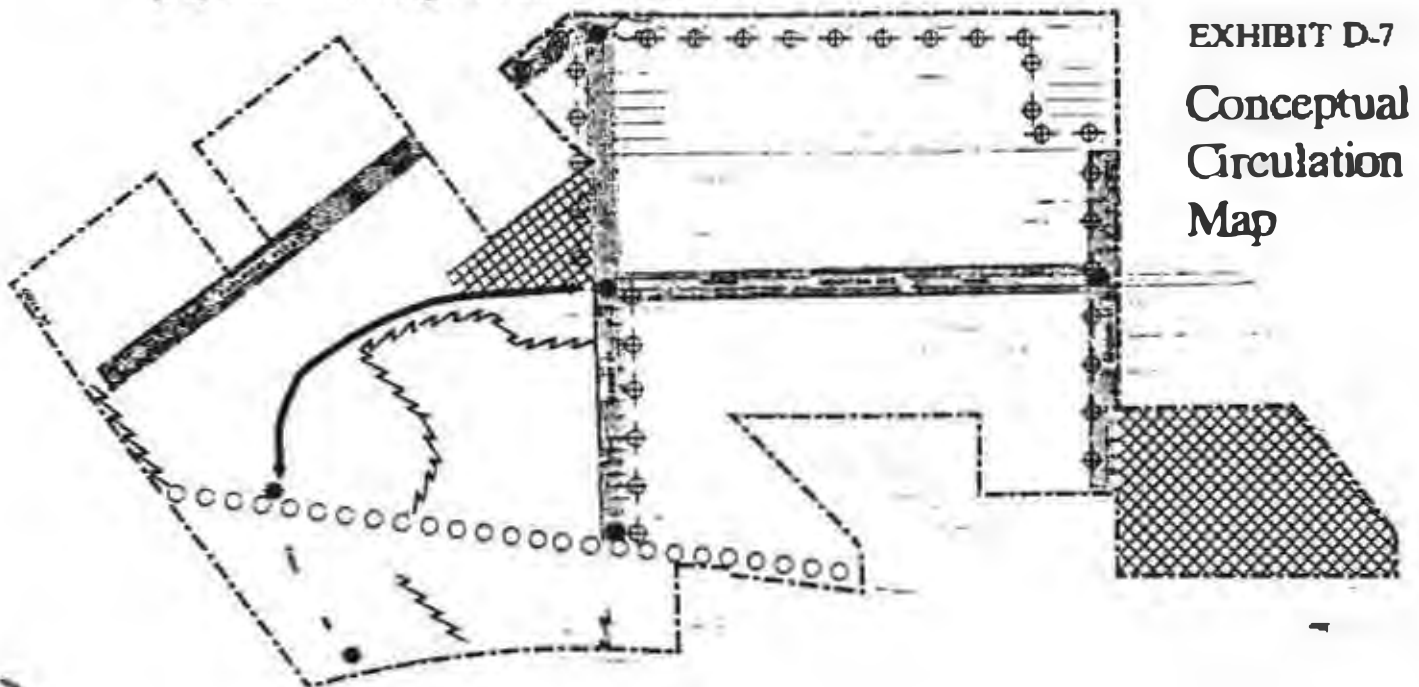
6. Provide for the extension of Mountain Avenue between First Street and the proposed extension of Yuma Drive.
7. Provide major intersection traffic control devices and landscape treatments.
8. Provide a street tree program on all Project Area roadways.
9. Provide entry monuments and signage to facilitate circulation within the Project Area.
10. Provide traffic control devices/signage and landscape treatments at all secondary intersections as required.
11. Provide for the vacation of various roadways.

Riding Trails

This Specific Plan acknowledges the importance of riding trails within the Project Area and makes provisions for trails that will meet existing City riding trail design criteria. As shown in Exhibit D-7, riding trails are designated along the following roadways:

- o Pacific Avenue
- o First Street*
- o Second Street*

* Riding trails will be maintained for as long as residential properties exist along First and Second Streets



LAND USE

Commercial District

As shown in Exhibit D2, the Project Area has been divided into four major land use Districts:

- 1) Commercial
- 2) Industrial
- 3) Office Park/Professional
- 4) Existing Residential

Each of these Districts and all subordinate designations are briefly described below.

In an effort to establish the Project Area as a viable local and sub-regional commercial area, the Plan provides for significant commercial opportunities along Hammer Avenue, First Street, Second Street and the proposed extensions of Yuma Drive and Mountain Avenue. The land use designations and permitted uses have been carefully selected to enhance the Project Area's marketability and to improve its overall economic condition.

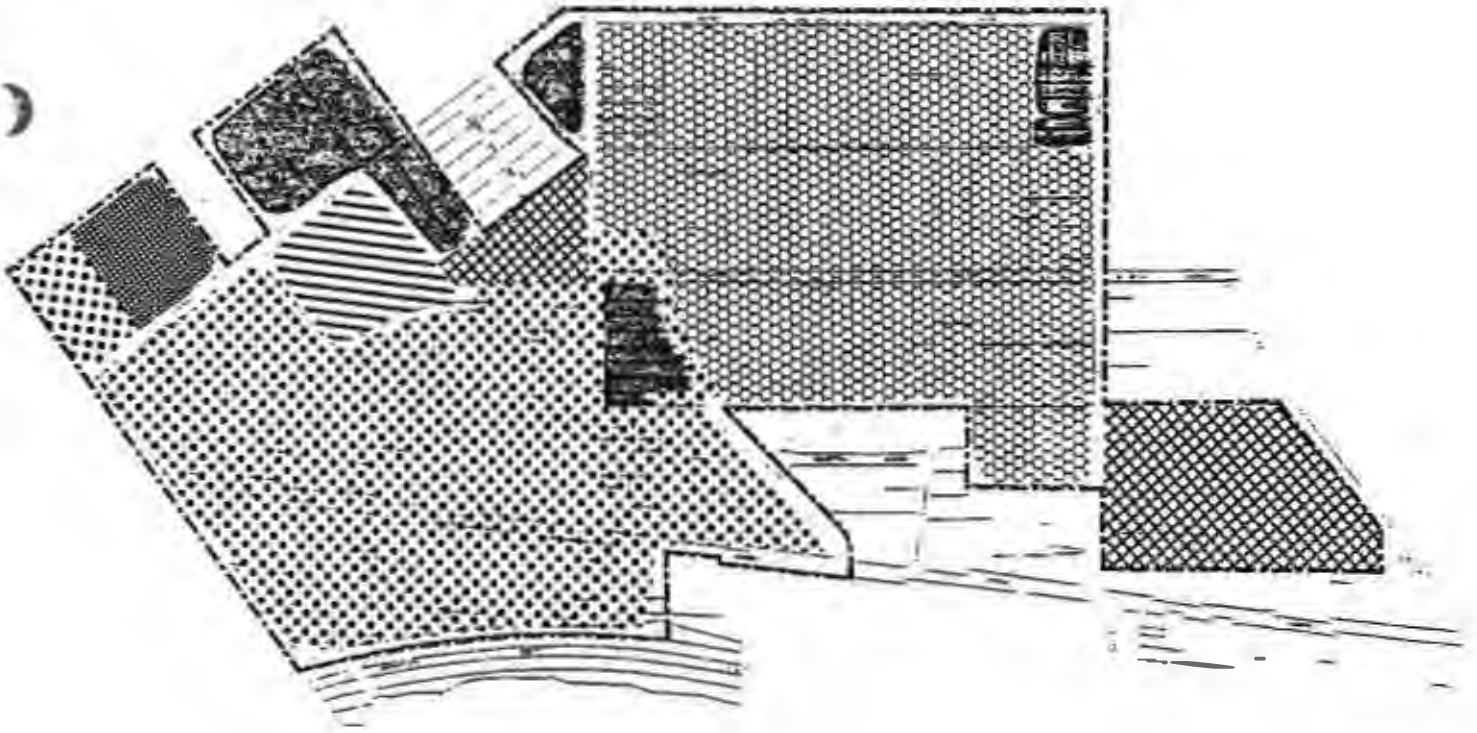


EXHIBIT D-8

Conceptual Land Use Districts

Office Park District

This land use designation will accommodate development of office buildings including single occupancy and garden office, and ancillary uses. This designation is found along Parkridge Avenue.

Industrial District

Light industrial, research and development (R&D) and ancillary facilities will be located in the Area east and west of Mountain Avenue and north of First Street. Designation of this area to accommodate industrial land uses allows maximum utilization of land area inward from the major traffic routes. Industrial uses allowed in this District will be "light" and "clean", primarily high-tech industry that generates little noise or air pollution; no heavy manufacturing will be permitted.

Residential District

This land use consists of A-1-20 and A-1-40 zoned areas which are located along Parkridge Avenue in the south-western section of the Project Area and along First and Second Streets in the western portion of the Project Area.

UTILITIES INFRASTRUCTURE

In an effort to mitigate existing infrastructure deficiencies and to prevent future deficiencies which would be exacerbated by more intensive land uses, a plan to effectively deal with the following engineering issues has been included as part of this Specific Plan:

- o Drainage/flood control
- o Lowering of Hamner Avenue
- o Water/Sewerage service system
- o Grading/topography

IMPLEMENTATION

To ensure that the Gateway Specific Plan Project Area develops successfully, the implementation section provides suggested methods of financing and phasing development of needed public improvements.

This section includes economic development strategies and a lot consolidation program which provides incentives for small lot owners to work together with the Redevelopment Agency to improve development potential in certain locations.

The Plan also recognizes the need to bring high sales volume businesses into the City, the implementation portion of the Plan highlights a Project Area Marketing Program. In addition, it is recognized that many existing businesses within the Gateway Specific Plan Project Area may need financial assistance to do their part in the Project Area's revitalization. As such, implementation of a small business assistance program is recommended.

Recognizing the need to maintain Project Area signage and landscaped areas, this Plan proposes the establishment of a sign and landscape maintenance district.

As a means of monitoring development within the Project Area, a monitoring program has been outlined for use by City/Agency staff.



GATEWAY SPECIFIC PLAN

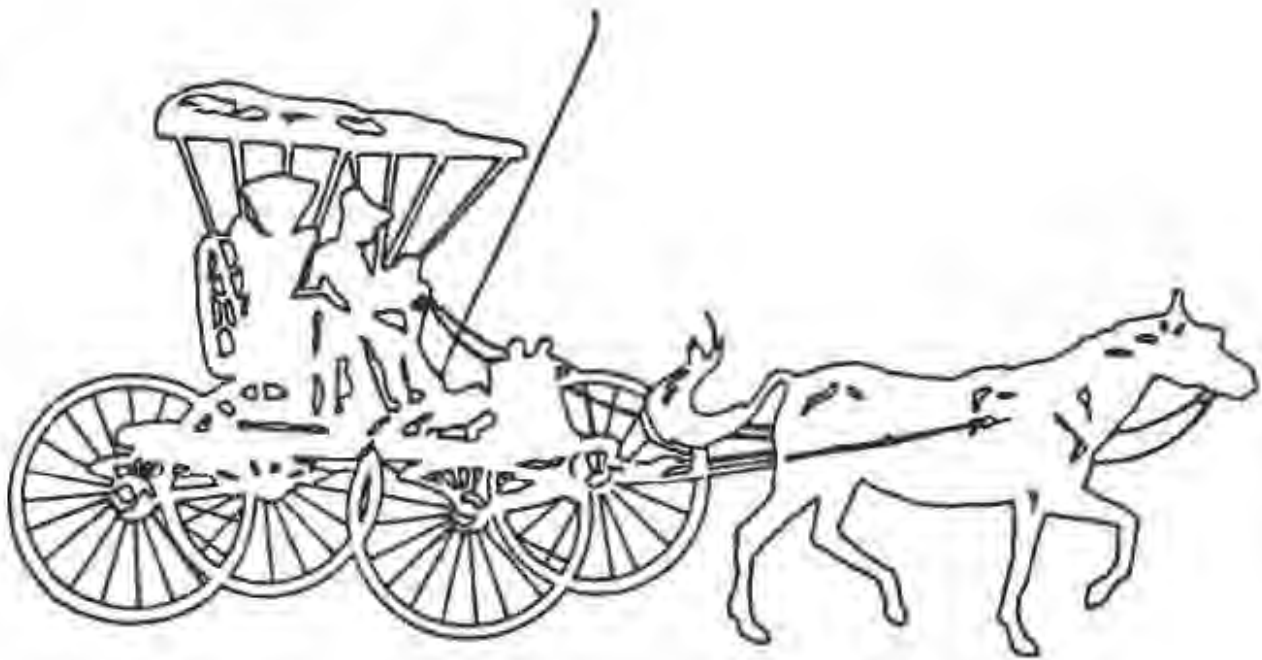
Prepared for
**THE CITY OF NORCO
COMMUNITY REDEVELOPMENT AGENCY**

**APPROVED BY PLANNING COMMISSION
FEBRUARY 1991**

**ADOPTED BY CITY COUNCIL
MAY 1991**

EFFECTIVE DATE: JULY 1991

Prepared by:
**URBAN FUTURES, INC.
IN ASSOCIATION WITH:
AGAJANIAN & ASSOCIATES
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EXECUTIVE SUMMARY

1.0

1.0 EXECUTIVE SUMMARY

1.1 PROJECT DESCRIPTION & LOCATION

The Gateway Specific Plan Project Area (the "Project Area") encompasses approximately 317 gross acres and is approximately 74 percent developed. The majority of existing buildings are single or two story, varying in age and condition. Generally, effective implementation of the Gateway Specific Plan (the "Plan") will achieve the following goals:

- Goal: To expand the economic base of the Project Area and the community as a whole through the active promotion and encouragement of commercial and industrial development in appropriate parts of the Project Area.
- Goal: To encourage the development and revitalization of economically viable commercial land uses along Hamner Avenue.
- Goal: Provide a high level of public services and facilities to all properties within the Project Area.
- Goal: Develop a circulation system which facilitates efficient and safe vehicular, equestrian, and pedestrian traffic, along with the enhancement of the community design character.
- Goal: Create a community design image for the Project Area that expresses and enhances the unique character and identity of Norco.
- Goal: To improve the relationship of differing land uses through physical and functional separation.

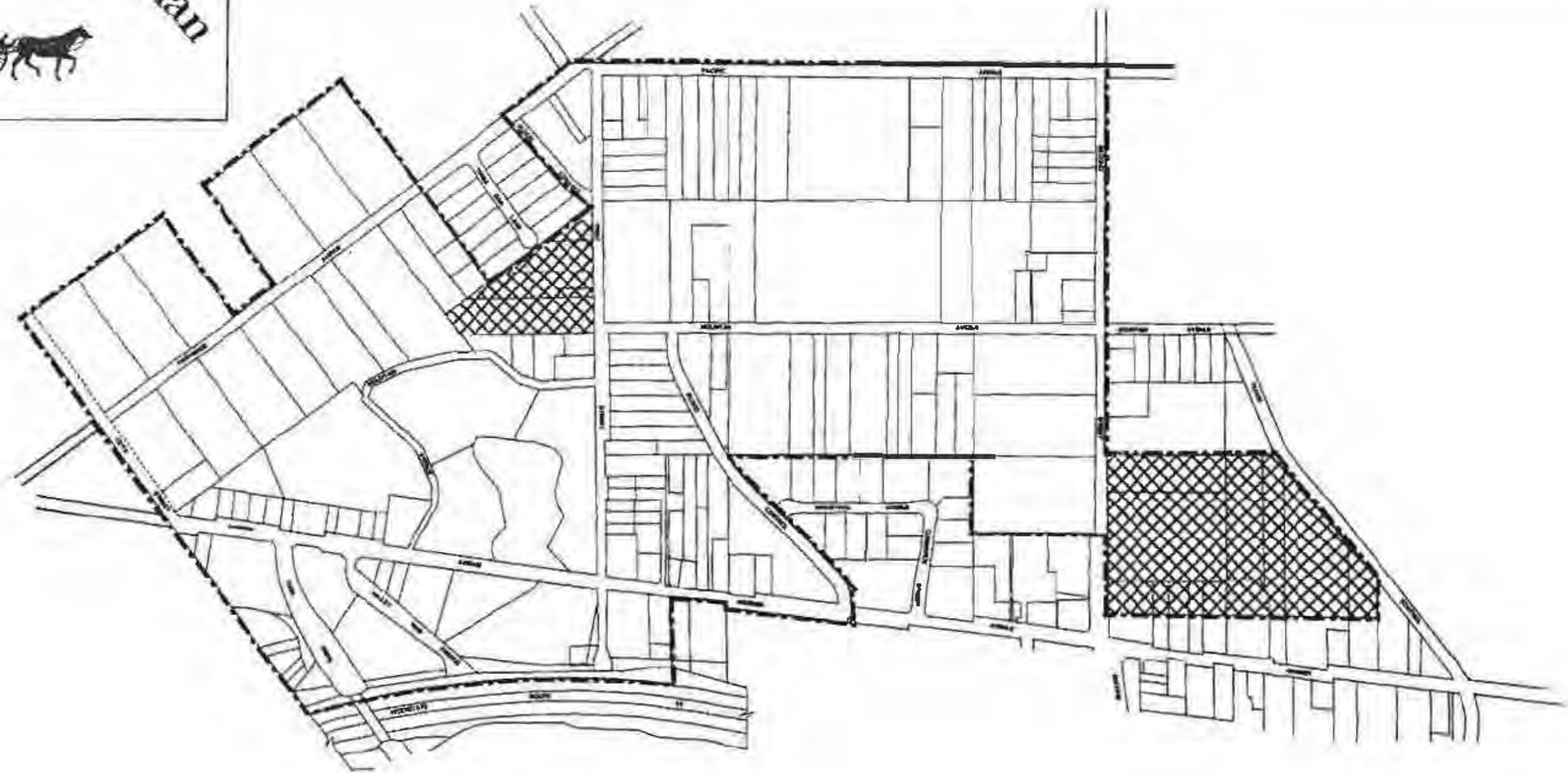
The Gateway Specific Plan Project Area (Exhibit 1) is located within the southern most portion of the City of Norco and incorporates territory east and west of Hamner Avenue; the Project Area lies directly west of the new I-15. Exhibit 2 shows the location of the City in its regional context.

Existing land uses within the Project Area include residential, commercial, industrial, park, pasture/agricultural, quasi-public and vacant land. Land uses adjacent to the Project Area generally include residential, commercial, industrial and vacant land.

1.2 PLAN AUTHORITY, DIRECTION AND SCOPE

The California Government Code authorizes cities to adopt specific plans by resolution as policy or by ordinance as regulation. Hearings are required by both Planning Commission and City Council, after which the Specific Plan must be adopted by the City Council to become operative.

Gateway Specific Plan



GATEWAY SPECIFIC PLAN PROJECT AREA MAP

XXXX NOT A PART

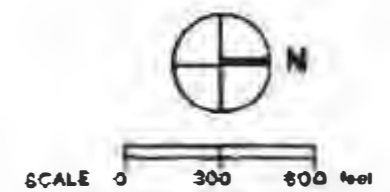


EXHIBIT 1

Gateway Specific Plan



REGIONAL LOCATIONAL MAP



The adoption of the Gateway Specific Plan as a regulatory plan will establish it as the zoning ordinance for the property within its boundary. Proposed development plans or agreements, tentative tract or parcel maps and any other development proposals and approvals must be consistent with the Gateway Specific Plan. Projects consistent with the Gateway Specific Plan will be deemed consistent with the Norco General Plan.

The regulations contained herein shall apply to the boundaries described in Appendix A to this plan.

Development of the Gateway Specific Plan was guided by the six major elements listed below:

- o Relationship to the Norco General Plan (see Section 6.0 for a summary of General Plan consistency);
- o Direction provided by the City Council/Community Redevelopment Agency and Planning Commission;
- o I-15 Corridor Study;
- o Staff participation;
- o Public participation; and
- o Existing conditions.

Each of these factors is discussed on the following pages in order to clearly illustrate the methodology for the Plan, its policies and its regulations.

Norco General Plan

A specific plan may not be adopted or amended unless the proposed plan or amendment is consistent with the city's general plan. Furthermore, a specific plan must evolve to conform to pertinent changes in the general plan. According to section 65359 of the California Public Resources Code, "Any specific plan...of the city or county that is applicable to the same areas or matters affected by a general plan amendment shall be reviewed and amended as necessary to make the specific...plan consistent with the general plan." A specific plan's statement of its relationship to the general plan is one means of demonstrating consistency.

The 1987 General Plan Guidelines set forth the following general rule for determining consistency: "An action, program, or project is consistent with the general plan if, considering all its aspects, it will further the objectives and policies of the general plan and not obstruct their attainment."

If a specific plan is primarily a planning policy document, its provisions should be refinements of, or derived from the general plan's goals, objectives, policies, and plan proposals (i.e., the general plan's statement of development policies). A policy-oriented specific plan conforms to the general plan if a decision making body can reach the same conclusions using either plan. If the city or county uses the general plan alone, however, its staff may have to make detailed site investigations as a prerequisite to a decision.

If a specific plan is regulatory in nature, the plan's regulations must likewise promote the general plan's statement of development policies. In particular, the regulations must be enactments resulting from and complying with the directives of the general plan's policies, plan proposals, or action programs.

Policy-oriented and regulatory specific plans differ in their approaches to implementing a general plan. Policy plans propose implementation measures while regulatory plans impose them. The most important point here is that a specific plan's provisions result directly from and conform to the general plan's policies and proposals.

Specific Plan Diagram Consistency

By law every specific plan must have a diagram or diagrams. Each diagram must be consistent with the general plan. Naturally, there is a question as to how closely a specific plan's diagram should match a general plan's. The consistency question is especially pertinent to a specific plan that has multiple development phases. Interestingly, a multiphased specific plan might meet the consistency requirement even though its illustrations of initial development phases do not match the general plan's long-term land use diagram.

For example, the general plan might call for the conversion of farmland to residential uses when contiguous parcels are urbanized. Although the general plan's land use diagram would designate the area for residences, the initial phases of the specific plan would map the area for continued agricultural use. The specific plan's latter phases, however, would show residential uses. The specific plan would be consistent with the general plan on both a short- and long-term basis.

The timing of development is obviously an important specific plan issue. To avoid diagram consistency questions, the general plan's policies should provide clear guidance for the pace of future development, perhaps by phasing plan proposals in five-year increments. Alternatively, a general plan might set forth conditions to be met before an area would be ready for particular types of uses.

There remains, however, a question as to the degree to which a specific plan's land use designations must duplicate those of a general plan when the diagrams of both plans address the same development phase. There is no explicit statutory answer. Nevertheless, the basic characteristics of the two plan types offer some clues.

A specific plan specifies in detail the distribution, location, and extent of land uses. In doing so it takes into account the individual features of a site on a parcel-by-parcel basis. By contrast, a general plan's land use element diagram need not be parcel-specific. Rather, it reflects jurisdiction-wide planning policies. It functions more as an illustration of appropriate land use relationships than as a precise map locating permitted activities. The boundaries of a specific plan's land use designations need not precisely match the generalized boundaries delineated on a general plan diagram.

A specific plan diagram, however, should reflect a reasoned approximation of a general plan's land use designations. Furthermore, the specific plan's land use relationship should not vary from those proposed by a general plan. If the latter shows only multiple family, neighborhood commercial, and open space uses adjacent to a single-family residential area, so should the specific plan. Industrial uses in that area would be inconsistent with the general plan.

In other words, a specific plan's diagram does not supersede a general plan. Rather, it adds detail to a general plan's statement of development policies.

The Gateway Specific plan is consistent with the City of Norco General Plan, as amended.

City Council/Redevelopment Agency and Planning Commission Direction

The City of Norco City Council/Community Redevelopment Agency and Planning Commission have given the following direction affecting this Plan:

- 1) Do not promote commercial/industrial development east of the I-15;
- 2) No multi-family housing;
- 3) The Project Area should be treated as the economic nucleus of Norco; and
- 4) Dislocate as few residential properties as possible while pursuing the economic development potential of the Project Area.

I-15 Corridor Study

The primary objective of this study was to provide the framework to assist City officials in guiding the long-term economic, land use and infrastructural development strategy for the entire I-15 Corridor Study Area; the Gateway Specific Plan Project Area is located at

the southern end of the I-15 Corridor Study Area. The I-15 Corridor Study should be referenced for detailed information related to the composite I-15 Corridor Study Area.

Staff Participation

Generally, City staff provided technical assistance and assisted the Project Team in gathering data, attending City Council/Redevelopment Agency and Planning Commission meetings, and public workshops.

Public Participation

Public workshops were held on the following days:

Thursday, September 7, 1989
Tuesday, September 12, 1989
Thursday, September 14, 1989

At these meetings "Steering Committees" were formed to work with the Project Team for the duration of the Project. Steering committee meetings were held on the following days:

Monday, October 9, 1989
Thursday, October 12, 1989
Monday, October 30, 1989
Thursday, November 2, 1989
Tuesday, November 28, 1989
Monday, December 11, 1989

It was clear that the people who attended the first three public workshops on the Specific Plan were confused as to the need for a specific plan. Most property owners and merchants in attendance did admit to circulation and parking problems as well as a general discontentment with current zoning restrictions, which, in fact they felt had served to lower property values and create an artificially slow market condition. The public stated that they wanted the Project Team to look closely at land uses which would have a beneficial effect on property values. Generally, those in attendance acknowledged the potential strength of the Project Area to provide the City with additional sales and property tax revenues, and themselves economic gain, because of the Project Area's location next to the newly opened I-15.

The Gateway Specific Plan Project Team has incorporated, to the best of its ability, the recommendations of all participating affected property and business owners.

1.3 BACKGROUND

With the recent completion of the I-15 Freeway, the City is now facing a challenge to ensure that inevitable future development will be complimentary to Norco. Because of this situation, the City has committed itself to the development of the Gateway Specific Plan. The Plan will optimize the long-term benefits for residents, land and business owners, the City of Norco and the community at large by formulating a market supportable development strategy, encouraging compatible land uses and generating property and sales tax monies to support City services such as Police and Fire Protection, trail improvements, etc., and tax increment funds to be used for infrastructure and roadway improvement projects, small business assistance programs, marketing strategies, etc.

As a basis for preparing the Plan, a number of problem conditions within the Project Area were identified by the Project Team and verified with City officials, property and business owners. Generally, these problem statements address: 1) the overall decline in the quality and market potential of the Project Area, 2) the underutilization of many parcels, 3) the large number of deteriorating structures and parcels, 4) the lack of adequate circulation system, 5) the large number of small and disaggregated parcels, 6) flood control and drainage problems, 7) inadequate utilities infrastructure, and 8) general uncertainty about land use policies, development standards and development direction for the Project Area. The Gateway Specific Plan addresses these concerns and others and provides a means to correct identified deficiencies while exploiting Project Area opportunities.

1.4 PURPOSE

The Gateway Specific Plan, when adopted by City Ordinance, serves both a policy function and regulatory function. It will be the device for implementing the long-term goals and objectives of the City of Norco General Plan within the Project Area. The Plan will also contain all applicable land use regulations and will thus constitute the zoning for the Gateway Specific Plan Project Area.

1.5 ORGANIZATION OF THE PLAN

Generally, the most important section of this Plan is Section 4.0, Development Regulations. It contains the rules by which certain uses will be permitted or prohibited, development standards which specify how structures must be located on building sites and presents design guidelines which define, for purposes of this Plan, what "quality" means as it is used in various policies guiding the Plan.

The legal foundation and context for the Plan is found in Section 1.2, Plan Authority, Direction and Scope.

Section 2.0, the Specific Development Plan, provides an overview of the Primary Concepts constituting the Plan and sets the policy direction for the detailed regulations found in Section

4.0. An important feature of this section is the summary of key policy direction and rationale of the Plan. These portions enable the reader to quickly grasp the essential ideas which make the Plan unique and responsive to the issues within the Gateway Specific Plan Project Area. This section of the Plan also provides the analysis of existing Project Area conditions, e.g., demographic, land use, economics, infrastructure and circulation.

Section 3.0, General Design Guidelines and Development Standards, outlines design guidelines and development standards that will govern future development within the Project Area. These general guidelines will govern architecture, landscape architecture, streetscape design, site planning, riding trails, and signage requirements; specific development projects will use these general guidelines as a starting point to generate more detailed design concepts.

The Plan Implementation section, Section 5.0, identifies the steps necessary to monitor progress in carrying out the Plan as well as actions which will facilitate implementation such as economic development strategies, infrastructure development and improvement funding mechanisms, small business assistance programs and maintenance districts.

Section 6.0, General Plan Consistency, describes the goals of each element of the Norco General Plan and how the Gateway Specific Plan implements those goals.

Section 7.0 consists of appendices pertinent to development and implementation of the Plan.

1.6 CALIFORNIA ENVIRONMENTAL QUALITY ACT (CEQA) COMPLIANCE

The Gateway Specific Plan is accompanied by a complete Environmental Impact Report as an integral component of the planning process. It has been prepared in accordance with the most recently adopted guidelines established by the State of California.

As a Program EIR, this document will address issues related to the ultimate development of the Project based on the most definitive Project data available. At each of the subsequent discretionary levels of project review, the program EIR will serve as the master environmental document for the conceptual land use plan being submitted as a part of this Specific Plan.

All subsequent approvals requested will be examined in light of the Program EIR to determine whether additional or more-detailed environmental analysis is necessary based upon the following:

- o If it is determined that the Program EIR adequately addressed environmental impacts associated with the proposed activity, no new environmental documentation would be required.
- o If the proposed project could potentially have a significant impact on the environment which was not addressed in the Program EIR, but the potential impact will be effectively mitigated, a Negative Declaration can be prepared.

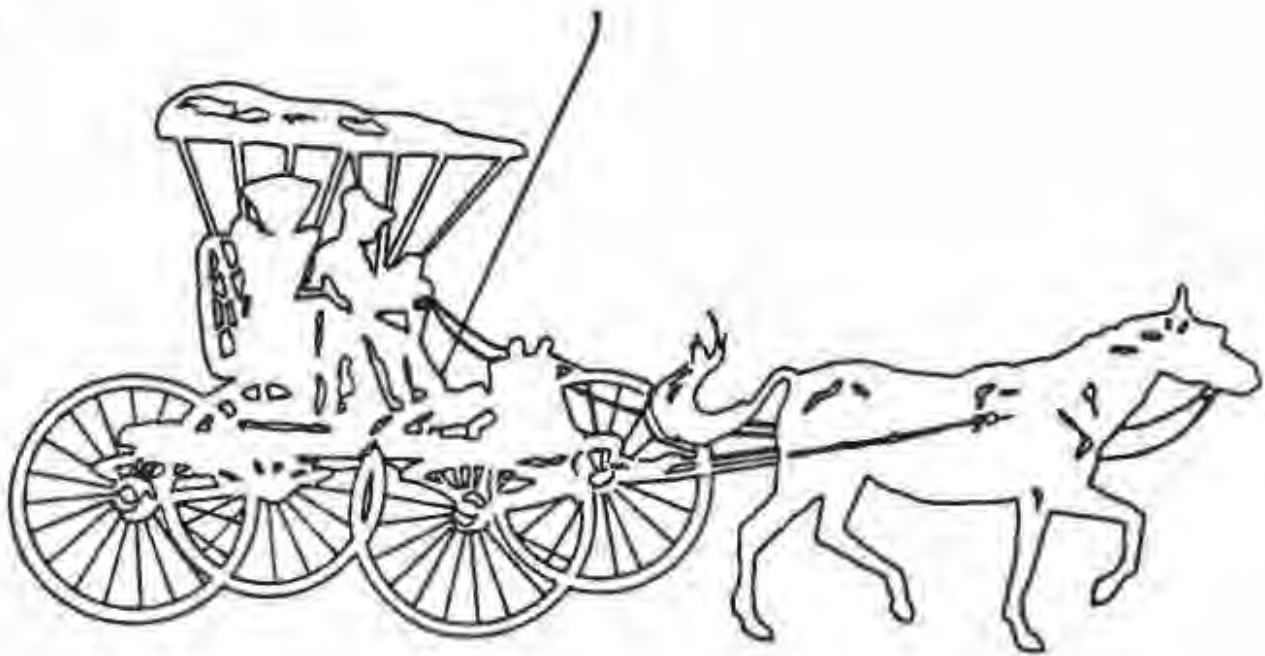
- o In the case where specific issues associated with an activity were not adequately addressed in the Program EIR, but only minor additions or changes are necessary to make the EIR adequate, a supplement/addendum to the EIR need contain only the information necessary to make the Program EIR adequate for the proposed project.
- o Further environmental review at more specific levels of project implementation may reveal additional mitigation measures which will be incorporated into project phases as they are implemented.

These options are in accordance with the provisions of the California Environmental Quality Act Guidelines, Section 15162, and are encouraged by the State, to be utilized in avoiding duplicative reconsideration of basic policy considerations.

Technical Appendices are included as Volume II of the Gateway Specific Plan Program EIR. The Appendices include technical data/analyses with regard to the following:

1. Civil Engineering
2. Traffic/Circulation
3. Geology
4. Acoustical Engineering
5. Air Quality
6. Economics
7. Biological Resources
8. Archaeological/Historical Resources

This document should be referenced to obtain detailed technical data/analyses pertinent to preparation and implementation of the Gateway Specific Plan.



SPECIFIC DEVELOPMENT PLAN

2.0

2.0 SPECIFIC DEVELOPMENT PLAN

2.1 INTRODUCTION

The Gateway Specific Plan (the "Plan") contains the essential components and the policy direction which serve as the foundation for development regulations and guidelines that will actually be used in reviewing and approving development projects proposed within the Project Area. Generally, the Plan is intended to, 1) promote an effective land use policy, 2) insure quality development standards, 3) to facilitate growth within the Project Area where market forces are favorable and, 4) to phase implementation of Project Area infrastructure and circulation system improvement projects and programs.

The basis for the Plan lies in 1) the Norco General Plan; 2) the analysis undertaken as part of the Specific Plan development process and I-15 Corridor Study; 3) direction provided by City staff, City Council, Planning Commission and other City officials; and 4) direction provided by affected property owners, business owners and citizens.

In order for the Project Area to function at its best, a partnership between the City/Redevelopment Agency, property owners, business owners and tenants will be essential. The Plan is intended to be the foundation for that partnership.

City Council adoption of the policies, land use regulations, site development standards and design guidelines demonstrates the City's commitment to the Plan to property owners, business owners and tenants within the Project Area, and to the citizens of Norco in general.

2.2 SUMMARY OF SPECIFIC PLAN POLICY DIRECTION

The following summary expresses the overall policy direction for which the remaining Plan components are supporting details.

- A. Decisions about participation in redevelopment programs and/or projects or about development of new uses on privately held property within the Project Area, shall rest with the individual property owner.
- B. Final decisions concerning development standards to be maintained or established within the Project Area shall rest with the City and shall be as prescribed in the Plan as it may be amended from time to time.
- C. The City of Norco and the City of Norco Community Redevelopment Agency shall give the first right of participation to owners/tenants currently within the Project Area. Further rights of participation shall be in accordance with the Redevelopment Plan for Project Area 1, as amended from time to time.

D. Interpretation of what constitutes "quality" development within the Project Area shall be a cooperative decision between the developer and the City/Redevelopment Agency. Quality, as referenced throughout the Plan, is a consideration of the following factors:

1. Property which is developed in uses permitted by the Plan;
2. Development which conforms to the site development standards in the Plan, particularly with respect to building height, bulk and floor area, setbacks, etc.; and
3. Projects which generally respond to the design guidelines in the Plan, whether they were established before or after Plan adoption.

E. The following primary land use Districts within the Project Area have been established: 1) Commercial, 2) Office Park, and 3) Industrial. Secondary residential and church related uses, as identified on the land use map, will remain as legal uses within the Project Area. Additional restrictions related to existing land uses are contained in Section 4.0 of this Plan.

F. This Plan provides guidance for development as it could occur within an approximate 15 year time period, or by the year 2005.

G. New development will not be allowed unless an appropriate expansion/upgrading of service infrastructure accompanies such development.

H. Effective planning for the Project Area must consider and respond to opportunities/constraints caused by the newly completed I-15.

2.3 GOALS, OBJECTIVES & POLICIES

The purpose of this section is to articulate goals, Objectives, and Policies for the Gateway Specific Plan and to build the foundation for subsequent sections of the Plan.

2.3.1 Definitions

- A. GOALS are broad statements that define the community's hope for the future. Goals are general in nature and do not prescribe "when" and "how".
- B. OBJECTIVES are statements of intent that generally guide future decisions in specific topic areas.
- C. POLICIES are specific statements of intent designed to deal with particular topics in a prescribed manner. They define the approach to be taken to achieve Plan objectives, and

are in themselves the first step in the development of a problem solution because they form the basis for standards and regulations.

2.3.2 General Goal Statement

The ultimate goal of the Plan is to create a Project Area that is attractive and of high quality with a unifying Project Area design theme reflective of community standards and identity, providing an economically viable setting for a balanced mixture of light industrial, commercial and office/professional uses, serviced by a safe and efficient circulation/transportation system, and to eliminate existing and prevent future service infrastructure deficiencies. All new uses must respect and accommodate existing uses that will remain both within and adjacent to the Project Area.

In an effort to further define and prioritize this general goal statement, the following Project Area Design, Land Use, Economic, Circulation, Environmental, and Infrastructure goals and objectives have been developed.

2.3.3 Project Area Design

GOAL: To create a Project Area design theme within the Project Area that expresses the unique character and identity of Norco.

Objectives

1. The architectural theme within the Project Area should be reflective of the City's adopted philosophy, "City living in a rural atmosphere". To this end, the Plan should incorporate architectural design guidelines that will ensure the development of a quality western/southwestern/early Californian design character within the Project Area.
2. Provide a system of vehicular gateway nodes which announce and identify entries into Norco and major land use districts and which help to achieve an overall positive identify for the area.
3. Promote compatible building elevations and construction materials.
4. Promote contemporary landscape treatments throughout the Project Area. Plantings should be primarily those that are low maintenance and drought tolerant.
5. Provide for the elimination of visually objectionable views such as outdoor storage, poorly maintained vacant parcels, underutilized properties and loading areas through the use of design guidelines.

6. Provide attractive and functional buffers for sensitive adjacent land uses. The buffers might include a combination of walls, plantings, earth berms, trees and varying setback depths.

Policies

1. Develop consistent streetscape and architectural palettes which are conducive to creating a gateway statement for Hamner Avenue and other parts of the Project Area. It is not the intent of this thematic overview to discourage innovative or contemporary architectural expressions, or to imitate the architecture of the past, but to promote the harmonious coexistence of architectural styles with an emphasis placed upon the western/southwestern/early Californian theme.
2. Require compliance with the Project Area design guidelines in plans prepared for new development, expansion or redevelopment, and make Project Area design standards a major consideration in the site plan review and approval process.
3. Utilize landscape materials within the Corridor which are drought tolerant, clean, safe and relatively low maintenance. Formal forms and configurations should be utilized at activity center nodes, such as major intersections, while less formal, natural planting patterns should be utilized throughout other parts of the Project Area such as in street medians and landscape setbacks.
4. Develop an incentive program which rewards private sector development for providing certain "extra" design amenities within their developments. Of particular interest are solar-control devices such as building overhangs, awnings and extra tree plantings in parking lot areas. Consideration should be given to special paving materials used in place of asphalt in vehicular areas, such as systems that allow water percolation.
5. Develop land use/site planning concepts that allow for adequate setbacks and land use buffering techniques to mitigate land use conflicts.
6. Provide a streetscape design for Hamner Avenue and other major roadways to include entry identification signage into the City, street tree programs, median landscape design concepts, landscape and hardscape standards, etc.
7. Consider economic incentives for owners who wish to architecturally rehabilitate, refurbish or upgrade design elements on existing properties.
8. Designate special landscape features at major intersection locations designed to promote a distinctive thematic character for these nodes. Changes in paving materials, plant materials, lighting, signing and siting of adjacent structures should occur at major intersections to enhance their distinctiveness and identity.

9. Designate new land uses that are sensitive to existing land use designations. Design appropriate buffers to mitigate potential conflicts caused by possible land use incompatibility.

2.3.4 Land Use/Fiscal

GOAL: Develop a specific plan which is sensitive to Project Area land use and fiscal needs.

Objectives

1. Maximize the economic potential of new and existing Project Area commercial and industrial activities, capturing local and subregional demand. The Norco Auto Mall is a particularly important area of economic potential.
2. Promote distinctive commercial activities which are complimentary to the characteristics and identity of the City as a whole.
3. Encourage or facilitate the assembly of smaller, fragmented lots into integrated, comprehensive development sites to optimize land use efficiency.
4. Segregate shopping-oriented retail commercial use and professional/office parks from industrial developments in order to provide a suitable environment for optimum growth of all segments of the business community.
5. Promote and encourage the use of private resources for commercial reinvestment in existing commercial developments.
6. Promote and encourage commercial activities that provide employment opportunities for all age and income groups.
7. Promote commercial activities that provide recreation and leisure opportunities to all age and income groups.
8. Require master planning at key sites/locations to assure comprehensive, integrated and compatible development consistent with this Plan.
9. Promote the upgrading of underutilized land functioning at less than market potential through incentives for rehabilitation and elimination of non-conformities.
10. Employ a land-use development concept that will effectively reduce the number of vacant properties, while eliminating mixed land uses of an incompatible nature.

11. **Employ design standards to improve land use compatibility where changes in land use are impractical.**

Policies

1. **Promote intensive commercial uses along roadways that have high visibility and good access, particularly along Hamner Avenue and adjacent to I-15 on/off ramps.**
2. **Prepare Development Regulations and Guidelines which regulate, simplify and facilitate the development review process.**
3. **Establish regulations and standards for mixed use developments which assure proper land use mixture, minimum lot size, access restrictions, buffering techniques, and master planning.**
4. **Establish an incentive program for lot consolidation of small, shallow parcels in the Project Area pursuant to Section 5.1.3 of this Plan, as a way to encourage development of larger, massed projects, as opposed to a linear strip commercial configuration.**
5. **Allow legal nonconforming uses to continue as pursuant to Section 4.5, to allow more flexibility in the continued use of property.**
6. **Encourage the formation of new redevelopment programs and incentives to assist retail and service businesses with property improvements.**
7. **Develop a light industrial land use nucleus centering around the existing Norco Ranch Facility.**
8. **Promote clustering of multiple, medium sized structures on large parcels rather than single, massive structures.**
9. **Allow maximum site development through liberal site development standards in return for well planned site plans which respond to established design guidelines.**
10. **Apply design guidelines to mitigate conflicts between uses where a change in land use is not practical.**
11. **Prepare a relocation Plan that will address the conditions of property owner relocation as needed on a case-by-case basis.**

2.3.5 Circulation

GOAL: Develop a circulation system which facilitates efficient and safe vehicular, equestrian and pedestrian traffic and enhances the Project Area's design character along Hamner Avenue and other major roadways within the Project Area.

Objectives

- 1. Limit vehicular access points on Hamner Avenue.**
- 2. Provide for, and phase necessary street improvements to maximize efficiency of the circulation system within the Project Area, and integrate the planned Yuma Drive/I-15 on/off ramp system.**
- 3. Minimize vehicular through traffic on adjacent residential streets, such as Pacific Avenue.**
- 4. Provide safe sidewalks and equestrian trails where there is a purpose for pedestrian and equestrian access.**

Policies

- 1. Reduce inter-site vehicular trips within the Project Area commercial areas by requiring private developments to secure Reciprocal Access Agreements prior to development when feasible.**
- 2. Allow shared parking and points of access to facilitate efficient parcel usage and to minimize traffic support facilities such as driveways (particularly those that affect traffic flow on major streets such as Hamner Avenue), parking spaces, etc.**
- 3. Discourage new commercial, industrial and office park developments from taking access from local residential streets, by developing internal circulation systems which direct traffic away from surrounding residential neighborhoods.**
- 4. Promote the installation of raised landscaped medians within Hamner Avenue, First Street, Second Street, and Mountain Avenue to insure efficient, safe and aesthetically pleasing traffic operations along these roadways and to provide a visual connection to Lincoln Street.**
- 5. Promote the vacating of Cota Street, Mountain Avenue (south of First Street), portions of Valley View Avenue and the most easterly segment of First Street, in order to reduce access to Hamner Avenue and to allow for maximum land utilization and contiguous development south of First Street.**

6. Promote the extension and new alignment of Mountain Avenue (south of First Street) Yuma Drive (west of Hamner Avenue) and the construction of additional roadways as shown in the Circulation Plan to promote access to future and existing land uses.
7. Promote the widening of Parkridge Avenue, First Street, the existing portion of Mountain Avenue, and Second Street between Hamner Avenue and the new connector street to be located west of the Norco Ranch Processing facility to facilitate increased levels of vehicular traffic.
8. Investigate the lowering of Hamner Avenue at First Street to provide a safer intersection at that location and to facilitate development of adjacent properties.
9. Promote intersection improvements at the following locations: 1) Hamner Avenue and Yuma Drive (proposed); 2) Parkridge Avenue and Yuma Drive (proposed); 3) Hamner Avenue and First Street; 4) Mountain Avenue and First Street; 5) Parkridge Avenue, First Street, Pacific Avenue and Lincoln Street; 6) Mountain Avenue and proposed connector street; and 7) Cota Street and Parkridge Avenue.
10. Promote the construction of pedestrian sidewalk facilities where appropriate within the Project Area to facilitate pedestrian activities. Promote the construction of riding trails along Pacific Avenue, First and Second Streets. Provide incentives to private development to incorporate such amenities into their development proposals.

2.3.6 Environmental

GOAL: Maintain the highest possible quality of environment within the Project Area by balancing the impacts of development with environmental sensitivity.

Objectives

1. Promote the involvement of landscape architects in future development to ensure environmental compatibility of all projects. For example, to reduce the impacts of flooding and to increase water percolation, increase the amount of pervious surfaces in parking lots, e.g., use of turf block; increase the number of landscaped areas, especially those incorporating trees.
2. Promote pedestrian and equestrian modes of transportation as a means of improving regional air quality.
3. Establish guidelines which preserve significant environmental features, such as mature trees.
4. Promote land use compatibility and measures to mitigate noise and visual impacts.

Policies

- 1. Limit development within the 100 year flood areas as shown on the latest City FIRM maps (or as may be amended) or provide flood protection measures e.g., improved flood control channel facilities, in accordance with City and Riverside County Flood Control and Water Conservation District requirements.**
- 2. Based on projected traffic volumes and land uses, map future noise contours, and establish land use restrictions and/or noise attenuation conditions for areas within noise impacted areas.**
- 3. Through the project approval process and the imposition of conditions or mitigation measures, pursuant to CEQA, ensure that all development within the Specific Plan area will not result in a decrease in environmental quality, and will wherever possible create a higher quality environment.**
- 4. Encourage that developments within the Project Area impacted by noise provide sufficient noise attenuation levels to maintain exterior and interior CNEL noise levels at acceptable levels.**
- 5. All site planning alternatives shall be reviewed and approved by a licensed landscape architect to ensure utmost environmental compatibility and to determine whether or not a more environmentally sensitive design alternative might be appropriate.**

2.3.7 Infrastructure

GOAL: Provide a high level of public services and service facilities to all residents, businesses and industrial uses within the Project Area.

Objectives

- 1. Phase future development to correspond with the construction of adequate utility infrastructure.**
- 2. Provide solutions to remedy deficiencies affecting the following: 1) drainage/flood control, 2) poor visibility at the intersection of Hammer Ave. and First Street, and 3) water and sewer service infrastructure.**
- 3. Utilize the resources of the Redevelopment Agency to assist in the finding of necessary public infrastructure improvement projects and to facilitate the timely development of economically beneficial land uses.**

Policies

1. Insure that all necessary public services and utilities are or will be available prior to completion of new development projects and prior to, or concurrently with the issuing of building permits.
2. Develop an implementation program for the improvement, phasing and financing of infrastructure within the Project Area.
3. Monitor utilization of public infrastructure systems and program improvements to ensure capacity for future planned development.
4. Encourage the formation of new redevelopment programs and incentives to assist funding of necessary public infrastructure.

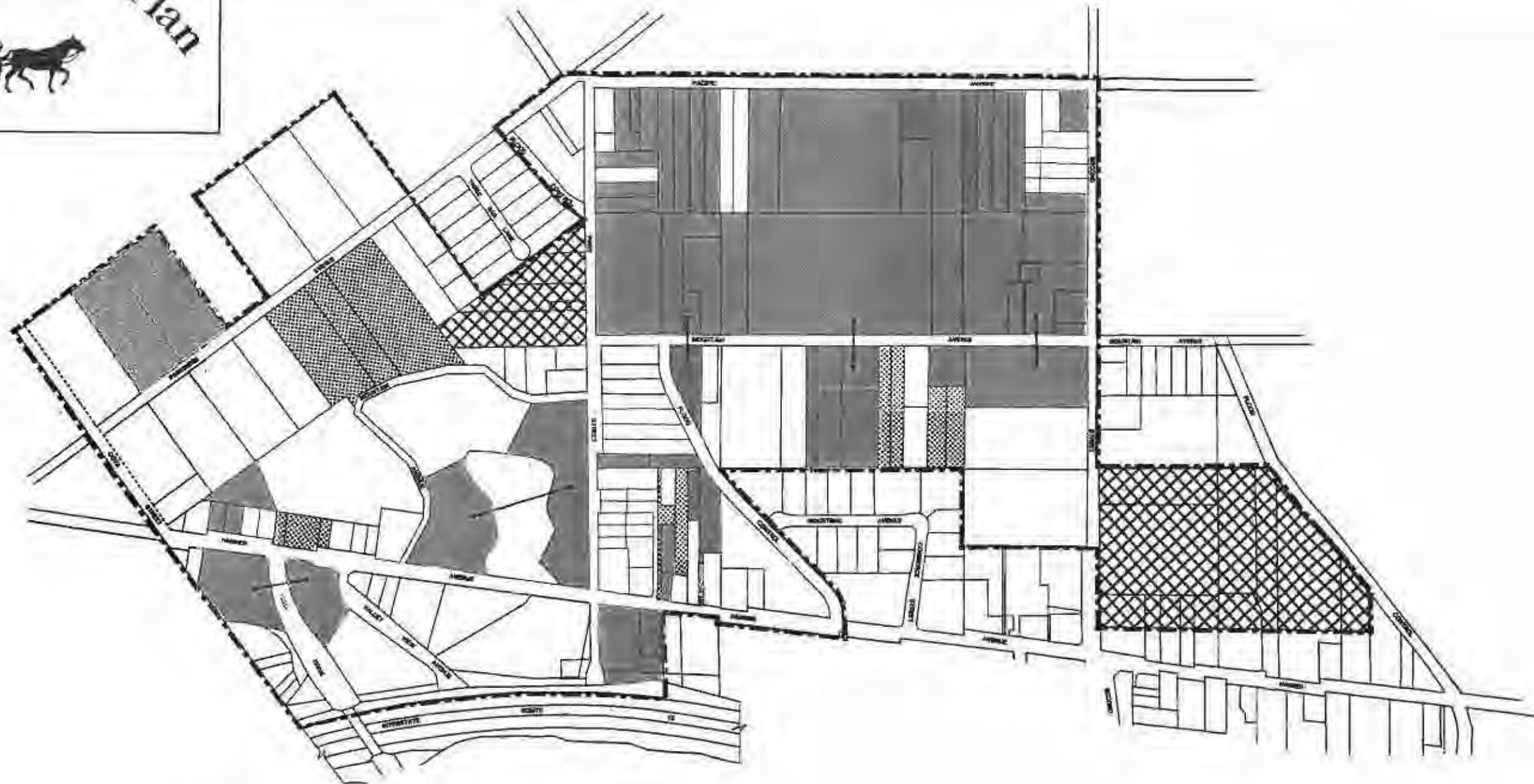
2.4 EXISTING CONDITIONS

2.4.1 General

The analysis of existing conditions within the Project Area is the result of input from numerous sources. General findings are as follows:

- A. As previously disclosed in Section 1.2, an economic study prepared for Interstate 15 Corridor Study (see Appendix B of the I-15 Corridor Study) revealed a significant sales leakage from the City of Norco into the City of Corona and other communities. That study also revealed strong market demand and an acceptable rate of absorption, over a 15 year period and beyond, for the land uses proposed within the Project Area. Land use distribution rates were guided to a significant degree by this economic analysis.
- B. Approximately 25% of the gross acreage within the Project Area is undeveloped. An additional 47 parcels are underutilized or in a deteriorated state. These areas represent areas of immediate development opportunity.
- C. For the purposes of discerning development opportunities and constraints, as well as to create a Project Area database, the Project Team studied ownership patterns, building conditions, lot consolidation opportunities, existing zoning and general plan designations. Exhibit 3 shows existing ownership patterns within the Project Area. Conclusions drawn from parcel ownership patterns included:
 1. As shown in Exhibit 3, the Norco Ranch owns a large amount of land within the Project Area. The Norco Ranch ownership generally includes the area between Pacific Avenue, Mountain Avenue, and First and Second Streets. This ownership also extends east of Mountain Avenue and south

Gateway Specific Plan



PROPERTY OWNERSHIP MAP

LEGEND

 Indicates Multiple Parcel Ownership by Single Entity



 Indicates Individual Parcel Ownership

 Ownership Connectors



SCALE 0 300 600 feet

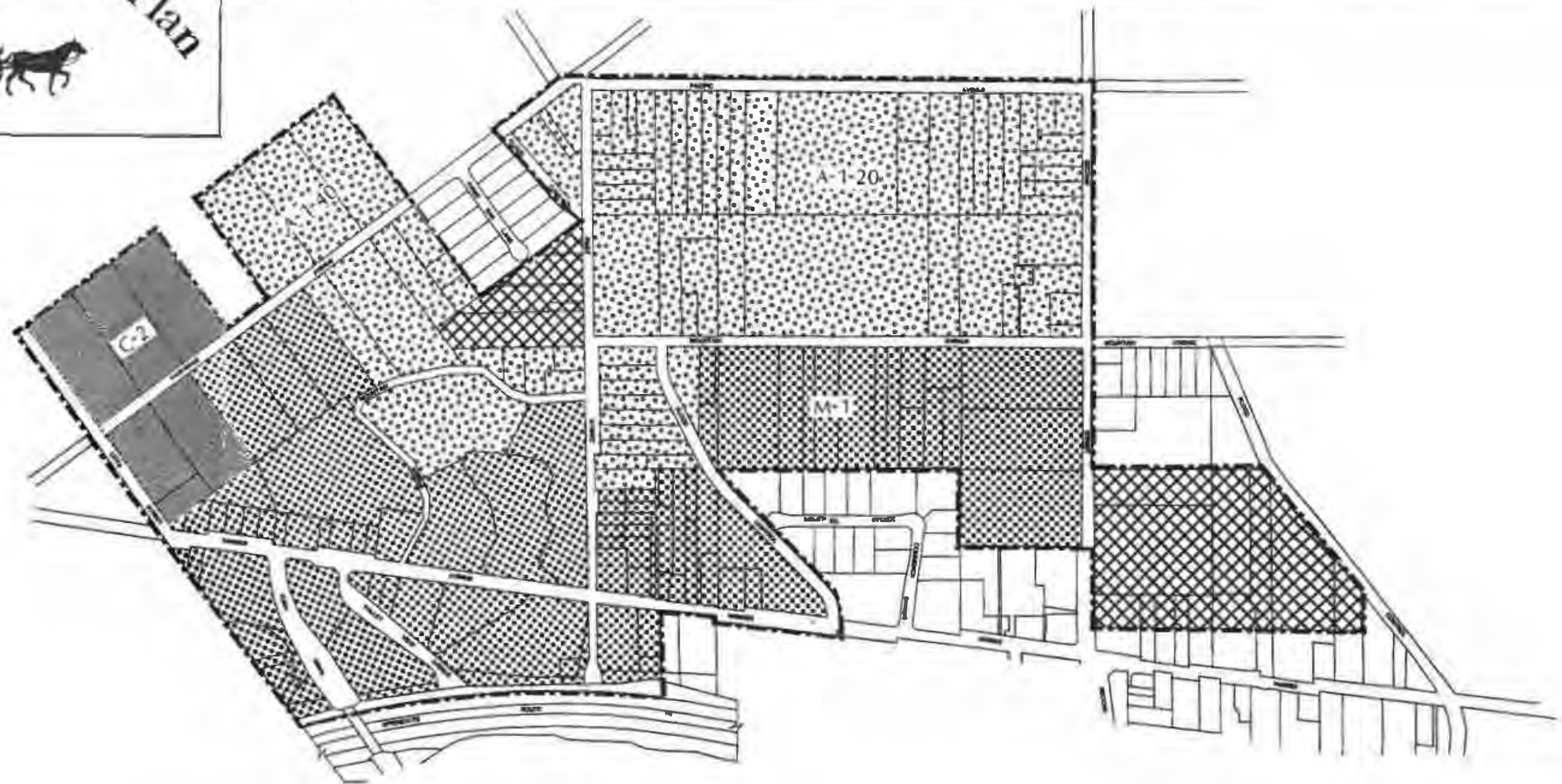
of First Street. Total acres owned by the Norco Ranch within the 41 parcels is approximately 87; this is approximately 30% of the total Project Area excluding public right-of-ways. Norco Farms is currently in the process of negotiating for additional parcels in close proximity to its existing holdings.

2. The second largest land holding occurs between Hamner Avenue and the unimproved portion of Mountain Avenue; this ownership totals approximately 9.84 acres.
3. The third largest land holding is located on the southwest side of Parkridge Avenue. This ownership consists of two large parcels totaling 8.62 acres.
4. Within the Project Area there are 113 property owners in possession of 165 parcels totaling approximately 272.49 acres.

D. Exhibit 4 shows existing zoning; Exhibit 5 shows existing General Plan land use designations.

1. The analysis of existing zoning and general plan land use designations has shown that numerous parcels are inconsistent with respect to existing zoning and existing General Plan Land Use designations.
2. Review of these maps also indicates that all properties adjacent to Hamner Avenue are designated for industrial development which is not the highest and best use for properties along Hamner Avenue.

Gateway Specific Plan



EXISTING ZONING MAP LEGEND

A-1-20 Agricultural /Residential

C-2 General Commercial

C-3 Heavy Commercial

M-1 Heavy Commercial /Light Manufacturing

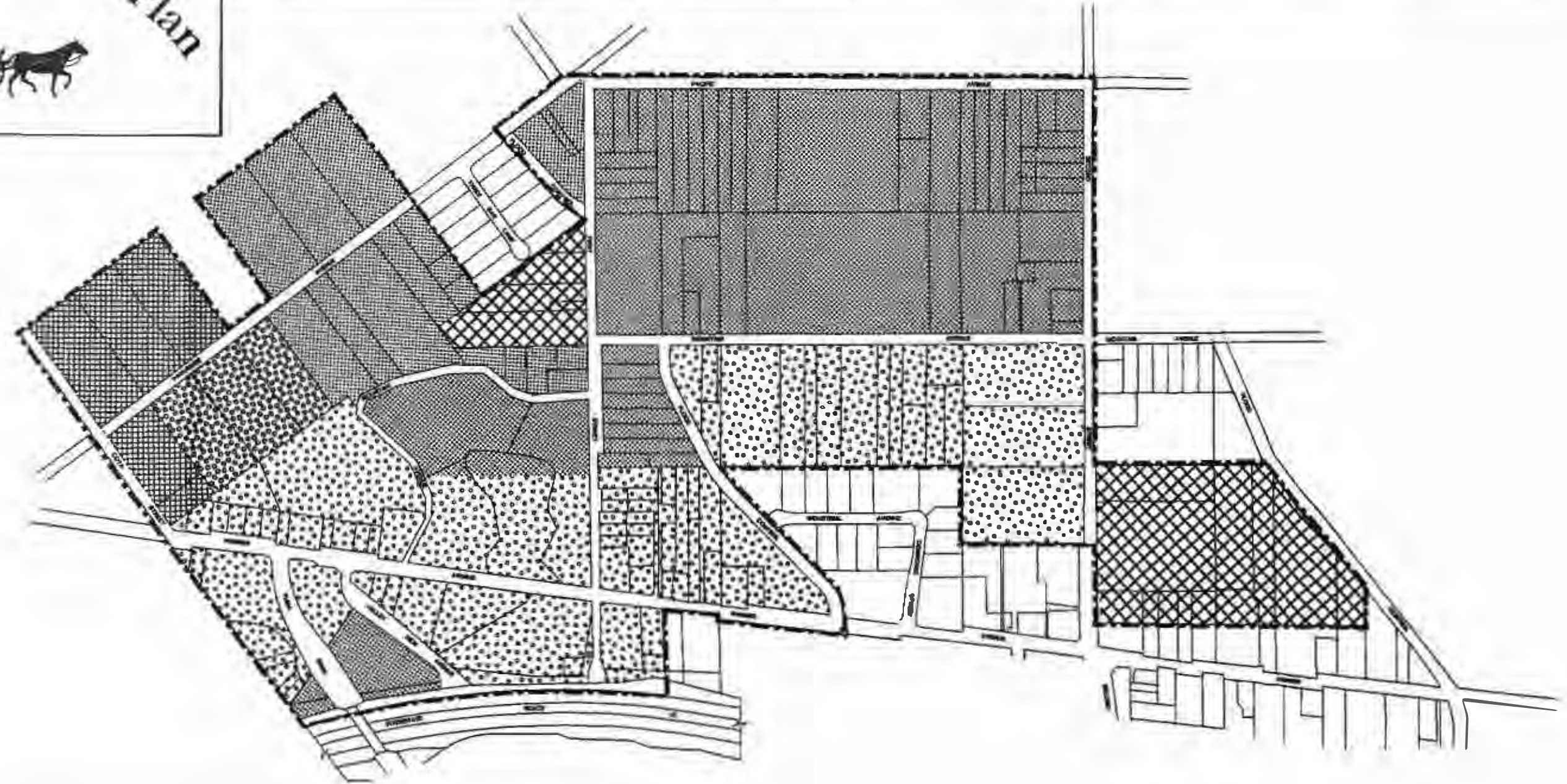
NOT A PART

The 'A' and 'R' zones each have sub-zones to designate lot size; the suffix number designates the required minimum lot size in either thousands of feet, or in acres. Any numerical suffix of 20 or more shall mean thousands of square feet; any numerical suffix of less than 20 shall mean acres.



SCALE 0 300 600 feet

Gateway Specific Plan



EXISTING GENERAL PLAN LAND USE MAP

LEGEND

Residential Low (0-2 units per acre)

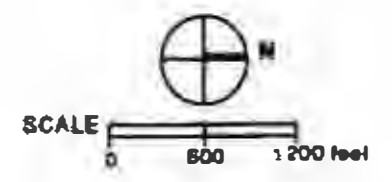
Commercial Neighborhood

Commercial Community

Industrial

Specific Plan

NOT A PART



- E. A Soils/Geology Study was prepared to analyze existing soils and geologic conditions and found that no adverse soils/geology constraints were present within the Project Area.
- F. A Biology Study was prepared to analyze existing biological conditions and found that no adverse biological constraints were present within the Project Area. However, a small riparian area was identified west of Hamner Avenue and Yuma Drive.
- G. An Archeological Study was prepared to research the possibility of archaeological or historical constraints; no significant archaeological/historical constraints were found to be present within the Project Area.
- H. Civil engineering studies were prepared to analyze existing conditions; issues studied are as follows:
- o drainage/flood control;
 - o elevation of Hamner Avenue;
 - o Project Area topography;
 - o adequacy of sewer and water service infrastructure
 - o costing of the Plan's development.
- I. A traffic/circulation study was completed. This study found that all Project Area intersections are presently operating at an acceptable level of service (LOS) of B or better. However, Plan related circulation improvements will not maintain an acceptable LOS. There will be a significant impact which cannot be completely mitigated to a level of insignificance.
- J. Analyses of existing acoustical and air quality conditions within the Project Area were prepared. No significant development constraints were found within the Project Area.
- K. A substantial number of structures within the Project Area were observed to be in a declining state. Table 1 provides an analysis of Project Area structures by land use.

2.4.2 Existing Land Use

Existing land uses are shown on Exhibit 6.
Existing land use acreages are shown on Table 2.

Gateway Specific Plan

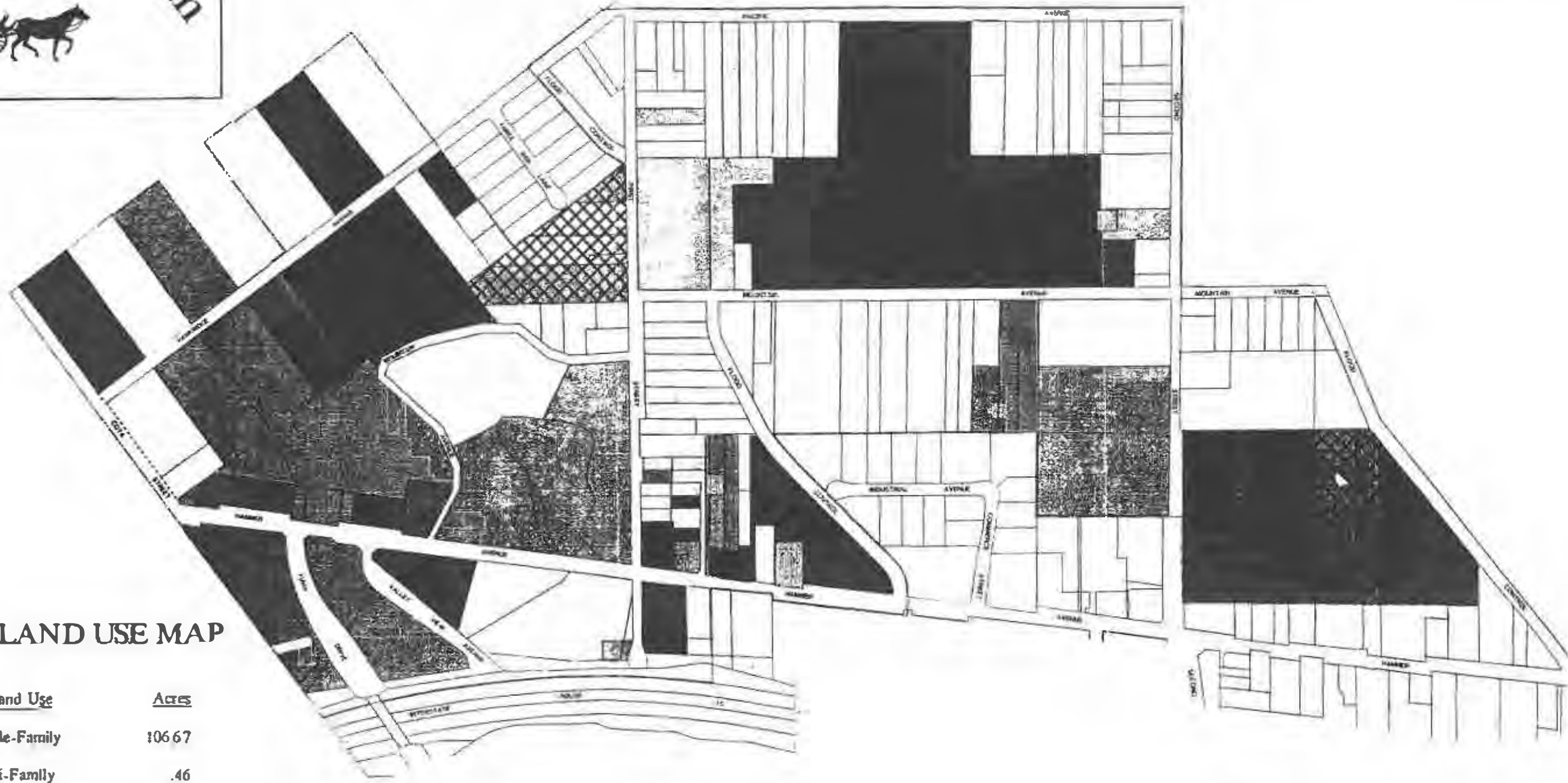


TABLE 1
EXISTING PROJECT AREA STRUCTURAL RATINGS

<u>Land Use</u>	<u>Rating Category</u>				<u>Total</u>
	<u>1</u>	<u>2</u>	<u>3</u>	<u>4</u>	
Residential	10	73	8	1	92
Multi-Family	0	2	0	0	2
Commercial	1	37	4	1	43
Industrial	0	9	1	0	10
Public	0	1	0	0	1
Total	11	122	13	2	148

Source: Urban Futures, Inc., 1990

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EXISTING LAND USE MAP

LEGEND

Land Use	Acres
Residential/Single-Family	106.67
Residential/Multi-Family	.46
Commercial	65.52
Industrial	8.64
Pasture/Agriculture	4.76
Quasi Public (Church)	8.69
Vacant	77.80
Public Rights-of-Way	44.86
TOTAL	317.35

Not A Part



SCALE

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TABLE 2
EXISTING LAND USE ACREAGES

<u>Land Use</u>	<u>Acres</u>
Residential/Single-Family	106.67
Residential/Multi-Family	.46
Commercial	65.52
Industrial	8.64
Pasture/Agriculture	4.76
Quasi Public (Church)	8.69
Vacant	77.80
Public Rights-of-Way	<u>44.86</u>
TOTAL	317.35

Source: Urban Futures, Inc., 1989

24.3 Existing Traffic/Circulation

Area Streets and Highways

Regional access to the Project Area is provided by the Riverside Freeway (State Route 91) and Interstate 15. Primary access to the Project Area is from the Riverside Freeway interchange with North Main Street (Hamner Avenue) in the City of Corona and the Interstate 15/Second Street interchange located in Norco. Upon its completion the Yuma Drive/I-15 Interchange will provide direct I-15 access into the Project Area.

Hamner Avenue (State Route 31) is classified as a major highway in the City of Norco Circulation Element of the General Plan and provides two through lanes in each direction with a two way left turn lane median area. Hamner Avenue is known as Main Street in the City of Corona. The roadway width varies from full improvements with curb and gutter to minimum width and unimproved highway edges. Hamner Avenue is signalized at the intersections of Parkridge Avenue, Yuma Drive and Second Street. Hamner Avenue is posted for a 40 MPH speed.

First Street extends from its intersection at Pacific and terminates west of the I-15 Freeway providing two lanes with unimproved road edges. First Street is signed with STOP signs at Pacific, Mountain and Hamner Avenue. First Street is classified as a collector in the City's Circulation Element.

Second Street is an east-west roadway, classified as a collector, extending from Parkridge Avenue on the west to Hillside Avenue, east of I-15. Second Street generally provides for one lane each direction except for multiple lanes from west of Hamner Avenue to the I-15 north bound freeway ramps. Four-way STOP controlled intersections exist at Pacific and Mountain Avenues, while multiple phased traffic signals control the intersections at Hamner Avenue and the Second Street/I-15 on/off ramp.

Pacific Avenue is a north-south street, classified as a local extending from Parkridge Avenue to the U.S. Naval Reservation. Two lanes are provided with unimproved roadway edges.

Mountain Avenue is a north-south local roadway extending from First Street to the U.S. Naval Reservation. Mountain Avenue and First Street is controlled by a three-way stop. Mountain Avenue also exists south of First Street as a dirt roadway curving easterly and intersecting with Hamner Avenue.

Parkridge Avenue is an existing two lane roadway having improved and unimproved roadside edges. Parkridge Avenue extends from Kips Komer

Road in the southeasterly direction, into the newly developing area of northeast Corona and intersecting with Yuma Drive.

Table 3 presents the key area intersection existing AM/PM peak hour Intersection Capacity Utilization/Level of Service (ICU/LOS) summary within the Project Area. Existing intersection conditions are relatively good with a majority of intersections operating with an LOS of B or better. Exhibit 7 shows the existing circulation system in the Project Area.

2.4.4 Existing Infrastructure

The Project Area is being served by sewer mains ranging in size from 8" to 27". Similarly, the existing water main sizes vary from 6" to 12". There are a number of abandoned water mains on Mountain Avenue, Hammer Avenue and Second Street. Final line sizes will be determined at the time of final engineering to assure adequate fire flow for the proposed developments. Proposed development projects will not be approved by the City without approval of an acceptable program/plan for providing sewer and water service infrastructure.



TABLE 3

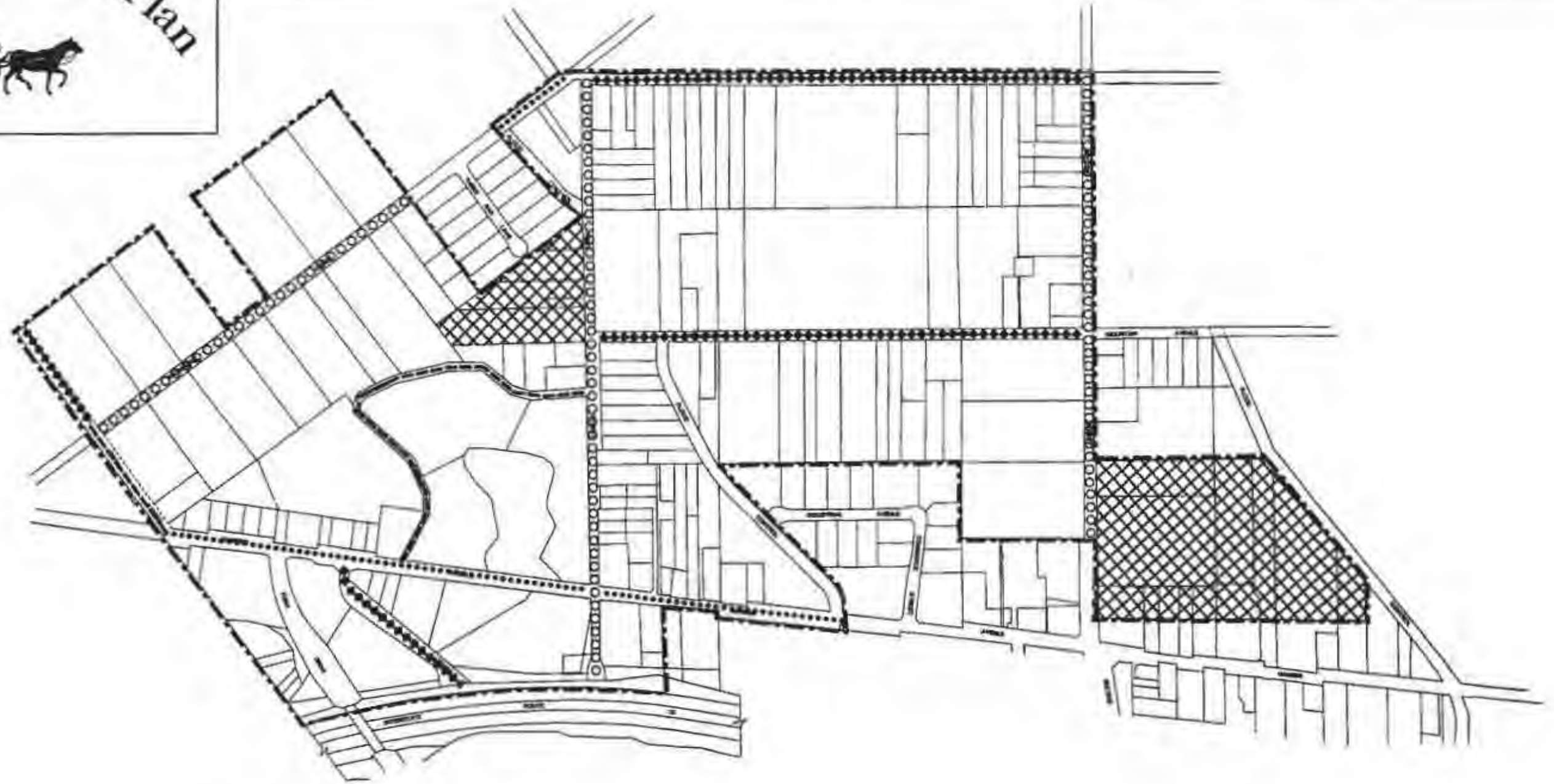
**INTERSECTION CAPACITY UTILIZATION/LEVEL OF SERVICE
EXISTING CONDITIONS SUMMARY
FOR PROJECT AREA KEY INTERSECTIONS**

<u>INTERSECTIONS</u>	<u>AM PEAK HOUR ICU/LOS</u>	<u>PM PEAK HOUR ICU/LOS</u>
Hammer Avenue		
@Parkridge Avenue	0.45/A	0.61/B
@Yuma Drive	0.40/A	0.49/A
@2nd Street	0.53/A	0.67/B
2nd Street		
@I-15 W/B Ramps	0.48/A	0.44/A
@I-15 E/B Ramps	0.47/A	0.62/B

N/A = not available; in process of taking existing counts

Source: Final Traffic Impact Report; Norco Gateway Specific Plan, I-15 Corridor Study, Norco, CA, April 1990.

Gateway Specific Plan



EXISTING CIRCULATION SYSTEM MAP Roadway Classifications

LEGEND

..... Major

◆◆◆◆◆ Local

XXXXX NOT A PART

ooooo Collector

——— Unimproved

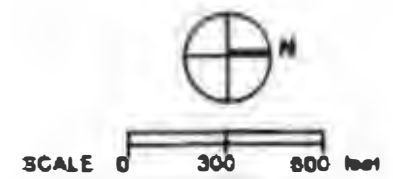


EXHIBIT 7

2.5 OPPORTUNITIES AND CONSTRAINTS

Of primary importance in the development of land use development strategies within the Project Area is the identification and location of existing and anticipated development opportunities and constraints. These opportunities and constraints may manifest themselves physically, economically or socially, becoming the foundation for future development of the Project Area. For instance, a large land form composed of a very hard rock is a physical constraint, conversely, a large number of land parcels under single ownership in an existing urbanized area, assuming social and economic conditions are acceptable, will generally be viewed as a physical and economic opportunity.

The analysis of opportunities and constraints considers existing and anticipated physical, social and economic constraints and opportunities, both within the Project Area and the surrounding territory that will either affect, or be impacted by the Project Area's long term development.

Methodology

The finding of opportunities and constraints involved field analysis, research of existing current and historical data, discussions with City staff, contributing sub-consultants, and property and business owners. The opportunities and constraints are described below and shown graphically on Exhibit 8.

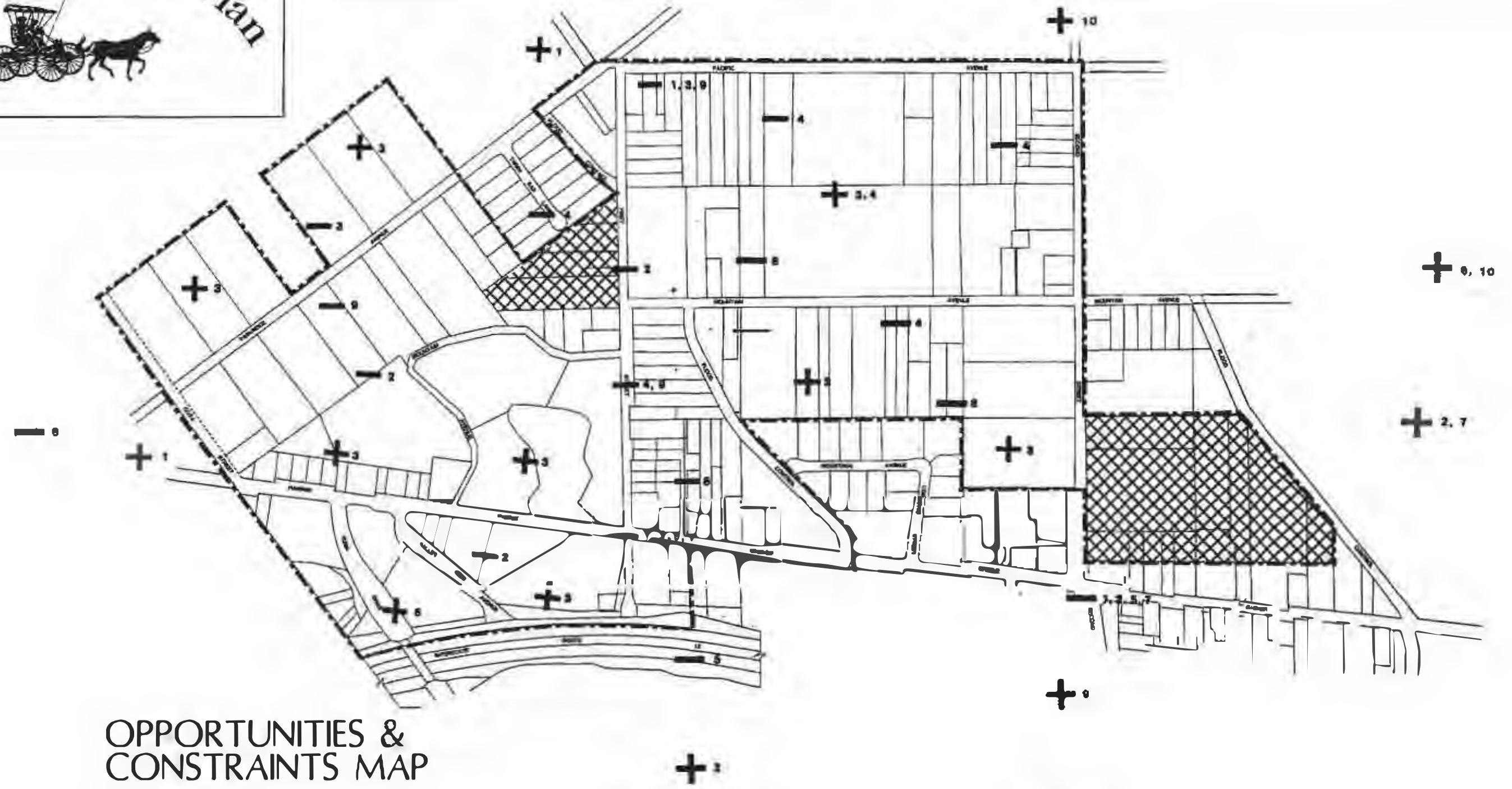
2.5.1. Opportunities

1. The Project Area's northern and southern points of entry from Hamner Avenue, and its western point of entry along First Street, provide indirect access into the Project Area from Orange County, the City of Riverside and other sub-regional economic trade areas.
2. Current plans for the Corona Ranch, the Northeast Corona Specific Plan and other planned developments within the City of Corona are expected to add over 10,000 new residential units over the next five year period.

The Northeast Corona Specific Plan is close to completion. The project encompasses 1,457 acres; approximately 5,800 units have been built or are being built. The Buey Corporation, through implementation of the Corona Ranch Specific Plan, is developing an approximately 712 acre area into a 3,410 dwelling unit tract. Grading commenced three years ago; construction is approximately 30% complete (this Project was previously known as the Woodlake project).

Three housing projects are currently being developed west of Hamner Avenue between Third and Fourth Streets in Norco. These include: Watt Development, 146 units;

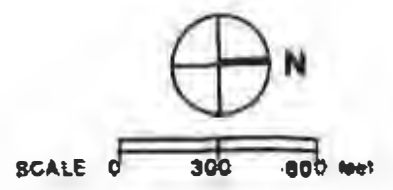
Gateway Specific Plan



OPPORTUNITIES & CONSTRAINTS MAP

LEGEND

- +** Opportunities
- Constraints
- 1, 2, 3... Reference Text
- NOT A PART



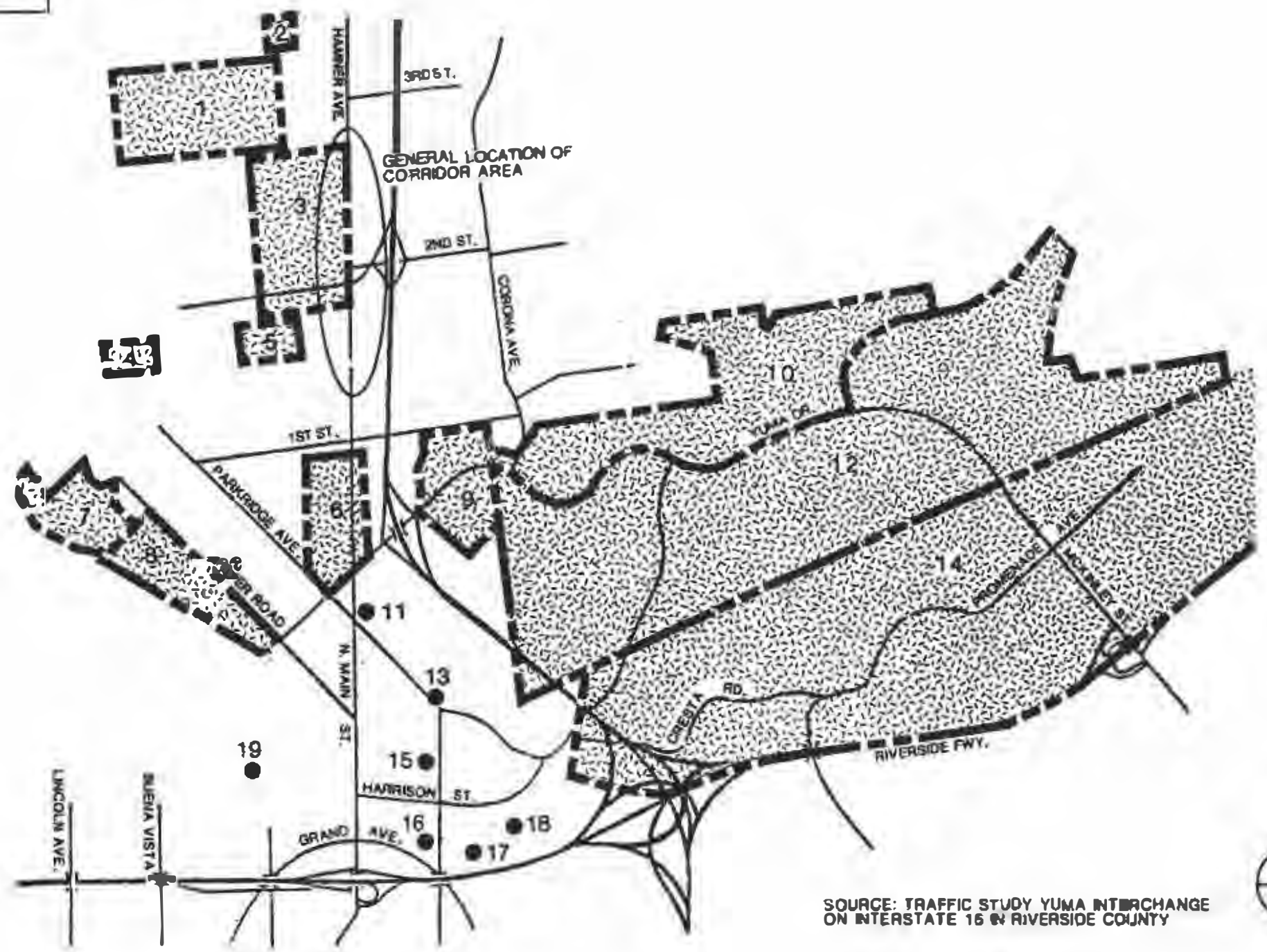
Lewis Homes, 100 units; and Woodcrest, 121 units. Families living in these homes will generate the need for additional commercial, retail and service oriented land uses, helping to support future development of the Project Area. Area housing developments will also generate short term employment opportunities and a long term labor force. Exhibit 9 shows the locations of all development projects within the vicinity of the Project Area which are presently planned or under construction. Table 4 quantifies the development shown on Exhibit 9. Exhibit 10 shows all major development projects within the I-15 Corridor that are planned or have been approved.

3. There are approximately 88 acres of vacant land within the Project Area; another 45 acres is made up of land parcels that are underutilized, or improperly developed or have deteriorating structures situated on them. Exhibit 11 shows these areas of development opportunity.
4. Norco Ranch owns approximately 87 acres generally located at the western border of the Project Area. This ownership represents approximately 30% of the total land, excluding public rights-of-way, within the Project Area. The owners of this facility are actively participating in the preparation of this Specific Plan.
5. The Norco Auto Mall is located immediately east of the Project Area, near the Second Street I-15 on/off ramps. The auto mall provides a regionally significant primary trip destination into the Area. People coming to the City to purchase automobiles may require other goods and services; appropriate spin-off goods and services can be developed on adjacent properties. These might include mechanic services, auto parts, tire dealerships, automotive accessories, body and paint shops.
6. U.S. Navy Weapons Fleet Analysis Center is located northwest of the Project Area and provides 800 full time jobs on-site and an additional 800 contract employee jobs off-site. This facility provides the opportunity for the development of additional retail, commercial and office space.
7. Construction of a Riverside Community College satellite campus is scheduled to begin in the immediate future. This facility will be located directly northwest of the Project Area and will be accessed by an extension of Third Street. Current estimates call for a total of 12,000 students to be enrolled at the campus. Students attending the campus will increase the demand for local services such as fast food, printing, entertainment, recreation, theatre, arts, sports, and other educational facilities.
8. Construction of a Yuma Drive on/off ramp is a short-term reality. City of Norco and Corona officials are presently working with Caltrans on the feasibility of this facility. When the on/off ramp is constructed it will make the southern end of the Project Area more easily accessible for commercial, retail, office and industrial development. This ramp will also facilitate access into new housing developments located in northeastern Corona along Yuma Drive.

Gateway Specific Plan



PLANNED DEVELOPMENT IN VICINITY OF I-15 CORRIDOR



SOURCE: TRAFFIC STUDY YUMA INTERCHANGE ON INTERSTATE 16 IN RIVERSIDE COUNTY

NOTE: REFER TO TABLE NO. 3 FOR DESCRIPTION OF AREAS OF DEVELOPMENT



NTS

Gateway Specific Plan



TABLE 4

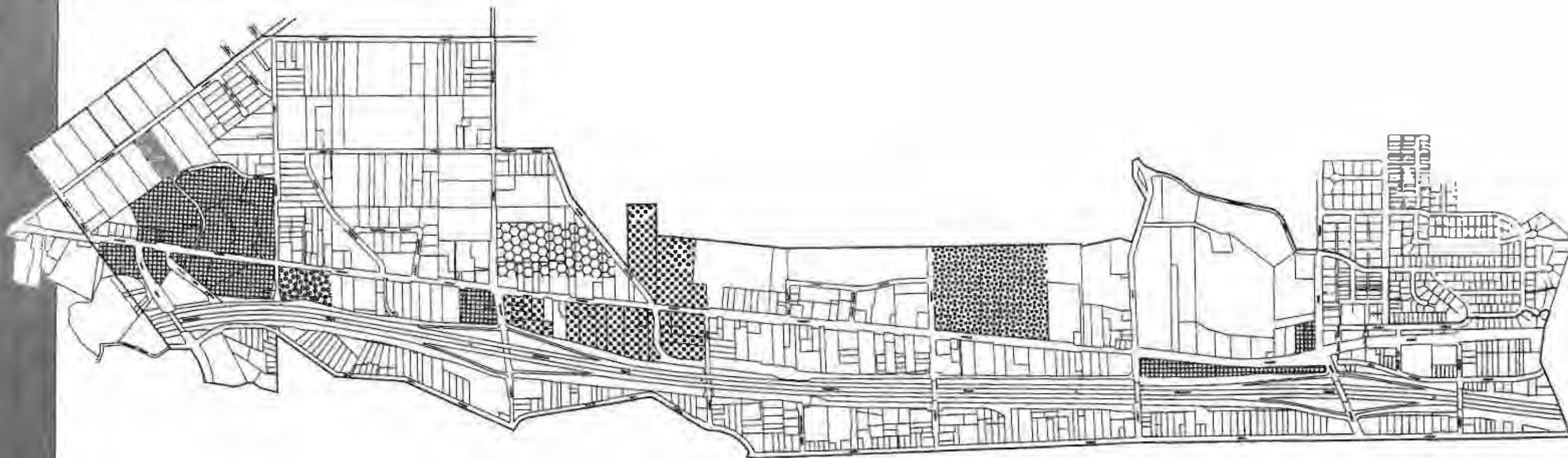
PLANNED DEVELOPMENT WITHIN VICINITY OF THE PROJECT AREA

<u>Location</u>	<u>Land Use</u>	<u>No. of Units</u>
1	Riverside City College	12,000 students
2	Residential - Townhomes	80 units
3	Norco Auto Mall	85 acres
4	Single-Family Residential	16 units
5	Single-Family Residential	0 units
6	Mixed Use Commercial	66 acres
7	Single-Family Residential	184 units
8	Mixed Density Residential	920 units
9	Single-Family Residential	100 units
10	Single-Family Residential	250 units
11	Apartments	88 units
12	Corona Ranch	3,410 units
13	Commercial	40 acres
14	Northeast Corona Specific Plan	
	Commercial	138 acres
	Single-Family Residential	6,100 units
15	Industrial	4.7 acres
16	Industrial	5.1 acres
17	Industrial	3.8 acres
18	Industrial	21 acres
19	Industrial	10 acres
TOTAL		
	Residential	11,234 units
	Commercial	244 acres
	Industrial	44.6 acres





Gateway Specific Plan



PROPOSED DEVELOPMENT WITHIN I-15 CORRIDOR



LEGEND

-  Auto Related Commercial
-  Commercial/Mixed Use
-  Office/Light Industrial
-  Highway Oriented Commercial

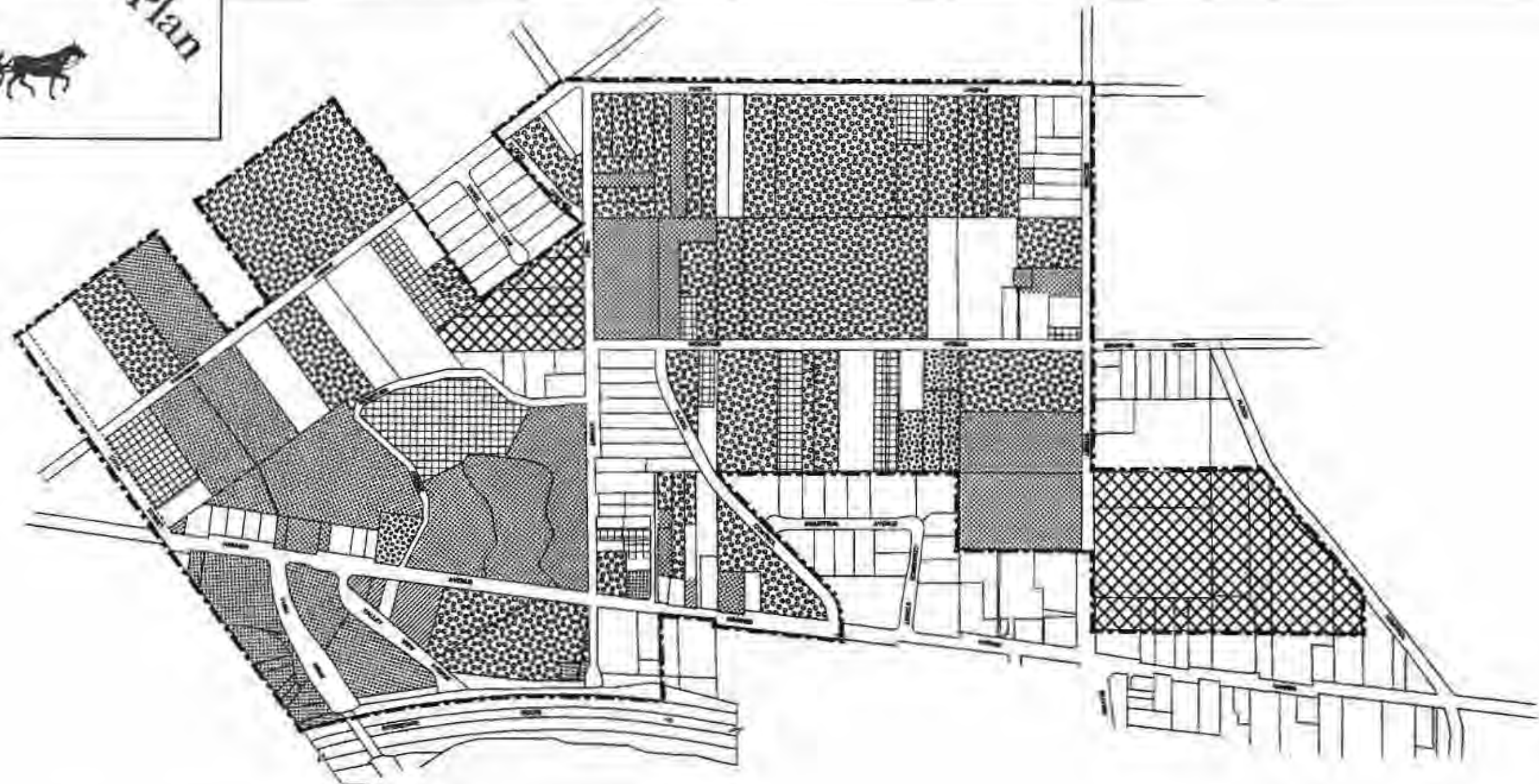
 Church

 Auto Mall



SCALE 0 600 1200 feet

Gateway Specific Plan



AREAS OF DEVELOPMENT OPPORTUNITY *

LEGEND

 Industrial *

 Residential *

 Commercial *

 NOT A PART

 Vacant

 Underutilized Parcels * +

* Parcels that have one or more deficient structures located on the property (shown by existing land use)

+ Based upon Consultant's Field Survey

* + Properties that are only partially developed or that do not demonstrate the use of property to its highest and best use



SCALE 0 300 600 feet

9. There is one existing on/off ramp connecting the I-15 to Hamner Avenue within the Project Area at Second Street. The ramp facilitates sub-regional access to local goods, services and commercial and industrial activities located within the Project Area. The ramps also provide the potential to establish goods and services to serve the travelers using I-15.
10. Existing residential neighborhoods both within and adjacent to the Corridor provide the purchasing power to support new development within the Corridor. Retail sales leakage has been estimated to be as high as 40% in the City of Norco, evidence that a balanced mix of goods and services is not being provided within the City.

2.5.2 Constraints

1. The health and safety of the general public traveling along Project Area roadways, particularly Hamner Avenue is a concern of City officials.

Cross traffic at intersections on First Street and Parkridge Avenue, as well as numerous minor streets, can be difficult. Intersection signalization is limited and visibility approaching First Street is poor because of the existing grade of Hamner Avenue.

2. Inadequate infrastructure within the Project Area is deterring development. In particular, drainage and flood control in the vicinity of Valley View Avenue, First Street and Mountain Avenue is inadequate. The Project engineer has identified areas within the Project Area that lack adequate water and sewer service. Lines need to be extended and/or capacities increased to accommodate future growth.
3. Land locked parcels, multiple ownership, and inadequate street access deter potential development. This is a particular problem around Mountain and Parkridge Avenues. Many streets are not wide enough, lack curbs, gutters and adequate traffic control devices.
4. Isolated residential parcels can make parcel consolidation difficult for economically viable commercial/industrial development.
5. Air and noise pollution impacts from Hamner Avenue and I-15 pose limitations for residential development in the Project Area.
6. Existing commercial centers along Main Street just south of the Norco City limits may hinder demand for new commercial/retail development in the short term. Market demand and absorption rates must be considered to lessen the possibility of oversaturation of like goods and services.

7. Traffic conditions along Hamner Avenue may worsen as growth of population and new development occurs. This may be particularly critical if the Yuma Drive/I-15 on/off ramp is not constructed and other street/circulation improvements are not made.
8. Adjacent property owners have indicated that existing traffic conditions along Parkridge Avenue and First Street are poor. These streets are too narrow and lack adequate traffic control facilities. The Parkridge Avenue/Pacific Avenue/First Street/Lincoln Avenue intersection demonstrates particular problems. Increased development within the Project Area will negatively impact these roadways unless appropriate improvements are made.

2.6 PROJECT AREA DESIGN CONCEPTS

This component of the Specific Plan establishes parameters with which the design character for the entire project Area can be created. To do so, a number of design concepts have been developed. The Project Area Design Plan is primarily focused upon the creation of aesthetic character within the parameters already established by the City of Norco. Its purpose is to further promote a visual environment that evokes a distinctive and unifying image which is unique to Norco. It is important to have a design statement for the Gateway Project Area that orchestrates and promotes a cohesive design concept design image for both the immediate Project Area and the larger I-15 Corridor Area.

The Project Area Design Concept is comprised of a hierarchy of urban design components. The intent of the Project Area design concept is to provide a broad-brushed overview of general design components which ultimately will lead to the development of specific design guidelines.

The overall Project Area design concept consists of the following components:

- o Activity Nodes
- o Architectural concept
- o Landscape architectural concept
- o Circulation concept

Activity Nodes

1. Vehicular Activity Nodes

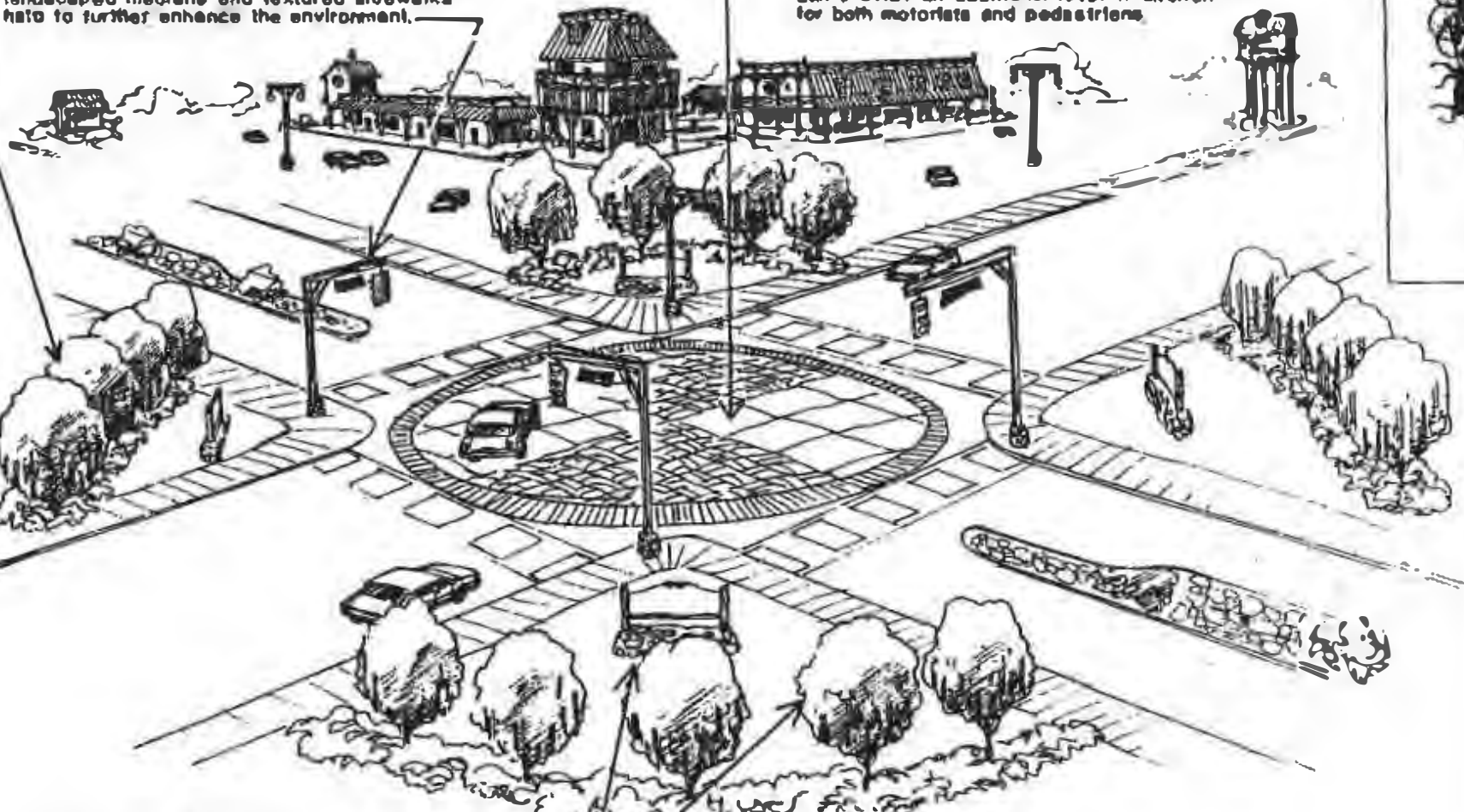
As shown in Exhibit 11A, these activity nodes will be oriented to people in transit. Aside from being attractive focal points serving to provide orientation for drivers, they will incorporate directional and locational signage. These nodes will incorporate appropriate building setbacks to allow for landscaping, entry monuments, special paving materials, patterns, etc.

Gateway Specific Plan



Major intersections can be visually enhanced through the use of decorative paving materials such as stamped concrete, pebble stones or brick. Eye catching patterns, as depicted here, can provide an additional level of caution for both motorists and pedestrians.

Other streetscape elements such as decorative themed street lights and signs, as well as landscaped medians and textured sidewalks help to further enhance the environment.



Landscaping elements and monument signs contribute to an aesthetically pleasing intersection. Trees are set back to allow clear lines of sight for motorists and signs can direct motorists to a particular destination.

VEHICULAR ACTIVITY NODE CONCEPT
(CONCEPTUAL ONLY)

EXHIBIT 11A

2. Pedestrian Activity Nodes

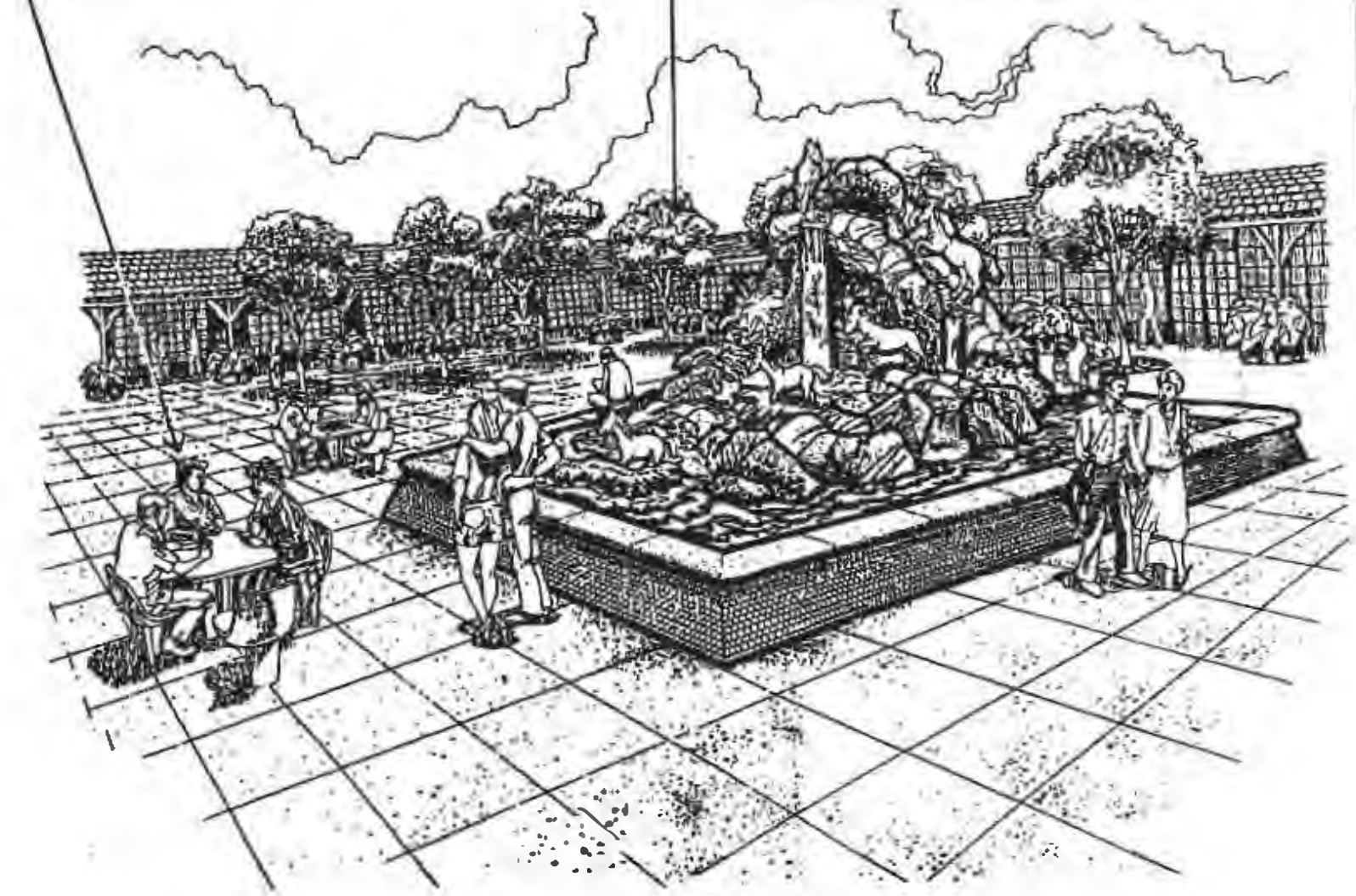
The Project Area is one that will be accessed primarily by automobiles. People will generally arrive by automobile at one of the commercial, office or industrial areas with a particular purpose in mind. Because of its vehicular orientation, and because the area does not contain a substantial residential element, the Gateway Project Area is not intended to function as a highly pedestrian oriented environment. However, each area of primary destination should provide for pedestrian amenities at the micro scale. For example, within the light industrial zone, each development should provide pedestrian areas designed for employees and visitors; areas should be provided for outside eating, passive exercise or for quiet and relaxation.

As shown in Exhibit 11B, each activity node will incorporate pedestrian amenities that might include benches, unique paving patterns/materials, landscaped areas, shade trees, fountains and turf areas.



Pedestrian oriented and user friendly spaces are successful when enjoyable activities are introduced.

An exciting outdoor retail space and rewarding shopping and pedestrian experience can be achieved through the use of active water features such as a fountain, sculpture, bench seating and paving materials that suggest a particular theme.



48

PEDESTRIAN ACTIVITY NODE
(CONCEPTUAL ONLY)

EXHIBIT 11B

Architcctural Concept

The overall architectural concept is characterized by architectural elements which complement the City of Norco's heritage as shown in Exhibits 12, 13A and 13B, the primary theme will be western/early California. However, a diversity of architectural styles will be allowed within the Project Area in an effort to promote a richness and diversity of architectural character. The promotion of architectural diversity will occur primarily within the industrial zone, while the western/early Californian theme will be most dominant within the Commercial and Office Park Districts.

Landscape Architectural Concept

The Landscape Architectural Concept for the Project Area proposes the use of a limited trees palette within designated areas, designed to reinforce both an urban and rural design character. Tree plantings will border all activity nodes and will define these nodes as focal points and areas of special interest. Informal plantings will provide a casual backdrop to these areas. The overall landscape concept as illustrated in Exhibit 14 shall incorporate a mixture of landscape, hardscape and signage features using native materials to the greatest extent possible. Plant species shall be drought tolerant to the greatest degree possible.

The circulation concept for the Gateway Specific Plan Project Area includes provision for landscaped street medians, as shown in Exhibit 15, along Hamner Avenue. The landscaped medians are intended to create an improved aesthetic quality on the Project Area's major roadways.

Gateway Specific Plan

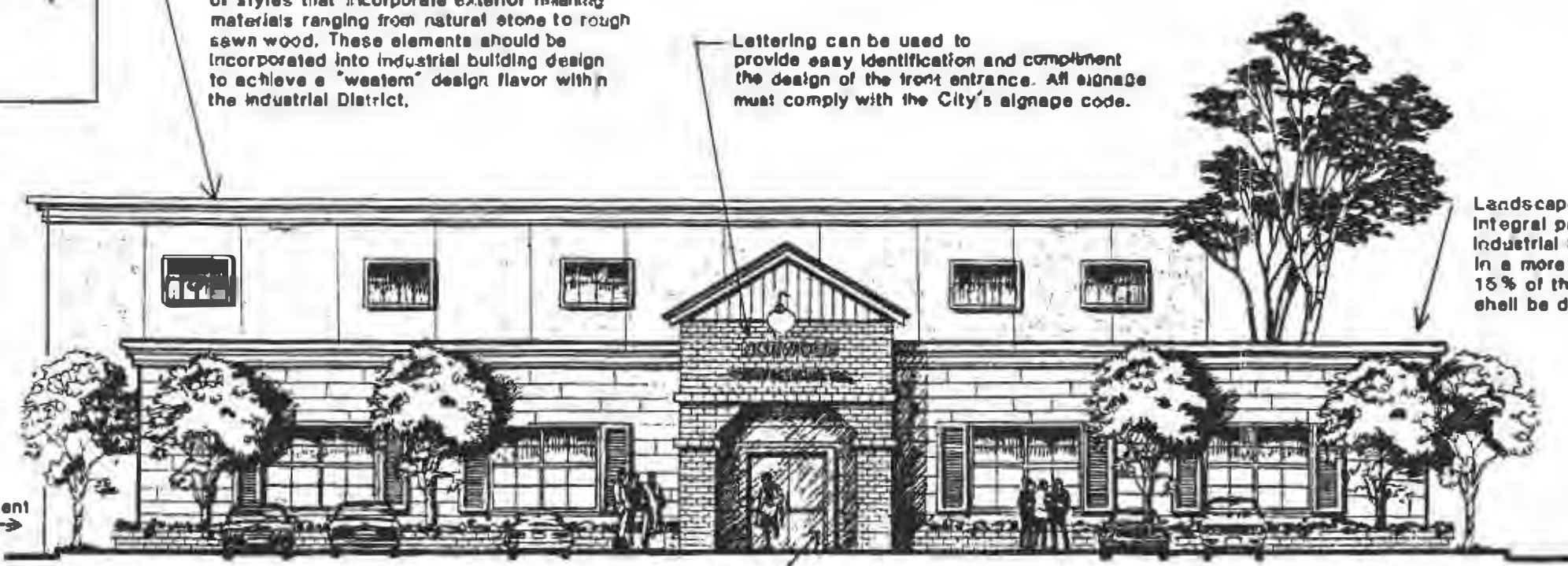


As with new commercial buildings, industrial buildings are designed in an increasing variety of styles that incorporate exterior finishing materials ranging from natural stone to rough sawn wood. These elements should be incorporated into industrial building design to achieve a "western" design flavor with the Industrial District.

Lettering can be used to provide easy identification and complement the design of the front entrance. All signage must comply with the City's signage code.

Landscape elements are an important and integral part of any development within the Industrial District, helping to set the buildi in a more natural setting. A minimum of 15% of the total of any development site shall be devoted to landscaping.

Today's increasingly competitive industrial building market has resulted in more architecturally innovative and sophisticated developments. The Gateway Specific Plan places a strong emphasis on design standards to effectively compete with surrounding communities in attracting industrial development to the Project Area.



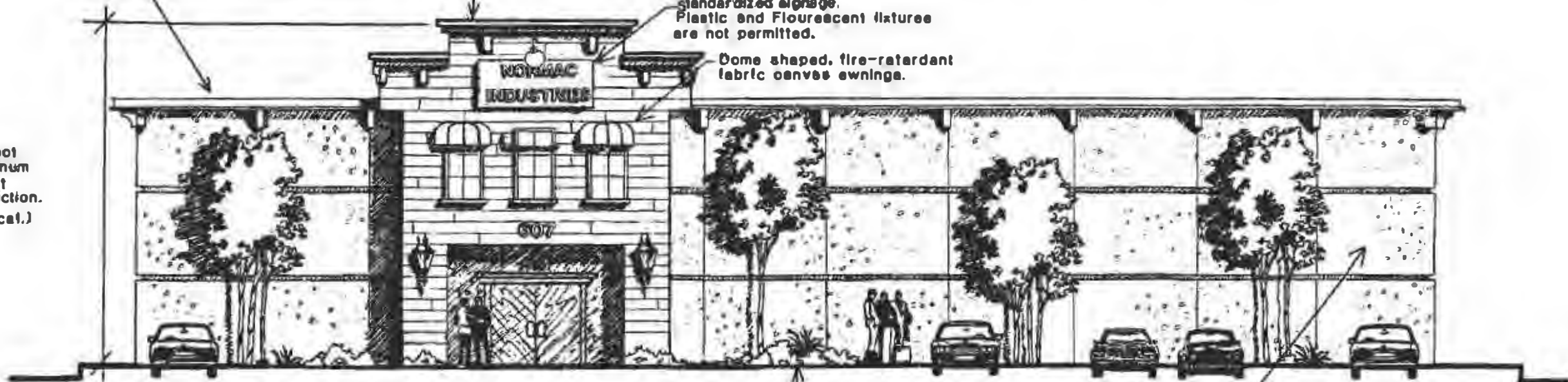
Grand entrances and functioning windows are elements that can be effectively used to capture the appropriate design character within the Industrial District. Cornices, pitched roofs and skylights can also be used.

Design elements that can be used to achieve a Western flavor consistent with the objectives of Norco are illustrated here.

standardized signage. Plastic and fluorescent fixtures are not permitted.

Dome shaped, fire-retardant fabric canvas awnings.

40 foot maximum height restriction. (Typical.)



Minimum of 15% of any development site shall be devoted to landscaping.

Suggested facade finishes can include pebble stone, aggregate concrete, textured stucco or red brick veneer.

CONCEPTUAL ARCHITECTURAL DESIGN ELEMENTS FOR INDUSTRIAL BUILDINGS

Gateway Specific Plan



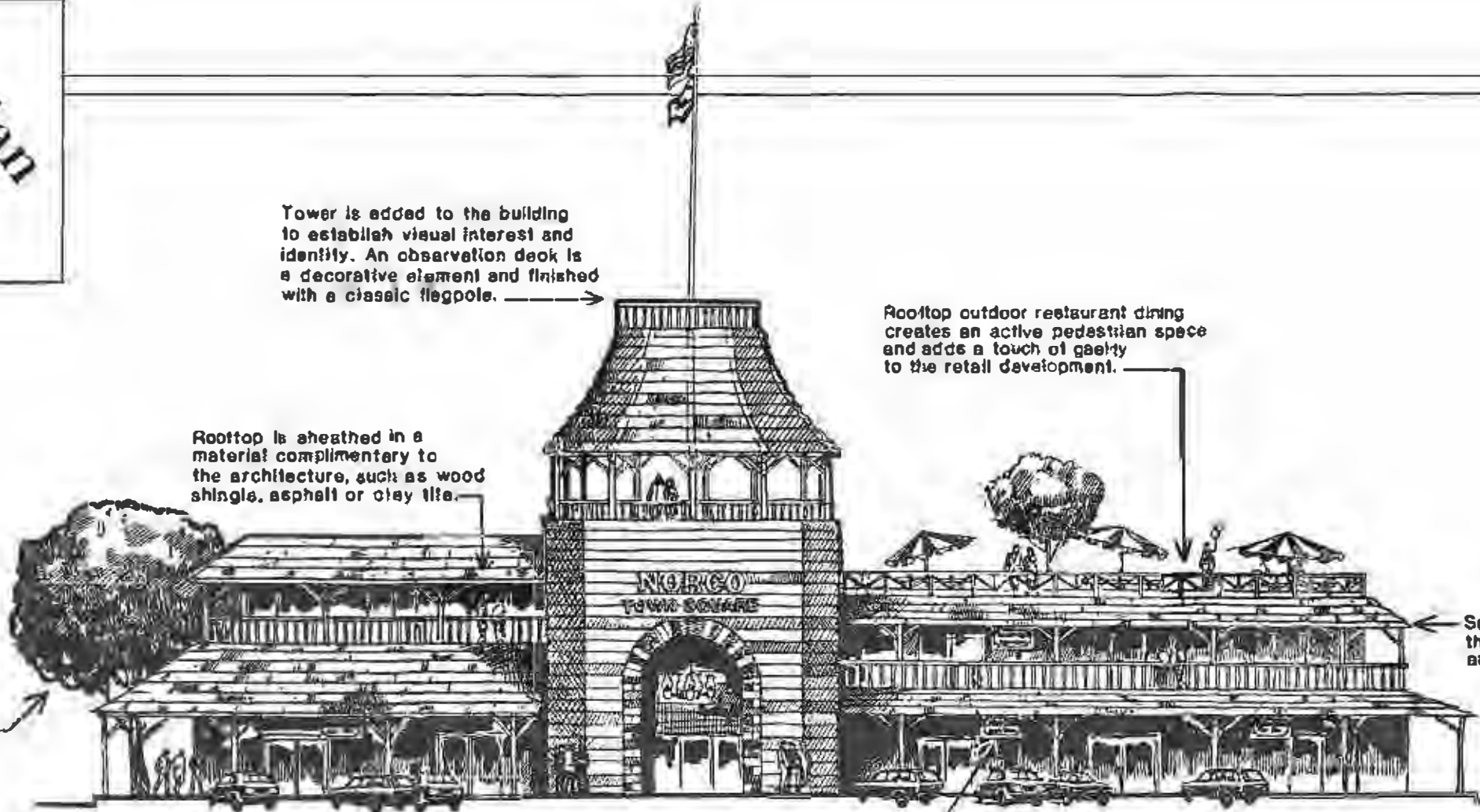
Tower is added to the building to establish visual interest and identity. An observation deck is a decorative element and finished with a classic flagpole.

Rooftop outdoor restaurant dining creates an active pedestrian space and adds a touch of gaiety to the retail development.

Rooftop is sheathed in a material complimentary to the architecture, such as wood shingle, asphalt or clay tile.

Landscape elements should be selected to enhance the building and should not compete with either the scale or architecture of the building.

Second floor balcony reinforces the recommended western architectural motif.

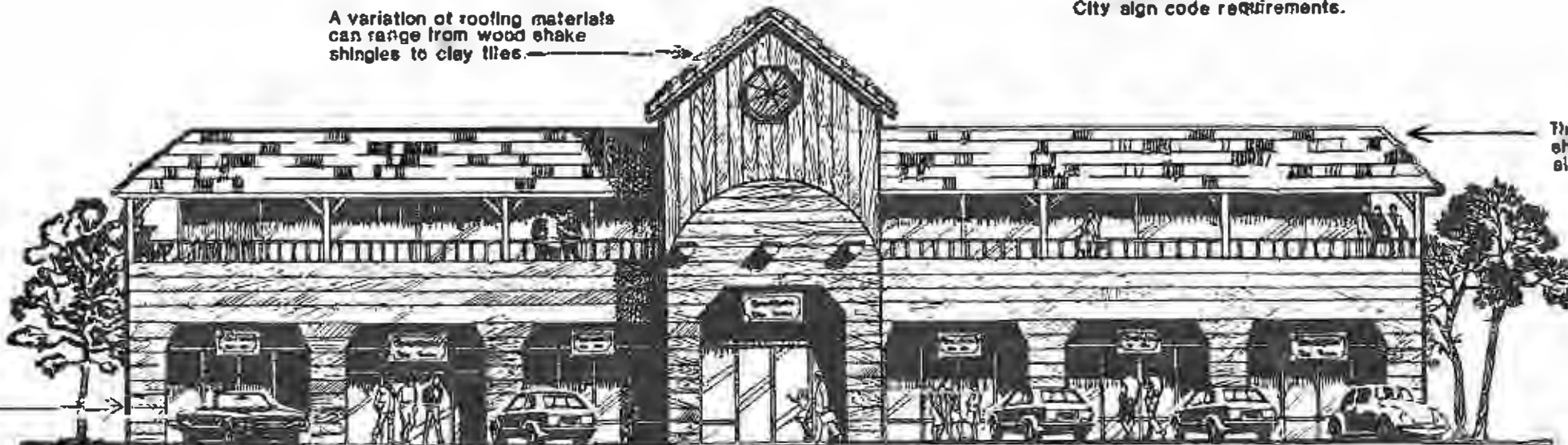


Storefront signage is located in areas that are visually identifiable and which enhance the facade of the building. All signage shall comply with City sign code requirements.

A variation of roofing materials can range from wood shake shingles to clay tiles.

The height of multi-story buildings should not overwhelm the development site or surrounding environment.

Exterior sheathing materials such as wood, natural stone or brick should be used to reinforce the architectural theme.



CONCEPTUAL ARCHITECTURAL DESIGN ELEMENTS FOR COMMERCIAL/OFFICE BUILDINGS

Gateway Specific Plan



Second floor balconies lend a casual yet functional design feature to the building.



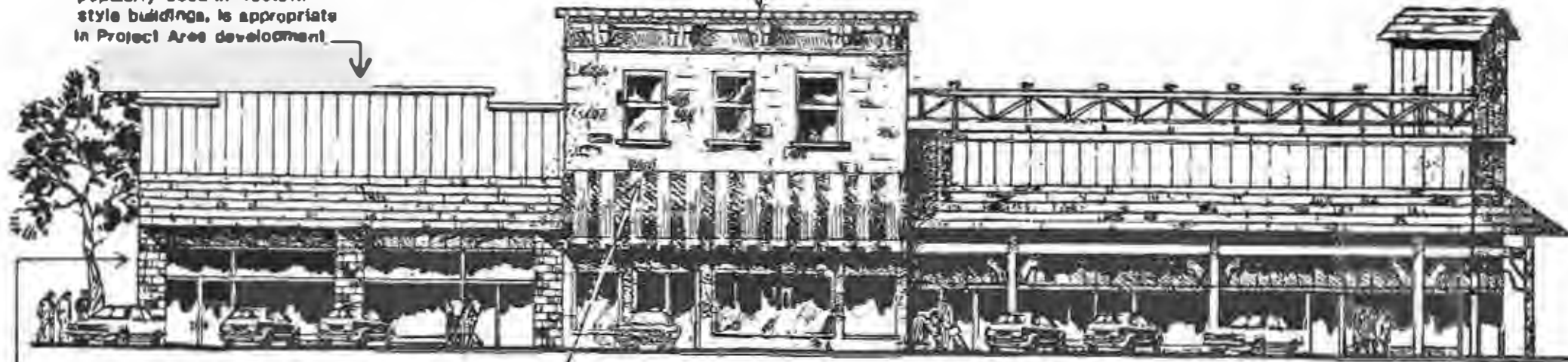
A rooftop "false front" design element imparts a Western look to the face of this building reminiscent of the old west architectural genre.

The use of trees, shrubs and other landscaping as design elements in a development are important features to soften the angularity and bulkiness of the hardscape.

A variation of a "Western" style commercial building designed to enhance the Norco country atmosphere.

A false front, such as was popularly used in western style buildings, is appropriate in Project Area development.

Cornice detailing imparts a classic, elegant look to any building. The use of cornices is encouraged wherever appropriate.



A combination of several finishing materials such as brick, wood siding and shingles can be used for contrast such as is depicted above.

The use of decorative storefront awnings should be used for contrast, color, aesthetics and as a sunshade device.

A covered colonnade adds a "Western" touch to this building's design also acting as a sunshade device.

CONCEPTUAL ARCHITECTURAL DESIGN ELEMENTS FOR COMMERCIAL/OFFICE BUILDINGS

Gateway Specific Plan



Formal tree groupings.
See Plant Palette Appendix B.
All intersection tree specimens
should be flowering.

PLEASE REFERENCE PG. 70 (EXHIBIT 23A)
FOR CHANGE IN SIGNAGE LAYOUT.

Mass shrub plantings to create understory.

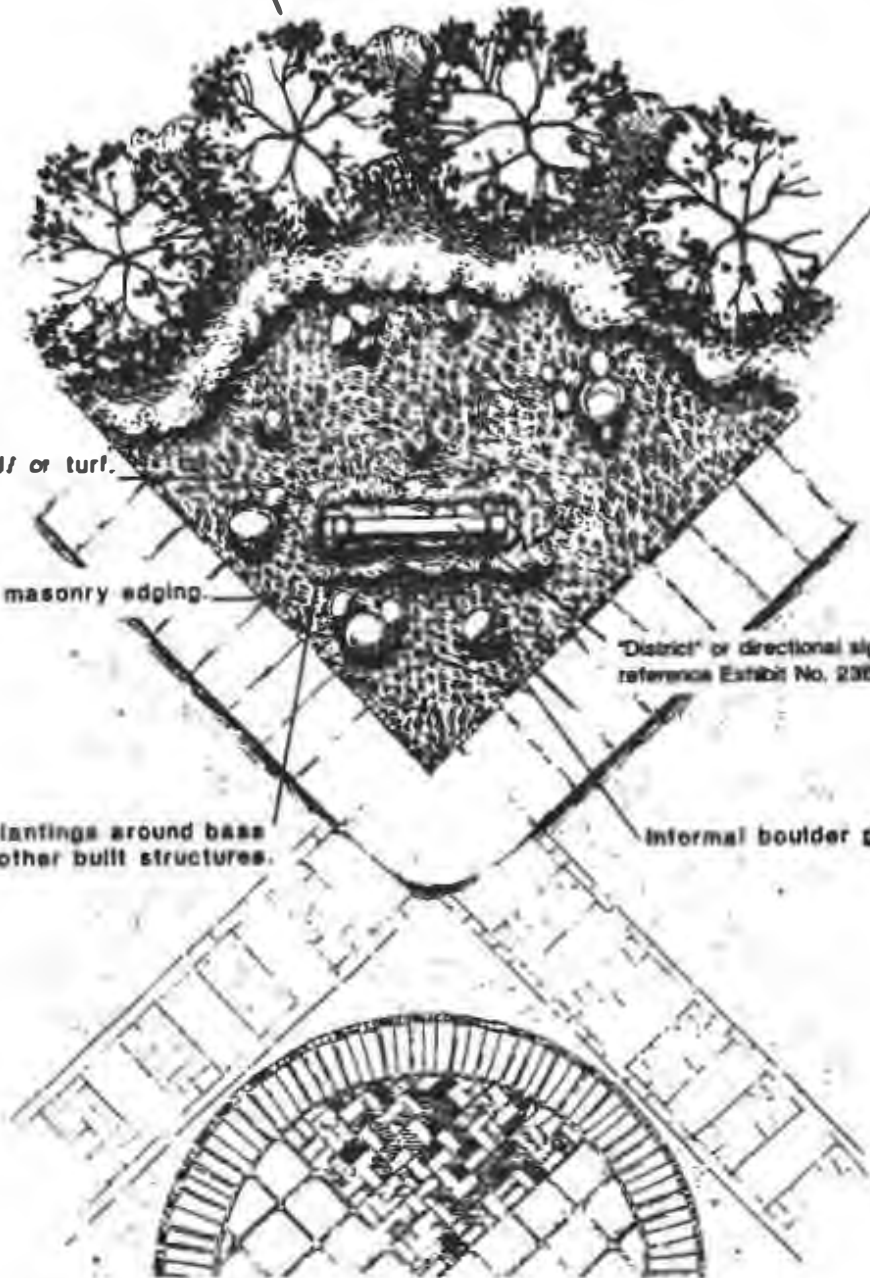
Ground cover and/ or turf.

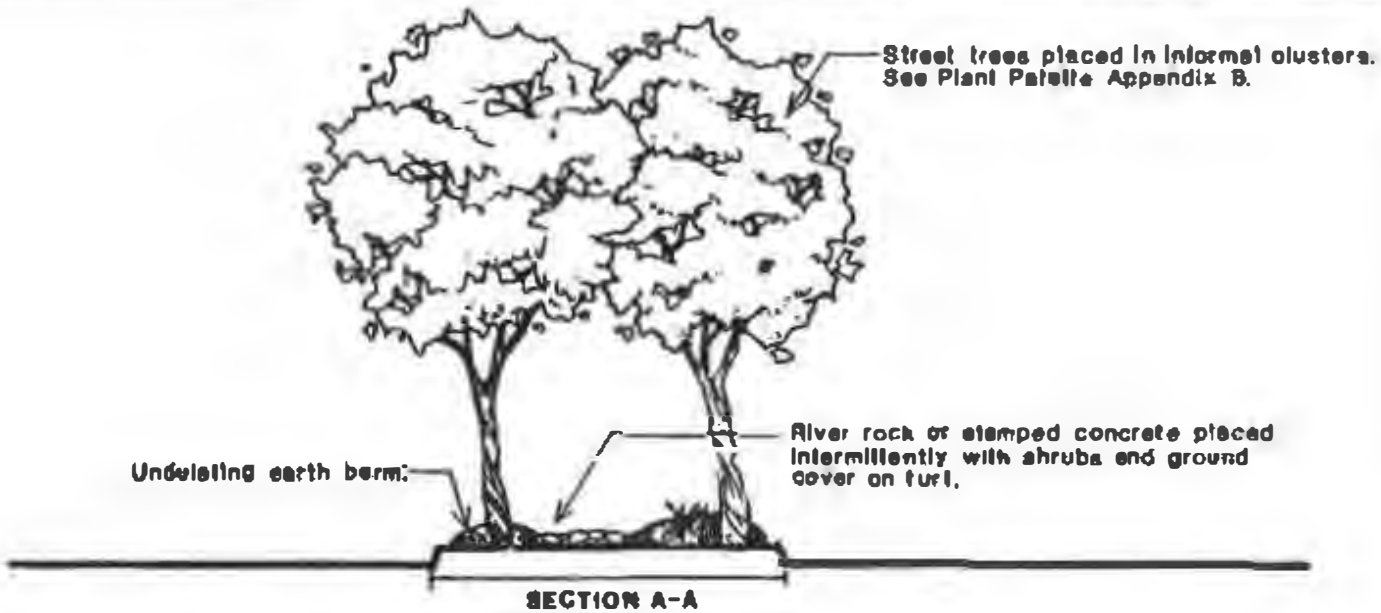
River rock or masonry edging.

"District" or directional signage
reference Exhibit No. 23B

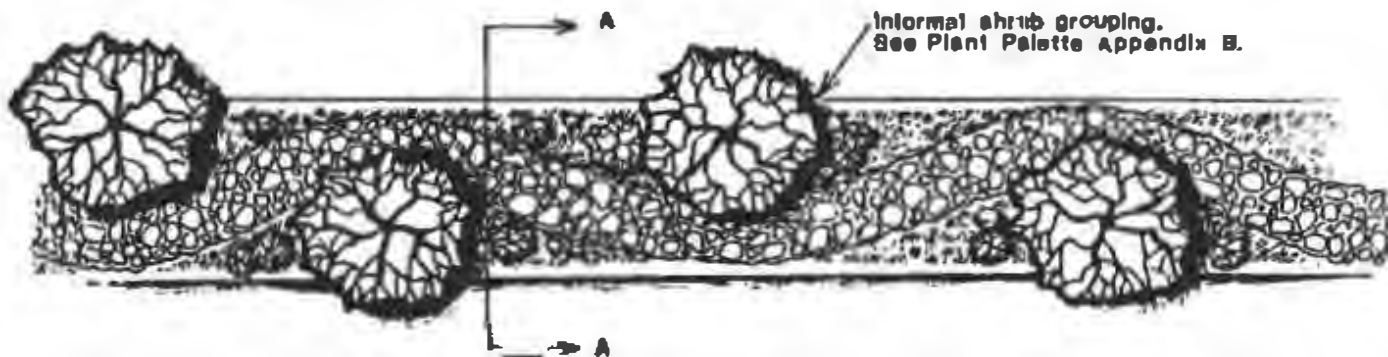
Mass shrub plantings around base
of signs and other built structures.

Informal boulder groupings.





**CONCEPTUAL LANDSCAPED MEDIAN ISLAND SECTION
(HAMNER AVENUE ONLY)**



**CONCEPTUAL LANDSCAPED MEDIAN ISLAND
(HAMNER AVENUE ONLY)**

EXHIBIT 15

2.7 LAND USE PLAN

2.7.1 Land Use Concept

The Land Use Concept is based on the premise of dividing the Project Area into four major land use "Districts"; 1) Commercial, 2) Office Park, 3) Industrial and 4) Residential. Districts were determined by analyzing such factors as existing Project Area development patterns, market absorption rates, adjacent development patterns, circulation routes, infrastructure capacity, geology and relationship to the I-15. Exhibit 16 illustrates the location of these four primary districts.

2.7.2 Land Use Categories

As shown in Exhibit 17, contained in the map pocket at the back of this Plan, the Conceptual Development Plan demonstrates land uses that provide a rich texture and diversity to the community as a whole while maximizing the Project Area's relationship to the I-15 and other significant roadways. Suggested Land Use Categories consist of the following designations:

2.7.2.1 Residential

Residential uses in the Gateway Specific Plan area will be designated for very low density (0-2 units/acre). Lot size will be a minimum of 20,000 sq. ft. (A-1-20) or a minimum of 40,000 square feet (A-1-40) depending on the location of the proposed residential districts.

2.7.2.2 Commercial

The Gateway Specific Plan area should contain a variety of commercial uses designed to promote diverse and unique shopping environments. In general, commercial land uses within the Project Area will range in size, type and character, based upon their respective relationship to location of the I-15, other major roadways and prevailing market conditions.

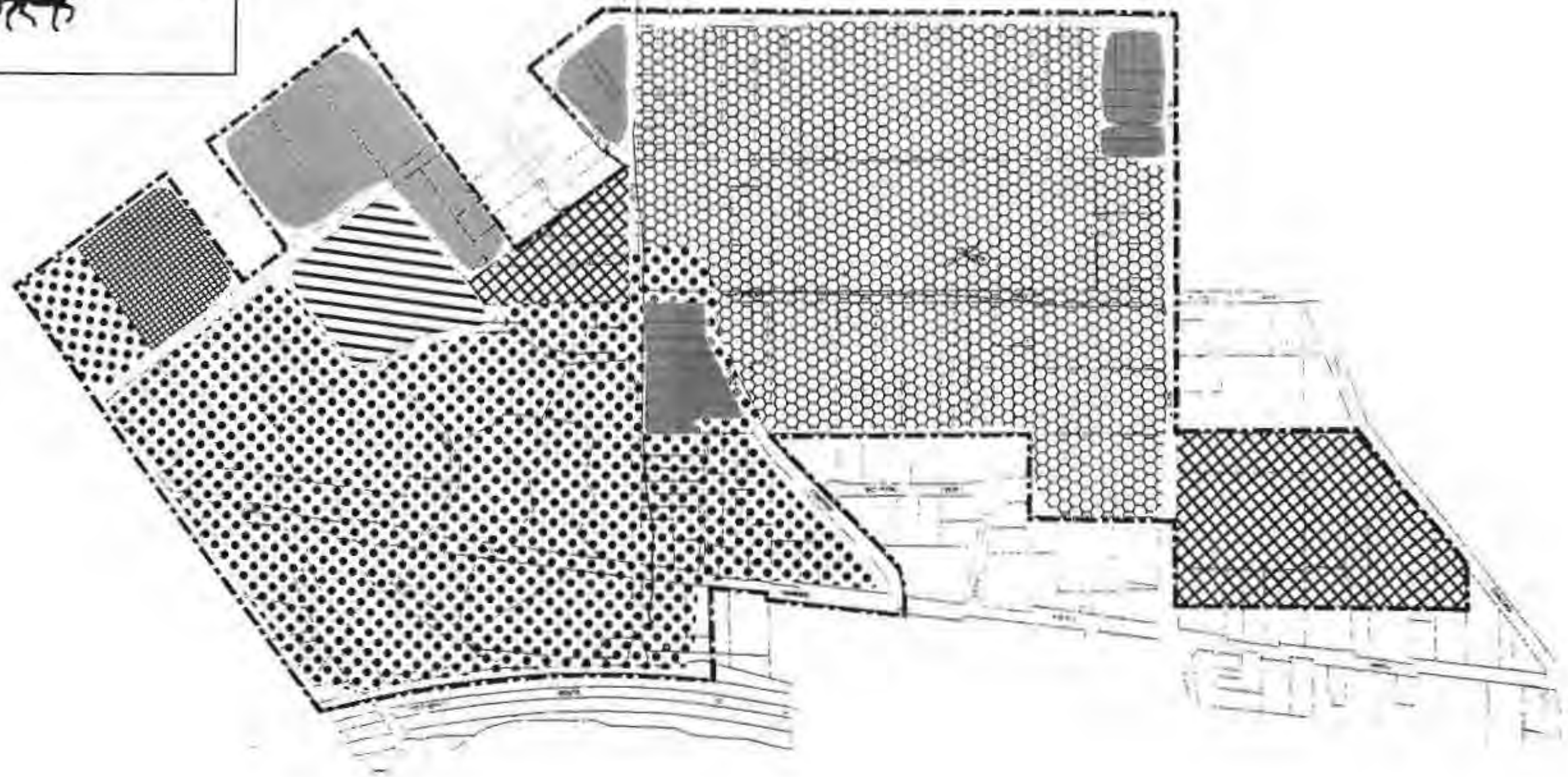
2.7.2.3 Office Park (OFF)

The Office Park District is primarily located southwest of Parkridge Avenue. The OFF designation will allow office development that will include garden and single occupant facilities, as well as ancillary uses such as restaurants, cafes, copy shops, etc. This classification is closely related to the City's existing C-0 zoning designation.

2.7.2.4 Industrial (I)

This District overlaps the south westerly most portion of the Project Area, that portion of the Project Area farthest from I-15 and Hamner Avenue. While this area will depend on good access to primary access routes such as the I-15, Hamner Avenue and Lincoln Street, the need for high visibility and quick access is not critical to the success of the District.

Gateway Specific Plan



CONCEPTUAL LAND USE DISTRICTS

LEGEND

Commercial District

ACRES

96.6

Industrial District

84.3

Office Park District

8.4

Residential

ACRES

18.1

Existing Church

12.9

NOT A PART

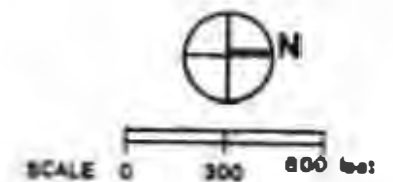


EXHIBIT 17

CONCEPTUAL DEVELOPMENT PLAN
(Please refer to map pocket
at back of this document)

The Industrial designation allows light industrial, research and development, and office uses. Permitted industrial uses include light manufacturing, custom manufacturing, assembly, fabrication and wholesaling. Permitted office uses are those necessary to support the administrative function of the primary permitted uses. This industrial classification is closely related to the city's existing C-4 and M-1 and industrial park zoning designations.

2.7.2.5 Existing Land Uses

A large church facility that is used primarily on weekends is located on the northeast side of Parkridge Avenue. This facility will continue to operate under the city's existing A-1-20/M-1 zone.

The Norco Ranch facility presently operates its administrative and egg processing facilities on an area of land located near the corner of Mountain Avenue and Second Street within the Plan's Industrial District. The facility will remain in place and is compatible with the Industrial designation.

Located at the corner of Second Street and Pacific Avenue is a cluster of residential properties. These homes will remain within a residential designation.

Located at the most southern tip of the Project Area is a newly constructed self-storage facility. This facility will remain because it is compatible with surrounding land uses.

There are 92 residential structures on approximately 107.13 acres presently existing in the Project Area.

Please refer to Section 2.4, Existing Conditions, for a complete analysis of existing conditions within the Project Area.

Project Review and Plan Consistency

The Plan is intended to allow for substantial flexibility compared with typical zoning restrictions, yet it does provide certain direction that is critical to successful long term development of the Project Area.

In determining whether or not a proposed project is consistent with the Specific Plan, the following sequence of considerations should be followed. Successful responsiveness of the project at each step in the sequence allows consideration of the following step. Failure on one or more counts at each step will require project modification accordingly.

1. The initial "screen" for plan consistency is in compliance with the development regulations in Section 4.0.

2. The second consistency factor is the extent to which the design guidelines are used in actual project design.
3. The third area of consideration is the extent to which the various incentives are appropriately used.
4. Finally, in the case of needed interpretation, the extent to which the development proposal responds to the Plan's Goals, Objectives and Policies will be evaluated.

2.8 CIRCULATION PLAN

The Project Area's proposed circulation system is shown conceptually on Exhibit 18. This upgraded system, while not dictating specific project roadway layouts, incorporates the existing roadway pattern to the greatest degree possible, but, incorporates numerous additions and improvements to that system. The cited improvements and additions will serve to mitigate existing circulation/traffic system deficiencies while allowing for future development to occur without significantly decreasing existing roadway and intersection service levels. The circulation plan will allow the City to achieve the following related transportation goals: 1) promotion of the conservation of energy and land; 2) acquisition and development of public roadways will be made with regard to the medium and long term needs of the City; and 3) reduction of through traffic on residential streets. The circulation plan includes roadway improvements that would be constructed as on site circulation improvements. These roadways are subject to design revisions based upon specific site development plans which will require City approval.

Generally, the proposed circulation system is intended to provide:

1. Improved access to all Project Area land uses;
2. Improved access to both the I-15 and 91 Freeway;
3. A more efficient circulation system with improved levels of service along major roadways and intersections;
4. A safer circulation system;
5. A median installation program that will create more aesthetically pleasing roadway;
6. Roadway/circulation system improvements designed to facilitate increased development within the Project Area and the surrounding locale while minimizing negative impacts to existing levels of service.

2.8.1 Circulation System Modifications

Existing conditions and future trends indicate that significant increases in vehicular traffic will occur within the Project Area. In an effort to minimize possible future traffic liabilities, the following traffic/circulation related actions, as shown in Exhibit 18 are necessary:

Center Median Program

Based upon traffic analysis, raised landscaped center medians are recommended along Hamner Avenue. The following benefits can be realized with the implementation of a raised landscaped center median program:

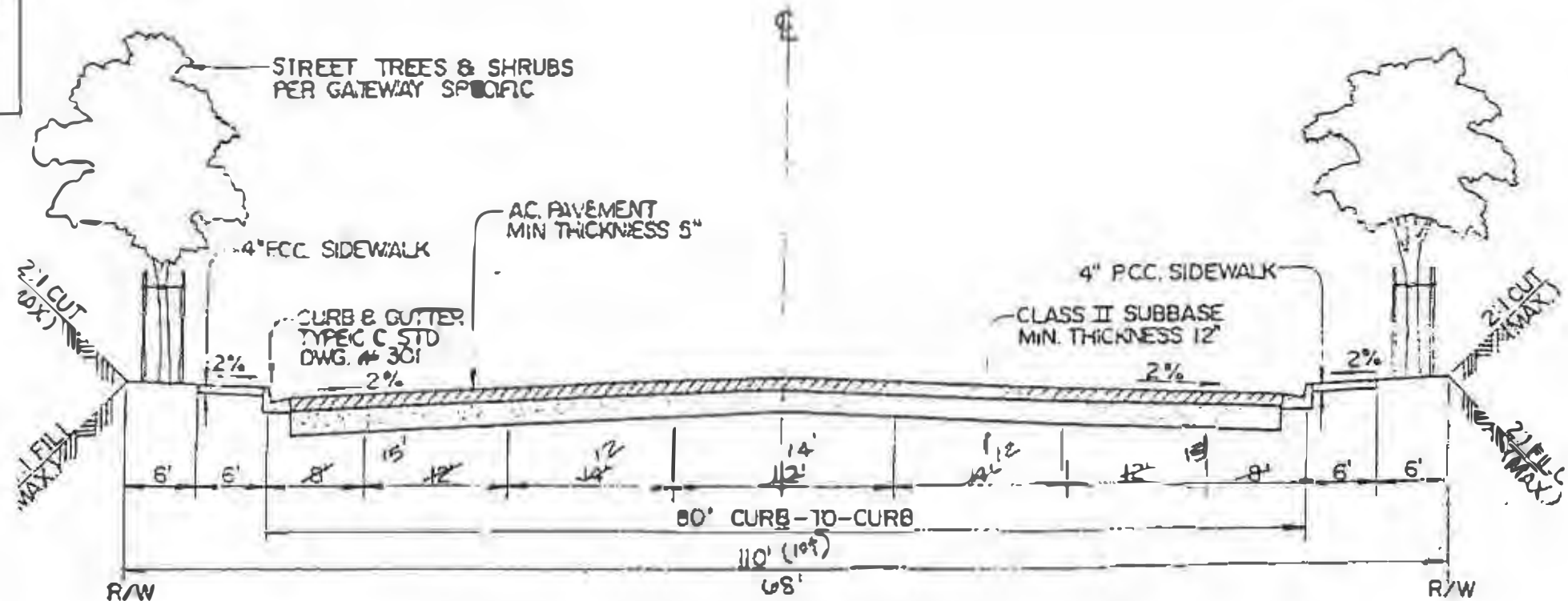
- o Increased vehicular capacity
- o Increased vehicular/pedestrian safety
- o Increased vehicular mobility
- o Increased vehicular level of service
- o Improved Project Area appearance

The median program (see Exhibit 17 for median location) is designed to maintain the highest level of service possible on Hamner Avenue. The enhancement of traffic flow and safety through the attainment of the lowest possible level of conflict among vehicles is essential to achieving the necessary service level.

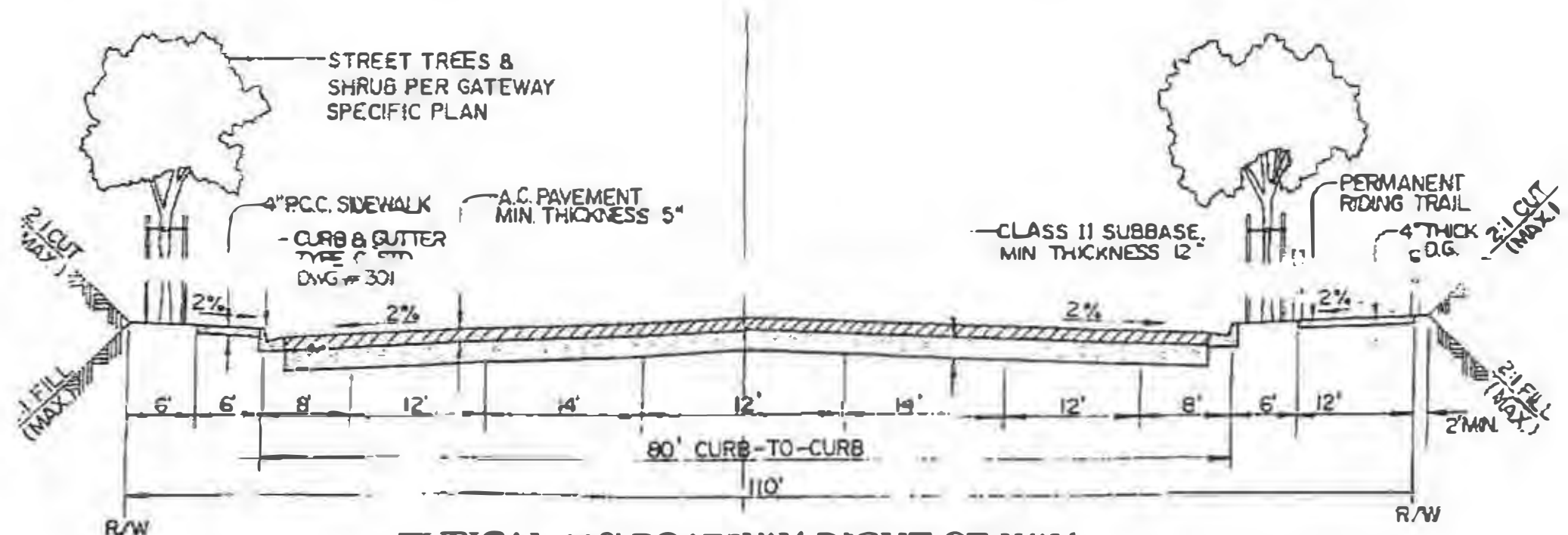
In order to preserve the level of service along Hamner Avenue, access to commercial properties should be primarily via the median breaks at signalized intersections. It will be necessary to accept an increased number of U-turns at these intersections. Additional median breaks, in addition to those at signalized intersections, will be considered in accordance with local business needs and City traffic standards. These breaks and emergency access across the medians will be subject to a detailed traffic safety analysis subject to review and approval by the City Engineer.

Exhibits 19 and 19A show typical roadway sections for Project Area roadways.

Gateway Specific Plan



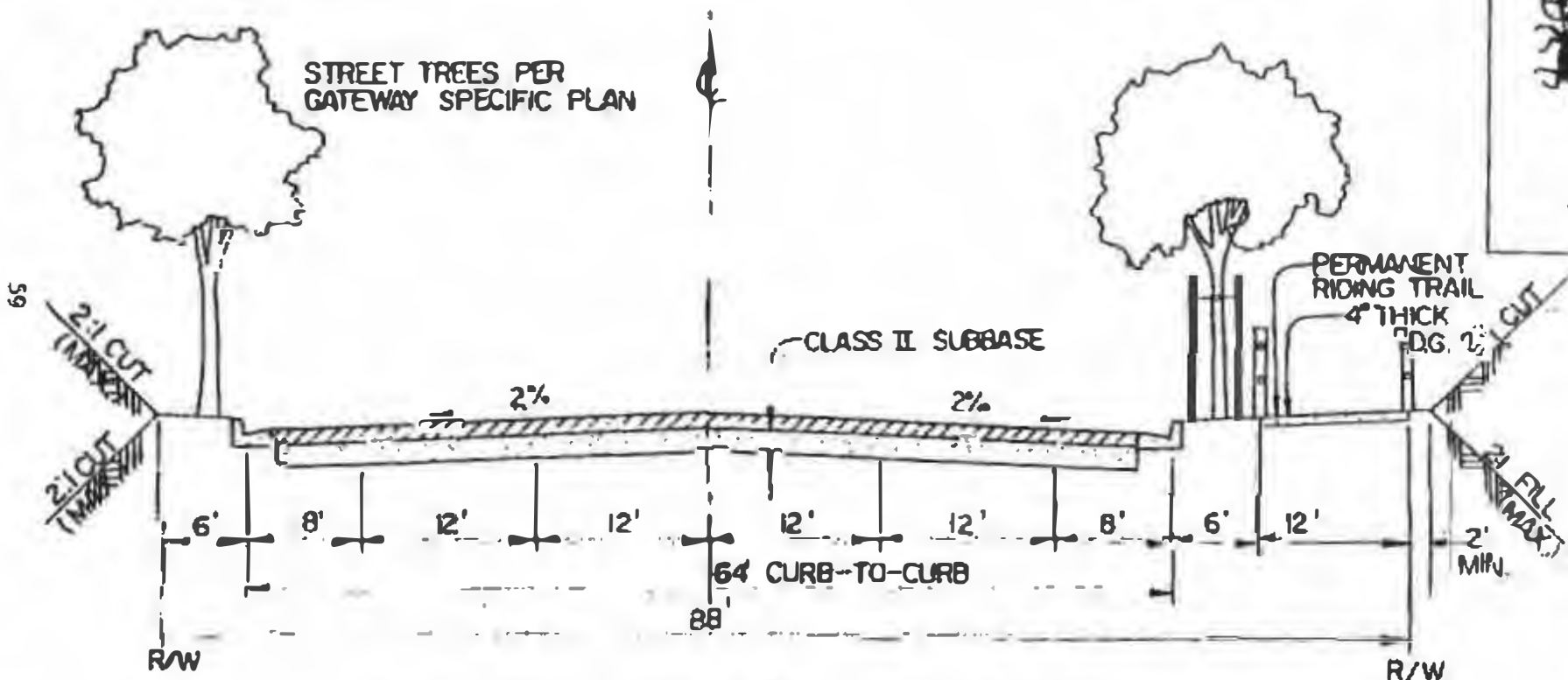
TYPICAL 68' ROADWAY RIGHT-OF-WAY



TYPICAL 110' ROADWAY RIGHT-OF-WAY



Gateway Specific Plan

TYPICAL 88' ROADWAY RIGHT-OF-WAY

Parkridge Widening

Presently, Parkridge Avenue is a two-lane roadway with a paved roadway width of 24 feet. Parkridge Avenue runs southeast/northwest with that portion between Cota Street and the Lincoln Street/Parkridge Avenue/First Street/Pacific Avenue intersection being within the Project Area. The roadway is too narrow to accommodate an appropriate level of service and pedestrian amenities do not exist. As part of this Plan, Parkridge Avenue will be widened to an 88 foot right-of-way to improve its level of service and to facilitate access to the 91 Freeway via Lincoln Street.

First Street Widening

Presently, First Street is a two lane roadway with a paved roadway width of 20 feet. First Street runs east/west and lies totally within the Project Area. The roadway is too narrow to accommodate planned land uses and increased service demands caused by future development projects. As part of this Plan, First Street will be widened to a 88 foot right-of-way to improve its level of service and to facilitate access from Hamner and Mountain Avenues to Project Area commercial and industrial land uses and to facilitate access to the 91 Freeway via Lincoln Street.

Second Street and Mountain Avenue Widening

Second Street provides direct I-15 access into the Project Area. Second Street presently exists as a two-lane roadway through most of the Project Area; the eastern most portion has been widened to include turn lanes at Hamner Avenue. As a part of this Plan, Second Street will be widened to a 88 foot right-of-way for the primary purpose of improving Project Area access to the I-15. To deter high volumes of through traffic from continuing west on Second Street into residential areas along the western portion of Second Street and Pacific Avenue, the width of Second Street will be reduced to a 66 foot right-of-way beginning just east of the new street located midway between Mountain and Pacific Avenues.

To facilitate access to commercial and industrial land uses adjacent to Mountain Avenue and to facilitate access to the 91 Freeway via Lincoln Street, Mountain Avenue will be widened to a new right-of-way width of 88 feet.

Cul-de-Sac of Pacific Avenue

Pacific Avenue presently exists as a two-lane, north/south running roadway. Pacific Avenue proceeds northward beyond the Project Area, but its southern terminus occurs within the boundaries of the Project Area at its intersection with Lincoln Street/Parkridge Avenue/First Street. As a part of this Plan, Pacific Avenue will be cul-de-saced just north of its intersection with First Street/Parkridge Avenue/Lincoln Street. This improvement will limit access into the industrial district east of Pacific Avenue and the residential area west of Pacific Avenue.

Mountain Avenue Extension/Vacation

As part of this Plan, the existing unimproved portion of Mountain Avenue will be vacated. From the intersection of Mountain Avenue (improved) and First Street, Mountain Avenue will be extended as an 88 foot wide right-of-way, in a south-easterly direction where it will intersect with the new extension of northeast/southwest running Yuma Drive. This extension will provide additional access to land uses on either side of the Mountain Avenue extension, thereby, reducing traffic volume along Parkridge Avenue.

Yuma Drive Extension

The present terminus of Yuma Drive is located at the Yuma Drive/Hammer Avenue intersection. As part of this Plan, Yuma Drive will be extended as an 88 foot right-of-way in a south-westerly direction where it will intersect with Parkridge Avenue, continuing southward from this intersection as a 66 foot right-of-way through the office park district located at the southern most portion of the Project Area. This improvement will facilitate access from Hammer Avenue and the I-15 to Mountain Avenue; the extension will also lessen the congestion at the existing Hammer Avenue/Parkridge Avenue intersection.

Cota Street Vacation

Cota Street presently exists as an unimproved northeasterly/southwesterly running roadway northeast of Parkridge Avenue. This right-of-way will be vacated. Southwest of Parkridge Avenue, Cota Street is improved to River Road; this portion of Cota Street will remain as is.

Lowering of Hammer Avenue at First Street

As a part of this Plan, Hammer Avenue may be lowered no less than five to six feet nor more than 17 feet at its intersection with First Street. This improvement may create a more functional Hammer Avenue/First Street intersection.

Intersection Improvements

Improvements to six (6) Project Area intersections are required to complete the Circulation Plan. Those intersections are shown on Exhibit 18 and are described in Table 5. Most intersection improvements will involve new or upgraded signalization, signage and pavement improvements. However, the Lincoln Street/First Street/Pacific Avenue/Parkridge Avenue intersection will be realigned.

Traffic Signal Control for Horseback Riders

Traffic signal control buttons for horseback riders shall be incorporated at intersections designated by City staff and shall be installed in accordance with existing City standards.

**TABLE 5
INTERSECTION IMPROVEMENTS**

Traffic signal modify, Yuma Drive/Hammer Avenue
Traffic signal install, First Street/Hammer Avenue
Traffic signal install, Second Avenue/Mountain
Traffic signal install, Parkridge Avenue/Pacific Avenue
Traffic signal install, I-15/Yuma Drive on/off ramps
Traffic signal install, First Street/Mountain Avenue

2.9 INFRASTRUCTURE/UTILITIES

Adequate utility service shall be provided to all properties within the Project Area. Telephone, gas and electrical service are currently available with adequate capacity upon application and payment of applicable fees and charges.

Water Service

Water service is provided by the City of Norco and will be available to the Project Area as necessary.

The average daily water consumption for the City is currently 8 million gallons per day. Exhibit 20 depicts improvements to the existing water system.

Fire Protection

The provision of water for adequate fire suppression to any particular property will involve upgrading several existing small connectors. Improvements should be provided to meet the requirements of the Insurance Services Office (ISO), fire underwriters. Fire flows must be at least 2,000 gpm. Minimum size mains shall be 8 inches. Any dead end mains should be corrected and reconnected into a looped, gridded system. Fire hydrants will be spaced 300 feet apart. Project engineers have estimated that an additional 40 fire hydrants are needed in the Project Area.

All new development shall be provided with fully automatic sprinkler protection or other reasonable fire/life safety protection as determined by the City's fire chief. All new development in areas adjacent to dead-end water mains shall include provisions for assessment in order to accomplish re-looping of such mains into the system. It is recommended that all new commercial and industrial development shall be assessed a fee per square foot of net increase in building area to mitigate the effect of the incremental additional demands on fire/life safety and paramedic services.

EXHIBIT 20

WATER SYSTEM IMPROVEMENTS
(Please see back map pocket)

Sewer System

Developments which significantly increase the demand on the sewer system cannot be accommodated without construction of relief sewer systems. The size, extent and cost of these systems depend greatly upon the level of property development. Because of this localized variation in capacity, it is imperative that project review include a careful analysis of sewer system impact caused by proposed development projects. Exhibit 21 identifies plan related improvements to the existing system.

Flood Control/Drainage

Flood control and storm drains in the Project Area are the responsibility of the City of Norco and the Riverside County Flood Control and Water Conservation District. Exhibit 22 identifies plan related improvements to the existing system.

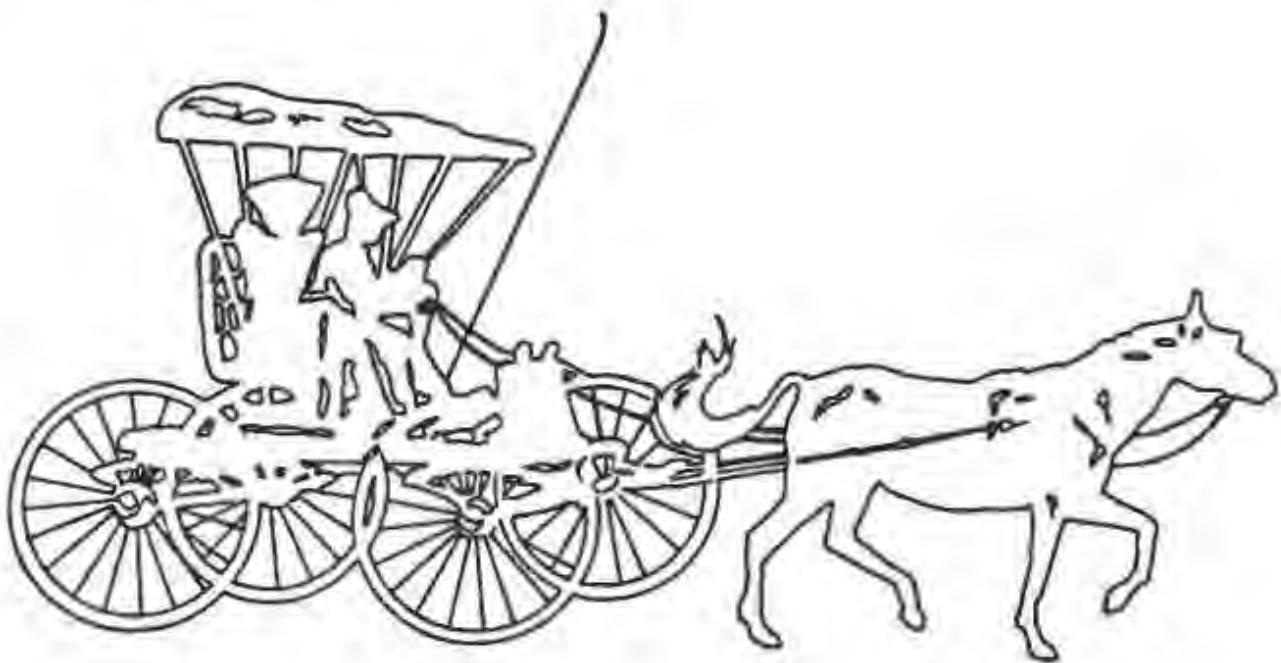
There are periodic street flooding problems in portions of the Project Area, particularly along the westend of First Street and east of Hamner Avenue near Valley View Avenue. Urban runoff is related to the amount of impervious surface; it is expected that the new development within the Project Area will increase the volume of runoff.

EXHIBIT 21

**SEWER SYSTEM IMPROVEMENTS
(see back map pocket)**

EXHIBIT 22

DRAINAGE/FLOOD CONTROL IMPROVEMENTS
(see back map pocket)



GENERAL DESIGN GUIDELINES AND DEVELOPMENT STANDARDS

3.0

3.0 GENERAL DESIGN GUIDELINES AND DEVELOPMENT STANDARDS

These General Design Guidelines and Development Standards address the architectural, landscape architectural and site development requirements that will create a quality environment for the Gateway Specific Plan Project Area.

The desired design character for the Project Area is western/early Californian. The City of Norco and its residents value the life style and atmosphere this theme represents. The City seeks to preserve the special qualities the life style is associated with; "City living in a rural atmosphere".

All new development in the Project Area must comply with the overall development guidelines contained herein and the Development Regulations (Section 4.0) to receive project development approval. These Guidelines will be used by the City as a basis for review of all proposed projects. Proposals that do not address these guidelines will not receive City approval.

There are key design elements which will contribute significantly to the overall visual impact of the Project Area. These include landscape design, architecture, and site planning criteria. This section addresses each of these elements in general terms and establishes the guidelines which will be used and expanded upon in more detail through the site planning and project approval process.

3.1 GENERAL GUIDELINES

This section is not intended to limit the work of designers, but to provide a flexible framework to accomplish a comprehensive design concept and to encourage quality development which will establish the Gateway Project Area as a special place to visit, work and/or own a business.

Design Goals

- o To create quality commercial, office park and industrial districts.
- o To create an identity for the Gateway Specific Plan Project Area specifically and the City of Norco in general.
- o To enhance the economic potential of the Project Area by attracting quality developments.

Design Guidelines

General qualities and design elements for buildings that are most actively encouraged within the Project Area are:

- o Richness of surface and texture
- o Play of light (shapes and shadows)

- o Equal void to solid building wall ratios
- o Multi-planned roofs and walls
- o High degree of varying wall articulation

The elements to avoid or minimize:

- o Large, extremely deep front setbacks, except where deeper setbacks are required, with vast areas of parking located there
- o Highly reflective surfaces
- o Large blank walls
- o Large metal surfaces
- o Exposed concrete block
- o Chain link fence, barbed wire
- o Inarticulated building facades

3.1.1 Business District Sign

Business district sign will be provided along Yuma Drive near the 1-15 on ramp (see Exhibit 23). This sign, similar to one shown in Exhibit 23A, is designed to announce the entrance into a specific area of the City. The business district sign will feature materials that reflect a western/early California look, such as wood and river-wash cobble.

3.1.2 Directory Sign

Directory signs will be provided at the following locations (see Exhibit 23):

- A. Northwest and southeast corners of First Street and Hamner Avenue; and
- B. Northeast corner of Yuma Drive and Hamner Avenue.

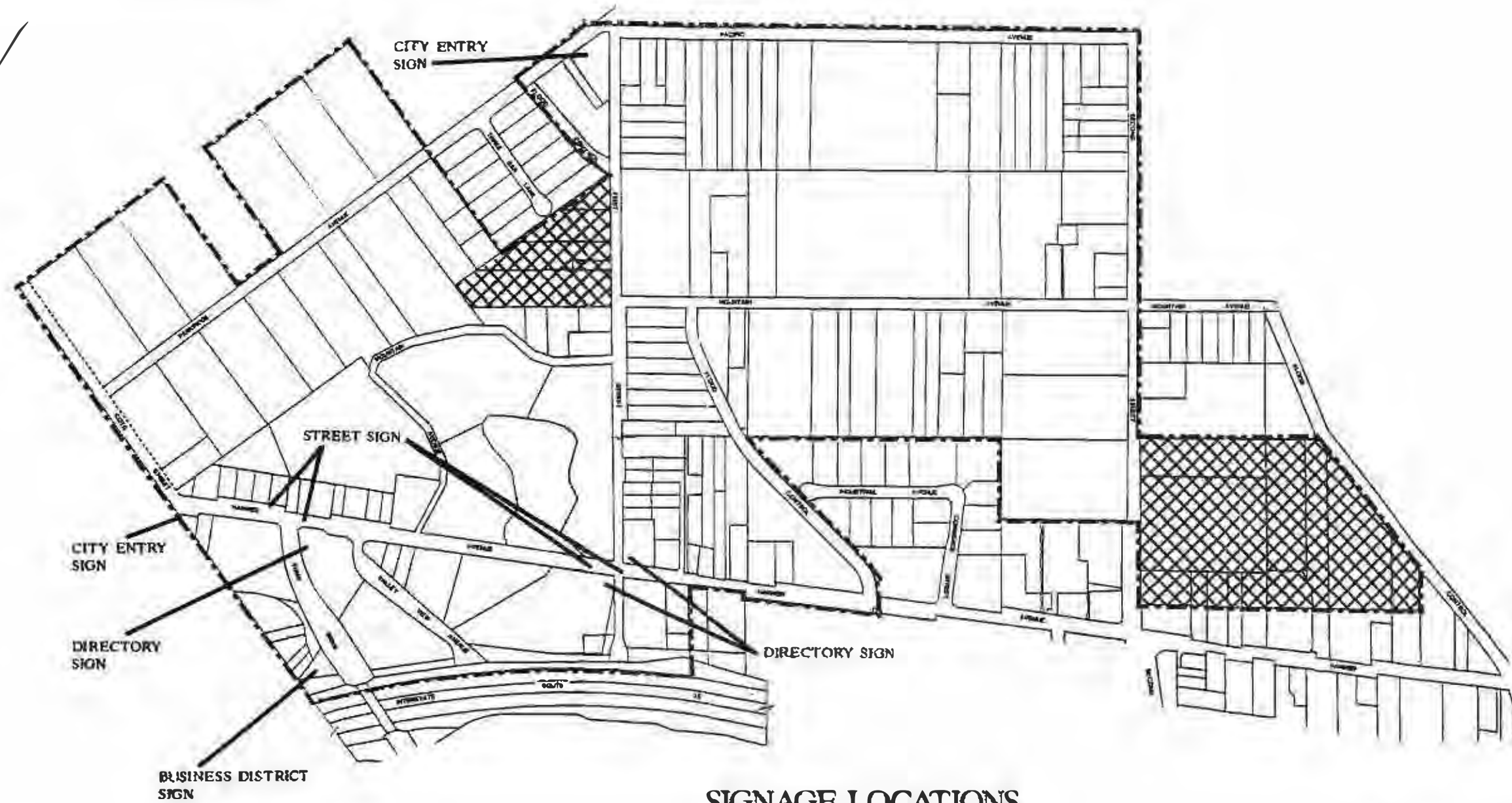
These signs, similar to the ones shown in Exhibit 23B, are designed to identify commercial businesses at that location. Directory signs will feature materials that reflect a western/early California look, such as wood and river-wash cobble.

3.1.3 Street Sign (median island sign)

Street signs will be provided at the following locations (see Exhibit 23):

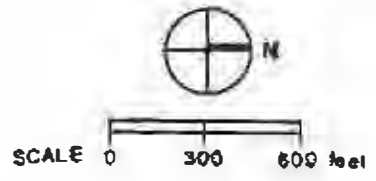
- A. Along Hamner Avenue at Yuma Drive; and
- B. Along Hamner Avenue at Second Street.

Gateway Specific Plan



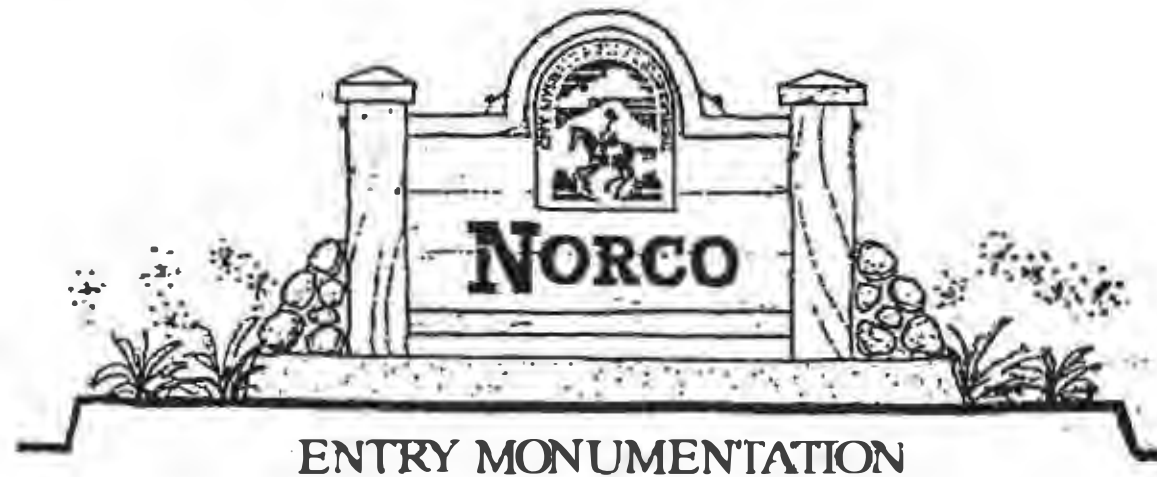
SIGNAGE LOCATIONS

XXXXX Not A Part

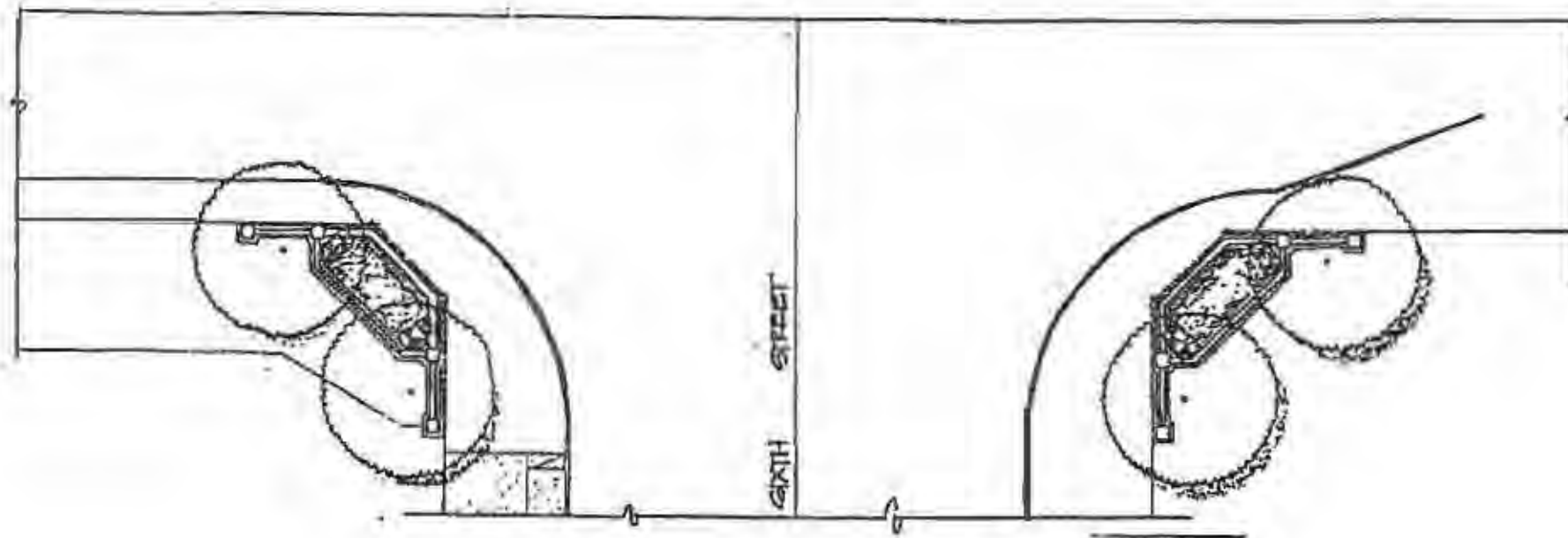


WHY DOES THIS MAP NOT INDICATE PACIFIC AS A CUL-DE-SAC?

Gateway Specific Plan



ENTRY MONUMENTATION



SIGNAGE SITE PLAN



BUSINESS DISTRICT SIGN

ELEVATION
SCALE: 1" = 1'-0"

Gateway Specific Plan




DIRECTORY SIGN



DIRECTORY SIGN



DIRECTIONAL/
LOCATIONAL SIGN

These signs, as shown in Exhibit 23B, are designed to show directions to other significant locations (Civic Center, Old Town Norco, etc.) within the City. Street signs will feature materials that reflect a western/early California look, such as wood and river-wash cobble.

3.1.4 Entry Monumentation

Entrance monumentation will be provided at the following locations:

- a. Hamner Avenue and Cota Street; and
- b. First Street and Pacific Avenue.

These monuments, as shown in Exhibit 23, are designed to announce the entrance into the City and will promote a clear sense of "progression" and transition into the Project Area and the City. Entry signs will feature materials that reflect a western/early California look, such as wood and river-wash cobble.

3.1.5 Site Planning

Project Area site planning standards are as follows:

- a. In situations where buildings are highly visible from side streets and/or adjacent parking areas, blank walls shall be prohibited and special emphasis placed on creating "architectural interest".
- b. In the case of multi-storied buildings, it is important that multi-storied buildings relate to the pedestrian scale. All ground story facades in these areas shall be designed to relate to a pedestrian scale as previously shown in Exhibits 12, 13 and 14. This can be accomplished through breaking the facade into bays and the signage brought down in size and location. Pedestrian scale shall also be established through the use of pedestrian canopies and awnings which add horizontal articulation to facades. Landscape elements and planting areas shall also be used to strengthen the pedestrian scale and to encourage pedestrian activities in the commercial and office park districts.
- c. Within the Commercial District, buildings shall be designed to be visually connected in order to eliminate a strip commercial appearance.
- d. Architecture and outdoor space within the commercial and office park districts shall be integrally designed and oriented toward the pedestrian experience. The experience should be visually diverse, stimulating, and include activities and a built environment that creates a sense of variety and excitement.
- e. Create plazas and open space within individual development projects to the greatest degree possible.

- f. Site and design buildings to minimize pedestrian/vehicle conflicts.
- g. Site and design structures to facilitate public access across sites where important pedestrian connections occur.
- h. Orient/screen all auto related facilities (i.e., working bays, storage, etc.) from public view.
- i. Bi-level landscape buffers shall be provided between existing residential land uses and proposed commercial office park and industrial land uses. As shown in Exhibit 24, the first buffering "level" shall consist of a seven to eight foot high masonry wall. The second "level" consists of a landscaped buffer.
- j. Avoid long linear vistas and building edges within the development envelope and along the streetscape through variations in setbacks.
- k. Public entrances and siting of structures should address major streets. Lots located adjacent to Pacific Avenue are required to take primary vehicular access from the new street paralleling Pacific and Mountain Avenues. Pacific Avenue is to be used only for emergency vehicles.
- l. Primary elements of effective site design are illustrated below:
 - a. controlled site access
 - b. separate service area and service access
 - c. separate employee parking
 - d. convenient public access and visitor parking
 - e. front yard landscaping
 - f. screening of storage and work areas
- m. Within the Gateway Specific Plan Project Area it is more acceptable to create a clustering of smaller building forms connected by open space and walkways than to create a massive single building form.

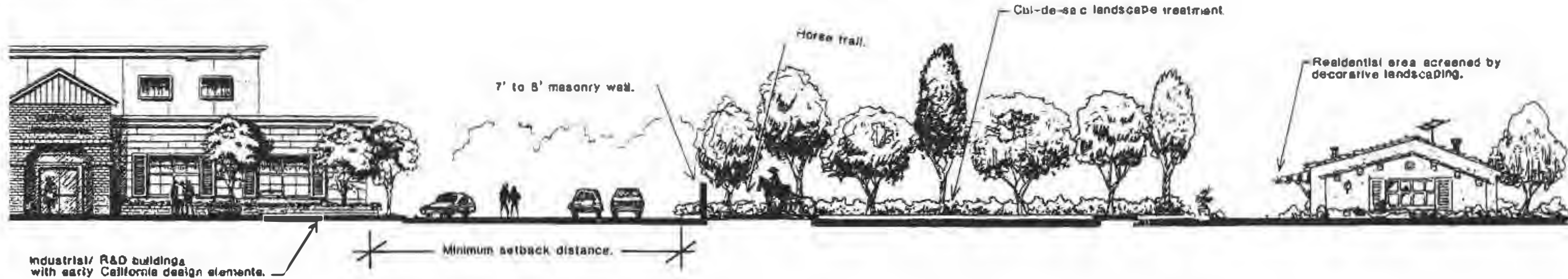
3.1.6 Architecture

Commercial and Office Park Districts

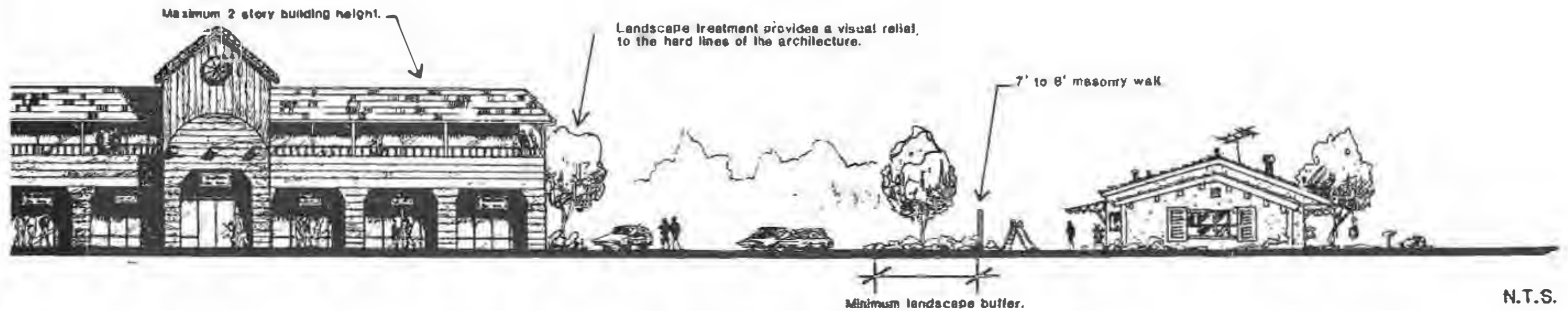
Generally, the design concept within the Commercial and Office Park Districts will emphasize building clusters with vehicular parking lots located away from pedestrian travel routes.

Buildings within these two districts should be diverse with a scale that is not overbearing relative to pedestrians; architectural style should retain a rural flavor consistent with the goal of Norco.

Gateway Specific Plan



PACIFIC AVENUE ROADWAY SECTION AND LANDSCAPE BUFFER DETAIL



TYPICAL COMMERCIAL LAND USE ADJACENT TO RESIDENTIAL LAND USE

EXHIBIT 24

The following architectural elements should be addressed when designing structures within the Commercial and Office Park Districts:

1. Roof Features
2. Clustering
3. Visual Connections
4. Fenestration (windows/doors and misc. openings)
5. Construction Materials
6. Color

Desirable application of the featured design elements are as follows (see Exhibit 25):

1. Roof Features/Visual Connections

- a. Buildings should have predominant roof features with varying roof lines created by: tower features, canopies, awnings, covered porches, exposed wood beams, and wood shingle or simulated wood shingle roof surfaces.
- b. Full roofs (rather than built-up roofs with facades) are encouraged for single-level structures, thus providing substantial roof mass.
- c. Extensive roof overhangs and extension of roof features (trellises) to accessory structures are encouraged.
- d. All elements on rooftops shall be attractively screened from public view. Mechanical equipment and duct work shall be depressed adequately within the roof structure or incorporated within special rooftop features. No equipment or duct work shall be allowed on the roof of any structure within view from any street, freeway or adjacent buildings. The screening shall be an integral part of the building (e.g., extended parapets), and shall be painted the same color as the rest of the building. Corrugated metal screening is not permitted.

2. Clustering

- a. The bulk of the building should be visually broken into at least two or three distinct masses.
- b. The clustering of groups of buildings connected visually by open space, covered walkways and trellises is encouraged.

3. Fenestration (windows/doors and misc. openings)

- a. Window treatments, door openings and exterior hallways, and detail should reflect the western/early Californian style.

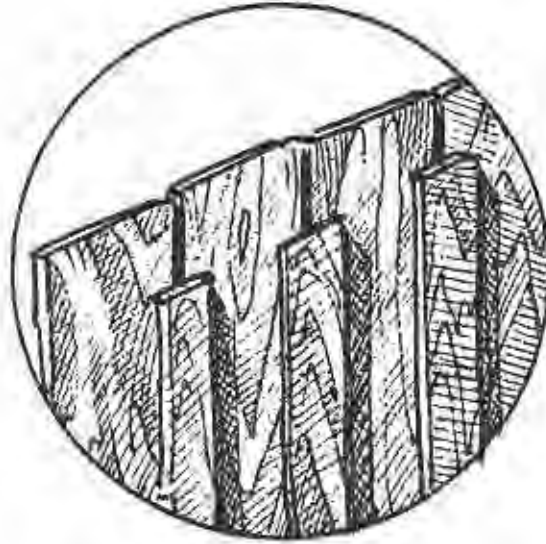
Gateway Specific Plan



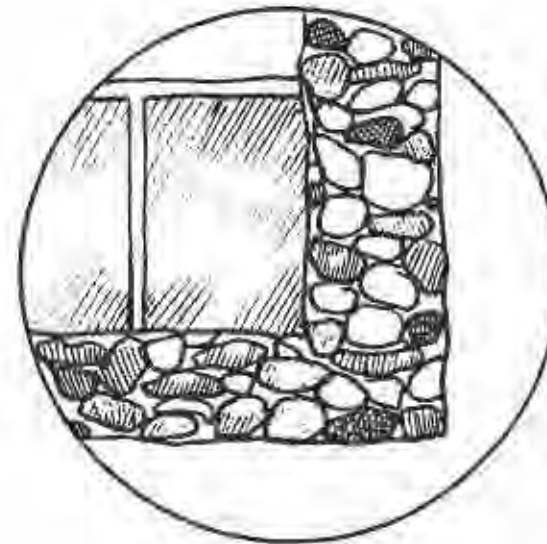
TOWER ROOFTOP FEATURE



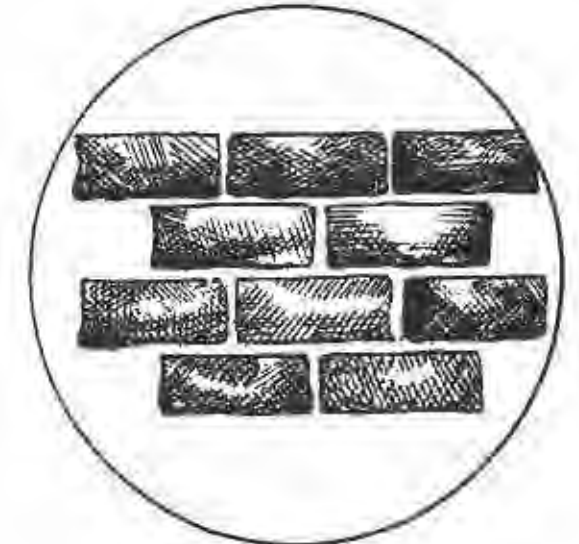
BOARD AND BATTEN



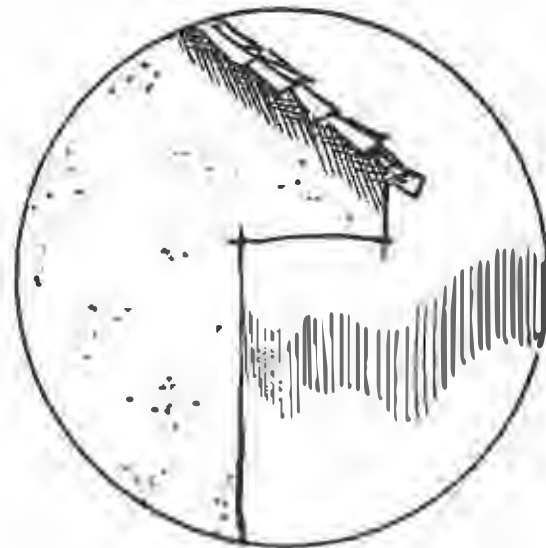
RIVER ROCK



MASONRY



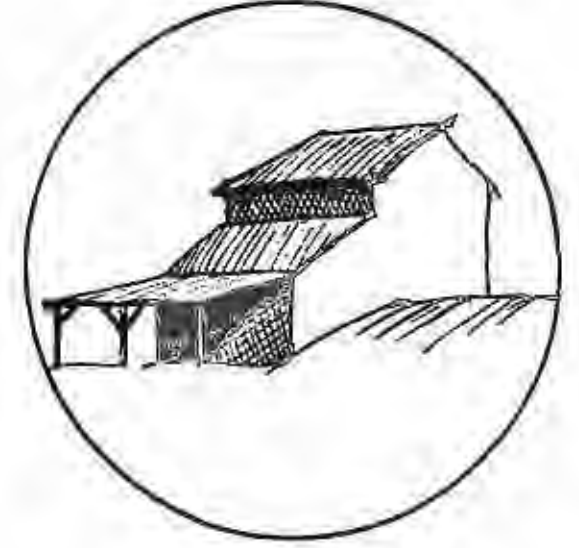
SHINGLES & RAFTER TAILS



TILE & STUCCO WALLS



HIP ROOF/GABLE ROOF



BARN SHED ROOF
& ROOF OVERHANGS

- b. Entry ways should be recessed or projected outwards.
- c. Use of predominant roof lines such as tower features should be focused around entry areas.

4. Construction Materials

The following general building materials alternatives are recommended to achieve a variation upon the desired architectural theme:

Wall Materials

- a. Exterior walls of all buildings are anticipated to be "off-white" or "tertiary earth tones of brown hues" and be constructed of Spanish style textured materials such as slump stone concrete block, Spanish style plaster, or adobe brick. The use of heavy appearing exposed wood beams is encouraged.
- b. Use of wood, brick and river rock exterior walls, wall and support elements are encouraged as an alternative to or as an adjunct to the Spanish style textured materials. Use of the "off-white" or "tertiary earth tones of brown hues" can be used to compliment and/or contrast with natural colors of unpainted wood, brick and rock work. Use of plant materials to compliment and weaken harsh vertical and horizontal lines are encouraged.

5. Roof Materials

- a. When visible, roofs shall be constructed of clay tile, wood shingle, wood shakes of appropriate color and texture of asphalt shingle. Wood shake and wood shingles may be used only by approval of the City Fire Chief.
- b. Where roof lines are not visible due to use of parapet walls, other types of roofing materials may be used pursuant to the requirements of applicable building and construction codes and regulations.

6. Color

- a. Colors for the exterior of the buildings must serve to integrate all elements of the commercial and office park districts and enhance the established style and character. The use of strong, earth tone colors which highlight the building structure is encouraged. Use of colors with a deep hue are encouraged in signage.

Industrial District

Buildings

Conventional industrial architecture can be characterized by simple one and two story rectangular box-type structures. Typical construction materials and/or veneers might include exposed aggregate concrete, river rock, brick/brick veneer and other masonry construction materials. The buildings can be trimmed with wood awnings, arbors, etc., to help create a more rustic quality within the Industrial District. These relatively low profile, simple shapes can comprise a strong element of continuity throughout the industrial district; fenestration details which will provide the greatest degree of variety should be employed. The following design elements should be addressed when designing buildings and structures within the Industrial District:

1. wall surfaces (texture and relief)
2. fenestration (windows, entrances)
3. colors
4. graphics
5. contrast of building materials
6. lighting

The varying and different applications of a variety of elements is only limited by the imagination and ingenuity of the architect. Applications of the described design elements are as follows:

1. Wall Surfaces

- o brick/brick veneer
- o exposed aggregate
- o scored split face block
- o river rock
- o wood veneer
- o opaque glass (no shiny reflective glazing is permitted)

2. Fenestration

- o recessed or projected entries
- o entry overhead elements
- o windows
- o clerestories
- o landscape planters
- o undulation of building massing

3. Colors

- o subtle warm tones

- o white
- o earth tone color bands and trim

4. Graphics

- o building address
- o directional
- o company names

5. Contrast

- o smooth to rough
- o scale
- o color
- o positive/negative space

6. Lighting

- o contrast
- o positive/negative
- o highlighting effects
- o directional

The following guidelines should be consulted when designing and reviewing future development anywhere within the Gateway Specific Plan Project Area:

1. Avoid long, inarticulated building facades. Buildings with varietal front setbacks are strongly encouraged.
2. Avoid blank front and side walls on street frontages.
3. Flat roofs with parapet walls to screen rooftop equipment are appropriate, although buildings with articulated varying roof planes are encouraged.
4. The use of prefab, all metal steel for sheathing of buildings is prohibited. This is not to preclude the use of metal detail within architecturally designed buildings such as "Cortin" steel.
5. Conceal all service areas and storage areas either within the building themselves or by screening walls by a solid masonry, concrete or stucco and wall of one color.
6. Ultra flat smooth facades, and polished reflective surfaces are not encouraged within the Project Area.

7. All screening shall be architecturally integrated with the building design and a roof parapet wall shall be used to screen roof mounted equipment.

The following architectural elements shall be prohibited within the Project Area:

- o Highly reflective surfaces (steel)
- o Large blank walls
- o Flat roofs except where extended parapet walls are used to achieve a "western" character
- o Reflective glass
- o Metal siding
- o Plastic or aluminum siding; and
- o Irregular, modernistic window shapes and rhythms.

3.1.7 Parking

Project Area parking standards are as follows:

- a. Parking access points, whether located on Hamner Avenue or other streets, should be located as far as possible from street intersections. Specific access points as proposed for individual site plans must be submitted for review and approval by the Director of Public Works/City Engineer.
- b. Design parking areas so that pedestrians walk parallel to moving cars. Minimize the need for the pedestrians to cross parking aisles.
- c. Driveways and parking areas should be separated from adjacent sidewalks or landscaped areas by a curb not less than six inches high.
- d. All parking spaces shall be clearly outlined on the surface of the parking facility.
- e. Link individual project parking areas with on-site driveways which are clearly identified and easily recognized as connectors.
- f. Use parking lot paving surfaces such as turf block or gravel that will allow water to percolate down wherever practical and desirable.
- g. As a condition of project approval, the applicant may be required to combine parking facilities and access to serve more than one individual project.

- b. Required parking or loading facilities shall not be located in any required setback areas unless specifically allowed by the development regulations.
- i. Loading areas and docks shall be permitted only in rear and side lot areas and shall be screened from public view.
- j. No part of a street shall be used for loading.
- k. Separate automobiles access and parking from service truck areas.

3.1.8 Landscape Architecture

Project Area landscape design standards are as follows:

- a. Establish a colorful landscape edge at the base of buildings and avoid asphalt edges at the base of structures as much as possible. Plant materials located in containers are appropriate.
- b. Landscaping should result in a low profile image, i.e., use of blade grass lawn areas and other ground covers, canopy trees in parking areas, use of hedges, and low walls and plantings to screen service areas.
- c. On-site plant masses should assume a non-uniform arrangement. The diversity of massing types should be great enough to provide interest, but kept to a level which evokes a relaxed, natural feeling.
- d. Water conserving irrigation systems and drought tolerant plants shall be used in all public and private landscaping whenever appropriate.
- e. Landscape materials should enhance the major architectural design elements through the coordinated use of shrub and leaf colors, tree forms, plant material masses, and lighting.
- f. Plant material and massing should be designed to compliment architectural elevations and roof lines through color, texture, density and form on both the vertical and horizontal planes.
- g. A combination of landscaping, fences and walls shall be used on the perimeter of properties to define property limits, separate use areas and provide on-site security.
- h. Landscaping, fences and walls, or a combination thereof; shall be used on the perimeter of properties to visually screen and/or physically enclose outdoor storage areas, loading docks and ramps, transformers, storage tank and other appurtenant items of negative visual quality.

- i. See Appendix B for the recommended Plant Selection Palette.

3.1.9 Riding Trails

The City of Norco has developed as a community which emphasizes equestrian activities. Existing City riding trail standards serve to compliment horseback riding while safeguarding the public through the provision of a regulated equestrian- pedestrian trail system.

Section 1828 of the Norco Municipal Code establishes a zone within existing public easements and rights-of-way, and within private access easements that governs this trail system.

Within the Gateway Specific Plan Project Area, riding trails shall be located along the following roadways until such time that existing residential properties are recycled to commercial or industrial uses:

Second Street
First Street

Equestrian riding trail along Pacific Avenue shall remain a permanent installation.

3.1.10 Signage

Project Area signage standards are as follows:

- a. Signage shall be simple and express an easy to read, direct message. Brand names of merchandise shall not be displayed.
- b. Storefront shall not have more than two signs - one primary and one secondary. The secondary sign, shall be pedestrian in scale and not be visible by motorists.
- c. Signs shall identify only the firm name or major enterprise on the premises on which the sign is located. Material and product advertising shall not be permitted except as primary identification of an establishment. Additional identifying logos of products may be incorporated within the signing but are considered part of the overall allowed sign area.
- d. Pedestrian-oriented signs may be applied directly to the face of the building and may not exceed city sign area requirements. The shape of the sign can be a positive feature.
- e. No signs shall extend higher than the ridgeline or be located on the roof of the structure.
- f. Window signs shall not obscure more than 25 percent of the display window area.

- g. All signs, including temporary signs, shall be in conformance with the City of Norco Municipal Sign Code.
- h. All signs shall be for the purpose of identification or direction only and shall contain no advertising.
- i. Signs suspended from canopies, balconies, etc., will be permitted; adequate head room must be provided.
- j. There shall be no revolving, flashing, or moving signs.
- k. Wall-painted signs will be permitted with prior approval of the Director of Community Development. Such signs will compliment the western character of the Project Area.
- l. No pennants or similar devices for commercial display shall be permitted.
- m. Roof signs will not be permitted.
- n. Plastic and fluorescent light signage fixtures are not permitted, except for special events.
- o. All buildings using rear parking lots shall clearly identify their businesses with rear wall signage. When site planning requires rear parking within commercial and office park districts, rear entries shall not be treated as an "after thought" but shall reflect appropriate design considerations that attract pedestrians and encourage entry. Appropriate design features would include awnings, potted plants, pavement treatments, window placement, signage, street furniture and decorative painting around doors, windows, etc. Safe nighttime entry shall be facilitated through the use of appropriate lighting standards.

All structures shall exhibit clear address information on the front facade and back facade when rear entries are used. (Recommended guideline: minimum address number size shall be 6" for a 1 story building and 10" for a 2 story building.) For nighttime identification, the numbers shall be internally or externally lit. Otherwise all signing shall comply with the Norco Sign Ordinance.

3.2 **STREETSCAPE DESIGN**

3.2.1 **Center Median Islands**

Center median islands shall be provided along Hamner Avenue.

These medians are designed to provide access control and a unifying landscape statement. As shown in Exhibit 26, the medians will incorporate rolling turf berms, low growing shrubs and

Gateway Specific Plan



Landscaped medians provide safety and roadway delineation in an eye pleasing fashion.

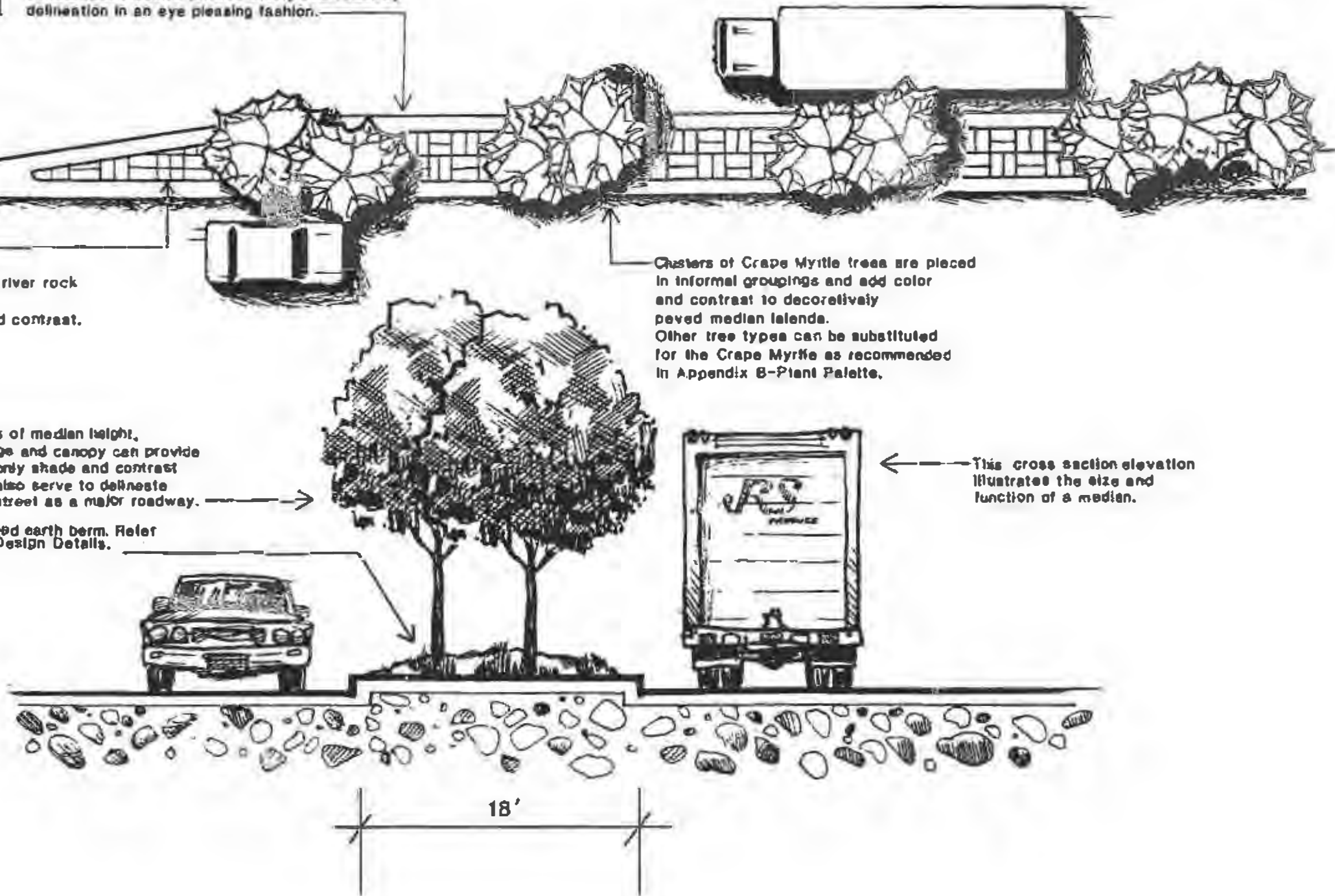
Median can be paved in stamped concrete as shown or natural materials such as river rock or other stone with areas of bermed landscaping to add contrast.

Clusters of Crape Myrtle trees are placed in informal groupings and add color and contrast to decoratively paved median islands. Other tree types can be substituted for the Crape Myrtle as recommended in Appendix B-Plant Palette.

Trees of median height, foliage and canopy can provide not only shade and contrast but also serve to delineate the street as a major roadway.

Suggested landscaped earth berm. Refer to Exhibit 15 for Design Details.

This cross section elevation illustrates the size and function of a median.



HAMNER AVENUE MEDIAN DESIGN
(CONCEPTUAL ONLY)

riverrock/cobblestone or stamped concrete, dominated by informal clusters of Crape Myrtle and Evergreen Pear or other trees as specified in Appendix B.

3.2.2 Streetscape Lighting

It is the intent of streetscape lighting to add a consistent, planned look to the Project Area during daylight hours and evening. Lighting can have a dramatic affect on the quality of streetscape design.

- a. All lighting fixtures in the public right-of-way shall be consistent and approved by the City Engineer following the recommendation of the Planning Commission. As shown in Exhibit 27, materials must be consistent and in keeping with the western/early Californian theme. Luminaries, poles and supporting hardware must also be consistent.
- b. Lights shall not be placed to cause glare or excessive light spillage on neighboring sites. A uniform light color is preferred for the area. The use of high pressure and energy efficient sodium lights is encouraged.
- c. Light standards shall not exceed 20 feet in height and in no case shall they exceed the height of the buildings on-site.
- d. The design of light fixtures and required structural support shall be architecturally compatible with the surrounding buildings.

3.2.3 Street Tree Program

All Project Area roadways shall incorporate a street tree planting program (see Exhibit 28); permitted street trees are those trees identified in Appendix B. Chapter 12.12 of the Norco Municipal Code provides tree installation, staking, trimming and maintenance requirements that will be applicable to the Project Area street tree program.

Gateway Specific Plan



Themed street light fixture mounted to a decorative wooden post.



Height of pole conforms to minimum requirements provided by the City.

Natural wood sign with burned or applied decorative lettering.



Directional/Locational street signage is suspended from street light fixture.

Decorative support base provides additional aesthetic elements to the streetscape materials such as natural stones and drought tolerant landscaping.



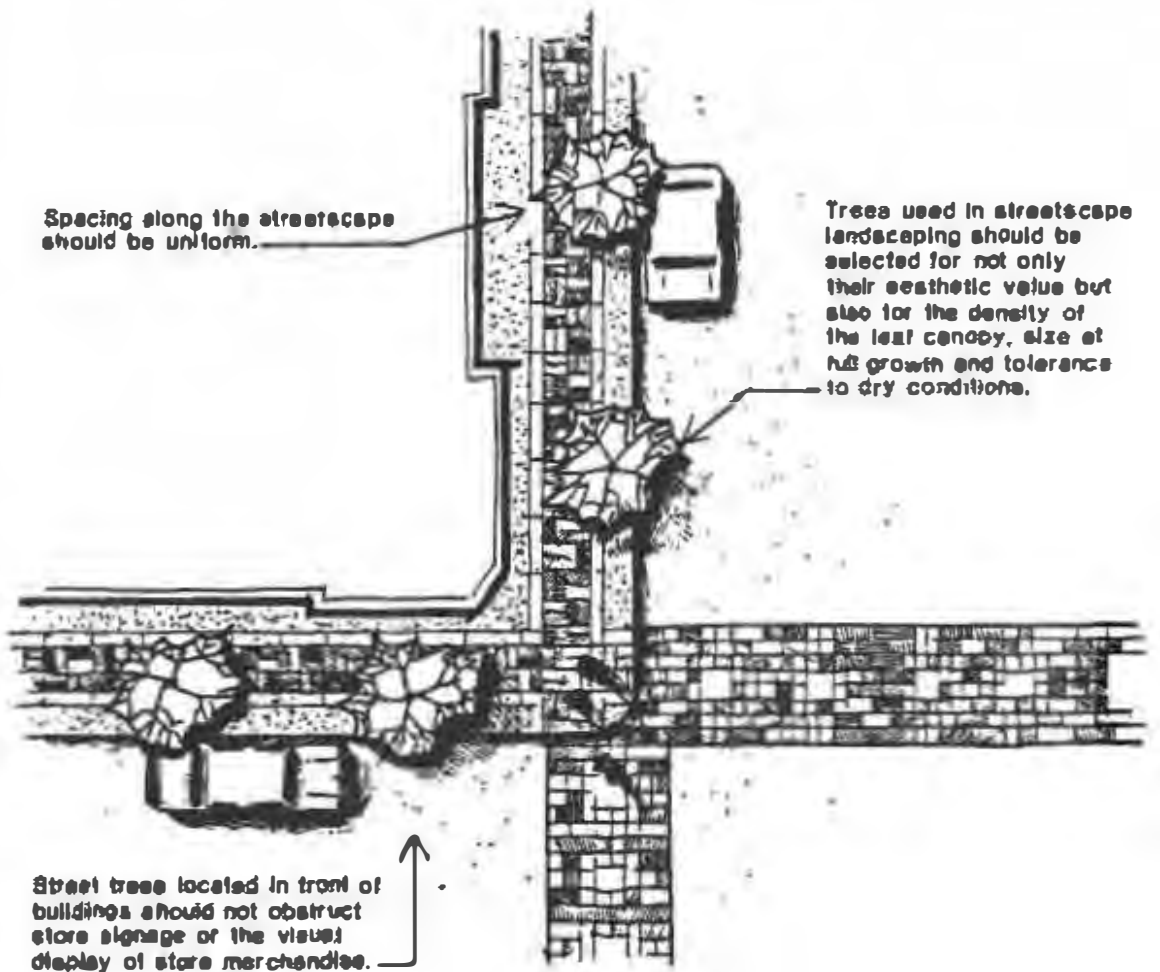
STREET TREE PROGRAM

Gateway Specific Plan



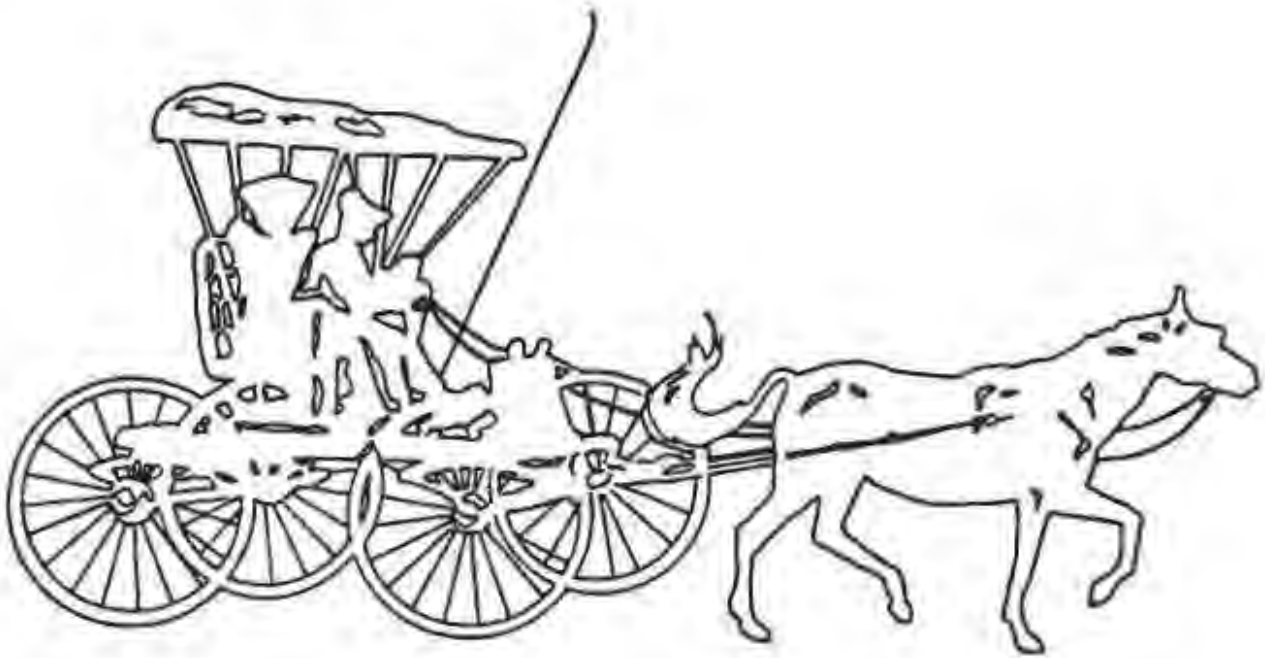
Spacing along the streetscape should be uniform.

Trees used in streetscape landscaping should be selected for not only their aesthetic value but also for the density of the leaf canopy, size at full growth and tolerance to dry conditions.



Street trees located in front of buildings should not obstruct store signage or the visual display of store merchandise.

STREETSCAPE LIGHTING DESIGN STANDARDS (CONCEPTUAL)



DEVELOPMENT REGULATIONS

4.0

4.0 DEVELOPMENT REGULATIONS

4.1 Introduction

The following Development Regulations implement the Gateway Specific Plan. The regulations are adopted by Ordinance pursuant to Article 8, Authority for Scope of Specific Plans, of the Planning and Government Code in compliance with Sections 65450 and 65503 of the Government Code. Land Use districts for regulatory purposes are shown on Exhibit 29.

4.1.1 Relationship to Other Regulations

The Plan will contain most of the information needed to guide the development of a particular parcel within the Project Area. However, areas not specifically covered by this Plan will continue to be governed by existing City regulations, and no provision of this Plan is intended to repeal, abrogate, annul, impair, or interfere with any existing City ordinance except as it specifically repealed by adoption of this Plan.

4.1.2 Conflict with Other Regulations

Whenever the provisions of this Plan impose more restrictive regulations or otherwise establish more restrictive regulations than are imposed or required by any other City ordinance, code or regulations, the provision of this Plan shall govern.

4.1.3 Relationship to the Norco Municipal Code

This Plan augments the development regulations and standards of the Norco Municipal Code. When an issue, condition, or situation occurs which is not covered or provided for in this Plan, the regulations of the Municipal Code that are most applicable to the issue, condition, or situation shall apply. In the event that the provisions of this Plan are in conflict with the Municipal Code, the provisions of the Plan shall prevail.

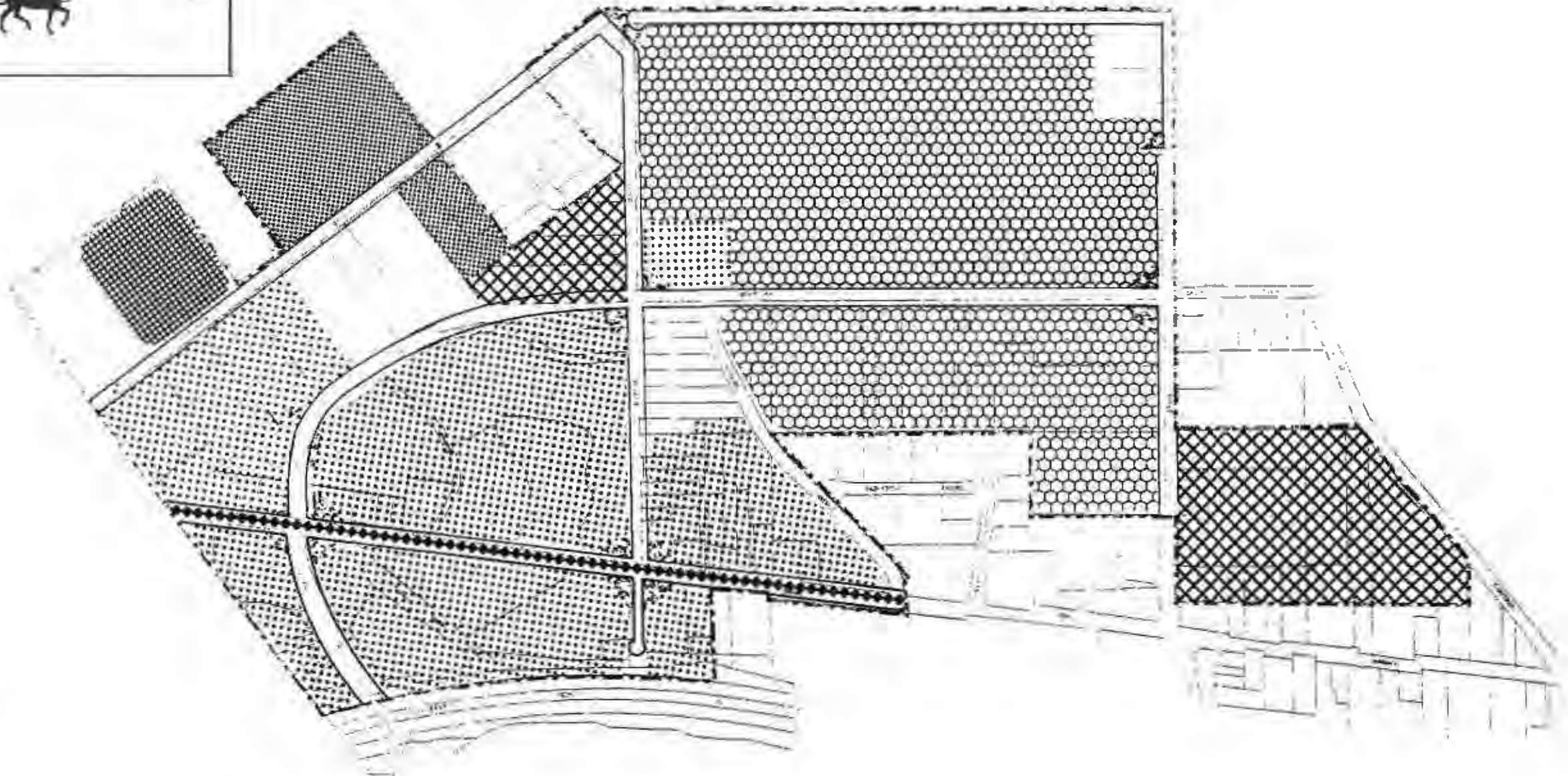
4.1.4 Enforcement

The Planning Director or his/her designee shall enforce provisions of this Plan and all applicable codes of all governmental agencies and jurisdictions in such matters including, but not limited to, building, mechanical, fire and electrical codes, codes pertaining to drainage, waste water, public utilities, subdivisions and grading.

4.1.5 Definitions





Words, phrases and terms not specifically defined herein shall have the same definition as provided in the Municipal Code.

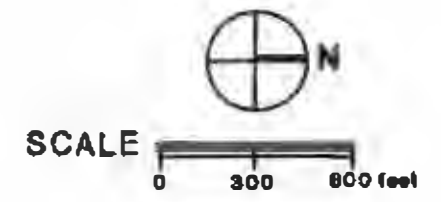
Gateway Specific Plan



LAND USE DISTRICTS LEGEND

-  Commercial District
-  Industrial District
-  Office Park District
-  Residential District

-  Landscape Feature
-  Landscaped Median
-  Maintain/Upgrade Existing Uses
-  NOT A PART



4.1.6 Severability

If any regulation, condition, program or portion thereof of this Plan is for any reason held invalid or unconstitutional by any court of competent jurisdiction, such portion shall be deemed a separate, distinct and independent provision and the invalidity of such provision shall not affect the validity of the remaining provisions.

4.1.7 Interpretation

The Planning Commission shall have the responsibility to interpret the provisions of the Plan. All such interpretations shall be in written form and permanently maintained. Any person aggrieved by such an interpretation may request that such interpretation be reviewed by the City Council.

4.1.8 Ambiguities

Unless otherwise provided, any ambiguity concerning the content or application of the Gateway Specific Plan shall be interpreted by the Planning Commission.

4.1.9 Agreements

The provisions of this Plan are not intended to interfere with or abrogate any easements, covenants, or other existing agreements which are more restrictive than the provisions of this Plan.

4.1.10 Public Hearing and Notification

All public hearing and notification requirements of the California Public Resources Code shall apply to the development within the Gateway Specific Plan Project Area.

4.2 General Purpose

The development regulations found in this Specific Plan provide the standards, criteria and procedures necessary to achieve the following objectives:

- a. Implement the provisions and policies of the Norco General Plan.
- b. Provide maximum opportunities for innovative Project Area design and site planning.
- c. Improve the visual image, economic well-being and overall integrity of the Gateway Specific Plan Project Area.

4.3 Application

- a. These development regulations shall be applied only in the Gateway Specific Plan Project Area.**
- b. If any portion of these regulations are, for any reason, declared by a court of competent jurisdiction to be invalid or ineffective in whole or in part, such decisions shall not affect the validity of the remaining portions thereof. The City Council hereby declares that it would have enacted these regulations and each portion thereof, irrespective of the fact that any one or more portions be declared invalid or ineffective.**
- c. Terms used in these regulations shall have the same definitions as given in the City of Norco Municipal Code.**

4.4 General Provisions

- a. All construction and development within the Specific Plan Project Area shall comply with applicable provisions of the Uniform Building Code and the various related mechanical, electrical, plumbing codes, grading and excavation code and the Subdivision Codes, as currently adopted by the City of Norco. In case of a conflict between the specific provisions of any such code and this Specific Plan, the provision which serves best to insure public safety and Project Area integrity shall apply. The Planning Director shall resolve any conflict by written determination in a manner consistent with the goals and policies of this Specific Plan.**
- b. The setback requirements are as specified by each district of this Specific Plan. All setbacks shall be determined as the perpendicular distance from the existing street right-of-way line, or property line, to the foundation point of the closest structure.**
- c. If an issue, condition or situation arises or occurs that is not sufficiently covered or provided for or to be clearly understandable, those regulations of the Norco Municipal Code that are applicable for the most similar issue, condition or situation shall be used by the Planning Commission as guidelines to resolve the unclear issue, condition, or situation. This provision shall not be used to permit uses or procedures not specifically authorized by this Specific Plan or the Norco Municipal Code.**
- d. This Specific Plan may be amended by the same procedure as it was originally adopted. Each amendment shall include all sections or portions of the Specific Plan that are affected by the change. An amendment may be initiated by the City Council or the Planning Commission. Any such amendment requested by a property owner shall be subject to the fee schedule adopted by the City Council.**
- e. Whenever a use has not specifically been listed as being a permitted use in a particular District Use classification within the Specific Plan Project Area, it shall be the duty of**

the Planning Commission to determine if said use is (1) consistent with the intent of the use district and (2) the said use is compatible with other listed permitted uses. Any person aggrieved by the determination may appeal that decision to the City Council.

- f. Automotive vehicles or trailers of any kind or type without current license plates that have been abandoned shall not be parked or stored on any property within the Project Area unless it is in a completely enclosed building or within a completely screened enclosure. However, in no case shall fixture auto dismantling operations, except as required for the permitted repair of automobiles within specific zones, be allowed in the Project Area.

4.5 Non-conforming Uses of Land

Where at the time of passage of this Specific Plan lawful use of land exists which would not be permitted by the regulations imposed by this Specific Plan, and where such use may be continued so long as it remains otherwise lawful, provided:

- a. No such non-conforming use shall be enlarged or increased, nor extended to occupy a greater area of land than was occupied at the effective date of adoption or amendment of this Specific Plan unless approved by a conditional use permit;
- b. No such non-conforming use shall be moved in whole or in part to any portion of the lot or parcel other than that occupied by such use at the effective date of adoption or amendment of this Specific Plan unless approved by C.U.P.;
- c. If any such non-conforming use of land ceases for any reason for a period of more than 360 days, any subsequent use of such land shall conform to the regulations specified by this Specific Plan for the district in which such land is located;
- d. Additional buildings may be erected in connection with such non-conformity use of land if less than 10 percent of market value of total improvement on land. Additional buildings may be erected by C.U.P. if over ten percent of M. V.
- e. Submittal for C.U.P shall be considered a minor application with appropriate fee as determined by City Council.

4.6 Non-conforming Structures

Where a lawful structure exists at the effective date of adoption or amendment of this Specific Plan that could not be built under the terms of these regulations by reason of restrictions on area, height, yards, its location on the lot, or other requirements concerning the structure, such structure may be continued so long as it remains in the same use and is otherwise lawful, subject to the following provisions:

- a. No such non-conforming structure may be enlarged or altered in a way which increases its non-conformity, but any structure or portion thereof may be altered to decrease or not affect its non-conformity;
- b. Should such non-conforming structure or non-conforming portion of a structure be destroyed by any means to an extent of more than 50 percent of its replacement cost at time of destruction, as determined by the City building officials, it shall not be reconstructed except in conformity with the provision of this Specific Plan;
- c. Should such structure be moved for any reason for any distance whatever, it shall thereafter conform to the regulations for the district in which it is located after it is moved.

4.6.1 Residential Transition Overlay Zone

Residential properties located within the Project Area shall be within the Residential Transition Overlay Zone. This zone allows the residential property owner greater flexibility in the use of his/her property than would typically be allowed for legal non-conforming land uses. Residential property owners within this zone may make improvements to, or construct new improvements upon the property, in conjunction with the existing residential use.

1. Enlarge or alter main dwelling units to an extent of not more than 25 percent of existing floor area;
2. Enlarge, alter or new construction of accessory structures.
3. Should a non-conforming structure be involuntarily destroyed to any extent, including total destruction, it may be rebuilt to the identical use and original floor area, providing design guidelines contained herein shall be respected, and further provided that onsite parking be replaced at the ratio existing at the time of such destruction.

Residential properties along the north side of First Street shall retain their present zoning classification of "A-1-20". The area shall be designated as a future commercial zone and upon application of the property owners, representing two-thirds (2/3) of the area, the City will process a zone change. The City shall process the zone change at no cost to the property owners; the City will not process or consider any zone changes on a "piece by piece" basis, without processing fees being paid.

4.7 Undergrounding of Utilities

All electrical, telephone, CATV and similar service wires and cables, which provide direct service to the property being developed within the exterior boundary lines of such property, shall be installed underground whenever possible. Each new development must underground utilities or prove to the City Engineer's satisfaction that undergrounding cannot be accomplished. Risers on poles and buildings are permitted and shall be provided by the developer or owner onto the pole which provides service to said property. The developer or owner is responsible for complying with the requirements of this section and shall make the necessary arrangements with the utility companies for the installation of such facilities.

For the purpose of this section, appurtenances and associated equipment such as, but not limited to, surface-mounted transformers, pedestal-mounted terminal boxes and meter cabinets, and concealed ducts in an underground system may be placed above ground provided such appurtenances and associated equipment are screened from view.

4.8 Property Maintenance

Property shall be maintained in acceptable visual condition. This includes irrigation, seeding, pruning and necessary replanting of landscaped areas; removal of trash, discarded materials and equipment; removal of boats, trailers or other vehicles either unrelated to the on-site business or in a deteriorated or incomplete condition; removal of containers or shipping materials after their function is accomplished; and painting or appropriate preservation of building facades and walls. A landscape maintenance district shall be formed within the Project Area to insure adequate maintenance of roadway landscape features as shown on the Conceptual Development Plan contained in the back pocket of this document.

4.9 Future CEQA Compliance

A program environmental impact report has been prepared and certified as adequate and complete for this Specific Plan; proposed development projects consistent with this Specific Plan shall, in most cases, be required only to reference the existing EIR. If localized impacts or changes are discovered in the process, they shall be addressed by means of a mitigated negative declaration or supplemental environmental impact report pursuant to Sections 15162 and 15163 of CEQA.

4.10 Land Use

The four primary land use districts permitted within the Project Area are listed below. Each district is further described by recommended specific land use categories. Refer to Exhibit 29 to determine the boundaries of each category.

Primary Land Use Districts

Residential District

Commercial

Office Park District

Industrial District

4.10.1 Primary Districts

4.10.1.1 Residential District

The residential district in the Gateway Specific Plan will consist of low density housing (A-1-20 and A-1-40). Most of the proposed housing will be located along Parkridge Avenue south of First Street. To a lesser extent housing is also proposed along Second Street near Mountain Avenue. Development shall be governed by requirements of underlying zone.

4.10.1.2 Commercial District

The Commercial District is intended to facilitate the development of underutilized and vacant parcels of land within the Project Area in such a way that they will contribute to the economic development of the City. The Gateway Specific Plan Project Area, because of its location to major roadways, the Norco Auto Mall and the Second Street/I-15 on/off ramp, and because of the availability of significant amounts of vacant and recyclable land, will become the primary commercial area within Norco.

a. Principle Uses Permitted

See Appendix C, Permitted and Conditionally Permitted Uses.

b. Limitations on Permitted Uses

Every use permitted in the Commercial District shall be subject to the following conditions and limitations.

- 1) No operations and uses conducted on the premises shall be offensive by reason of noise, dust, mud, odor, smoke, vibrations or other similar causes; and
- 2) All uses in the Commercial District shall be conducted completely within a fully enclosed building except:
 - a. Recreational activities customarily conducted in the open;
 - b. Exterior storage with the approval of a conditional use permit;
 - c. Outdoor dining areas; and
 - d. Surface parking lots.

- 3) All uses in the Commercial District shall comply with all other applicable City, County, State and Federal laws and regulations.
- 4) No automobile dismantling businesses are allowed in this district except as required for the permitted repair of automobiles within specific zones.
- 5) Small auto service and repair uses with up to four (4) service bays are permitted within the auto related commercial designation. Small auto service and repair uses with up to two (2) service bays are permitted within the Highway Oriented Commercial designation. Projects proposing a greater number of bays shall be referred to the Planning Commission for consideration in the form of a conditional use permit.

c. Permitted Accessory Uses

The following uses are permitted in the Commercial District when developed and used in conjunction with one or more of the principal permitted uses.

- 1) Employee recreation facilities and play areas;
- 2) Retail sales and services;
- 3) Permitted wholesale sales and services ("willcall" types of business)
- 4) Surface parking lots.

d. Site Development Standards

Any "integrated" commercial center(s) shall be exempt from lot size and frontage requirements; said centers shall be subject to discretionary actions of the Planning Commission.

1) Lot Area

Every lot in the Commercial District shall be a minimum of 13,125 square feet and have a minimum frontage on a dedicated street of 75 feet.

2) **Height**

No building or structure within the Commercial District shall exceed a height of 35 feet. Buildings utilizing a parapet wall in order to hide rooftop equipment or buildings incorporating rooftop architectural features shall not exceed 50 feet at the highest point. Building heights may be increased through the provision of a conditional use permit at the discretion of the City's Planning Commission.

3) **Front Yard Setback Requirements**

When no front yard parking is included within the site plan a 25 foot minimum, suitably landscaped setback shall be provided. When front yard parking is included within the site plan a 60 foot minimum setback shall be provided; this setback shall include a fifteen (15) foot minimum, suitably landscaped area along the lot frontage.

4) **Side Yard Setback Requirements**

No side yard setback is required unless the following condition exist:

- a. Such yard abuts a dedicated street, in which case a fifteen (15) foot minimum, suitably landscaped setback shall be provided. Refer to Sub-section (d)(3) (Front Yard Setback Requirements), if parking facilities are located in the side yard setback.

5) **Rear Yard Setback Requirements**

No rear yard setback is required, unless such rear yard abuts a dedicated street in which case a fifteen (15) foot minimum, suitably landscaped setback is required.

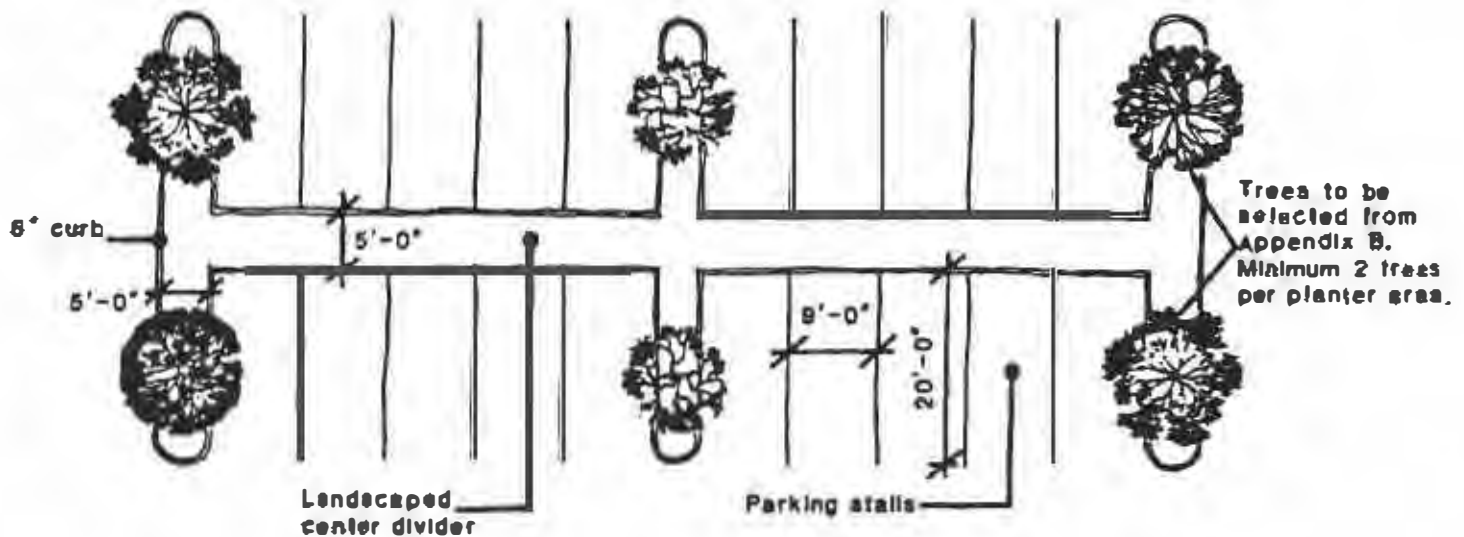
e. **Landscape Requirements**

1) **In the Commercial District, landscape requirements shall be as follows:**

- a) Landscaping for the purpose of this chapter shall consist of trees, shrubs, vines, annual and perennial flowers, ground coverings or any combination thereof. Drought tolerant

species shall be used whenever feasible. A recommended plant palette is included in Appendix B of this Plan.

- b) A minimum of 15 percent of the total of any development site shall be devoted to landscaping which includes all landscaped setbacks. One-third of the required landscaping shall be within parking lot areas.
- c) All landscaped areas shall be provided with permanent watering facilities.
- d) All landscaped areas shall be maintained in a neat, clean and healthy condition. This shall include proper pruning, weeding, removal of litter, fertilizing and replacement of plants when necessary.
- e) Landscape design shall include a variety of open space areas, utilizing earth mounds of variable heights where feasible with a variegated grouping pattern of trees, shrubs and groundcover.
- f) Within vehicle parking and outdoor display areas the design guidelines shown below are recommended. However, these guidelines are not intended to prohibit flexibility in creating various shapes (circular, triangular) or varying sizes. Reference Section 4.11, Parking Requirements, for general parking stall design provisions.



Vehicle Parking Stall Design

4.10.1.3 Office Park District

The Office Park District is intended to serve local and sub-regional office and office park needs. This district provides for the development of garden office, single occupant office and ancillary uses.

The District is intended to accommodate professional/administrative office and personnel services. Typically, uses consist of executive, management, administrative, or clerical uses including the establishment of branch offices, data processing centers and the provision of consultation establishments of a professional nature. Additional office land uses consist of activities which cater to business support and personal services. Uses typically include medical and health care clinics, travel agencies, insurance agencies, copy centers, and other like land uses.

a. Principal Uses Permitted

See Appendix C, Permitted and Conditionally Permitted Uses.

b. Limitation on Permitted Uses

Every use permitted in the Office Park District shall be subject to the following conditions and limitations:

1. All uses in the Office Park District shall be conducted completely within a fully enclosed building except:
 - a. Recreational facilities customarily conducted in the open;
 - b. Exterior storage provided that it is completely surrounded by a masonry wall at least seven feet in height;
 - c. Outdoor dining areas and employee parking; and,
 - d. Surface parking lots.
2. All uses in the Office Park District shall comply with all other applicable City, County, State and Federal laws and regulations.

c. Permitted Accessory Uses

The following uses are permitted in the Office Park District when developed and used in conjunction with one or more of the principal permitted uses.

1. Surface parking lots
2. Eating establishments; no bars or lounges are permitted

d. Site Development Standards

Any "integrated" office park(s) shall be exempt from lot size and frontage requirements; said office parks shall be subject to discretionary actions of the Planning Commission.

1. Lot area

Every lot in the Office Park District shall be a minimum of 13,125 square feet. Minimum width shall be 75 feet; minimum depth shall be 175.

2. Building Height

No building or structure within the Office Park District shall exceed a height of two stories or 35 feet. Buildings utilizing a parapet wall in order to hide rooftop equipment, or buildings incorporating rooftop architectural features shall not

exceed 50 feet at the highest point. The following exception is applicable to that area designated as Office Park located northeast of Parkridge Avenue and southeast of the Three Bar Lane residential area:

The building height limitation shall be one (1) story or twenty (20) feet.

3. **Front Yard Setback Requirements**

In the Office Park District the front yard setback shall not be less than fifteen (15) feet from the Parkridge Avenue right-of-way; this setback shall be fully landscaped.

The front yard setback shall not be less than twelve (12) feet from the right-of-way of all other streets; this area shall be fully landscaped.

4. **Side Yard and Rear Yard Setback Requirements**

There shall be no requirement, except where any rear yard is provided, there shall also be provided a side yard of twelve (12) feet on at least one side of the lot, or a recorded vehicular access easement from a public street to said rear yard.

e. **Landscape Requirements**

1. In the Office Park District, landscaping requirements shall be as follows:

- a. Landscaping for the purpose of this section shall consist of trees, shrubs, vines, annual and perennial flowers, ground coverings or any combination thereof. Drought tolerant species shall be used whenever feasible. A recommended plant palette is included in Appendix B of this Plan.
- b. A minimum of 15 percent of the total of any development site shall be devoted to landscaping which includes all landscaped setback areas. One-third of the required landscaping shall be within parking lot areas.
- c. All landscaped areas shall be provided with permanent watering facilities.
- d. All landscaped areas shall be maintained in a neat, clean and healthy condition. This shall include proper pruning, weeding, removal of litter, fertilizing and replacement of plants when necessary.
- e. Landscape design shall include a variety of open space areas, utilizing earth mounds of variable heights were feasible with a variegated grouping pattern of trees, shrubs and groundcover.

- f. Within vehicle parking areas, standards as required in Section 4.10.1.2 (e)(I)(f) shall be applicable.

4.10.1.4 Industrial District

The Industrial District is intended to facilitate the economic development of the City by creating an expanded employment base. This District provides for the development of light industrial land uses which generally includes research & development facilities, light manufacturing activities, custom manufacturing, assembly, fabrication and wholesaling with related office and administrative functions.

a. Principal Uses Permitted

See Appendix C, Permitted and Conditionally Permitted Uses.

b. Limitations on Permitted Uses

Every use permitted in the Industrial District shall be subject to the following conditions and limitations:

- 1) No operations and uses conducted on the premises shall be objectionable by reason of noise, dust, mud, odor, smoke, vibrations or other similar causes; and
- 2) All uses in the Industrial District shall be conducted completely within a fully enclosed building except:
 - a) Recreational facilities customarily conducted in the open;
 - b) Exterior storage provided that is completely surrounded by a masonry wall at least seven feet in height;
 - c) Outdoor dining areas and employee parking; and,
 - d) Surface parking lots.
- 3) No automobile dismantling businesses are allowed in this district.
- 4) Small professional offices up to 15,000 square feet in floor area may be the principal permitted use except that, in conjunction with a permitted manufacturing, light industrial, warehousing, distribution, light assembly, processing or mixed use the size may be greater.

c. Permitted Accessory Uses

The following uses are permitted in the Industrial District when developed and used in conjunction with one or more of the principal permitted uses.

- 1) Employee recreation facilities and play areas.
- 2) Wholesale sales and distribution.
- 3) Surface parking lots.
- 4) Open storage incidental to a principal use provided the storage is screened from public view by a solid masonry or stucco stud wall of one color to be not less than seven feet in height. Such storage shall not be stacked above the height of the wall and shall not be located within the required parking area.

d. Site Development Standards

Any "integrated" industrial center(s) shall be exempt from lot size and frontage requirements; said centers shall be subject to the discretionary actions of the Planning Commission.

1) Lot Area

Every lot in the Industrial District shall be a minimum of 43,560 square feet (1.0 acre). Minimum width shall be 125 feet; minimum depth shall be 250 feet.

2) Building Height

No building or structure within the Industrial District shall exceed a height of 35 feet, however that building utilizing a parapet wall in order to hide rooftop equipment shall not exceed 40 feet. For buildings located within 75 feet of Pacific Avenue right-of-way, a one (1) story or twenty (20) feet building height limitation shall apply.

3) Front Yard Setback Requirements

In the Industrial District the front yard setback shall not be less than fifteen (15) feet from the street right-of-way. These setbacks shall be fully landscaped and maintained in accordance with sub-section (e) Landscape Requirements. No parking shall be allowed in this setback area. The following exception is applicable:

- a. All front yard setbacks abutting Second Street between Mountain Avenue and Pacific Avenue, shall not be less than 25 feet. No parking shall be allowed in this setback area. These setbacks shall be fully landscaped and maintained in accordance with sub-section (c), (Landscape Requirements).

4) Side Yard Setback Requirements

No side yard setback is required unless one of the following conditions exist, in which case the stated setback requirement shall be applicable:

- a) Such side yard abuts one of the following dedicated roadways:

<u>Street</u>	<u>Setback</u>
Pacific Avenue	Fifty (50) feet minimum
Second Street, between Mountain and Pacific Avenues	Twenty (20) feet minimum (fully landscaped)
First Street	Twenty (20) feet minimum (fully landscaped)

- b) Such side yard abuts any other dedicated street in which case a ten (10) foot suitably landscaped setback is required.
- c) Where the abutting property on the side yard is in another district in which case a minimum fifteen (15) foot suitably landscaped setback is required. Landscaped setback shall include a minimum seven (7) foot high masonry wall; location of this wall shall be approved by the City Planning Department. No parking shall be allowed in this setback.

5) Rear Yard Setback Requirements

No rear yard setback is required, unless the abutting property is in a different district or adjacent to a dedicated street in which case an minimum fifteen (15) foot suitably landscaped setback is required. Landscaped setback shall include a seven (7) foot high masonry wall; location of this wall shall be approved by the City Planning Department. For property abutting Pacific Avenue, a fifty (50) foot setback is required with at least five (5) feet of landscaping adjacent to the right-of-way.

6) Yard Area

Where two or more legally established building sites at the time of adoption of these regulations are combined into one site, the minimum yard area for the new

site shall be the aggregate of yard areas that would have been required for each of the original sites under these regulations.

g. Landscape Requirements

- 1) In the Industrial District, landscaping requirements shall be as follows:
 - a) In the front yard setback, an at-grade or raised landscape planter shall be employed except in front yard setbacks fronting First or Second Street, between Mountain Avenue and Pacific Avenue, where the planting areas shall consist of undulating earth berms not less than 42" in height. A permanent drought tolerant ground cover such as turf, ivy or gazania, and trees are the basic planting materials recommended. At a minimum, one (1) 15 gallon tree shall be provided for every 25 feet of lot frontage on a dedicated street. Tree groupings shall be informal. A recommended plant palette is included as Appendix B of this Plan.
 - b) Landscaping is not required in side yard setbacks unless the side yard abuts a dedicated street or abuts a different zoning district.
 - c) Landscaping for the purpose of this chapter shall consist of trees, shrubs, vines, annual and perennial flowers, ground cover or any combination thereof. Drought tolerant species shall be used whenever feasible.
 - d) A minimum of 15 percent of the total of any development site shall be devoted to landscaping which includes all landscaped setback areas. One-third of the required landscaping shall be in the parking lot areas.
 - e) All landscaped areas shall be provided with permanent watering facilities.
 - f) All landscaped areas shall be maintained in a neat, clean and healthy condition. This shall include proper pruning, weeding, removal of litter, fertilizing and replacement of plants when necessary.
 - g) Landscape design shall include a variety of open space areas, utilizing earth mounds of variable heights where feasible with a variegated grouping pattern of trees, shrubs and ground cover.
 - h) Within vehicle parking areas, standards as required in Section 4.10.1.2 (e)(1) (f) shall be applicable.

4.11 Parking Requirements

4.11.1 General Provisions

- a. Parking facilities shall be designed so that a car within a facility will not have to enter a street to move from one location to any other location within the same facility.
- b. Wheel stops shall be provided on all parking spaces abutting a sidewalk or property line unless a 2'-6" landscaped area is provided in lieu of wheel stops as shown below.
- c. Parking facilities shall be designed in such a manner that any vehicle on the property will be able to maneuver as necessary so that it may exit from the property traveling in a forward direction.
- d. Off-street parking facilities, shall be provided for any new building constructed, for any new use established, and for any addition or enlargement of an existing building or use. See Table 6 for the number of off-street parking spaces required.

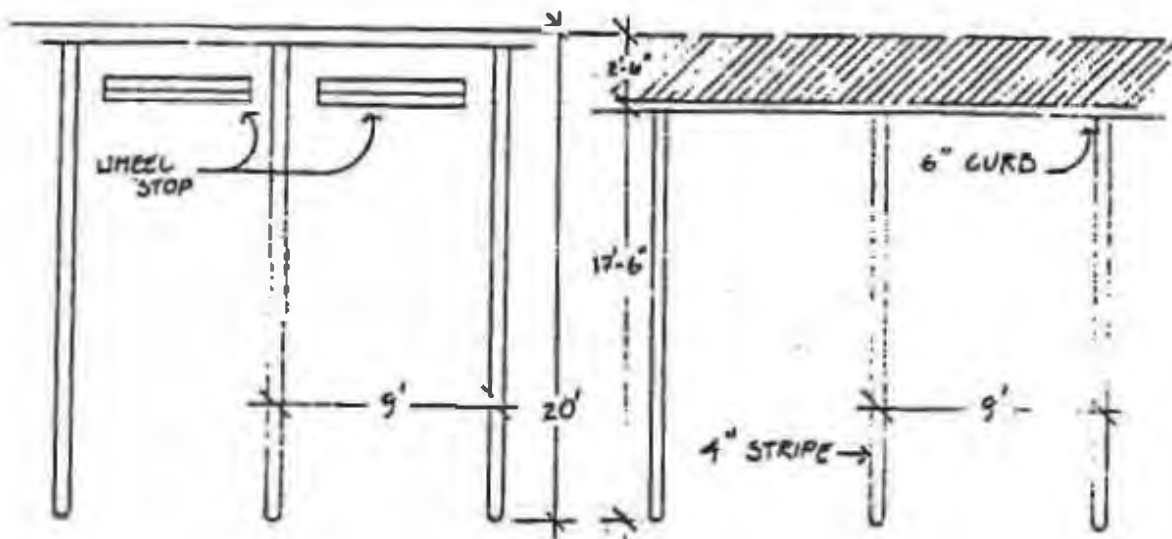




TABLE 6
AUTOMOBILE OFF-STREET PARKING SPACES REQUIRED

<u>USE</u>	<u>PARKING REQUIREMENT</u>
a. Light Manufacturing & Light Industrial	1 space/400 square feet of gross floor area devoted to manufacturing plus 1 space for every 250 square feet of office floor area.
b. Research and Development (office with on site testing facilities)	1 space/400 square feet of gross floor area. 1 space for every 250 feet of office floor area.
c. Warehouse	1 space for every 750 square feet of warehouse or storage floor area.
d. Stores, Shops and other commercial uses	1 space/250 s.f. of gross floor area or portion thereof.
e. Restaurants, Bars, Cafes, Dining Rooms	1 space/100 s.f. of gross floor area.
f. Offices	1 space per 250 s.f. of gross floor area.
g. Hotel/Motel	1 space for each living or sleeping unit; plus 1 for each 5 units. For each unit with a kitchen, add 2 spaces in a garage or carport.
b. Auto Sales/Rentals/ Retail Nurseries	Not less than 1 parking space for every 1,000 square feet of gross land area used for open display, not to exceed 10,000 square feet.

- e. No building or use of land lawfully existing on this effective date of this ordinance shall be considered non-conforming solely because of the lack of off-street parking facilities required by this Specific Plan.
- f. Structures existing at the time of the adoption of this Specific Plan will not be required to provide parking for new uses in accordance with this provision as long as the new use is of the same general nature as the prior use and no expansion of floor area is proposed.
- g. Where the application of the following parking schedules results in a fractional space, then the fractions shall be rounded to the next higher whole number.
- h. Parking requirements for uses not specifically listed in the following schedule shall be determined by the City Planning Department for the proposed use on the basis of requirements for similar uses.
- i. Shared Parking facilities may be allowed for uses with significantly different peak hours of operation. Requests for shared parking must meet the following requirements:
 - 1) A parking study shall be submitted by the applicant to the Planning Commission demonstrating that there will not exist substantial conflict in the peak hours of parking demand for the uses for which joint use is proposed.
 - 2) The number of parking stalls which may be credited against the requirements of the structures or uses involved shall not exceed the number of stalls reasonably anticipated to be available during differing hours of operation.
 - 3) A written agreement shall be drawn to the satisfaction of the City Attorney and executed by all parties concerned assuring the continued availability of the number of stalls designed for joint use.
 - 4) This provision is suggested as an administrative guideline. The Planning Commission shall ultimately determine the size of allowable parking reductions.
- j. Parking spaces shall be developed in such a manner as not to be detrimental to surrounding properties. Parking facilities adjoining an existing residential zone shall be screened with a masonry wall of a seven (7) foot minimum height.

4.11.2 Design of Parking Spaces

a. Visitor Spaces

Visitor or non-tandem spaces shall be a minimum of 9 feet in width by 20 feet in length (see Exhibit 30).

b. Parallel Parking

Parallel parking spaces shall be a minimum of 8 feet in width by 24 feet in length.

c. A maximum 2 1/2 foot overhang into the landscaped area in front of wheel stops may be counted in calculating space length.

d. Minimum Dimensions for regular non-tandem parking layouts are shown below. A reduction of parking stall size must be approved by the City Planning Commission.

<u>Angle</u>	<u>Curb Length</u>	<u>Stall Length</u>	<u>Aisle</u>	<u>Bay Width</u>	<u>Interlock Bay Width</u>
30°	18'	17'-10"	12'	47'-8"	39'
40°	14'	19'-9"	13'	52'-6"	45'-7"
45°	12'-9"	20'-6"	14'	55'	48'-8"
50°	11'-9"	21'-1"	15'	57'-2"	51'-5"
60°	10'-5"	21'-10"	18'	61'-8"	57'-2"
90°	9'	20'	25'	65'	65'
A	B	C	D	E	F

e. Any driving aisle that is designated by the Fire Department as a fire lane shall be a minimum of twenty (20) feet wide.

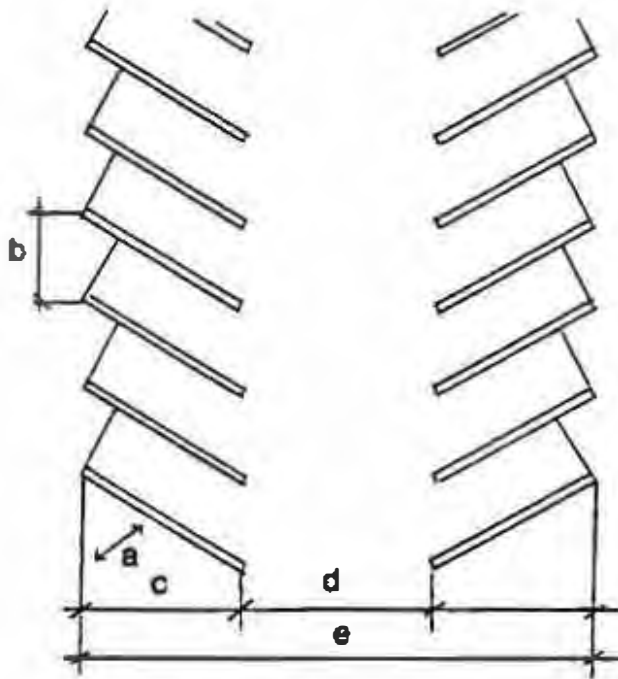


EXHIBIT 30

Fire equipment turning radius shall be designed to the satisfaction of the Fire Department with a minimum turning radius of twenty (20) feet and a minimum outside turning radius of forty-five (45) feet.

4.11.3 Off-Street Loading Facilities

- a. Each loading space shall not be less than twelve (12) feet in width, twenty-five (25) feet in length, and with an overhead clearance of at least fifteen (15) feet. All or any portion of the loading dock may be allowed within the building.
- b. Such spaces must be located on the side or in the rear of buildings whenever possible.

- c. Such space shall be so designed that it will not interfere with vehicular circulation.
- d. Such space shall be sited to avoid views from public streets or other sensitive receptors.
- e. Required loading spaces for commercial, industrial, warehouse and manufacturing uses shall be provided as set forth in Table 7:

4.12 Site Plan Review Procedures

All regular provisions of the Norco Municipal Code shall apply to all properties within the Specific Plan Project Area, including but not limited to conditional use permit, variance, time limit, public notice and hearing provisions. The provisions herein add to or supplement the above referenced provisions.

4.12.1 Site Plan Review

The Gateway Specific Plan shall be implemented through the use of site plan review. A site plan shall be required for all rehabilitation, redevelopment, expansion of existing use of structures and new development within the Specific Plan Project Area requiring a building permit. A site plan review will not be required for rehabilitation of a structure where there is no footage increase or use intensification or for residential development. This requirement is instituted for the following reasons:

- 1) To ensure consistency with the intent of the Gateway Specific Plan.
- 2) To encourage innovative commercial and industrial design and development.
- 3) To assure substantial long range compliance with the Norco General Plan.
- 4) To promote the highest contemporary standards of site design.
- 5) To adapt to specific or special development conditions that occur from time to time while continuing to implement the Specific Plan.
- 6) To facilitate complete documentation of land use entitlements authorized and conditions pertinent thereto.



**TABLE 7
REQUIRED LOADING SPACES**

Commercial, Industrial and Warehouse Buildings

	<u>Building Floor Area</u>	<u>No. of Loading Spaces Required</u>
(a)	Under 20,000 SFGFA*	1
(b)	Between 20,001 & 40,000 SFGFA	2
(c)	Between 40,001 & 80,000 SFGFA	3
(d)	Between 80,001 & 120,000 SFGFA	4
(e)	Between 120,001 & 160,000 SFGFA	5
(f)	Over 160,000 SFGFA	6

Hotels and Restaurants

1

Office Buildings

	<u>Building Floor Area</u>	<u>No. of Loading Spaces Required</u>
(a)	Under 50,000 SFGFA	1
(b)	Between 50,001 and 100,000 SFGFA	2
(c)	Over 100,000 SFGFA	3

* Square feet of gross floor area 4.12.3

Exemptions

4.12.2 Applicability

Approval of a Site Plan shall be required prior to or concurrent with a Tentative Tract or Parcel map for all proposed projects within the Specific Plan Project Area. Where no Tentative Tract or Parcel map is required, approval of a site plan shall occur prior to issuance of building permits. Authority for approval of a site plan shall rest with the Planning Commission.

The following is a list of activities which are exempt from the site plan review process. This list is not all-inclusive, the Planning Director may exempt other special activities not covered by this listing.

- o All interior changes, alterations, construction
- o Repainting
- o Reglazing, new mullions
- o Relandscaping of existing structure
- o Re-roofing with similar style roofing materials
- o Exterior mechanical (heating, air conditioning, water heater)
- o Demolition

4.12.3 Exemptions

The following is a list of activities which are exempt from the site plan review process. This list is not all-inclusive, the Planning Director exempt other special activities not covered by this listing.

- o All interior changes, alterations, construction
- o Repainting
- o Reglazing, new mullions
- o Relandscaping of existing structure
- o Re-roofing with similar style roofing materials
- o Exterior mechanical (heating, air conditioning, water heater)
- o Demolition

4.12.4 Procedures

Site plans which contain plans, drawings, illustrations, designs, reports and other detailed information as required herein, shall be submitted to the City staff for review and comment. Applicants are encouraged to submit preliminary plans for review and comment by the Planning Department prior to the final preparation of a site plan. Comment from other City departments and service agencies shall be sought by the Planning Department prior to preparing a recommendation on the finalized site plan.

Applicants should ensure that they have obtained a copy of the design guidelines which supplement this Specific Plan. This will assist the developer in achieving consistency with the Specific Plan and generally facilitate a quality project.

Upon determination that the site plan complies with the provisions of the Specific Plan and the review factors described in the design guidelines, the staff shall prepare a staff report with recommendations which shall be submitted along with the site plan to the Planning Commission at the earliest possible regular meeting. The Planning Commission shall approve, deny or conditionally approve the site plan.

4.12.5 Environmental Determination

The site plan review process is discretionary, not ministerial, and is therefore subject to the requirements of the California Environmental Quality Act (CEQA). A program Environmental Impact Report (EIR) was prepared as a part of the Plan adoption process; however, if during the preparation of an initial study, or during review of the program EIR it is determined that new information is available which discloses and affects upon the existing environmental setting, or if it is determined that a proposed development project would have impacts not previously examined in the program EIR, additional environmental assessment may be required prior to the Project's approval.

4.12.6 Revisions

Revisions that are minor in nature or reasonable extensions other than those applied as a condition of approval by the Planning Commission, shall be submitted for review and approval administratively by the Planning Director. Significant changes, additions or omissions shall be submitted for review and approval by the Planning Commission.

4.12.7 Site Plan Requirements

A site plan shall consist of plans, drawings, illustrations, designs, reports and other detailed information as required to determine compliance with the provisions of the Gateway Specific Plan and responsiveness to design guidelines. The following list of plans and information is required:

- a. Lot dimensions;
- b. All existing and proposed building and structures, including their location, size, height, proposed use, design and construction material;
- c. All existing and proposed yards and spaces between buildings and structures;
- d. All existing and proposed walls, fences and landscaping including the location, height, area, nature and type of design and material composition for the walls

and fences and the type landscaping vegetation and irrigation system proposed for such;

- e. All existing and proposed off-street parking, including the location, number of parking spaces, dimensions of the entire parking area and individual parking spaces, the arrangement of spaces, internal circulation pattern for pedestrian, equestrian, and vehicular traffic, and the landscaping thereof;
- f. All existing and proposed access to the lot, including pedestrian, equestrian, and vehicular access; the points of ingress and egress to the lot, the width, location and description of the access areas and of the streets from which access and ingress is proposed;
- g. All existing and proposed signs, including the location, size, height, location and nature of supports and material composition of sign and supports;
- h. All existing and proposed loading, including the location, area dimensions, number of loading spaces and the internal vehicular traffic circulation on the site for loading vehicles;
- i. All existing and proposed lighting, including the location and general nature of both offsite and onsite lighting; the proposed intensity thereof and diffusion thereof;
- j. All existing and proposed street or trail dedications, and improvements thereon, including the location, and nature of street or trail improvements;
- k. All existing and proposed outdoor and indoor storage activities, including but not limited to the nature of such storage, its location, proposed height and type of screening for such including the design and material composition thereof;
- l. All existing and proposed drainage and grading on site and offsite, including the location of the drains, their type and dimensions;
- m. All existing and proposed waste disposal facilities, including the results of any percolation test for onsite septic tank use; and
- n. Such other data as may be required by the Planning Director to enable the Planning Commission to make a proper review and take action thereon.

4.12.8 Procedure for site plan review and approval

All requirements for site plan review and approval as established in the Norco Municipal Code, Section 18.40.10, are applicable to this specific plan.

4.12.8.1 Mandatory Findings for Site Plan Approval

The Planning Commission shall make the following written findings before approving or conditionally approving any site plan affecting this Specific Plan.

- 1) The proposed project is compatible with the intent and purpose of the Specific Plan.
- 2) The Project will not have an adverse impact on the public health, safety, interest, convenience or the general welfare.
- 3) The Project is compatible with the intent and purpose of the regulations and design guidelines of the Gateway Specific Plan.



PLAN IMPLEMENTATION

5.0

5.0 PLAN IMPLEMENTATION

5.1 Economic Development Strategies

This section outlines the Economic Development strategies incorporated in this Specific Plan. These strategies are intended to provide the framework for financing public and private improvements within the Project Area, which can bring the circulation, land use, utilities and design components of the Plan into reality and establish other mechanisms which will ultimately promote the viability of business activity along within the Project Area.

In following brief notes on the purpose of an economic development strategy, the discussion turns to five key components which constitute the economic blueprint for the Project Area. Each component is described in terms of its rationale, potential phasing, private and public responsibilities, and appropriate funding sources.

An Economic Development Strategy is only one part of the overall Specific Plan, though it is crucial in achieving a land use pattern which is viable in the marketplace while supporting the design and regulatory objectives of the Plan. It is one of the focused blueprints for implementation of goals and objectives. In addition, it functions as a coordinating device between such features as traffic, design character, and land use. Finally, it enumerates the various administrative and financial resources which will be required for effective implementation.

Economic development covers just one of the many policy actions necessary to project the Plan's design, land use, public utilities infrastructure and circulation improvement programs into reality. Policies aimed toward retail and service land use, will eventually be evaluated on whether or not they strengthened the local economy, and also provide retail shopping opportunities.

This strategy is founded upon the findings of an economic study prepared for the Interstate 15 Corridor Study Area, which includes the Gateway Specific Plan Project Area (reference Appendix C of the I-15 Corridor Study) and in the goals and objectives established with citizen, staff and public officials input.

Finally, an Economic Development Strategy enumerates the conditions under which various types of public assistance may occur. It also specifies what is expected of both private and governmental participants in the implementation process. Because resources for implementation are always limited, it is essential to prioritize objectives for the future and to re-examine these as time brings changes to the Project Area. Economic development within the Project Area can be successfully initiated by the private sector. Participation by local government will be limited to providing a receptive climate in response to private sector proposals assisting in parcel consolidation, relocation assistance, roadway and infrastructure improvement projects and to install special mechanisms that foster private action among retail and service businesses.

Implementation policies may be divided into two types: 1) administrative mechanisms, and 2) financial incentives. Some changes and the administrative climate for economic development are generally necessary to make use of specific incentive devices. Moreover, local government policy should give priority to improving the climate for the local economy rather than on the provision of financial subsidies.

5.1.1 Redevelopment Agency Participation

With the adoption of the Gateway Specific Plan the Norco Redevelopment Agency (the "Agency") has the opportunity to consider targeting specific public actions commensurate with its ability to finance those actions.

Consideration of such should commence with a special report to the Agency outlining areas of potential public involvement, potential costs and priorities; this report should be considered by the Agency Board within 90 days from the date of adoption of the Gateway Specific Plan. This report, many parts of which are currently under consideration by City and Agency staff, should address such potential policy areas as:

1. Participation in underwriting of public street and intersection construction and reconstruction projects;
2. Public design treatment as a positive force within the Project Area;
3. Elimination of physical and bureaucratic obstacles to private development/redevelopment projects;
4. Public participation in parcel consolidation;
5. Participation in underwriting of public infrastructure and utilities improvements Projects;
6. Joint public-private participation ventures;
7. Applicable financing programs;
8. Redevelopment program schedules; and
9. Relocation assistance.

The primary objective of this policy is to provide a mechanism for public participation in areas where public action is necessary to eliminate major obstacles to the proper development of the Project Area and to encourage future economic investment within the City of Norco.

5.1.2 Infrastructure Assessment District Development and Improvement

Generating an enhanced economic climate for the Gateway Specific Plan Project Area will require public sector commitments as well as private development activity. Public participation will be required in the development of flood control/drainage improvements, street widening and construction, and intersection reconstruction/traffic control device installation.

Storm drain/flood control and traffic/circulation system investments are the heart of the Specific Plan infrastructure improvements plan. These projects (see Appendix D for a detailed Projects List) are proposed to be completed in two phases:

Phase I, Public improvements south of First Street will be completed in Phase I. Phase I Projects will include:

- A. Roadway/Circulation System Improvements
 1. Extension of Mountain Avenue southward to its intersection with the existing portion of Mountain Avenue between First and Second Streets;
 2. Widening and resurfacing, installation of curbs, gutters, etc., of Parkridge Avenue;
 3. Improvements of those portions of Hamner Avenue within the Project Area, including an 18-foot wide raised landscape median.
 4. Widening, resurfacing, installation of curbs, gutters, and sidewalks along First Street;
 5. Traffic signal improvements/modifications at the following intersections:
 - a. Yuma/Hamner
 - b. First/Hamner
 - c. Parkridge/Pacific
 - d. I-15/Yuma
 - e. First/Mountain
 6. Miscellaneous signing, striping and pavement marking;
 7. Miscellaneous street lights; and
 8. Installation of a riding trail along the southern side of First Street. This riding trail will remain as long as residential properties are located along First Street;

B. Storm Drain/Flood Control Improvements

1. Catch basins as required;
2. Curb openings as required; and
3. Installation of the boxed portions or open channel of the South Norco flood control channel (approximately 800 linear feet);

C. Sanitary Sewers and Water Distribution

1. Installation of new mains as required south of First Street only;
2. Installation of new main holes as required;
3. Miscellaneous laterals as required; and
4. Installation of new fire hydrants as required.

Phase II. Public improvements north of First Street will be completed in Phase II. Phase II Projects will include:

A. Traffic/Circulation System Improvements

1. Cul-de-sacing of Pacific Avenue;
2. Widening, resurfacing, installation of curbs, gutters and sidewalks;
3. Widening, realignment, resurfacing, installation of curbs, gutters and sidewalks. Roadway width will be 66-feet west of the new connector street/Second Street intersection. No median will be installed within the 66-foot right-of-way;
4. Traffic signal improvements/modifications at the following intersections:
 - a. Second Street/Hammer Avenue;
5. Miscellaneous striping, signing and pavement marking;
6. Miscellaneous street lights; and
7. Installation of riding trails along the south side of Second Street and along the east side of Pacific Avenue. The riding trail along Second

Street will remain as long as residential properties are located along that street. The riding trail along Pacific will be a permanent installation.

B. Storm Drain/Flood Control Improvements

1. Catch basins as required;
2. Curb openings as required;
3. Improvements to the open sections of the east/west flood control channel;

C. Sanitary Sewers and Water Distribution

1. Installation of new mains as required;
2. Installation of new manholes as required;
3. Miscellaneous laterals as required; and
4. Installation of new fire hydrants as required.

Funding for storm drain/flood control improvements could come from a Redevelopment Agency bond issue or a Mello-Roos Community Facilities District. The Mello-Roos Act has become a significant method of financing public capital facilities serving new development through bond issues authorized by special taxes. Additionally, the City will examine the possibility of either assessment district proceedings or a separate redevelopment bond issue to finance its portion of these improvements.

5.1.3 Incentive Program for Lot Consolidation

Problems of traffic congestion and marginal land use are often associated with a pattern of lots that are of an irregular form, shape or inadequate size for proper usefulness and development. Highly fragmented ownership and disjointed parcel configurations seriously hinder plans for renovation or expansion of businesses operating within these areas.

Therefore, it is recommended that an incentive program for small-lot consolidation within the Project Area be established. Assistance to support lot consolidation would apply under the following condition:

- a. Three or more parcels are involved; and
- b. Plans are submitted for a new development which exceeds the combined valuation of the land and improvements on the affected properties.

The Redevelopment Agency will work with applicants to consolidate parcels thereby stimulating eventual recycling of existing uses in affected areas. Agency assistance would be applied on a case-by-case basis, depending on the current pattern of development and the proposals submitted.

5.1.4 Project Area Marketing Program

The Economic Development Strategy prepared by Agajanian & Associates identified a number of retail and service activities which are under-represented in the Project Area.

Eliminating this leakage will be an ongoing implementation activity, having a greater potential for positive results after infrastructure improvements are in place. Types of businesses within the Commercial District which would be targets of the marketing effort include high sales tax and employment generators.

In order for this economic development program to be successful, it will be necessary to make Project Area development sites more attractive to private investment firms. The Redevelopment Agency may need to be a part of this effort for the purposes of land assembly, land writedowns, its power of eminent domain, and infrastructure and roadway improvement projects.

5.1.5 Small Business Assistance Program

New business promotion and redevelopment activity do not mean that needs of existing Project Area businesses will be neglected. The City is committed to improving the operation of those firms which are viable contributors to serving local and sub-regional consumer demands.

In order to assist the small business sector, the Redevelopment Agency will work toward establishing a program for use by businesses to finance facade upgrading and on-site improvements. The program would be open to owners of commercial property.

The following criteria should be used in establishing a small business Assistance Program within the Gateway Specific Plan Project Area:

- o Improvements shall promote and not be in conflict with the goals and objectives of the Gateway Specific Plan.
- o Eligible improvement projects may include repainting, re-stuccoing, re-roofing, landscape improvements, resurfacing parking facilities, or other minor, on-site upgrading projects.
- o All facade and on-site improvements shall be approved through the Design Review process.
- o Improvements for non-conforming uses and structures satisfy provisions established within the Gateway Specific Plan.

- o Addition of new building and site features (not to include additional floor or parking area) which reduce the degree of non-conformity of existing uses encouraged under this program.

Other small business assistance programs which the City can investigate include the establishment of a local development corporation in conjunction with the City of Norco Chamber of Commerce. Initial funding for such a program is available through Community Development Block Grant Section 108 provisions and the Small Business Administration 504 Program.

The function of this nonprofit organization would be to stimulate the health and expansion of small businesses through low-interest loans or grants. This funding would support:

- o building construction;
- o leasehold improvements;
- o renovation and modernization;
- o machinery and equipment; and
- o land acquisition.

5.1.6 Sign and Landscape Maintenance District

The City should consider the establishment of a Maintenance Assessment District for the Project Area which will accumulate funds to improve and maintain public landscaped areas, including landscaped medians, and signage. The funds will be accumulated by charging a special fee for signage and landscaped areas. The fee will be charged at the design review and building permit stage.

5.1.7 Monitoring Program

A monitoring program is incorporated in the Plan in order to track the progress of the Specific Plan and to cue the City for any needed improvements. This information will also assist in determining budgetary needs for the Project Area. In addition, it is desirable to verify periodically that the plan is working in accord with City expectations.

The Project Area will be monitored during the City's site plan review process. Exhibit 31 is an example of the type of form to be used for the monitoring procedure. A summary should be analyzed once a year to determine what changes are occurring and if they require additional City action. This review should occur prior to the City budget preparation so that in the event additional City resources are required, they can be allocated at that time.

The following steps will be necessary to make the monitoring system operational:



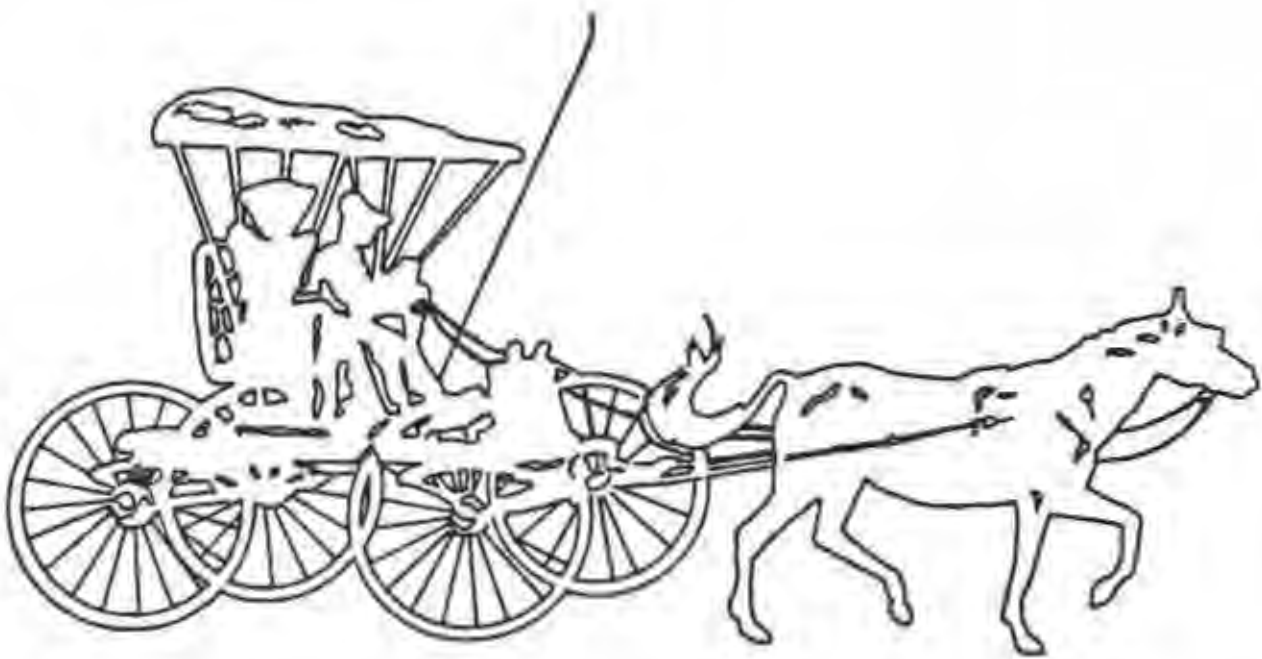
Land Use District

EXHIBIT 31

GATEWAY SPECIFIC PLAN
MONITORING PROGRAM WORKSHEET

DATE	CASE NO.	PARCEL SIZE	LAND USE	PARKING SPACES ADDED(COST)	LOADING BAYS ADDED(COST)	PUBLIC FACILITY ADEQUACY	COMMENTS; ACTIONS REQUIRED
TOTALS							

1. Set up files;
2. Establish a set of summary maps to log site plan cases;
3. Enter cases on worksheets as they are filed;
4. Enter cases on summary map as they are approved; and
5. Summarize aggregate activity in each land use district annually. Identify particularly:
 - a. Parcel consolidation, if any.
 - b. Parking/loading bay increases or reductions.
 - c. Requirements for public facility improvements (particularly sewer enlargement).
 - d. Notable problems, if any, with any aspect of the Specific Plan which may suggest amendment consideration.



GENERAL PLAN CONSISTENCY

6.0

6.0 GENERAL PLAN CONSISTENCY

The Gateway Specific Plan is intended to be consistent with all the elements of the Norco General Plan. The Plan includes policies, guidelines, standards, regulations and implementing actions for various policies of the General Plan. The Gateway Project Area has unique development potential, and the policies and regulations contained herein are meant to guide the development of this area, within the context of the City's General Plan. This Specific Plan should not be construed to be in conflict with, or in any way to modify the General Plan. Actions carried out under the jurisdiction of this Plan must be found consistent with the City's General Plan.

The Gateway Specific Plan is designed to implement the goals and intent of the various elements of the Norco General Plan. The General Plan is divided into seven elements, each of these are addressed herein. The goals of each element are listed, followed by an explanation of how they are implemented by the Gateway Specific Plan.

6.1 LAND USE ELEMENT

6.1.1 Goals

- a. To promote the health, safety and well-being of the people of Norco by adopting standards for the proper balance, relationship and distribution of the various types of land uses.
- b. To maintain a well balanced equestrian oriented community offering residential zones, offering large lots for the keeping of horses and small animals, an adequate parks and recreation system for all age groups, facilities for cultural pursuits including equestrian path dedications, planned commercial areas and industrial areas and a coordinated circulation system for the fast, safe and efficient movement of people and commodities.
- c. To encourage replacement of obsolete buildings and infrastructure with well planned, quality developments with emphasis on environmental and aesthetic standards.
- d. To encourage development of property zoned for commercial manufacturing and special commercial uses with projects of the highest quality.

6.1.2 Method of Implementation

The Gateway Specific Plan will implement these goals by providing specific development standards, regulations and guidelines which will assure quality development in the Project Area. Supporting circulation system and public facility improvements are identified.

6.2 CIRCULATION ELEMENT

6.2.1 Goals

- a. The circulation system should promote conservation of energy and land.
- b. The opportunity for movement should be increased for the elderly and other transportation needy groups in the community.
- c. Integration into developing regional public transit systems should be pursued.
- d. Acquisition and development of public roadways should be made with regard to the medium and long-term needs of the City.
- e. Alternative modes of travel to the private auto should be considered.
- f. Through traffic on residential streets should be minimized where it is likely to cause congestion or conflict with equestrian activities.

6.2.2 Method of Implementation

A traffic and parking study prepared by Linscott, Law and Greenspan Engineers accompanies this Specific Plan. This study outlines the existing traffic conditions and how those conditions will be affected by the implementation of the Gateway Specific Plan. Mitigation measures for any adverse conditions are outlined to ensure an efficient circulation system within the Project Area that is adequate to accommodate increased development activities.

6.3 HOUSING ELEMENT

6.3.1 Goals

- a. Adequate Provision of Housing;
- b. Housing and Neighborhood Preservation;
- c. Housing Accessibility;
- d. Preserving Affordability; and
- e. Standards and Plans for Adequate Sites.

6.3.2 Method of Implementation

The Gateway Specific Plan is adjacent to existing residential areas that are located to the west, north, northwest and eastern boundaries of the Project Area. The development regulations within this Plan are designed to mitigate the impacts of future industrial, commercial and office park development upon these residential areas.

There are 110.56 acres of existing residential land and 92 residential structures located within the Specific Plan Project Area. Long-term implementation of the Gateway Specific Plan could negatively impact these existing residential properties. Those residential properties that have not been rezoned are being buffered to lessen the potential impacts of future commercial, office and industrial land uses.

A relocation Plan has been developed in realization of the need to address the impacts upon those residential property owners whose property has been rezoned to a commercial, office park or industrial land use. Implementation of the policies established within that Plan will be achieved on a case by case basis.

6.4 NOISE ELEMENT

6.4.1 Goals

To protect the health and welfare of the community through the identification, control and abatement of noise.

6.4.2 Method of Implementation

The Gateway Specific Plan provides a land use plan which strives to provide compatible land uses within the Project Area while being responsive to adjacent land uses. Current adopted noise standards and criteria will remain applicable to this area. In addition, a noise analysis was prepared for the Project by Gordon Bricken and Associates. This analysis has identified sensitive noise receptors and has provided ways to mitigate potentially significant noise impacts upon those receptors.

6.5 SEISMIC AND PUBLIC SAFETY ELEMENT

6.5.1 Goals

- 1. To minimize injury and the loss of life from hazardous natural events created by either man or nature.**
- 2. To minimize damage to public and private property resulting from hazardous natural events caused by either man or nature.**

3. To minimize social and economic dislocations resulting from injury, loss of life, and property damage caused by hazardous natural events

6.5.2 Method of Implementation

The Gateway Specific Plan provides for an evaluation of public health and safety-oriented services to the Project Area. This includes fire protection, law enforcement, circulation, water supply, sanitary sewers, and drainage/flood control. An efficient/ effective system for each of these services will contribute to public health and safety. Constructing, staffing, and equipping an additional City fire station in the southern portion of the City will correct a recognized imbalance in community fire protection services, and subsequently insure the best possible fire protection. Elimination or reduction of circulation conflicts is a major thrust of the Plan, thus providing direct safety improvements. Additionally, a soils geology survey has been prepared for the Project Area by Geo-Etka, Inc., which identifies hazardous soil conditions and seismic safety hazards. Appropriate mitigation measures have been provided to mitigate potentially significant impacts.

6.5.3 Evaluation of Fire Protection Services to the Project Area

A building/structure fire in the Project Area receives the following initial or first alarm assignment: one three-person structural engine company and one two-person company (rescue squad vehicle) from City Fire Station #12, which is located at 1281 Fifth Street in the City's eastern portion; one three-person structural engine company from City Fire Station #11, which is located at 3367 Corydon Avenue in the City's western portion; and, through a formal automatic aid agreement, and when available, one two-person wildland engine company from County Fire Station #14, which is located at 1511 Hamner Avenue in the City's southern portion; as well as one City Chief Officer, when available.

No local ladder company service is currently available, although the City's 5-year Capital Improvement Program includes provision for the FY 1990-91 purchase of a combination pumper-ladder truck, which would replace the aforementioned pumper at Fire Station #11. Twenty-four-foot extension ladders are currently the longest pumper-borne ground ladders.

Nationally recognized sources have established that built-up areas of the City should be within 1-1/2 miles of a first-due engine company.

The current deployment of City fire stations/fire companies does not meet this standard in the southern portion of the City, in which the Project Area is situated. First-due engine company response distances to major intersections in the Project Area include:

<u>Intersection</u>	<u>Response Distance</u>
2nd Street at Hamner Avenue	1.90 miles
1st Street at Hamner Avenue	2.40 miles
Yuma Drive at Hamner Avenue	2.70 miles
Hamner Avenue at South City Limits	2.85 miles
2nd Street at Pacific Avenue	2.10 miles
1st Street/Pacific Avenue at Parkridge Avenue	2.40 miles
Cota Street at Parkridge Avenue	3.00 miles
2nd Street at Mountain Avenue	2.25 miles
1st Street at Mountain Avenue	2.65 miles

As identified in the City's Seismic and Public Safety Element, constructing, staffing and equipping an additional City fire station in the southern portion of the City will correct this recognized imbalance in community fire protection services, and subsequently insure the best possible fire protection.

Capital expenditure funding for this third City fire station is proposed at the time of this writing through a desired method of assessment districting involving both the Norco Hills and Gateway Specific Plan areas.

6.6 RESOURCE ELEMENT

The Resource Element was adopted by the City in 1973. The Gateway Specific Plan implements the following:

6.6.1 Goals

- a. Implement programs that will encourage and contribute to the economic, social, psychological and physical health, safety, welfare and convenience of the community.
- b. Improve the quality of and increase the amount of open space and recreational lands and opportunities within the City, including the development of highway medians and greenbelts.
- c. Revitalize, upgrade and improve substandard areas of the City and develop flood control facilities and eliminate the erosion of soils and open space.
- d. Establish guidelines for adequate standards in the building codes and zoning ordinances to insure humane, safe and aesthetically sound living and working conditions and to prevent community deterioration.

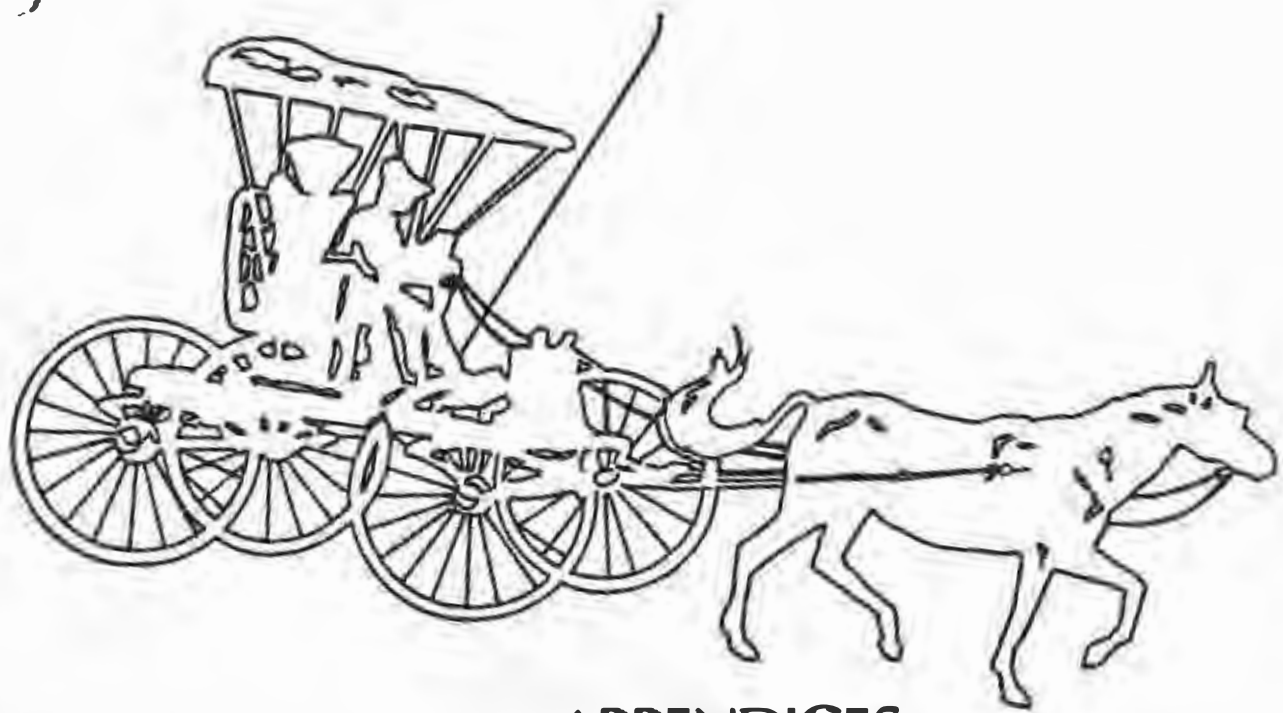
6.6.2 Method of Implementation

The Gateway Specific Plan provides for the development and utilization of measures to control flooding and to prevent erosion of soils. While the Specific Plan does not designate lands to be used for open space and active recreational uses, it does plan for the improvement and upgrading of the City's circulation system which includes landscaped median strip greenbelts and street tree programs. The Plan also provides for the development of riding trails along First and Second Streets and Pacific Avenue, in accordance with existing City policy. The Plan also calls for the general upgrading of the visual and aesthetic quality of the Project Area and the City of Norco in general.

6.7 SCENIC HIGHWAYS ELEMENT

The Scenic Highways Element was adopted by the City in 1975. While State law requires a Scenic Highways Element, there are no mandatory requirements regarding the content of the element.

Because there are no Scenic Highway corridors within the Project Area or the City of Norco, and none are planned for future consideration, this element of the Norco General Plan has not been addressed in the preparation of the Gateway Specific Plan.



APPENDICES

7.0

APPENDIX A

Gateway Specific Plan
Boundary Descriptions

Beginning at a point on the city limit line at the I-15 Freeway to a point approximately 320 feet northerly of First Street and then along the easterly R/W of Hammer Avenue to the North side of the South Norco Channel R/W,

Then to the west along the north side of the South Norco Channel R/W to a point approximately 250 feet west of Industrial Avenue,

Then northerly along the westerly property line of the properties on the west side of Industrial Avenue to a point approximately 660 feet south of Second Street,

Then east approximately 400 feet along the property line that is 660 feet south of Second Street,

Then north along the property line to Second Street R/W (North side),

Then east approximately 425 feet to a point that is approximately 120 west of Hammer Avenue,

Then north approximately 1050 feet to a property line,

Then west along the property line to the south R/W line of the North Norco Channel,

Then ~~southwesterly~~ along the south R/W line of the North Norco Channel to property line approximately 850 away,

Then southerly to the northerly R/W line of Second Street,

Then westerly along the northerly R/W line of Second Street to the westerly R/W line of Pacific Avenue,

Then southerly along the westerly R/W line of Pacific Avenue to the southerly R/W line of Parkridge Avenue,

Then easterly along the southerly R/W line of Parkridge Avenue to the easterly R/W line of the South Norco Channel,

Then ~~northwesterly~~ along the easterly R/W line of the South Norco Channel to an angle point approximately 500 feet away,

Then approximately 160 feet to the southerly R/W line of First Street,

Then southeasterly along the rear property line of those lots at the end of Three Bar Lane (a cul-de-sac),

Then to the southwest along the rear property lines and projection to the southwest of the properties on the southerly side of Three Bar Lane to the southerly City Limits,

Then along the southerly City Limits line to the City Limits angle point in the easterly R/W line of Cota Street,

Then along the City Limits line to the I-15 Freeway R/W the point of beginning.

APPENDIX B

APPENDIX B

RECOMMENDED GATEWAY SPECIFIC PLAN PROJECT AREA
PLANT PALETTE

<u>SPECIES</u>	<u>COMMON NAME</u>	<u>HEIGHT/SPREAD (In Feet)</u>	<u>GROWTH RATE</u>	<u>EVERGREEN (E) DECIDUOUS (D)</u>	<u>COMMENTS</u>
<i>Acacia baileyana</i>	Bailey Acacia	20-30/20-40	Fast	E	Short-lived (20-30 years)
<i>A. longifolia</i>	Sydney Golden Wattle	20/20	Very Fast	E	
<i>Aesculus californica</i>	California Buckeye	40/20		D	Drops leaves in July if without water. Tolerant of urban pollution.
<i>Albizia julibrissin</i>	Silk-Tree	30-50/30		D	Good canopy. Can be messy leaf and pod litter.
<i>Bauhinia variegata</i>	Purple Orchid Tree	20-35/		D	Light pink to orchid purple flowers in spring.
<i>Brachychiton scarifolium</i>	Flame Tree	60/	Moderate	D	Produces great clusters of small red/orange flowers.
<i>Brachychiton populneus</i>	Bottle Tree	30-50/30	Moderate	E	
<i>Callistemon citrinus</i>	Lemon Bottlebrush	20-25/	Fast	E	
<i>Callistemon viminalis</i>	Weeping Bottlebrush	20-25/	Fast	E	Fire retardant
<i>Calocedrus decurrens</i>	Incense Cedar	50-70/		E	
<i>Casuarina</i> spp.	She-Oak			E	Tolerant of dry or wet soil
<i>Casuarina cunninghamiana</i>	River She-Oak	70/		E	Tallest and largest of Casuarina
<i>Catalpa speciosa</i>	Western Catalpa	40-60/30-40	Fast	D	Needs occasional deep watering. Pest free.

<u>SPECIES</u>	<u>COMMON NAME</u>	<u>HEIGHT/SPREAD</u> <u>(In Feet)</u>	<u>GROWTH</u> <u>RATE</u>	<u>EVERGREEN (E)</u> <u>DECIDUOUS (D)</u>	<u>COMMENTS</u>
<i>Cedrus deodara</i>	Deodar Cedar	80/40	Fast	E	May need pruning. Heavy needle drop. May react to reflected heat.
<i>Carstonia siliqua*</i>	Carob, St. John's Bread	30-40/30-40	Moderate	E	Grows 20 feet in 10 years. Fire retardant.
<i>Cercidium</i> spp.	Palo Verde	25/75	Fast	D	Summer dormant. Fire retardant.
<i>Cercis occidentalis</i>	Western Redbud	10-18/			Leaves yellow Fall color. Deep rooted.
<i>Chorisia speciosa</i>	Floss Silk Tree	30-60/	Moderate	E	Pink, purplish rose or burgundy, large showy flower.
<i>Cinnamomum camphora*</i>	Camphor Tree	30-50/30-50		E	Casts deep shade.
<i>Opuntia acardioloba</i>	Carrot Wood	30/20	Slow	E	Gives heavy, dense shade.
<i>Opuntia arborescens</i> <i>Glabra</i>	Smooth Arizona Cactus	40/		E	Conical shape. Grow in full sun.
<i>Eriobotrya japonica</i>	Loquat	15-30/15-30		E	Grow more slender than round if in shade. Fruits. Doesn't do well in reflected heat. Susceptible to fire blight. Fire retardant.
<i>Eucalyptus</i> all species except: <i>E. Picifolia</i> <i>E. Saligna</i>		20-100/	Fast	E	Good dense crown. Fast grower and long lived.
<i>Ficus retusa</i>	Indian laurel Fig	25-30/	Moderate	E	
<i>Ficus rubiginosa</i>	Rustyleaf Fig	20-50/		E	

<u>SPECIES</u>	<u>COMMON NAME</u>	<u>HEIGHT/SPREAD (In feet)</u>	<u>GROWTH RATE</u>	<u>EVERGREEN (E) DECIDUOUS (D)</u>	<u>COMMENTS</u>
<i>Fraxinus velutina</i>	Arizona Ash			D	Deep-rooted; pyramidal when young; spreading later.
<i>Geijera parviflora</i>	Australian Willow	25-30/20	Moderate	E	Needs pruning to correct form. Deep-rooted. Casts light-shade.
<i>Ginkgo biloba</i>	Maidenhair Tree	50-80/40		D	Deep-rooted. Needs occasional deep watering. Pest-free.
<i>Gleditsia triacanthos</i> esp	Thornless Honey Locust	35-70/35-70		D	Yellow fall color. Needs full sun.
<i>Grevillea robusta</i>	Silk Oak	50-60/	Fast	E	Pyramidal when young; broad-topped when old. Requires frequent caking from leaf fall.
<i>Heteromeles arbutifolia</i>	Toyon	15-25/		E	Multi-trunked. Fire retardant.
<i>Jacaranda acutifolia</i>	Jacaranda	25-40/15-30		D	High light penetration.
<i>Roulstaneria paniculata</i>	Golden Rain Tree	20-35/10-40	Slow-Med	D	Open branching; slight shade. Deep-rooted.
<i>Lagarstroemia indica</i>	Grape Myrtle	6-30/6-30	Moderate	D	
<i>Leptospermum laevigatum</i>	Australian Tea Tree	30/		E	Grow in sun.
<i>Liquidambar styraciflua</i> esp	American Sweet Gum	60/20-25	Moderate	D	Brilliant fall foliage. Narrow and erect shape until mature.
<i>Liriodendron tulipifera</i>	Tulip Tree	60-80/40	Fast	D	
<i>Lyrothamnus floribundus</i> 'Asplenifolius'	Fernleaf Santa Cruz Ironwood	25-60/20-40		E	Needs well-drained soil.

<u>SPECIES</u>	<u>COMMON NAME</u>	<u>HEIGHT/SPREAD (In Feet)</u>	<u>GROWTH RATE</u>	<u>EVERGREEN (E) DECIDUOUS (D)</u>	<u>COMMENTS</u>
<i>Maytenus toaria</i>	Mayten Tree	30-50/20	Slow-Mod	D	Will take some drought once established.
<i>Magnolia grandiflora</i> ssp	Southern Magnolia	60/40		E	
<i>Melaleuca linarifolia</i>	Flaxleaf Paperbark	30/			Crown-umbrella-like.
<i>Melaleuca styphalioides</i>	Prickly Melaleuca	20-40/			
<i>Olea europaea</i> 'Fruitless'		25-30/25-30	Slow	E	Fruitless varieties available.
<i>Parkinsonia aculeata</i>	Jerusalem Thorn	15-30/15-30	Fast at first, slow	D	Filters rather than blocks sunlight. Fire retardant.
<i>Pinus canariensis</i>	Canary Island Pine	60-80/	Fast	E	
<i>Pinus eldarica</i>		30-80/	Moderate	E	
<i>Pinus halepensis</i>	Modell Pine	30-80/	Moderate	E	
<i>Pistachia chinensis</i> *	Chinese Pistache	50/50	Moderate		Subject to verticillium wilt.
<i>Platanus acerifolia</i>	London Plane	70-100/70-100		D	Pollution tolerant.
<i>Platanus racemosa</i> *	California Sycamore	40-90/	Fast	D	Best in riparian areas.
<i>Populus fremontii</i>	Fremont Cottonwood	40-60/	Very Fast	D	Riparian tree. Natural areas only.
<i>Prunus caroliniana</i>	Carolina Laurel Cherry	35-40/		E	Litter from flowers & fruit is problem when planted over paved areas.

<u>SPECIES</u>	<u>COMMON NAME</u>	<u>HEIGHT/SPREAD (In Feet)</u>	<u>GROWTH RATE</u>	<u>EVERGREEN (E) DECIDUOUS (D)</u>	<u>COMMENTS</u>
<i>Prunus cerasifera atropurpurea</i>	Plum	15-20/		D	Short-lived.
<i>Prunus ilicifolia</i>	Hollyleaf Cherry	20-30/	Moderate	E	Extremely drought- tolerant. Fruits.
<i>Prunus Lyoni</i>	Catalpa Cherry	10-45/		E	Fruits. Fire retardant.
<i>Pyrus kawakamii</i>	Evergreen Pear	15-25/	Moderate	E	Requires pruning and spraying.
<i>Quercus agrifolia*</i>	Coast Live Oak	20-70/70	Mod-Fast	E	Dense; sheds leaves in Spring. Can grow 25 ft. in 10 years.
<i>Q. chrysolepis</i>	Canyon Live Oak	20-60/		D	
<i>Q. douglasii</i>	Blue Oak	50/50		D	Low branching.
<i>Q. engelmannii</i>	Wine Oak	60/		E	Native to Southern California.
<i>Q. ilex*</i>	Holly Oak	40-70/40-70	Moderate	E	Best for sunny areas and faster growing than other Quercus.
<i>Q. kelloggii</i>	California Black Oak	30-80/	Moderate	D	
<i>Q. lobata</i>	Valley Oak	70/70	Mod-Fast	D	Where ground-water can be tapped, grows 1 1/2-3 ft. per (defoliation in Dec.)
<i>Q. suber*</i>	Cork Oak	70-100/70-100	Moderate	E	Needs good drainage. Thick corky bark.
<i>Rhus lancea</i>	African Sumac	25/	Slow	E	Open, spreading habit. Fire retardant.

<u>SPECIES</u>	<u>COMMON NAME</u>	<u>HEIGHT/SPREAD</u> <u>(In feet)</u>	<u>GROWTH</u> <u>RATE</u>	<u>EVERGREEN (E)</u> <u>DECIDUOUS (D)</u>	<u>COMMENTS</u>
<i>Salix molle</i>	California Pepper Tree				
<i>S. torreyana</i>	Brazilian Pepper	30/30	Moderate	E	Fire retardant.
<i>Sophora japonica</i>	Chinese Scholar Tree	50-75/50-75		D	Fruits.
<i>Tipuana tipu</i>	Tipu Tree	25-50/	Fast	D	Needs occasional deep soaking. Apricot to yellow flowers in June-July. Brittle branches.
<i>Tilia tomentosa</i>	Silver Linden	40-50/20-30	Slow	D	Average deciduous period (defoliation in Oct.-Nov.).
<i>Tristania conferta</i>	Brisbane Box	30-60/	Mod-Fast		Chlorosis is problem in Los Angeles area.
<i>Ulmus parviflora</i>	Chinese Elm	40-50/40-50		E/D	Fruits.
<i>Umbellularia Californica</i> Walnut (Juglans)	California Bay Laurel	20-25/20-25		E	Pest-free.
<i>J. californica</i>	Southern California Black Walnut	15-30/		E	
<i>Washingtonia filifera</i>	California Fan Palm	50-60/		E	
<i>Washingtonia robusta</i>	Mexican Fan Palm	80-100/	Moderate	E	
<i>Zelkova serrata</i>	Japanese Zelkova	50-80/50-80		D	Red Fall color.

* Recommended street trees.

<u>SPECIES</u>	<u>COMMON NAME</u>	<u>EVERGREEN (E)</u> <u>DECIDUOUS (D)</u>
	<u>SHRUBS</u>	
Abelia grandiflora	Glossy Abelia	E
Agapanthus africanus	Lily of the Nile	E
Aloe spp.	Aloe	
Arctostaphylos densiflora 'Howard McMillan'	McMillan Manzanita	E
A. edmundsii	Little Sur Manzanita	E
A. Emerald Carpet	Emerald Carpet Manzanita	E
A. Festival	Festival Manzanita	E
A. Greensphere	<u>Greensphere</u> Manzanita	E
A. hookerii	Monkey Manzanita	E
A. Indian Hill	Indian Hill Manzanita	E
A. manzanita	Common Manzanita	E
A. SandSprite	SandSprite Manzanita	E
A. Sea Spray	Sea Spray Manzanita	E
A. uva-ursi	Bearberry	E
A. Winterglow	Winterglow Manzanita	E
Artemisia sp.		E
Baccharis pilularis Twin Peaks	Coyote Bush	E
Quercus sp.	Boxwood	E

<u>SPECIES</u>	<u>COMMON NAME</u>	<u>EVERGREEN (E)</u> <u>DECIDUOUS (D)</u>
<i>Callistemon citrinus</i>	Lemon Bottle Brush	E
<i>Calceanthus occidentalis</i>	Western Spice Bush	
<i>Carpenteria californica</i>	Bush Anemone	E
<i>Cassia</i> sp.	Senna	E or D
<i>C. arvensis</i>	Feathery Cassia	E
<i>Ceanothus</i> (all species)	Ceanothus	E
<i>Cercocarpus betuloides</i>	Mountain Mahogany	E
<i>Cistus corbariensis</i>	White Rock Rose	E
<i>C. purpurascens</i>	Orchid Spot Rock Rose	E
<i>Collinsonia pulchra</i>	Pink Dillies	E
<i>Camarostaphylos</i> <i>diversifolia</i>	Summer Holly	E
<i>Convolvulus crinitus</i>	Bush Morning Glory	E
<i>Cordylone australis</i>	Uruciera	E
<i>Cotoneaster glaucophylla</i>	Bright-bead Cotoneaster	E
<i>C. lacteus</i>	Red Clusterberry	E
<i>C. rotundifolia</i>	Redbox Cotoneaster	E
<i>Cordylone</i>		
<i>D. rigida</i>	Bush Poppy	E
<i>Diplazis longiflorus</i>	Shubby Monkey Flower	E
<i>Ordnassa viscosa</i>	Hops and Bush	E

<u>SPECIES</u>	<u>COMMON NAME</u>	<u>EVERGREEN (E)</u> <u>DECIDUOUS (D)</u> <u>PERENNIAL</u>
<i>Echium fastuosum</i>	Price of Madara	
<i>Elaeagnus angustifolia</i>	Russian Olive	D
<i>E. pungens</i>	Thorny Elaeagnus	E
<i>Erigeron arborescens</i>		E
<i>E. giganteum</i>	St. Catherine's Lace	E
<i>Feijoa sellowiana</i>	Pineapple guava	E
<i>Fremontodendron</i> California Glory	Flametal Bush	E
<i>F. mexicanum</i>	Southern Flametal Bush	E
<i>Garrya elliptica</i>	Coast silktassel	E
<i>Gaultheria shallon</i>	Salal	E
<i>Grevillea lanigera</i>	Woolly Grevillea	E
<i>G. rosmarinifolia</i>	Rosemary Grevillea	E
<i>G. thelemanniana</i>	Hummingbird Bush	E
<i>Hakea laurina</i>	Sea Urchin Tree	E
<i>H. suaveolens</i>	Sweet Hakea	E
<i>Hebe</i> spp.	Hebe species	E
<i>Helianthus russularium</i>	Sunrose	E
<i>Heteromeles arbutifolia*</i>	Tuyon	E
<i>Juniperus</i> species	Junipers	E
<i>Latana</i> species		E

EVERGREEN (E)
 DECIDUOUS (D)
 Semi-D

COMMON NAME

SPECIES

COMMON NAME	SPECIES	EVERGREEN (E) DECIDUOUS (D) Semi-D
English Lavender	<i>Lavandula officinalis</i>	
Australian Tea Tree	<i>Leptospermum laevis</i>	E
Tea Rose	<i>Caryophyllum frutescens</i>	E
Privet species	<i>Ligustrum</i> spp.	D
Hydrangea species	<i>Hydrangea</i> sp.	E
Oregon Holly Grape	<i>Ribes aquifolium</i>	E
Longleaf Mahonia	<i>M. nervosa</i>	E
California Grapes Holly	<i>M. pumila</i>	
Myrtle	<i>Myrtus communis</i>	E
Heavenly Bamboo	<i>Nandina domestica</i>	E
African Boxwood	<i>Buxus africana</i>	E
Flax	<i>Linum catharticum</i>	E
Chinese Photinia	<i>Photinia chinensis</i>	E
Tobira	<i>Pittosporum tobira</i> esp.	E
Orange Plumbeago	<i>Plumbago capensis</i> or <i>P. auriculata</i>	6cm-D
Bird of Paradise Bush	<i>Strelitzia reginae</i>	E
Caroline Laurel Cherry	<i>Prunus caroliniana</i>	E
Pomegranate	<i>Punica Granatum</i>	D

<u>SPECIES</u>	<u>COMMON NAME</u>	<u>EVERGREEN (E)</u> <u>DECIDUOUS (D)</u>
<i>Pyracantha</i> species	Firethorn	E
<i>Raphiolepis indica rosea</i>	Pink Indian Hawthorn	
<i>R. californica</i> Eve Case	California Coffeeberry	E
<i>R. ovata</i> *	Sugar Bush	E
<i>Ribes speciosum</i>	Goniberry	Semi-D
<i>R. viburnifolium</i>	Evergreen Currant	E
<i>Rosmarinus officinalis</i> *	California Blackberry	D
<i>Salvia greggii</i>	Autumn Sage	D
<i>S. leucantha</i>	Mexican Sage	
<i>S. leucophylla</i>	Purple Sage	Semi-D
<i>Senecio cineraria</i>	Costy Miller	Perennial
<i>Simmondsia chinensis</i>	Jojoba	E
<i>Taxodium cruttiana</i>	Bush Geranium	E
<i>Trichostema lanatum</i>	Woolly Blue Curl	E
<i>Viburnum</i> spp. (refer to <u>specific species</u>)	<i>Viburnum</i> species	D
<i>Xylocopa congestum</i>	Shiny Xylocopa	E or D

VINES

<i>Bougainvillea</i>	<i>Bougainvillea</i>
<i>Campis</i> spp.*	Trumpet Vine
<i>Clematis armandii</i>	Evergreen Clematis

SPECIES

pumila
era semper riana
um femino lano
aria capens is
aria spp.

ire retardant

TREE NAME

Croton Fig
Trumpet Honey suckle
Rotal Vine
Cape Honey suckle

GROUNDCOVER

Chilopsis tomentosa
Jugosa reptans
Arctostaphylos uva
Arctostaphylos uva

Arctrotheca calendula
Artemisia Caucasica
Atriplex cuneata
Atriplex semibaccata

Baccharis pilularis
var. prostrata

Baccharis pilularis cv.
Carpobrotus edulis

Carpobrotus edulis
Ceratium tomentosum
Cistus discolor Hybrid

Cremula lactea
Dioscorea Alba

Diosanthemum flouburdus
Diosanthemum speciosum
Erythraea fortunei var.

Malva
F. uria chilensis is
Rosa uniflora
Rosa is

Woolly Yarrow
Carpet Bugle
Blackberry
cv. Point Reyes
Point Reyes manzanita

Cape Weed
Caucasian Artemisia

Crawling Australian
Salt Bush
Dark Coyote Bush

Twin Peaks
Green Carpet - natal plus
Notburned fig
Sage Carpet
Snow in Summer
Hybrid rockrose

White Trailing Iceplant
Diosanthemum Iceplant

Common Winter Creeper

Wild Strawberry
Trailing Gazania
Algerian Ivy
English Ivy
Aaron's Beard
Iceplant

<u>SPECIES</u>	<u>COMMON NAME</u>
<i>Lamproloma spectabilis</i>	Trailing Iceplant
<i>Lantana montevidensis</i>	Trailing Lantana
<i>L. swallowiana</i>	
<i>Conocera japonica</i> cv. Halliana	Hall's Moneybush
<i>Halephora crocea</i>	Croceum Iceplant
<i>Halephora Crocea</i> var.	
purpurea - crocea	
<i>Myoporum parvifolium</i> var. prostrata	
<i>Osteospermum fruticosum</i>	Trailing South African Daisy
<i>Parthenocissus tricuspidata</i>	Boston Ivy
<i>Parthenocissus puberula foliata</i>	Virginia Creeper
<i>Pelargonium peltatum</i>	Ivy Geranium
<i>Potentilla verna</i>	Spring Diquefoil
<i>Rosmarinus officinalis</i> var. prostratus	Dwarf Rosemary
<i>Santolina chamaedryf viridis</i>	Lavender Cotton
<i>Santolina viridis</i>	Green Lavender Cotton
<i>Sedum acre</i>	Goldress Sedum
<i>Sedum folium</i>	Green Stonecrop
<i>Sedum confusum</i>	
<i>Sedum rubrotinctum</i>	Brown Bean
<i>S. guatemalense</i>	
<i>Taxodium chamaedryf</i>	Germander
<i>Thymus serpyllum</i>	Mother of Thyme
<i>Thymus serpyllum</i> var. <i>laughranensis</i>	Wooly Thyme
<i>Verbena peruviana</i>	Peruvian verbena
<i>Vinca major</i>	Periwinkle
<i>Vinca minor</i>	Dwarf Running Myrtle

The City of Narco is in Sunset Climate Zone 18, herein classified as an "interior climate".
(Sunset New Western Garden Book, Lane Publishing Co., 1983)

Source: Urban Futures, Inc., 1990

APPENDIX C

APPENDIX C

SUMMARY OF PERMITTED (X) AND CONDITIONALLY PERMITTED (O) USES

	DISTRICTS	
	COMMERCIAL	OFFICE INDUSTRIAL
**** COMMERCIAL RETAIL USES ****		
Antique Shops	X	
Apparel:		
a) Boutiques	X	
b) General	X	
Appliance Stores and Repair	X	
Art, Music, and Photographic Studios and Supply Stores	X	X
Auto Service Stations	O	O
Auto Service (including motorcycles, boats, trailers, campers):		
a) Sales (with ancillary repair facilities)	O	
b) Rentals	O	
c) Automobile Repair and Service	O	
d) Car-wash Washing	O	
e) Automatic Washing	O	
f) Parts and Supplies	O	
Bakeries (retail only)	X	X
Barber and Beauty Shops	X	X
Bed & Breakfast	O	O
Bicycle Shops	X	
Blueprint and Photocopy Services	X	X
Book, Gift and Stationery Shops (other than adult related material)	X	X

Per Specific Plan 90-01 Amendment 1,
"Egg Processing Facility, not including poultry" is allowed at the Industrial zone.

Per Specific Plan Amendment 2,
"Community Halls" is a conditionally permitted use in the Commercial zone.

SUMMARY OF PERMITTED (X) AND CONDITIONALLY PERMITTED (O) USES

DISTRICTS

	COMMERCIAL OFFICE	INDUSTRIAL
Candy and Confectioneries	X	X
Catering Establishments	X	
China and Glassware Stores	X	
Christmas Tree Sales lot	X	
Churches	X	
Cleaning and Pressing Establishments	X	
Cocktail lounge (bar, lounge, tavern) including related entertainment	X	
Commercial Recreation		
a) Indoor uses such as bowling and billiards	O	O
b) Outdoor uses such as tennis and basketball	O	O
Convalescent Facilities and Hospital	O	O
Curtain and Drapery Shops	X	
Day Care Centers	O	O
Delicatessens and Speciality Food Stores	X	X
Drug Stores and Pharmacies	X	
Farmers Market	X	
Floor Covering Shop	X	
Florist Shops	X	X
Furniture Stores	X	
Hardware Stores	X	O
Health and Athletic Clubs/Gyms and Weight Reducing Clinics	X	

SUMMARY OF PERMITTED (X) AND CONDITIONALLY PERMITTED (O) USES

DISTRICTS

	COMMERCIAL OFFICE	INDUSTRIAL
WHOLESALE USES		
Wholesale Commercial Outlets	X	
ENTERTAINMENT AND CULTURAL USES		
Arcades	O	O
Cultural/Artist Exhibit		
a) Indoor gallery and art sales	X	O
b) Outdoor art exhibit	O	
Discos/Clubs	O	
Theatres		
a) Dinner Theatre	O	
b) Music Theatre	O	
OFFICE AND ADMINISTRATIVE USES		
Administrative, Business, and Professional Office	X	X
Banks, Finance Service and Institutions		
- without drive-thru	X	X
- with drive-thru	O	O
Business and Office Services	X	
Design Firms	X	X
Medical/Dental Offices and Related Health Clinic	X	X
Optician and Optometrical Shops	X	X
Realtor and Real Estate Offices	X	X
Travel Agencies	X	X

SUMMARY OF PERMITTED (X) AND CONDITIONALLY PERMITTED (O) USES

DISTRICTS

COMMERCIAL OFFICE INDUSTRIAL

****** PUBLIC USES ******

Transit Facilities

Public Utility Installations

****** HOTEL USES ******

Hotel/Motel **X**

Hotel Facilities **X**

Ancillary Uses:

- | | |
|-------------------------------------|----------|
| a) Beauty/Barber Shop | X |
| b) Cafes | X |
| c) Catering Services | O |
| d) Cocktail Lounge | O |
| e) Conference/Convention Facilities | O |
| f) Florist Shops | X |
| g) Gift Shops | X |
| h) Newspaper/Magazine Stores | X |
| i) Pharmacies | X |
| j) Restaurants (sit down) | X |
| k) Tourist Information | X |
| l) Travel Agencies | X |

SUMMARY OF PERMITTED (X) AND CONDITIONALLY PERMITTED (O) USES

DISTRICTS

	COMMERCIAL OFFICE	INDUSTRIAL
Hobby Shops	X	
Ice Cream Stores and Soda Fountains	X	
Janitorial Services and Supplies	X	
Jewelry Stores	X	
Laundry (Self Service)	X	
Leather Goods and Luggage Stores	X	
Liquor Stores	X	
Message and Wire Service	X	
Music, Dance and Martial Arts Studios	X	X
Newspaper and Magazine Stores	X	
Nurseries & Garden Supplies Stores within enclosed areas	X	X
Office, Business Machine and Computer Component Stores	X	
Paint, Glass and Wallpaper Stores	X	
Pet Shops	X	
Photocopy	X	X
Record and Tape Stores	X	

SUMMARY OF PERMITTED (X) AND CONDITIONALLY PERMITTED (O) USES

DISTRICTS

	COMMERCIAL	OFFICE	INDUSTRIAL
Restaurants (sit down) :			
a) With entertainment and/or serving of beverages	O		O
b) Incidental serving of beer and wine (without a cocktail lounge bar, entertainment, or dancing)	X		O
c) Cafe, limited to 20 seats (including outdoor seating)	O		
d) Fast food: with drive-thru	O		

Shoe Stores, Sales and Repair	X		X

Specialty Retail	O		

Sporting Goods Stores:			
a) Specialty; backpacking, tennis, skiing, mountaineering, fishing-etc.	X		
b) General; Encompassing a variety of sports equipment	X		

Supermarkets	X		

Swimming Pool supplies	X		

Teller Shops	X		X

Television, Radio, VCR, Stereo and Cd Components	X		

Toy Stores	X		

Variety Department Stores, Junior Department Stores	X		

Veterinary (domestic)			
a) Non-boarding	X		X
b) Boarding	O		O

Watch and Clock Repair Shops	X		

Yardage Goods Stores	X		

SUMMARY OF PERMITTED (X) AND CONDITIONALLY PERMITTED (O) USES

DISTRICTS

**** Manufacturing ****	COMMERCIAL OFFICE	INDUSTRIAL
Small Professional Offices involved in industrial, related activities, eg. engineering, industrial design and consultation	X	
Warehouse/Distribution		X
Light Assembly and Processing		X
Mixed Use Projects including commercial, office and light industrial uses where the light industrial uses make-up at least 75% of the total square footage.		X
Wholesaling and Warehouse		X
Research Development and Testing Laboratories		X
Manufacturing, compounding, processing, packaging, or treatment of products such as:		
a) Bakery Goods		X
b) Candy		X
c) Cosmetics		X
d) Dairy Products		X
e) Drug Products		X
f) Food Products(excluding fish and meat products, beer/brew, vinegar, yeast and the rendering of fats and oils)		X
g) Fruit and Vegetable packaging		X
h) Automobiles		X
i) Manufacturing/Maintenance of electric/ neon signs		X
j) Toiletries		X
k) Novelties		X
l) Retail Lumber		X
m) Shoes		X
n) Stone monument works		X
Ambulance Service		O
Animal hospitals and shelters		O
Delicatessens		O
Resale, bulk station, and terminal		O
Recreational club		O
Stables and riding academies		O

APPENDIX D

**APPENDIX D
GATEWAY SPECIFIC PLAN
CITY OF NORCO**

MAY 23, 1990

**PREPARED FOR
CITY OF NORCO**

**PREPARED BY
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NORCO GATEWAY SPECIFIC PLAN

GRADING AND DRAINAGE

The topography of the Gateway Specific Plan area was determined from review of existing aerial topography from Riverside County Flood Control and Water Conservation District (Riverside County Flood Control & Water Conservation District) and improvement plans prepared by Caltrans for the I-15 freeway project.

Review of this information indicate that there are several localized depressed areas (sumps) that do not have positive drainage. A conceptual grading layout has been prepared to mitigate these areas, and earthwork quantities were estimated. A reduced copy of the conceptual grading drainage plan is included for reference at the back of this appendix.

Improvement plans for the existing drainage systems within the specific plan area were reviewed. Plans for the proposed improvements of storm drain lines SB and SB-1 by Riverside County Flood Control & Water Conservation District were reviewed. Proposed drainage improvements for line SB and SB-1 extensions in Yuma Drive and across Hamner Drive to I-15 freeway have been included in the infrastructure cost estimate, and are shown on the reduced copy of the conceptual grading/drainage plan for reference at the back of this appendix.

The grading of Phase I consists of lowering Hamner Avenue at First Street approximately 18 feet and the property adjacent to that intersection. The soil from this area would be used to raise the grade near the intersection of Parkridge Avenue and Yuma Drive to help relieve drainage problems as shown on topographic maps. Hamner Avenue and First Street near Hamner Avenue are the only streets where a significant change of grade is proposed. No grading is proposed for the church site on Parkridge Avenue or the existing residential areas near Parkridge Avenue and First Street.

No grading is proposed for the area south of Parkridge Avenue and Yuma Drive now being used as a self-storage business. The proposed grading of Phase I would generate 70,000 CY of excess material. This soil would be stockpiled within the Phase II area which is north of First Street. The cost of stockpiling this material is included in the cost estimate included at the end of this appendix.

The area northerly of First Street near Hamner Avenue needs to be lowered to conform with the proposed grade of Hamner Avenue as part of the Phase II grading. The material from this area would be used to fill where there are drainage problems. The 70,000 CY of excess material from Phase I would be used as fill within the Phase II area. No grading is proposed for the residential area southeast of Pacific Avenue and Second Street or the Norco Ranch.

Approximately 1,800 feet of concrete lined trapezoidal channel has been included in the Phase II drainage costs. Additional local drainage facilities will be required with new streets and site specific development.

The drainage improvements for Phase I contains the construction of a reinforced concrete box culvert for storm drain lines SB and SB-1 in the area of Parkridge Avenue and Yuma Drive as proposed by Riverside County Flood Control and Water Conservation District. Catch basins are included where the box culvert crosses major existing streets. A section of box culvert for the South Norco Channel northeast of First Street and Mountain Avenue is included as part of Phase I. Additional drainage would probably be required as part of Phase I when commercial development and additional streets are constructed, but are considered to be "local" in nature, and would be built in conjunction with site specific developments.

STREETS

The street improvements for Phase I consist of the following items:

- The widening of Hamner Avenue with median for landscaping.
- Removal of the existing streets southerly of First Street and Hamner Avenue.
- Traffic signals, pavement striping and traffic signs.
- Improvement of Parkridge Avenue, Yuma Drive and First Street.
- Extension of Mountain Avenue between Yuma Drive and First Street.

Street improvements for Phase II include the following items:

- Demolition of Mountain Avenue and Second Street.
- Improvement of Mountain Avenue and Second Street with islands in the median and landscaping.
- Proposed traffic signal at the intersection of Hamner Avenue and Second Street.
- Pavement striping and marking and traffic signs.

SEWER AND WATER

Improvement Plans for the existing water and sewer infrastructure within the Norco Gateway Specific Plan area were obtained from the Department of Public Works, City of Norco. At present, this area is being served by sewer mains ranging in size from 8" to 27". Similarly, the existing water main sizes vary from 6" to 12". There are a number of abandoned water mains on Mountain Avenue, Hammer Avenue and Second Street.

The existing 6" water mains need to be replaced by new water mains. Twelve inch lines were assumed for cost estimate purposes. Final line sizes will be determined at the time of final engineering to assure adequate fire flow for the proposed developments.

It has been assumed that the Specific Plan would be developed in two phases. Phase I included all sewer and water improvements on Hamner Avenue from Yuma Drive to South Norco Channel, First Street, Parkridge Avenue, Yuma Drive and Mountain Avenue up to First Street. Phase II including all improvements within Second Street and Mountain Avenue northerly of First Street to Second Street. The division of phases is shown on the utility plan.

In Phase I, it would be necessary to remove the existing water and sewer mains due to the proposed grading. Secondly, it is assumed that the minimum main size for sewer and water will be 12". The final size of these improvements will be determined at the time of final design and plan preparation. Once Phase I is constructed, sewer and water lines in the adjacent area (Phase II) can be connected without any service interruption. A section of the existing 18" sewer main in the South Norco Channel will have to be relocated due to the proposed storm drain box improvements.

All new streets included in Phase II are assumed to have 12" sewer and water mains. These would connect to the improvements placed during Phase I of the Specific Plan development. For the sewer line, a maximum spacing between manholes of 350' was assumed for cost estimate purposes. All lots are proposed to be serviced by a 6" lateral and a single 2" water meter supply. Fire hydrants are assumed to be spaced at 300' intervals within Phases I and II. Reduced copies of the existing and proposed sewer/water exhibits are included at the back of this appendix.

COST ESTIMATES

A detailed cost estimate by phase has been prepared for the Specific Plan area. This estimate utilizes the assumptions noted in the above discussions, and is included at the end of this appendix. Cost for traffic signal improvements are based upon input from the traffic consultant for the Specific Plan.

(Reports2\29053.440)

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 Telephone (714) 884-8804

PRELIMINARY
 ***** CONSTRUCTION COST ESTIMATE *****

Job: NORCOL
 Gateway Specific Plan, Phase I

Estimate date: 26 March 1990
 By: BECK/BEERS

	Quantity	Unit Cost	Item Total
** SITE PREPARATION & GRADING **			
Demolition/Removal:			
Remove First St.....	LS		10,000.00
Remove Valley View.....	LS		10,000.00
o Hamner (Yuma-So. Norco Chann.)	LS		32,000.00
o First St. (Hamner-Mountain)...	LS		2,500.00
Demo Parkridge (North of Cota).....	LS		5,500.00
Demo Cota (West of Parkridge).....	LS		4,000.00
Clearing & Grubbing.....	102.6 AC	800.00	82,080.00
Water Mains (Removal) 1st., W/Mtn.	1350 LF	5.00	6,750.00
Water Mains (Removal) 1st., E/Mtn.	1350 LF	5.00	6,750.00
Water Mains (Removal) Hamner.....	3600 LF	5.00	18,000.00
Water Mains (Removal) Parkridge....	3200 LF	5.00	16,000.00
Sanitary Sewer (Removal) 1st. W/Mtn	1300 LF	5.00	6,500.00
Sanitary Sewer (Removal) 1st. E/Mtn	1300 LF	5.00	6,500.00
Sanitary Sewer (Removal) Hamner....	3250 LF	5.00	16,250.00
Sanitary Sewer (Removal) Parkridge.	400 LF	5.00	2,000.00

Demolition/Removal subtotal:			230,830.00
Relocation/Restoration:			
Sanitary Sewer, S/First st.....	500 LF	20.00	10,000.00
Sanitary Sewer, Parkridge.....	700 LF	20.00	14,000.00
Sanitary Sewer, N/Parkridge.....	1000 LF	20.00	20,000.00

Relocation/Restoration subtotal:			44,000.00
Rough Grading:			
Mass Excavation.....	197000 CY	1.10	216,700.00
o Back Excavation.....	43000 CY	7.00	301,000.00

Rough Grading subtotal:			517,700.00
SITE PREPARATION & GRADING total:			792,530.00

Construction Cost Estimate - NORCO1

Quantity Unit Cost Item Total

*** SANITARY SEWERS ***

Mains:

12" Pipe, VCP, First St., Mtn-Hamm.	1300 LF	25.00	32,500.00
12" Pipe, VCP, Yuma, Parkr.-Mtn....	650 LF	25.00	16,250.00
12" Pipe, VCP, Yuma, Mtn.-Hamner...	500 LF	25.00	12,500.00
12" Pipe, VCP, Yuma, E/Hammer.....	950 LF	25.00	23,750.00
12" Pipe, VCP, Hammer.....	3250 LF	25.00	81,250.00
12" Pipe, VCP, Parkridge.....	2050 LF	25.00	51,250.00
18" Pipe, VCP, First, Pacific-Mtn..	650 LF	35.00	22,750.00
18" Pipe, VCP, Mountain, N/1st.....	350 LF	35.00	12,250.00

Mains subtotal: 252,500.00

Manholes:

Standard Manhole.....	30 EA	1,500.00	45,000.00
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Manholes subtotal: 45,000.00

Miscellaneous:

Laterals.....	30 EA	500.00	15,000.00
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Miscellaneous subtotal: 15,000.00

SANITARY SEWERS total: 312,500.00

*** WATER DISTRIBUTION ***

Mains:

12" Pipe, Hammer	3600 LF	35.00	126,000.00
12" Pipe, First, Parkridge-Mtn.....	1350 LF	35.00	47,250.00
12" Pipe, First, Mtn.-Hamner.....	1350 LF	35.00	47,250.00
12" Pipe, Parkridge.....	2600 LF	35.00	91,000.00
12" Pipe, Yuma, E/Hammer.....	1000 LF	35.00	35,000.00

Mains subtotal: 346,500.00

Meters:

Master Meter.....	LS		500.00
Service Meters.....	26 EA	125.00	3,250.00

Meters subtotal: 3,750.00

Special Assemblies:

Fire Hydrants.....	28 EA	1,600.00	44,800.00
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Special Assemblies subtotal: 44,800.00

Construction Cost Estimate - NORCO1

	Quantity	Unit Cost	Item Total
Miscellaneous, Laterals.....	26 EA	50.00	1,300.00

Miscellaneous subtotal:			1,300.00
WATER DISTRIBUTION total:			396,350.00

*** STORM DRAINAGE ***

Catch Basins: Curb Opening, 10'-wide.....	16 EA	3,000.00	48,000.00

Catch Basins subtotal:			48,000.00
Miscellaneous: 12XB RCB, SB Line & S. Norco Ch....	2400 LF	785.00	1,884,000.00
8X6 RCB, SB Line.....	700 LF	500.00	350,000.00
8X4 RCB, SB1 Line.....	2000 LF	425.00	850,000.00

Miscellaneous subtotal:			3,084,000.00
STORM DRAINAGE total:			3,132,000.00

*** ROADWAY/STREET IMPROVEMENTS ***

Pavements:			
Hamner AC & AB.....	265000 SF	1.75	463,750.00
Hamner Curb & Gutter.....	6980 LF	8.00	55,840.00
Hamner Medians.....	3280 LF	50.00	164,000.00
Parkridge, AC & AB.....	127200 SF	1.50	190,800.00
Parkridge, C & G.....	4240 LF	8.00	33,920.00
Mountain, AC & AB, 1st.- Yuma.....	90000 SF	1.50	135,000.00
Mountain, C & G, 1st.- Yuma.....	3000 LF	8.00	24,000.00
Mountain, Medians, 1st.-Yuma.....	240 LF	50.00	12,000.00
First, AC & AB, Parkridge-Htn.....	80000 SF	1.50	120,000.00
First, C & G, Parkridge-Htn.....	2650 LF	8.00	21,200.00
First, Medians, Parkridge-Htn.....	1100 LF	50.00	55,000.00
First, AC & AB, Mtn.-Hamner.....	80000 SF	1.50	120,000.00
First, C & G, Mtn.-Hamner.....	2650 LF	8.00	21,200.00
First, Medians, Htn.-Hamner.....	1100 LF	50.00	55,000.00
Yuma, AC & AB, Parkridge-Mtn.....	36000 SF	1.50	54,000.00
Yuma, C & G, Parkridge-Mtn.....	1200 LF	8.00	9,600.00
Yuma, AC & AB, Mtn.-Hamner.....	30000 SF	1.50	45,000.00
Yuma, C & G, Htn.-Hamner.....	1000 LF	8.00	8,000.00
Streets (R/W = 88 & 110) C & G.....	15340 LF	8.00	122,720.00
Streets (R/W = 88 & 110) Medians...	2500 LF	50.00	125,000.00

Pavements subtotal:			1,836,030.00

Construction Cost Estimate - NORCO1

Quantity Unit Cost Item Total

Miscellaneous:

Traffic signal modify, Yuma/Hammer	LS		85,000.00
Traffic signal install, First/Hmnr.	LS		110,000.00
Traffic signal install, Coda/Parkr.	LS		90,000.00
Traffic signal install, Parkr./Pac.	LS		90,000.00
Traffic signal install, Mtn./Conn..	LS		90,000.00
Traffic signal install, I-15/Yuma..	LS		130,000.00
Traffic signal install, First/Mtn..	LS		95,000.00
Striping, signing & pavmt. marking	LS		10,000.00

Miscellaneous subtotal: 700,000.00

ROADWAY/STREET IMPROVEMENTS total: 2,536,030.00

*** UTILITIES ***

Miscellaneous:

Street Lights.....	35 EA	2,000.00	70,000.00
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Miscellaneous subtotal: 70,000.00

UTILITIES total: 70,000.00

*** CONSULTANT FEES & SERVICES ***

Civil Engineering Design:

Geotechn., Civil Eng. & Surv. (103)	LS		895,000.00
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Civil Engineering Design subtotal: 895,000.00

CONSULTANT FEES & SERVICES total: 895,000.00

Construction Cost Estimate - NORCO1

*** MAJOR CATEGORY TOTALS (without contingencies):

SITE PREPARATION & GRADING:	792,530.00
SANITARY SEWERS:	312,500.00
WATER DISTRIBUTION:	396,350.00
STORM DRAINAGE:	3,132,000.00
ROADWAY/STREET IMPROVEMENTS:	2,536,030.00
UTILITIES:	70,000.00
CONSULTANT FEES & SERVICES:	895,000.00

*** SUMMARY:

CONSTRUCTION COST TOTAL:	8,134,410.00
CONTINGENCIES @ 10%:	813,441.00
OTHER COSTS:	0.00

TOTAL ESTIMATED COST:	8,947,851.00

NOTES:

1. Since C M Engineering & Associates, Inc. has no control over the cost of labor, materials, or equipment, or over the contractor's methods of determining prices, or over competitive bidding or market conditions, our opinions of probable project cost or construction cost provided for herein are to be made on the basis of our experience and qualifications and represent our best judgment as design professionals familiar with the construction industry, but C M Engineering & Associates, Inc. cannot, and does not, guarantee that proposals, bids, or the construction cost will not vary from opinions of probable cost prepared by the firm.

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PRELIMINARY
 ***** CONSTRUCTION COST ESTIMATE *****

Job: NORCO2
 Gateway Specific Plan, Phase II

Estimate date: 26 March 1990
 By: BECK/BEERS

	Quantity	Unit Cost	Item Total
*** SITE PREPARATION & GRADING ***			
Demolition/Removal:			
Demo Mountain (First St.-Second St)	LS		16,000.00
Demo Second St. (Hammer-E. of Pac.)	LS		12,500.00
Grading & Grubbing.....	138.5 AC	800.00	110,800.00
Water Mains (Removal) Mtn., 1st-2nd	2600 LF	5.00	13,000.00
Sanitary Sewer (Removal) Second St.	2300 LF	5.00	11,500.00
Sanitary Sewer (Removal) Mountain..	2400 LF	5.00	12,000.00
			175,800.00
Demolition/Removal subtotal:			
Relocation/Restoration:			
Sanitary Sewer, East of Mountain...	800 LF	20.00	16,000.00
Water Mains, Second, Pacific-Mtn...	1300 LF	20.00	26,000.00
			42,000.00
Relocation/Restoration subtotal:			
Rough Grading:			
Mass Excavation.....	320000 CY	1.10	352,000.00
Rock Excavation.....	70000 CY	7.00	490,000.00
			842,000.00
Rough Grading subtotal:			
SITE PREPARATION & GRADING total:			1,059,800.00

Construction Cost Estimate - NORCO2

	Quantity	Unit Cost	Item Total
* SANITARY SEWERS ***			
Mains:			
12" Pipe, VCP, Mountain, 1st.-2nd..	1000 LF	25.00	25,000.00
12" Pipe, VCP, Second St., Pac-Mtn	1050 LF	25.00	26,250.00
12" Pipe, VCP, Second St., Htn-Hamm	1050 LF	25.00	26,250.00

	Mains subtotal:		77,500.00
Manholes:			
Standard Manhole.....	12 EA	1,500.00	18,000.00

	Manholes subtotal:		18,000.00
Miscellaneous:			
Laterals.....	29 EA	500.00	14,500.00

	Miscellaneous subtotal:		14,500.00
	SANITARY SEWERS total:		110,000.00

WATER DISTRIBUTION ***

Mains:			
12" Pipe, Second St., Htn-Hammer...	1100 LF	35.00	38,500.00
12" Pipe, Mountain, 1st-2nd.....	2600 LF	35.00	91,000.00

	Mains subtotal:		129,500.00
Meters:			
Service Meters.....	29 EA	125.00	3,625.00
Master Meter.....	LS		500.00

	Meters subtotal:		4,125.00
Special Assemblies:			
Fire Hydrants.....	17 EA	1,600.00	27,200.00

	Special Assemblies subtotal:		27,200.00
Miscellaneous:			
Laterals.....	29 EA	50.00	1,450.00

	Miscellaneous subtotal:		1,450.00
	WATER DISTRIBUTION total:		162,275.00

Construction Cost Estimate - NORCO2

Quantity Unit Cost Item Total

*** STORM DRAINAGE ***

Miscellaneous:

12X6 Lined Channel, S. Norco Ch. . . 1800 LF 200.00 360,000.00

Miscellaneous subtotal: -----
360,000.00

STORM DRAINAGE total: 360,000.00

*** ROADWAY/STREET IMPROVEMENTS ***

Pavements:

Mountain Ave., AC & AB, 1st-2nd.... 153600 SF 1.50 230,400.00
 Mountain Ave., C & G, 1st-2nd..... 5120 LF 2.00 40,960.00
 Mountain Ave., Medians, 1st-2nd.... 2155 LF 50.00 107,750.00
 Second St., AC & AB, Pacific-Mtn. . 70000 SF 1.50 105,000.00
 Second St., C & G, Pacific-Mtn. 2650 LF 8.00 21,200.00
 Second St., Medians, Pacific-Mtn. 570 LF 50.00 28,500.00
 Second St., AC & AB, Mtn.-Hamner... 97600 SF 1.50 146,400.00
 Second St., C & G, Mtn.-Hamner..... 3250 LF 2.00 26,000.00
 Second St., Medians, Mtn.-Hamner... 1340 LF 50.00 67,000.00

Pavements subtotal: -----
773,210.00

Miscellaneous:

Traffic signal modify, 2nd/Hamner . LS 40,000.00
 Striping, signing & pavmt. marking LS 15,000.00

Miscellaneous subtotal: -----
55,000.00

ROADWAY/STREET IMPROVEMENTS total: 828,210.00

*** UTILITIES ***

Miscellaneous:

Street Lights..... 45 EA 2,000.00 90,000.00

Miscellaneous subtotal: -----
90,000.00

UTILITIES total: 90,000.00

Construction Cost Estimate - NORCO2

	Quantity	Unit Cost	Item Total
** CONSULTANT FEES & SERVICES ***			
Civil Engineering Design:			
Geotechn., Civil Eng. & Surv. (10%)	LS		323,000.00

Civil Engineering Design subtotal:			323,000.00
CONSULTANT FEES & SERVICES total:			323,000.00

*** MAJOR CATEGORY TOTALS (without contingencies):

SITE PREPARATION & GRADING:	1,059,800.00
SANITARY SEWERS:	110,000.00
WATER DISTRIBUTION:	162,275.00
STORM DRAINAGE:	360,000.00
ROADWAY/STREET IMPROVEMENTS:	828,210.00
UTILITIES:	90,000.00
CONSULTANT FEES & SERVICES:	323,000.00

*** SUMMARY:

CONSTRUCTION COST TOTAL:	2,933,285.00
CONTINGENCIES @ 10%:	293,328.50
OTHER COSTS:	0.00

TOTAL ESTIMATED COST:	3,226,613.50

NOTES:

1. Since C M Engineering & Associates, Inc. has no control over the cost of labor, materials, or equipment, or over the contractor's methods of determining prices, or over competitive bidding or market conditions, our opinions of probable project cost or construction cost provided for herein are to be made on the basis of our experience and qualifications and represent our best judgment as design professionals familiar with the construction industry, but C M Engineering & Associates, Inc. cannot, and does not, guarantee that proposals, bids, or the construction cost will not vary from opinions of probable cost prepared by the firm.

APPENDIX E

**MARKET FEASIBILITY OF THE GATEWAY SPECIFIC PLAN
Norco, California**

Prepared for:

**City of Norco
3954 Old Hammer Avenue
Norco, California 91760**

Prepared by:

**AGAJANIAN & Associates
Development Economic and Planning Consultants
666 Baker Street, Suite 369
Costa Mesa, California 92626**

January 24, 1990

The purpose of this brief report is to evaluate the market feasibility of the land uses proposed in the Gateway Specific Plan. The proposed Gateway Specific Plan (GSP) land uses are evaluated in this report to assess if and when they may be expected to develop. The proposed GSP land uses are considered market feasible if there is sufficient near term market demand to support their development.

This report presents a summary of our evaluation and findings in three sections : 1) a description of the land uses proposed in the GSP, 2) a description of the market demand for the I-15 corridor, and 3) an evaluation of the market feasibility of the proposed GSP land uses.

The comments in this report are based largely upon information from three documents:

1. Interstate 15 Corridor Study (Preliminary Draft, December, 1989)

A planning study prepared to help formulate a preferred development strategy for the corridor study area.

2. Gateway Specific Plan (Preliminary Draft, January, 1990)

A proposed plan for the beneficial long term development of the GSP area. The GSP area is a subarea of the I-15 Corridor Study.

3. I-15 Freeway Corridor Development Strategy (November 11, 1989)

A market analysis estimating long term land use absorption within the corridor study area based upon evaluation of development trends, competitive opportunities and constraints, and projected growth of market demand factors.

These documents should be consulted for detailed site and market information.

Our findings, based upon the market feasibility evaluation conducted, can be summarized as follows:

1. The proposed GSP area is expected to develop both earlier and faster than the northern portion of the I-15 corridor. This is likely since the GSP area is better situated in the path of development and offers a broader range of competitive sites.
2. The proposed GSP land uses are expected to reach buildout shortly after the year 2000. In aggregate, the types, amounts and locations of the proposed GSP land uses appear market supportable, and therefore feasible.
3. Nearly all of the 128.6 acres of developable land within the commercial district proposed in the GSP are expected to be absorbed by the year 2000. All commercial district land uses are, however, expected to be fully developed shortly after the year 2000.
4. All of the 21.1 acres of developable land area within the proposed GSP office park district are expected to be fully developed before the year 2000.
5. The 81.5 acres of land area within the proposed GSP industrial district is expected to be fully developed by the year 2005.

PROPOSED LAND USE DEVELOPMENT

The GSP proposes the long term development of 231.2 acres of commercial, office and industrial uses. Of this amount, 87.4 is currently vacant. The remaining 143.8 acres of land targeted for new development are currently improved but are expected to be redeveloped over the long term period.

As indicated on Table 1, the 231.2 acres of new development in the GSP area represents about 53% of the 437.5 acres of new development expected in the I-15 corridor. The remaining 206.3 acres of new development is expected to occur in the I-15 corridor north of the GSP area.

Three land use districts are identified in the GSP: commercial, office park and industrial. The commercial land use district is composed of six recommended land use categories including community service retail, leisure commercial, auto related commercial, highway commercial, local service retail and mixed use. Each of these recommended land use categories are described below (items 1 through 6). The office park land use district is expected to be developed as an office park, as described below (item 7).

The industrial land use district is expected to be developed with principally light industrial uses, as described below (item 8).

**Table 1
PROPOSED DEVELOPMENT ACREAGE
Gateway Specific Plan Area**

Land Use	1-15 Corridor	Gateway S.P.	North Corridor
Commercial			
Community Service Retail	103.8	43.7	60.1
Leisure Commercial	18.6	18.6	-
Auto Related Commercial	22.7	22.7	-
Highway Oriented Commercial	30.0	9.8	20.2
Local Service Retail	13.3	2.7	10.4
Mixed Use	66.8	31.1	35.7
Total Commercial	255.0	128.6	126.4
Office Park	40.9	21.1	19.8
Industrial	127.5	81.5	46.0
Residential	5.9	-	5.9
Public	8.2	-	8.2
Total Acreage	437.5	231.2	206.3

Source: AGAJANIAN & Associates

The recommended land uses for the three GSP districts are summarized as follows:

1. Community Service Retail (CSR)

This land use includes a wide variety of uses which provide higher priced and less frequently needed goods and services which typically include apparel stores, furniture stores, home appliance shops, discount department stores,

building material suppliers, hardware stores and other related community level goods and services. Typically, large community service retail facilities are designed to accommodate the needs of more than one community and include one or more major tenants accompanied by a variety of multi-tenant uses.

2. Leisure Commercial (LC)

The Leisure Commercial designation is intended to provide the leisure and recreational needs of residents in the community and the larger sub-regional market area. Uses allowed under this classification will include entertainment facilities, such as movie theaters, video arcades, dinner theater restaurants, health clubs, cafes, and leisure activity stores such as hobby shops, high end equestrian supply stores, specialty gardening stores and antique shops.

3. Auto Related Commercial (AC)

The Auto Related Commercial use is intended to support the existing Norco Auto Mall which is located directly east of this area. Goods and services provided here will augment those provided by auto dealerships with the Auto Mall. Uses permitted here included auto parts supply stores, auto, motorcycle, boat and RV sales, wheel and tire stores, specialty parts shops, and auto detailing service shops.

4. Highway Commercial (HC)

Highway Commercial uses are intended to primarily serve travelers on the I-15. Uses here would typically include hotels, motels and necessary support facilities such as restaurants, cafes, service stations, and tourist related retail uses such as gift shops, fast food stores, etc.

5. Local Service Retail (LSR)

Local Service Retail uses are designed to accommodate the needs of the local area. They will provide low dollar, regularly used goods and services. This category includes a wide variety of uses which typically include local markets, drug stores, variety stores, liquor stores, video stores and personal care shops.

6. Mixed Use (MU)

The MU designation will allow maximum flexibility in areas that could successfully support either commercial, retail or office oriented land uses. Rather than limiting the potential of these areas, the MU designation will afford the greatest response to fluctuating market demands in the region.

7. Office Park District (OFP)

The OFP District designation will allow office development that will include garden and single occupant facilities, as well as ancillary uses such as restaurants, cafes, copy shops, etc.

8. Industrial District (I)

The Industrial District designation allows light industrial, research and development, and office uses. Permitted industrial uses include light manufacturing, custom manufacturing, assembly, fabrication and wholesaling. Permitted office uses are those necessary to support the administrative function of the primary permitted uses.

Each of these eight recommended land use categories will be evaluated after the absorption estimates from the market analysis are presented in the next section.

ESTIMATED I-15 CORRIDOR ABSORPTION

The long term absorption for the GSP land uses were estimated as part of the market analysis conducted for the I-15 corridor and documented in the I-15 Freeway Corridor Development Strategy report. The estimated market demand for non-residential land uses for the entire I-15 corridor are presented on Table 2. These demand estimates reflect market supportable non-residential development for both 5 and 10 year periods. The entire I-15 corridor is expected to reach buildout soon after the year 2005.

The demand estimates presented on Table 2 are the same as those appearing in the market analysis report. The land use categories have, however, been changed to coincide with the land use categories used in the GSP. All land use categories used in the GSP and in the market analysis are equivalent with the following exceptions:

1. The "community service" retail land use in the GSP includes a portion of the "community serving" retail use and all of the "specialty subregional" retail use as presented in the market analysis.
2. The "auto related" commercial land use in the GSP reflect the remaining part of the "community serving" retail use from the market analysis.
3. The "mixed use" commercial land use is assumed to be equally developed with office and retail uses, though their final disposition will depend greatly on future trends and conditions.

Table 2
ABSORPTION ESTIMATES
I-15 Freeway Corridor Study Area

Use	0-5 Year Period	6-10 Year Period
Commercial District		
Community Service Retail	20 acres	20 acres
Leisure Commercial	10 acres	10 acres
Auto Related Commercial	10 acres	10 acres
Highway Oriented Commercial	10 acres	10 acres
Local Service Retail	5 acres	5 acres
Total Commercial	55 acres	55 acres
Office Park District	15 acres	15 acres
Industrial District	40 acres	40 acres

Source: AGAJANIAN & Associates

The market analysis did not estimate residential demand. The demand estimates presented in Table 2 are for the entire I-15 corridor. However, the market analysis identified important competitive factors which would promote earlier and faster development in the GSP area. These factors include:

1. The GSP is located in the path of historic development, northward along Hamner Avenue.
2. The GSP area is experiencing greater development and development interest than the northern portion of the I-15 corridor.
3. The GSP has a broader range of potential sites which would more competitively attract larger users.

These competitive factors are discussed in greater detail in the next section as they apply to the evaluation of specific land uses proposed in the GSP.

MARKET FEASIBILITY OF PROPOSED GSP USES

The market feasibility of the recommended GSP land use categories can be best evaluated by comparing the amount of proposed development in the GSP area to the amount of estimated market demand available in the corridor. This comparison is done for each of the six commercial land use categories and the office park and industrial districts as described earlier in this report.

The evaluation involves several considerations including the following:

1. The amount of each recommended land use category proposed for development in the GSP area.
2. The amount of market demand available in the corridor to support development of each land use.
3. The share of the corridor demand which the GSP area would capture by virtue of its competitive market factors.

If the proposed GSP land use acreage can be reasonably expected to be absorbed within the next 10 year period, then we may conclude that there is sufficient market demand for the plan to be considered feasible. Each of the six commercial land uses and the office park and industrial districts are evaluated separately below and then are summarized at the end of this section.

1. Community Service Retail (CSR)

The GSP proposes development of 43.7 acres of CSR uses while the market analysis indicates that 40 acres of demand are available for the I-15 corridor as a whole. Given the access, growth patterns in the corridor, and the availability of large single parcels, it is reasonable to expect that most, if not all, of the CSR uses will locate in the GSP area. Thus, the CSR uses in the GSP area will likely be developed within a 10-12 year period.

2. Leisure Commercial (LC)

The market analysis indicates that 20 acres of LC land uses can be supported in the corridor. Since no LC land uses are planned in the northern portion of the corridor, all of the GSP proposed 18.6 acres of LC land uses can be expected to be absorbed before the year 2000.

3. Auto Related Commercial (ARC)

The 22.7 acres of ARC land uses are limited to the GSP area because of their competitive need to locate near the Norco Auto Mall. These ARC

land uses are expected to predominantly serve the community level trade area with automotive services and can be considered as a portion of the "community service" retail uses identified above. The market analysis allocated 20 acres of market demand for ARC uses in light of the success of the Norco Auto Mall. Thus, it is reasonable to expect to reach buildout of ARC uses by the year 2000.

4. Highway Oriented Commercial (HOC)

The GSP proposes 9.8 acres of HOC land uses. The market analysis estimates that 20 acres of HOC land uses may be supported in the corridor by the year 2000. Most of the estimated demand is expected to occur nearer the 6th Street and 2nd Street on/off ramps with the I-15. For this reason, it is reasonable to expect that 10 acres of HOC development could be supported in both the northern portion of the corridor (near 6th Street) as well as the GSP area (near 2nd Street). Thus the 9.8 acres of proposed HOC uses in the GSP area appear to be market supportable by the year 2000. Demand for HOC land uses is expected to increase in the GSP area upon completion of the Yuma on/off ramp.

5. Local Service Retail (LSR)

The market analysis estimates that 10 acres of LSR uses can be market supportable in the corridor by the year 2000. The GSP proposes only 2.7 acres of LSR uses, indicating that most of these LSR uses will occur in the northern portion of the corridor, closer to Norco's residential areas. However, only 10.4 acres of LSR are proposed in the northern portion of the corridor. Since the estimated market demand exceeds available land, all of the proposed LSR acreage in the corridor will likely be developed before the year 2000.

6. Mixed Uses (MU)

The GSP proposes 31.1 acres of mixed uses to accommodate either retail or office uses. This land use is intended to serve as a land bank ready to absorb unanticipated growth in the area. Given the availability of developable retail and office sites it is reasonable to expect that the MU areas will be the last to develop in the GSP. Based on the absorption rates for both retail and office uses it appears that the MU areas will be developed in the 2000-2005 period.

7. Office Park District (OPD)

The market analysis identifies demand for both smaller garden offices and larger single occupant office buildings. Combined, office demand in the corridor is estimated at 35 acres of development by the year 2000. The GSP proposes 21.1 acres of office park district uses while 19.8 acres of

office uses are proposed for the northern portion of the corridor. Given the estimated office absorption it is reasonable to assume that all of the proposed office land uses will reach buildout about the year 2000.

8. Industrial District (I)

The GSP proposes 81.5 acres of industrial land uses. An additional 46 acres of industrial land uses are proposed for the northern portion of the corridor for a total of 127.5 acres. The market analysis estimates that 40 acres of industrial development may be market supportable within the corridor every 5 years. All of the proposed industrial acreage can therefore be expected to reach buildout by the year 2005.

Given the highly competitive site opportunities presented by the Norco Egg Ranch it is reasonable to expect that the GSP area will develop both earlier and faster than the industrial sites in the northern portion of the corridor. Consequently, the GSP area can be expected to reach buildout shortly after the year 2000.

Based upon the preceding evaluation of the market feasibility for each land use, we can summarize the findings as follows:

1. The proposed GSP area is expected to develop both earlier and faster than the northern portion of the I-15 corridor. This is likely since the GSP area is better situated in the path of development and offers a broader range of competitive sites.
2. The proposed GSP land uses are expected to reach buildout shortly after the year 2000. In aggregate, the types, amounts and locations of the proposed GSP land uses appear market supportable, and therefore feasible.
3. Nearly all of the 128.6 acres of developable land within the commercial district proposed in the GSP are expected to be absorbed by the year 2000. All commercial district land uses are, however, expected to be fully developed shortly after the year 2000.
4. All of the 21.1 acres of developable land area within the proposed GSP office park district are expected to be fully developed before the year 2000.
5. The 81.5 acres of land area within the proposed GSP industrial district is expected to be fully developed by the year-2005.