

DELHICOMMUNITY PLAN



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1.0 Introduction

The Delhi Community Plan is a comprehensive update of the Delhi Community Specific Plan and its land use map, the Delhi Specific Urban Development Plan (Delhi SUDP). The Delhi SUDP, first adopted in 1978, set forth designated land uses and circulation patterns for growth and development of the Community. The Community Specific Plan, adopted in 1982, contained local policy direction to guide the Community's growth. Since then, Delhi SUDP's boundary, and the Community's land use and circulation maps have been amended several times in response to growth pressures. Recent pressures for development in and around Delhi attest to the need for further expansion of the Community and for a comprehensive update and reorganization of the Delhi Community Specific Plan and Delhi SUDP.

The Delhi Community Plan is a long-range vision and a land use strategy to guide growth and development for the Community of Delhi through the Year 2020. The Plan is developed based on a thorough analysis of Delhi's physical and environmental conditions, and from extensive input from the Delhi's governing bodies, business, landowners and the Community-at-large.

The Delhi Community Plan consists of goals, objectives, policies, implementation measures, and design guidelines and standards to guide the Community's development. The Community Plan identifies necessary infrastructure improvements and a strategy for ensuring the Plan's implementation. Most importantly, the Community Plan, which is in compliance with the overall Merced County General Plan growth and development policies, ensures growth and development will be coordinated in an orderly manner.

1.1. Community Plan Area

Delhi, the northern gateway of Merced County, is located along Highway 99, north of the Merced River and south of the Stanislaus-Merced County Line (refer to Figure 1.1, Regional Map). The Community is approximately 5 miles north of the City of Livingston and 20 miles north of the City of Merced. The City of Merced is the County Seat of Government and a major regional commercial and service center. The City of Turlock, also a major regional commercial and service center, is located across the County line to the north.

Delhi is the largest unincorporated community in Merced County. The Delhi Community Plan Area encompasses approximately 2,500 acres or roughly 4 square miles. The Plan Area is generally bound by August Road / Second Avenue to the south, Merced Avenue to the west, and Bradbury Road on the north between Early Dawn Road and Vincent Road. Its eastern boundary is irregular, moving southeast from the intersection of Vincent Road and Bradbury Road to Second Avenue east of Petaluma Street (refer to Figure 1.2, Community Plan Area).

1.2. Purpose

The Delhi Community Plan serves as a bridge between County-wide goals, objectives, policies and implementation measures provided in the Merced County General Plan and the specific needs of Delhi. Adopted as an integral chapter of the Merced County General Plan, the Delhi Community Plan refines and supplements County-wide goals, objectives, policies, and implementation measures to specifically address the needs of Delhi based on its physical, demographic and economic characteristics.

The Community Plan serves as the basis for land use and other actions. For implementation, the Plan relies on tools such as the Merced County zoning and subdivision codes, capital improvement plans, and a variety of special purpose ordinances and programs. Development requiring County approvals such as zoning, subdivision maps, plot plans, use permits and other permits must be consistent with the Delhi Community Plan and its design guidelines and standards.



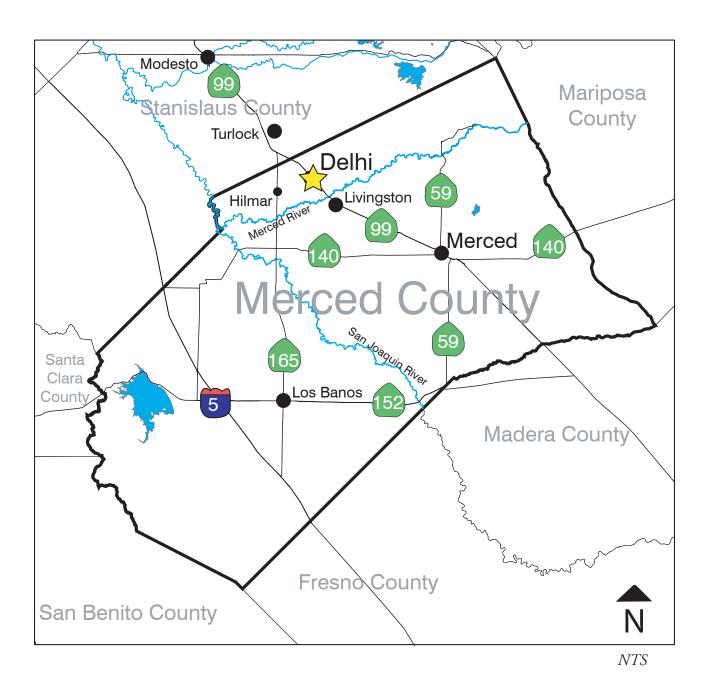


FIGURE 1.1 REGIONAL MAP



1.3. Authority

Content, adoption and subsequent amendment of a community plan are governed by California Government and Public Resources Codes. A community plan is adopted as a general plan amendment and may only be amended four times per calendar year, as per Section (§) 65358 of the California Government Code.

Section 21083.3 of the California Public Resources Code requires a community plan to include or provide reference to each of the seven mandatory elements of a general plan specified in §65302 of the California Government Code. A community plan need not address all the issues specified in §65302, if the overall general plan, in this case, the Merced County General Plan, satisfies these requirements. A community plan, however, must contain specific development policies relative to the plan area and identify measures to implement those policies.

The Delhi Community Plan includes the following Merced County General Plan elements or chapters — Land Use, Circulation, Open Space / Conservation, Noise and Safety. In addition, the Community Plan includes a Community Character and Design Guidelines Chapter, and a Public Services Chapter. The Merced County General Plan Housing and Agricultural Chapters, while containing important data, are not included in the Delhi Community Plan. Both Chapters are incorporated by reference.

Community plans should be updated periodically to conform to changes in California law and other legal requirements, and to reflect changes in local population, land development patterns, and public sentiment. In addition, the conditions and assumptions that form the basis of a community plan may change due to fluctuations in population, the economy, development in the surrounding region, and other factors. This Community Plan responds to the current and projected changes in population, economics, land use designations, and public interests since the Delhi Community Specific Plan and the Delhi SUDP were originally adopted.

1.4. A Mitigated Plan

During the Delhi Community Plan preparation, environmental studies were undertaken to identify potential environmental effects and allow for plan modifications, which either mitigated or avoided potential environmental effects. The intent was to minimize the Plan's environmental effects while providing realistic Community growth and development opportunities.

The Delhi Community Plan Environmental Impact Report (SCH #2004081181) analyzed the potential environmental effects of the Community Plan that inpart already responds to potential environmental issues. By engaging in multiple phases of environmental analysis during the Community Plan process, solutions to environmental issues, in the form of policies, implementation measures and design standards have been woven into the fabric of the Delhi Community Plan.

As such, a number of policies, implementation measures and design standards in this Community Plan serve a dual purpose of implementing the Plan, as well as, avoid potential environmental effects. These policies, implementation measures and design standards have been incorporated into this Plan to mitigate or avoid environmental effects that might otherwise result from implementing the Community Plan. Policies, implementation measures and design standards that serve a dual purpose are marked with (EM) to reflect their dual role. These policies, implementation measures and design standards cannot be amended without additional environmental analysis performed to determine the potential environmental affects a proposed amendment to a policy, implementation measures or design standards may have.

1.5. Community Organizations

The Community of Delhi is represented by a number of local agencies and appointments. The Delhi Municipal Advisory Council (Delhi MAC) advises the Merced County Board of Supervisors on planning, public works, public health and on public safety matters affecting the Community. The Delhi MAC, which consists of 15 Delhi residents appointed by the Merced County Board of Supervisors, meets monthly to address and make recommendations on issues relevant to the Community.

The Delhi County Water District, governed by a locally elected Board of Directors, provides municipal water and sewer services to a majority of the Community. The District's wastewater treatment plant is located outside of the Community Plan Area, south of Second Avenue, east of Highway 99. Municipal water is supplied by groundwater wells.

The Delhi Unified School District and the Turlock Unified School District provide educational services to the Community. Delhi Unified School District operates 3 elementary schools (grades K-5), 1 combined middle



Introduction

(grades 6-8) and high school (grades 9-12) education center, and a continuation school. The District is also responsible for operating and maintaining the Community's library and the Community's park and recreational facilities in cooperation with Merced County.

The area north of North Avenue and Swanson Road is within the Turlock Unified School District. This District has no current facilities within Merced County and students within the Plan Area are bussed to school facilities located in the City of Turlock.

1.6. Public Participation

The Delhi Community Plan is the result of a collaborative consensus building process involving the citizens of Delhi, Merced County staff, local agencies and a team of consultants. Community participation was an essential component of the Community Plan process. Public participation included a community-wide survey and a series of community workshops where residents, business

owners, and organizations of Delhi participated in identifying community needs, and on the direction and character of growth in Delhi. Monthly meetings were also held in association with the Delhi MAC over a period of 18 months.

1.7. Community Vision

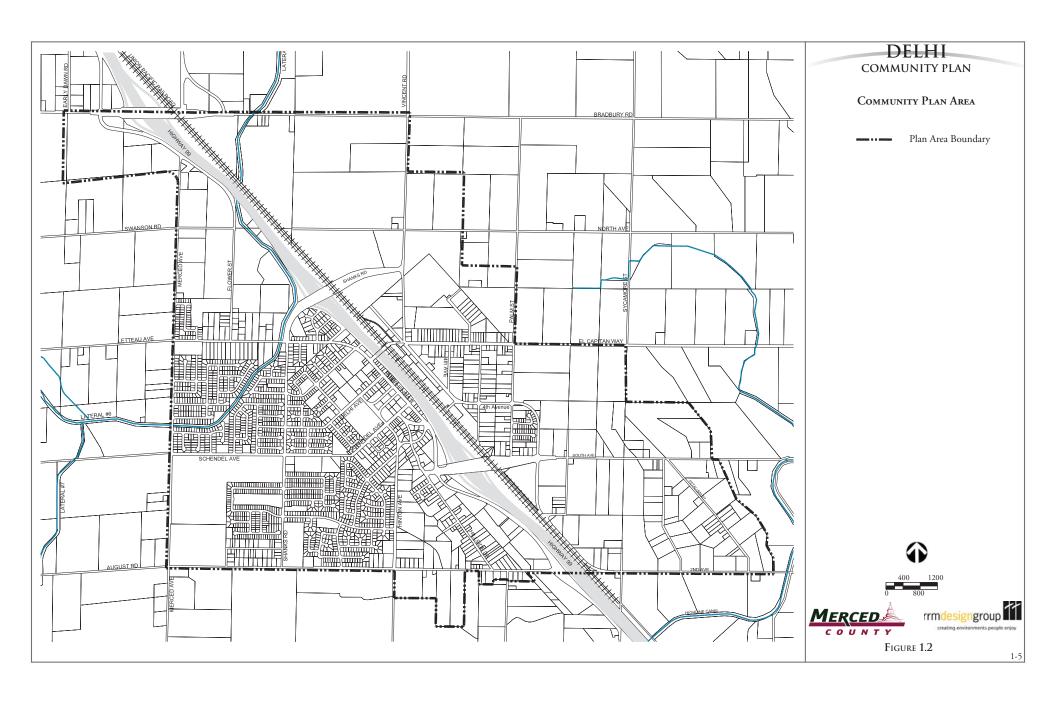
This Plan reflects the Community's priorities of promoting balanced growth, and an overall vision for Delhi's future. As a statement of the Delhi's priorities and vision, this Community Plan serves as a guide for directing and coordinating planning decisions, and guiding physical changes within the Community of Delhi. As a guide for directing and coordinating Delhi's future, this Plan also defines the desired character and quality of development, as well as, directing the process for how development should proceed. The goals, objectives, policies and implementation measures in this Community Plan define the manner in which Delhi's priorities and vision are to be achieved.

Delhi Vision 2020

In the fertile and plentiful agriculture fields of the great Central Valley lies Delhi, the northern gateway to Merced County. It is here, that a small town atmosphere fuses with the conveniences of urban living. Residents are able to enjoy many opportunities to shop locally, work within the Community, and call this place home. Residents have the opportunity to walk to and from their neighborhood park, school, and shopping center though inviting neighborhoods lined with trees and sidewalks intertwined by landscaped trails that connect the Community's public spaces together, creating a walkable community with a sense of place.

Through well planned commercial and industrial development, Delhi envisions a prosperous economic and employment base for incorporation. Delhi strives to create a sense of place within an expanding region. As the sun rises over the Central Valley, prosperity and opportunity embrace Delhi.





2.0 Background & General Growth Assumptions

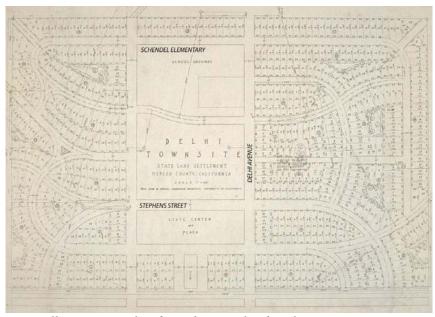
Delhi dates back to 1919, when the State of California started the second of two land settlement projects. Delhi's location was chosen because it was between two major railroads (Southern Pacific Railroad, now the Union Pacific Railroad, and the defunct Tidewater Southern) and its proximity to a state highway (now Highway 99). The original land settlement consisted of 8,400 acres of land that the State purchased and provided to qualified buyers to settle and begin agriculture production. Many of Delhi's initial residents were veterans of World War I and others who served in the United States Military whom were given preference in the ambitious State settlement plan.

The State Land Settlement Board, which had oversight of the project, worked with the Turlock Irrigation District on developing an irrigation system for farming. By 1923, a 143 mile underground pipeline system was in place to bring water to the area. Of the 8,400 acres the State purchased, 300 acres was dedicated to the Delhi Town Site. The Land Settlement Board aggressively pursued development within the town site. The town site included a pipe manufacture that produced the pipes needed to irrigate crops. Other smaller businesses followed to support the agrarian activities and community needs.

By the mid-1920s, the Delhi land settlement began experiencing difficult times as market prices from fruits and vegetables, to poultry and dairy products decreased substantially. While the State assisted many of the settlers through this difficult time, it was through the help of the federal government that many of the residents were able to remain in Delhi and continued to farm. The State withdrew from the land settlement project in 1931.

In subsequent years, agriculture began to flourish. By 1950, Delhi's population exceeded 500 inhabitants. By 1980, over 2,000 residents lived in the Community and by 1990, the Community's population increased to nearly 4,000.

The real population and housing construction boom in the Community began in the 1990s. A majority of the housing construction occurred south and west of Highway 99. During this time, Delhi also experienced a number of other significant changes. The stretch of Highway 99 through Delhi was rebuilt and improved to "freeway" status, cutting through the Community's central (downtown) district. The Highway 99 improvements resulted in the loss of over 50 percent of the commercial businesses operating in the Community's downtown area.



Delhi town site plan from the State land settlement project.





One of the first homes built during the Delhi land settlement period. This home is still in existence today.

By 2000, Delhi had become a bedroom community to cities in Merced and Stanislaus Counties, and to other urban centers as far away as the San Francisco Bay Area with over 8,000 residents. With agriculture continuing as a mainstay of the Community, Delhi has transformed from a small to a large unincorporated community.

2.1. Physical Features

Delhi is relatively flat, with few slopes, an elevated Highway 99, canals and laterals. The elevated highway creates a "window" into Delhi, as the Downtown is visible by passing motorists. Turlock Irrigation District Lateral #6 intersects the western and northern portions of the Plan Area delivering water to the region for irrigation purposes.

2.2. Land Use

Delhi is predominately a residential community surrounded by agriculture (refer to Figure 2.1, Land Use Inventory). The majority of commercial uses are located in the center of the community. Industrial designated parcels are scattered throughout Delhi. Parks are typically combined with schools.

Outside the built environment of Delhi are farmland, dairies, poultry farms and ranches. The rich farmland supports a diversity of agricultural activities including grain and cultivated crops, as well as, nut and fruit orchards. A substantial amount of land outside the Plan Area is under Williamson Act contracts. In addition to the numerous farms under Williamson Act contracts, an increasing number of property owners are placing

voluntary private agricultural conservation easements on their land, especially to the east and south (refer to Figure 2.2, Land Use Opportunities and Challenges). These easements have protected farmland around Delhi and prevented the encroachment of urban uses into productive farmland.

2.3. Redevelopment

In June 2001, Merced County commissioned a redevelopment feasibility study to evaluate the potential of establishing redevelopment areas within unincorporated communities. Redevelopment areas that were recommended for Delhi included property along Letteau Avenue and Stephens Street and surrounding residential neighborhoods. Other areas targeted by the study included east of Highway 99 along Vincent Road, 4th Street and 6th Street. Although the County has yet to designate these areas redevelopment zones, the opportunity exists to pursue redevelopment at a later date.

2.4. Circulation

Circulation within Delhi is constrained by Highway 99 and the Union Pacific Railroad, which bisects the Community. Access across Highway 99 and the Union Pacific Railroad is restricted to four crossings located at Bradbury Road, Shanks Road, El Capitan Way and South Avenue.

Access to Highway 99 is provided by three diamond interchanges located at Bradbury Road, Shanks Road and South Avenue. These interchanges were constructed in the 1980s and 1990s and have limited capacity to support growth within Delhi.

Roadways within Delhi that have been identified as important to provide circulation include August Avenue, Bradbury Road, El Capitan Way, Fourth Street, Gaddy Street, Hinton Avenue, Letteau Avenue, Merced Avenue, Schendel Avenue, Shanks Road, South Avenue, Stephens Street and Vincent Road (refer to Figure 2.3 Circulation Opportunities and Challenges). These roadways have limited capacity and will need to be upgraded in order to support growth within Delhi.

Circulation in Delhi is further constrained by some previously developed subdivisions that did not interconnect between neighborhoods.





DELHI COMMUNITY PLAN

Existing Land Use Inventory

Plan Area Boundary

Agriculture

Dairy

Residential - 1 home

Residential - 2-3 homes

Residential - 4+ / Apartments

Commercial

Heavy Commercial

Office

Industrial

Public Facility

Quasi Public

Park

Vacant

Note: Existing land use inventory based on May 2002 field observations and major subdivisions constructed between May 2002 and May 2005

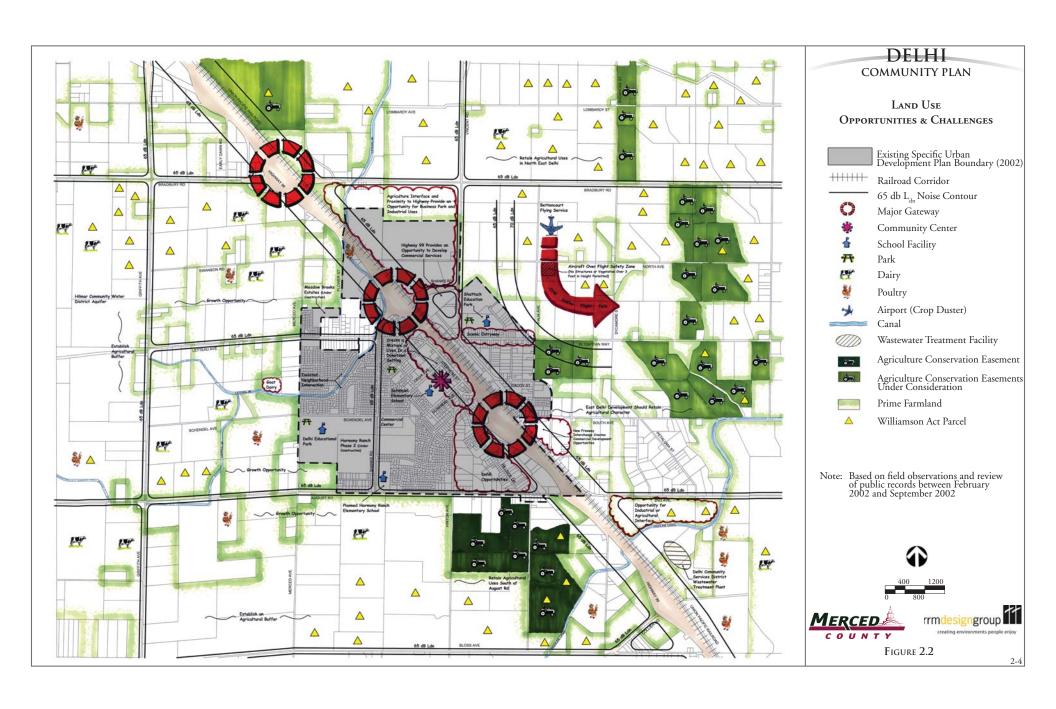








FIGURE 2.1



2.5. Community Growth Forecast

Delhi, over the past two decades, has roughly doubled in size and population every 10 years. Based on United States Census Bureau and State of California Department of Finance data, Delhi's estimated population in 1980 was 2,636 and by 1990, the Community's population increased to 3,886. By 2000, an estimated 8,022 people were living in Delhi, becoming Merced County's largest unincorporated community.

As shown in Table 2.1, Population Growth Projections, Delhi could continue to double in population every 10 years. Continued growth, however, typically translates into a demand for outward expansion of communities. As with many Central Valley communities, outward expansion also translates into the loss of productive farmland. To avoid expansion into productive farmland, the Delhi Community Plan increases development opportunities within the central core of the Community, and steers growth away from Williamson Act contract land and areas subject to agriculture conservation easements.

To evaluate potential "build-out" conditions of Delhi, modeling techniques were used to estimate likely new future development. The model provided a "mid-point" scenario between potential high and low forcasts of the possible future development. The data provided included potential population, dwelling units, and commercial / industrial square footage for each of the applicable land use designations of the Delhi Community Plan. With these figures, other models estimated needs and potential effects on traffic, schools, parks, sewer and water, fire, and other services. These figures serve as the assumed build out of the plan.

Responding to population and housing demands estimated by the land use model, the Delhi Community Plan encourages, and provides for, a range of housing densities. Growth, in the form of low density residential development opportunities, has been directed to the northern and eastern expansion areas of the Community. High density residential uses are directed to Delhi's downtown area to promote and support increased commercial viability and development. In addition, the Delhi Community Plan promotes neighborhood commercial uses near new residential areas. The Plan also encourages schools and parks in new neighborhood areas.

The residential neighborhood / commercial enclaves offer the opportunity to live, work, play, and shop in relatively small geographic area, thereby fostering a sense of community and neighborhood identity.

2.6. Growth Challenges & Strategies

The Community Plan process included an opportunities and constraints analysis. In 2001 and 2002, Merced County Community Development and Planning Department and its consultants reviewed relevant data. The analysis included an inventory of existing land uses, identification of vacant and opportunities, existing and pending agriculture conservation easements and existing Williamson Act contract parcels. This analysis also identified existing and potential circulation corridors, pedestrian and bicycle corridors, and areas subject to circulation conflicts. Along with the review of relevant materials, a community survey was compiled and incorporated the views of Delhi's citizens.

The following is a list of challenges identified during the early stages of Community Plan process and strategies to overcome those challenges. These strategies form the foundation of the Plan's objectives, policies, implementation measures, and design guidelines and standards.

Land Use

Challenge

Present land use development pattern create community edges that conflict with surrounding agriculture operations.

Strategy

Establish and promote urban land uses at the Community's edge which are harmonious with agriculture.

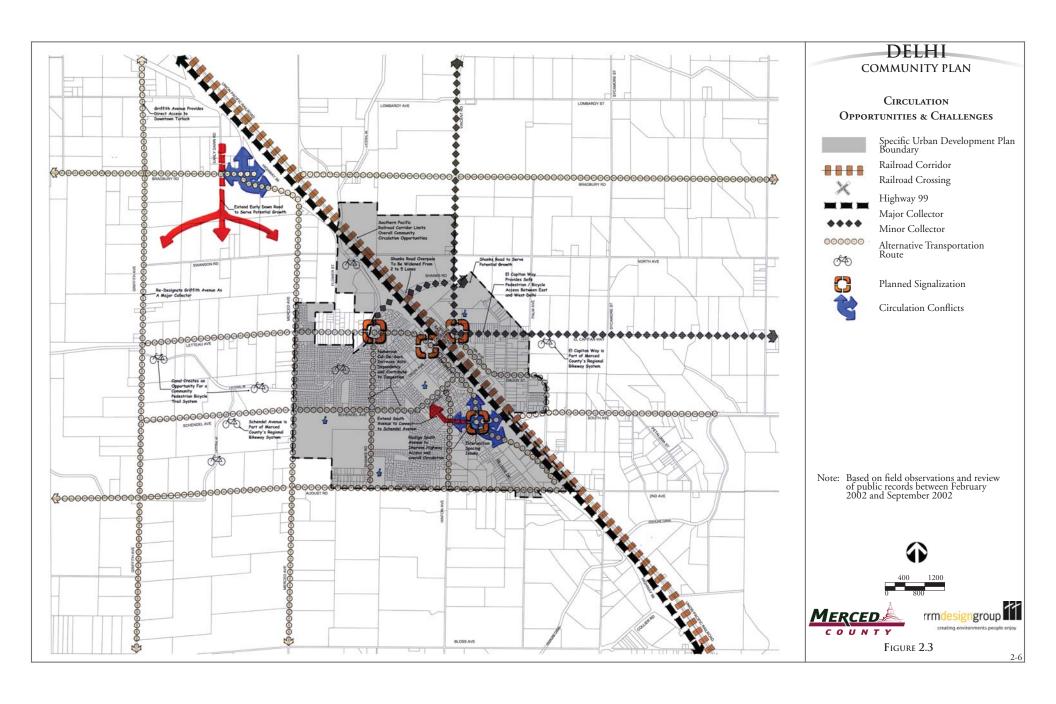
Challenge

Present land use development patterns in some areas do not establish orderly or cohesive neighborhoods.

Strategy

Establish land use objective, policies and implementation measures that ensure orderly and cohesive development.





40,000 35,000 30,000 25,000 Population 20,000 15,000 10,000 5,000 0 2000 2005 2010 2015 2020 8,022 9,300 10,781 12,498 14,489 Projection 1 9,372 10,950 12,792 14,945 Projetction 2 8,022 8,022 10,700 15,500 17,500 ♣ Projection 3 13,000 10,410 13,509 17,531 22,750 Projection 4 8,022 34,203 8,022 11,527 16,564 23,802 * Projection 5 Year

Table 2.1

Population Growth Projections

Projection 1 based on 1982 Delhi Community Specific Plan Growth Projection (3.0% Annual Growth Rate)
Projection 2 based on Census Bureau 1980-1990 Population Growth (3.16 % Annual Growth Rate)

Projection 3 based on MCAG, Regional Transportation Plan (RTP), July 2004 (4.2% Annual Growth Rate)

Projection 4 based on Census Bureau 1980 - 2000 Population Growth (5.35% Annual Growth Rate)

Projection 5 based on Census Bureau 1990 - 2000 Population Growth (7.52% Annual Growth Rate)

Year 2000 Population Based on on U.S. Census Bureau Year 2000 Census Data



BACKGROUND

Challenge

Inadequate commercial and industrial development to support local employment opportunities or tax revenue.

Strategy

Provide adequate land and infrastructure for commercial, business park, and industrial development to support local employment opportunities and generate tax revenue sufficient to support an acceptable standard of public services.

Public Services

Challenge

Existing infrastructure (e.g., water, sewer and drainage) within Delhi has limited capacity to support growth.

Strategy

Develop community-wide infrastructure master plans to address public services such as water, sewer and storm drainage necessary to support growth in the Community. Upgrade and expand existing infrastructure to meet planned Community growth.

Challenge

Additional school sites are needed to serve present and future residents in the Community.

Strategy

Promote additional school sites within new neighborhoods to serve current and future residents.

Open Space

Challenge

The existing community of Delhi is underserved for both neighborhood and especially community parks. While new development will provide for their pro-rate share of these facilities, the provision of adequate parks for the older existing neighborhoods will still be needed to serve existing residents.

Strategy

Promote a variety of parks to meet the Community's passive and active recreation needs.

Circulation

Challenge

Existing roadways have limited capacity to support growth within Delhi.

Strategy

Design and improve critical roadways and intersections to support growth in the Community.

Challenge

Existing neighborhood circulation limits the number of connections to other neighborhoods, schools and other amenities.

Strategy

Promote neighborhoods that provide connectivity and interconnect with surrounding neighborhoods, schools, parks and other amenities.

Challenge

Circulation is constrained by Highway 99 and the Union Pacific Railroad and limited highway capacity.

Strategy

Establish a community-wide circulation network that promotes a safe pedestrian and bicycling environment.

Challenge

Present circulation lacks major east-west road connections within the Community.

Strategy

Establish a hierarchical level of streets to serve the Community's needs.

Challenge

Existing subdivisions "back on" along community roads that promote a "walled" environment.

Strategy

Establish design standards that promote an "eyes on the street" approach with homes facing the street to avoid "walled" environments.



3.0 LAND USE

The residents of Delhi want a community that, overtime, will eventually become an incorporated city supporting a balance of housing, commercial, and employment opportunities, while creating recreational opportunities and preserving productive agricultural lands around it. The Land Use Chapter encourages a variety of development opportunities and recreational areas, while responding to farmland preservation issues through urban infill. School facilities, increased parks and recreation, and accommodation for public services were all factored into the Delhi Community Plan Land Use Diagram (refer to Figure 3.1). The Plan seeks to accommodate projected growth while increasing the quality of life through strategic design of the built environment. The following land use principles guide the physical location of the various land uses:

	Support a	variety o	of resid	lential	land	use opportunities	in ord	ler to	respond	to growth	pressures.
--	-----------	-----------	----------	---------	------	-------------------	--------	--------	---------	-----------	------------

Support a variety of	of commercial	and industrial	designations to	provide	opportunities	for business	growth
and employment.							

	D 11 C		ı
_	Provide for	recreational	i iises.

Recognize that the growth of the Community will occur in phases over the span of the Community Plan.
Because of the increase in population and infrastructure pressures, development standards shall be set forth
by corresponding master plans within specified plan areas.

3.1. Land Use Designations

The Delhi Community Plan establishes two new land use designations—Mixed Use and Business Park. The Mixed Use designation is intended to meet current and future uses of Delhi's downtown area that contribute to the diversity and scale of the Community. The Business Park designation is intended to encourage office and light industry development in a low intensity, campus-like setting. While the Delhi Community Plan establishes the intent and character of the Mixed Use and Business Park land use designations, the Merced County Zoning Code (Title 18) will need to be amended to establish zoning districts and regulations, as well as, development standards consistent with Mixed Use and Business Park land use designations.

The Delhi Community Plan Land Use Diagram (Figure 3.1) depicts the physical location of the various land uses in the Community. The generalized land use patterns provide the basis for the more specific districts, requirements, and standards of the Merced County Zoning Code.

It is important to consider the following points when referring to the Delhi Community Plan Land Use Diagram:

- ☐ The designated land use boundaries are generalized.
- ☐ The Delhi Community Plan Land Use Diagram indicates the primary use of land recommended in that general area and does not preclude minor deviations from the designated pattern. However, the intent of the predominant land use designation is to be followed.
- Densities and floor area ratios specified are expressed in dwelling units / gross area and building square footage / gross area, respectively.

Table 3.1, Land Use Summary, summarizes the Community's land uses by type, acreage and percentage distributions.



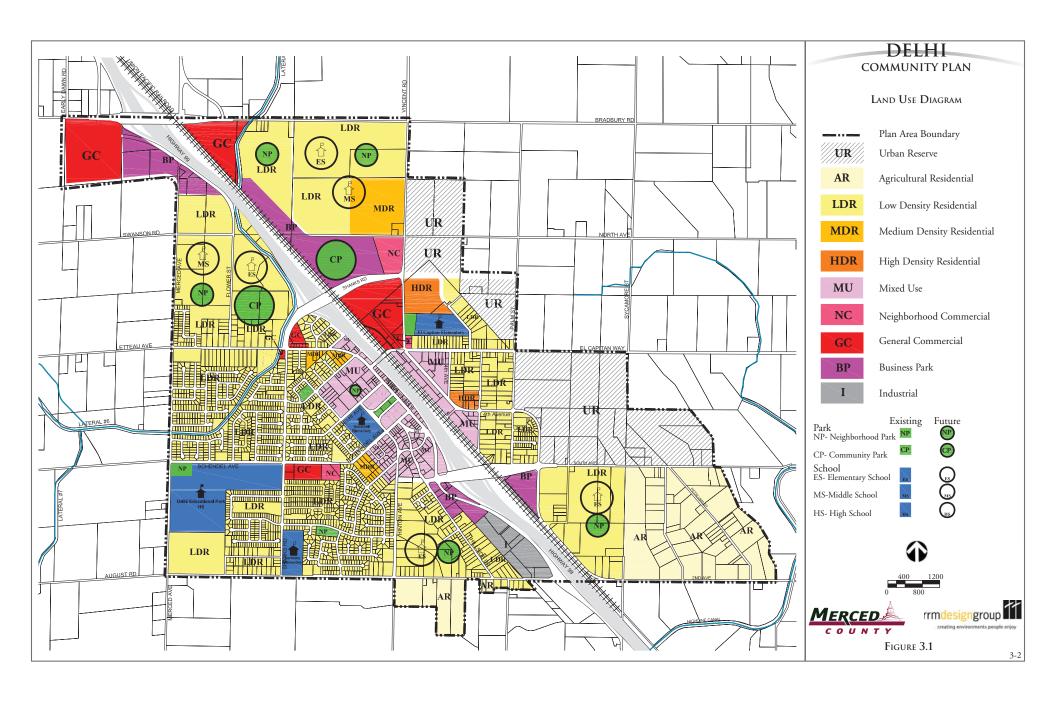


Table 3.1
Land Use Designation Summary

Land Use Designation	Total Acreage	Land Use Distribution	Units Per Acre ¹	Total Units ¹	Population ²	FAR ³	Potential Building Square Footage ⁴
			1 CI /ICIC	Cilits	1 opulation	1711	oquare 1 ootage
Urban Reserve (UR)	286	14%					
Agriculture Residential (AR)	204	10%	1.0	55	203		
Low Density Residential (LDR)	860	43%	4.5	3,750	13,629		
Medium Density Residential (MDR)	46	2%	9.0	524	1,939		
High Density Residential (HDR)	24	1%	20.0	515	1,906		
Mixed Use (MU) ⁵	88	4%	15.0	748	2,768	0.60	426,991
Neighborhood Commercial (NC)	14	1%				0.30	191,403
General Commercial (GC)	112	6%				0.25	1,345,121
Business Park (BP)	87	4%				0.30	1,133,649
Industrial (I)	26	1%				0.20	221,535
Schools ⁶							
Elementary	74	4%					
Middle School	40	2%					
High School	66	3%					
Parks ⁷							
Neighborhood Park	35	2%					
Community Park	30	2%					
Total	1,992	100%		5,592	20,444		3,318,699

Assumptions:

- ¹ Units per acre and total units are representative of typical or median range of residential density allowable and reflects the level of development expected during the planning period of this Community Plan.
- ² Population is based on the United States Census Year 2000 Delhi area data of 3.7 persons per household.
- 3 Floor area ratio (FAR) is the ratio of building area to parcel area. The FAR is representative of the typical or median range of building area allowable.
- Potential building square footage is derived by multiplying the typical FAR and the land use designation acreage total (excluding existing uses) and reflects the average intensity of development expected during the planning period of this Community Plan.
- ⁵ Land Use Map assumes 50 percent of the Mixed Use land use designation will be residential uses and 50 percent will be commercial, office, civic and government uses.
- ⁶ Land Use Map projects service seven (7) elementary (grades K-6) school sites, two (2) middle schools (grades 7-8) and one (1) high school (grades 9-12).
- ⁷ Land Use Map projects an additional five (5) neighborhood parks, and two (2) community parks.

3.2. Residential

The Delhi Community Plan Land Use Diagram includes four specific residential land use designations plus one additional land use designation (Mixed Use) that identify areas that are appropriate for housing. The following residential land use designation addresses the overall type and density of housing permitted within each designation.

Each residential designation establishes a minimum and maximum density, expressed in the numbers of homes (dwelling units) per gross acre. A gross acre includes not only the area required for the actual residential use (dwelling and yard), but also the area required for local streets and utilities necessary to serve the residential area. Uses such as arterials, major and minor collectors, neighborhood and community parks, schools and other uses that serve the residential neighborhood and community at large are not included in the gross acreage.

3.2.1. Agricultural Residential

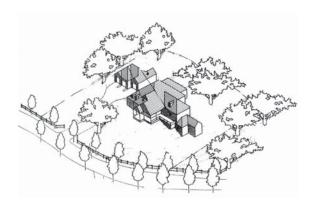
The Agricultural Residential (AR) designation is intended to maintain and / or provide for parcels of an acre or more in size where residents want the proximity and amenities of urban life, yet are able to enjoy some of the benefits of a more rural environment. Property designated Agricultural Residential typically consist of single family homes on estates or ranchettes. This designation provides a transition between the urban environment and the surrounding agriculture.

3.2.2. Low Density Residential

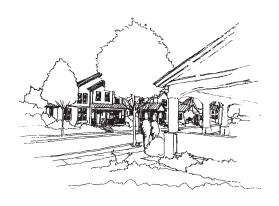
The Low Density Residential (LDR) designation is intended to promote single-family residential neighborhoods. Densities may range from a minimum of 3.5 to a maximum of 8.0 dwelling units / gross acre. Neighborhood amenities such as parks, schools and religious assemblies are encouraged.

3.2.3. Medium Density Residential

The Medium Density Residential (MDR) designation encourages a variety of detached and attached single-family, and multi-family (e.g., duplexes, triplexes, patio / courtyard homes, town homes) residential uses. The densities within this designation may range from a minimum of 8.1 to a maximum of 15.0 dwelling units / gross acre. Neighborhood amenities such as parks, schools and religious assemblies are encouraged.



Agricultural Residential concept.



Low Density Residential concept.



Medium Density Residential concept.



3.2.4. High Density Residential

The High Density Residential (HDR) designation encourages a variety of multi-family residential uses. Housing types encouraged include patio / courtyard homes, townhomes, apartments and condominiums. Densities within this designation range from 15.1 to 33.0 dwelling units / gross acre. This designation is suitable for areas near parks, schools, commercial / business centers and major streets.

3.3. Commercial and Industrial

The Delhi Community Plan Land Use Diagram includes two specific commercial land use designation plus one additional land use designation (Mixed Use) that identify areas that are appropriate for retail and professional services. The Delhi Community Plan Land Use Diagram also identifies two specific industrial land use designations. The following commercial and industrial land use designations include both a range of floor area ratio (FAR) and a typical FAR. The FAR is intended to be used as a guide for estimating development intensity.

3.3.1. Mixed Use

The Mixed Use (MU) designation is intended to meet current and future uses of Delhi's downtown area that contribute to the diversity and scale of the Community. Delhi's downtown area today is comprised of established single - and multi - family residential, commercial, office, civic and government services. The objective of the Mixed Use designation is to continue to support a complementary mix of commercial, office, civic and government services, and, more importantly, residential as continued and essential use in Delhi's downtown area.

The Mixed Use designation encourages an expansive range of residential densities and uses on both sides of Highway 99. This designation encourages a mixture of small lot detached and attached single - family homes, patio / courtyard homes, apartments, town homes and condominiums. The residential density ranges from a minimum of 4.5 to a maximum of 33.0 dwelling units / gross acre.

A full range of commercial and personal services, including small markets, apparel and specialty shops, restaurants, entertainment, financial institutions, medical and professional offices, and other similar uses are



High Density Residential concept.



An example of Mixed Use.



A residential over retail uses concept.



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permitted on both sides of Highway 99. In addition, the Mixed Use designation east of Highway 99 permits more intensive commercial uses such as motor vehicle body and repair, and appliance repair. The FAR within this designation may range from 0.20 to 1.5. The typical FAR is 0.55.

3.3.2. Neighborhood Commercial

The Neighborhood Commercial (NC) designation is intended for neighborhood retail and service uses such as a supermarket, pharmacy, bakery, dry cleaner, barber / beauty shop, video store, restaurant, business and professional offices, and other uses that generally serve nearby residential areas and carry products or offer services used by households on a regular basis. Commercial uses should be compatible in design and scale with neighboring residential uses, promote pedestrian and bicycle access, and provide adequate landscaping. The FAR within this designation may range between 0.25 to 0.75. The typical FAR is 0.30.

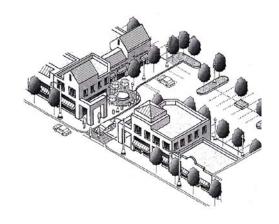
3.3.3. General Commercial

The General Commercial (GC) designation is intended for community-wide and regional retail services. Along with providing services to the consumer, retail uses are a major source of sales tax revenue that support public services and infrastructure. General Commercial areas allow for retail and service such as restaurants, durable goods, specialty shops, food and drug stores and similar uses that serve a community-wide or regional market. The FAR within this designation may range between 0.20 to 0.60. The typical FAR is 0.25

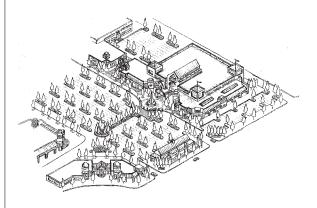
The General Commercial designation south of Shanks Road, west of Vincent Road is intended for communitywide and regional commercial uses.

The General Commercial designation south of Bradbury Road, east of Highway 99 is specifically intended to serve motorists traveling Highway 99. Development within this area is limited to uses such as auto / truck service stations, hotel / motel, restaurants, fast food and other similar uses serving motorists traveling Highway 99.

The General Commercial designation south of Bradbury Road, west of Highway 99 is intended for regional commercial uses and services. These uses may include a "big box" center, home furnishing store, entertainment center and other similar uses that serve the regional market of northern Merced County. This area is also intended to serve motorists traveling Highway 99.



An example of Neighborhood Commercial concpet.



An example of a General Commercial concept.

3.3.4. Business Park

The Business Park (BP) designation is intended for clean, non-noxious uses such as professional offices, research and development facilities, warehouse and distribution centers, mini storage, light manufacturing and fabrication (limited to indoor activity), and other similar uses located in a low intensity, landscaped setting with high design and development standards. Incidental employee-serving retail / service and ancillary on-site retail may also be permitted. The FAR may range between 0.15 and 0.75 depending on the location and nature of the business park. The typical FAR is 0.30.

3.3.5. Industrial

The Industrial (I) designation is intended for large-scale, more extensive types of industrial / manufacturing uses. This designation permits manufacturing, food processing, motor vehicle service and repair, contractor yards, warehousing and storage facilities, wholesale uses, construction supplies and building material facilities, and other similar industrial and heavy commercial uses. Incidental employee-serving retail services may also be permitted. Adequate landscaping should be provided to surrounding residential uses. The FAR may range between 0.15 and 0.75. The typical FAR is 0.20.

3.4. Urban Reserve

The Urban Reserve (UR) designation is intended for urban land uses to be determined in the future, depending on the growth needs and availability of urban services, utilities and facilities. Areas designated Urban Reserve are to remain agrarian in character and zoned for agriculture until designated for an urban use. It is anticipated that some areas designated Urban Reserve may develop within the time frame of this Community Plan (Year 2020). However, additional planning, environmental and feasibility studies will be required prior to development of these areas. Please refer to Implementation Measures LU 3.4.b, 3.4.c and 3.4.d for criteria regarding conversion of UR to other uses.

3.5. Parks

The Park designation provides for multiple recreational opportunities in the Community. The Park designation has two distinct forms: Neighborhood Park (NP) and Community Park (CP). Planned or future parks are identified in the Delhi Community Plan Land Use Diagram (refer to Figure 3.1) as circles, which present conceptual



An Industrial concept.



A Neighborhood Park concept.



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or floating designations. The exact size and locations are to be determined at the master plan and subdivision map stages of Community Plan implementation and shall be consistent with park standards set forth in Chapter 6, Open Space / Conservation.

Neighborhood parks are strategically located throughout the Community to serve residential neighborhoods within walking or biking distance. Other smaller parks within residential areas, such as tot lots, may be provided to serve the needs of small children and their parents. Community parks are larger parks that service the whole Community. The Plan includes 2 community parks, and 10 neighborhood parks.

3.6. Master Plan Areas

Some areas within Delhi will require the preparation and adoption of a master plan to ensure coordinated development and infrastructure (refer to Figure 3.2, Master Plan and Special Plan Areas).

The intent of a master plan is to implement the goals and policies of the Delhi Community Plan. Master plans are to be consistent with the Community Plan and with Section 18.33.040 of the Merced County Zoning Code. Each master plan will be required to follow the guidelines established in this chapter for master plans (refer to Implementation Measure LU 3.5.i).

It is recognized that land use designations contained in the Delhi Community Plan Land Use Diagram (refer to Figure 3.1) can be blended within the master plan neighborhoods as long as the acres, the variety of housing densities, and public amenities identified in Table 3.2 have been met. Land use acreages and densities for residential, non-residential, and public facilities for master plan areas are shown for each category (refer to Table 3.2, Master Plan and Special Plan Area Land Use Summary).

The number of potential dwellings, and non-residential square footages in the following sections and within Table 3.2 are based on assumptions used in the modeling for the plan. While these build-out numbers do not represent the maximum allowed by the plan they are thresholds that if exceeded may require further review and analysis under CEQA.

3.6.1. North Delhi Gateway Master Plan Area

The North Delhi Gateway Master Plan Area is bisected by Highway 99 in northern Delhi. This area is the northern gateway for Delhi and Merced County. Land uses in the Gateway Plan Master Area include General Commercial (GC) and Business Park (BP). To promote various commercial uses, especially those focused on serving passing Highway 99 motorists, the GC designation is subject to land use development performance guidelines (refer to Implementation Measure LU 2.1.i). The North Delhi Gateway Master Plan Area is anticipated to provide approximately 1.1 million square feet of commercial and business park development.

3.6.2. Flower Street Master Plan Area

Located in the northeast quadrant, approximately 160 acres in size the Flower Street Master Plan Area is primarily residential and includes future schools and parks, and could have 525 or more dwellings proposed for future development. It is the only master plan area with a significant amount of existing development and will serve as a crucial link between the existing town and the North Gateway Drive.

3.6.3. Bradbury Master Plan Area

Located in the northeast quadrant, approximately 260 acres in size, while primarily residential both low and medium density, the Bradbury Master Plan Area also includes a business park and neighborhood commercial along with a fture school, neighborhood parks and a community park. Depending on the ultimate designs this area could have 900 or more dwellings. It is the largest master plan area and provides a significant amount of new residential development east of Highway 99.

3.6.4. South Avenue Master Plan Area

Located in the southeast quadrant, approximately 110 acres in size, the South Avenue Master Plan Area is primarily Low Density Residential but also includes approximately 10 acres of Business Bark land along the freeway. It could have 600 or more homes, both an elementary school and a neighborhood park are proposed, which may be co-located together. The lots are generally smaller and like the Flower Street Master Plan Area, there are a number of existing residences.



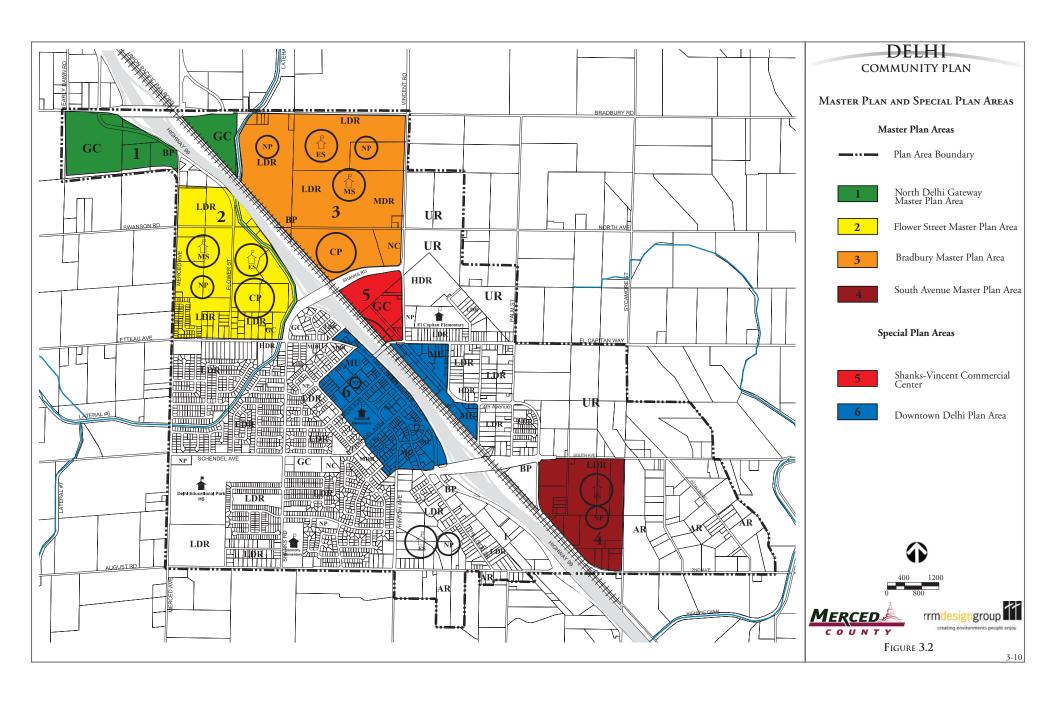
Table 3.2 Master and Special Plan Areas Land Use Summary

	Master 1	Plan Areas		Special Plan Areas			
	North Delhi Gateway	Bradbury	Flower Street	South Avenue	Downtown Delhi ² (West of HWY 99)	Downtown Delhi (East of HWY 99)	Shanks-Vincent Commercial Center
Residential							
Low Density Residential							
Acres ¹	0	105	108	87	0	0	0
Assumed Dwelling Units	0	590	528		0	0	0
Medium Density Residential							
Acres	0	41	0	0	0	0	0
Assumed Dwelling Units	0	279	0	0	0	0	C
Non-Residential							
Mixed Use							
Acres	0	0	0	0	61	27	C
Assumed Square Feet	0	0	0	0	500,743	182,163	C
Assumed Dwelling Units	0	0	0	0	383	103	C
Neighborhood Commercial							
Acres	0	10	0	0	0	0	0
Assumed Square Feet	0	136,680	0	0	0	0	0
General Commercial							
Acres	66	0	0	0	0	0	35
Assumed Square Feet	718,741	0	0	0	0	0	380,579
Business Park							
Acres	22	50	0	10	0	0	C
Assumed Square Feet	244,153	457,380	0	125,322	0	0	(
Public Facilities							
Schools							
Acres		30	30	10			
Facility Type		1 Elementary (K-5)	1 Elementary (K-5)		1 Existing		
		1 Middle School (6-8)	1 Middle School (6-8)	1 Elementary (K-5)	Elementary (K-5)		
Parks							
Acres		25	20	5	2		
Facility Type			1 Neighborhood Parks		1 Neighborhood		
		1 Community Park	1 Community Park	Park	Park		
Total Acres	88	261	158	112	63	27	35
Total Dwelling Units	0	869	528	409	383	103	(
Total Non-Residential Sq. Ft.	962,894	594,060	0	125,322	500,743	182,163	380,579

Notes:

¹ Acres represents gross acreage and excludes Highway 99, Union Pacific Railroad, arterials, major and minor collectors and canals.

² Downtown Delhi includes existing development and circulation improvements.



3.7. Special Plan Areas

The Community Plan also establishes other special plan areas requiring more attention to details in the permit or subdivision map process (refer to Figure 3.2).

3.7.1. Shanks-Vincent Commercial Center

The Shanks-Vincent Commercial Center is a special plan area. This area includes 35 acres of General Commercial that is intended to support community and regional commercial needs. The area is bounded by Highway 99 to the west, Shanks Road to the South, Vincent Road to the east, and El Capitan Way to the south. This center will provide approximately 380,000 square feet of commercial development.

3.7.2 Downtown Delhi Plan Area

The Downtown Delhi Plan Area is a special plan area. This area is designated Mixed Use. The Plan includes a neighborhood park and Schindel Elementary School. The objective of the Plan Area is to develop a pedestrian friendly, small town atmosphere within the Downtown Area. The Plan Area will include a mixture of housing, shops, markets, and professional offices. The Downtown Delhi Plan Area is divided into two sub districts—east and west of Highway 99. The west side of the Highway 99 is focused on providing a traditional "downtown" setting with commercial and higher density residential. The east side of Highway 99 is anticipated to be more residential in character, with a mixture of light industrial uses and businesses.

3.8. Goals, Objectives, Policies and Implementation

Goal:

Create an independent community through a balance of attractive and orderly development.

Objective:

LU 1.0 Provide for the housing needs of a broad range of socio – economic groups.

Policies:

LU1.1 The Delhi Community Plan shall provide a range of residential dwelling types, densities and amenities by designating land for Agricultural Residential, Low Density Residential, Medium Density Residential, High Density Residential and Mixed Use.

LU 1.2 A variety of housing types is encouraged to meet the broadest range of resident needs, including single family detached and attached homes and multi-family housing (e.g., duplexes, tri- and fourplexes, apartments, townhomes, and condominiums).

Implementation Measures:

LU1.1.a Residential housing designs shall be consistent with guidelines and standards set forth in the Community Character and Design Guidelines (Chapter 4).

LU 1.1.b Land designated Medium or High Density Residential should be located near schools, parks, commercial and business centers, and major streets.

LU1.1.c Residential uses above retail or other commercial uses shall be permitted in Mixed Use designated areas subject to the standards set forth in the Community Character and Design Guidelines (Chapter 4).

LU 1.1.d Land designated High Density Residential should be discouraged adjacent to land designated Agriculture or Agricultural Residential.

LU1.1.e Merced County shall work with the Delhi Community on renovating existing housing stock and improving Community appearance.

LU1.1.f Residential development shall be consistent with the density standards identified in Table 3.3, Residential Density Standards.



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Objective:

LU2.0 Promote economic vitality through attracting and retaining various commercial and industrial enterprises within Delhi.

Policy:

LU 2.1 Provide a mix of commercial and employment generating land use designations with infrastructure and services to meet the present and future needs of Community residents including Neighborhood Commercial, Commercial, Business Park, Industrial, as well as, providing economic opportunity in Mixed Use designated areas.

Implementation Measures:

LU 2.1.a Commercial development shall be designed with standards set forth in the Community Character and Design

Guidelines (Chapter 4).

LU 2.1.b Commercial development shall be consistent with floor area ratio (FAR) standards identified in Table 3.4, Non-Residential Floor Area Ratio Standards.

LU 2.1.c Downtown Delhi and Neighborhood Commercial designations shall contain neighborhood and community orientated businesses.

LU 2.1.d Neighborhood Commercial and Mixed Use land use designations shall support neighborhood retail, restaurants, and services.

LU 2.1.e Excluded uses in Neighborhood Commercial designation include "big box" retail centers greater than 65,000 square feet, auto repair and indoor / outdoor storage.

Table 3.3
Residential Density Standards

Residential Land Use Designation	Minimum Density	Maximum Density
Agricultural Residential (AR)	>1 unit/Acre	1 unit/Acre
Low Density Residential (LDR)	3.5 units/Acre	8 units/Acre
Medium Density Residential (MDR)	8.1 units/Acre	15 units/Acre
High Density Residential (HDR)	15.1 units/Acre	33 units/Acre
Mixed Use (MU)	4.5 units/Acre	33 units/Acre

Table 3.4
Non-Residential Floor Area Ratio (FAR) Standards

Non-Residential Land Use Designation	Minimum FAR	Maximum FAR
Neighborhood Commercial (NC)	0.25	0.75
General Commercial (GC)	0.20	0.60
Business Park (BP)	0.15	0.75
Industrial (I)	0.15	0.75
Mixed Use (MU)	0.20	1.50



- LU 2.1.f General Commercial designated areas shall be oriented to uses such as restaurants, durable goods, specialty shops, food and drug stores, and other similar uses that serve a community-wide market. Uses in this designation shall be those that generate sales tax revenue.
- LU 2.1.g Uses prohibited in General Commercial designated areas include motor vehicle, farm and heavy equipment repair and services, and indoor / outdoor storage centers.
- LU 2.1.h Regional and community serving commercial uses such as a "big box" retail center, specialty shops, etc. should be encouraged in the Shanks-Vincent Commercial Center and the North Delhi Gateway Master Plan Area.
- LU 2.1.i The type, quantity and distribution of general commercial land uses in the North Delhi Gateway Master Plan Area shall be consistent with Table 3.5, North Delhi Gateway Master Plan Area General Commercial Uses.
- LU 2.1.j Highway Regional serving commercial uses such as "big box" and specialty retailers along with restaurants, gas stations, truck stops, lodgings with highway oriented commercial are encouraged in the North Delhi Gateway Master Plan Area west of Highway 99.

- LU 2.1.k Within the North Delhi Gateway Master Plan Area, uses with automobile and agricultural commercial orientations (e.g., automobile, recreational vehicle, farm equipment repair and sales, heavy equipment repair and sales, and agriculture related industries) should be directed to the limited to the area, east of Highway 99..
- LU 2.1.l Provide visually attractive commercial development along the Community Gateways (Highway 99 interchanges at Bradbury Road, Shanks Road and at South Avenue) to enhance the image of Delhi to motorists on Highway 99 and to attract business.

Policy:

LU 2.2 Business Park and Industrial land use designations should provide employment generating land use activities.

Implementation Measures:

- LU 2.2.a Designate and service sufficient amount of Business Park, Industrial, and General Commercial land uses to support the population of Delhi and provide a strong local tax base.
- LU 2.2.b Business Park and industrial development shall be consistent with the floor area ratio (FAR) standards identified in Table 3.4, Non-Residential Floor Area Ratio Standards.

Table 3.5 North Delhi Gateway Master Plan Area General Commercial Uses

General Commercial Coco			
East of Highway 99	Acres	Percentage	
Highway Oriented Commercial	10	23%	
Auto / Ag Oriented Commercial	22	50%	
General Commercial	12	27%	
Total	44	100%	
West of Highway 99	Acres	Percentage	
Highway Oriented Commercial	10	45%	
General Commercial	12	55%	
Total	22	100%	



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- LU 2.2.c Uses in Business Park designated areas should strive to generate 20 jobs per acre. These uses should include office centers, research and development facilities, distribution centers, light manufacturing and other similar uses in a landscaped setting. with a goal of providing 20 jobs per acre.
- LU 2.2.d Mini-storage may be allowed within the Business Park designation subject to these limitations: No more than 30% of the Business Park designation within the North Delhi Gateway Master Plan may be allowed to develop as mini-storage and; only that area of the Bradbury Master Plan designated Business Park, north of North Road within 200 feet of the eastern property line of the Union Pacific Railroad may be allowed to develop as ministorages. Land developed for mini-storage, exceeding 10% of the aggregate of the entire Delhi Community Plan Business Park designation may be subject to CEQA analysis.
- LU 2.2.e Uses in Industrial designated areas may include light manufacturing, motor vehicle service and repair, contractor yards, truck yards, warehousing, and storage facilities.

Policy:

LU 2.3 Create business and economic opportunities through encouraging proper location and innovative urban design.

Implementation Measures:

- LU 2.3.a The Downtown Delhi Plan Area shall incorporate a mixed-use approach coupled with higher density housing.
- LU 2.3b The Delhi Municipal Advisory Council should work in conjunction with the Delhi Chamber of Commerce to attract businesses into the downtown area and foster business development with this area.

- LU 2.3.c Merced County shall work closely with the Delhi Municipal Advisory Council to attract and retain businesses to designated commercial, business park and industrial areas to provide job opportunities in the Community.
- LU 2.3.d Non-residential development shall be buffered from adjacent agricultural land uses outside the Community Plan boundary by landscaping or similar improvements.

Objective:

LU 3.0 Maintain a comprehensive Community Plan in order to provide vision, sustain a high quality of life, and foster consistent land uses decisions for Delhi growth and development.

Policy:

LU 3.1 Ensure Merced County Zoning Code (Title 18) and zoning maps are consistent with the Delhi Community Plan.

Implementation Measures:

- LU 3.1.a Amend the Merced County Zoning Code (Title 18) to conform to the Delhi Community Plan.
- LU 3.1.b Amend Merced County Zoning Maps consistent with and upon adoption of the Delhi Community Plan Land Use Diagram except those areas within Master Plans or designated with either a Mixed Use or Business Park land use designation.
- L.U. 3.1.c Land within a Master Plan shall be rezoned concurrent with the Master Plan approval process.

Policy:

LU 3.2 Establish a planning zone for the Mixed Use land use designation.



Implementation Measures:

LU 3.2.a Merced County shall amend the Merced County Zoning Code (Title 18) by establishing a "Mixed Use" zoning designation. Allowable land uses, permit requirements and development standards shall be consistent with the Downtown Delhi Guidelines and standards set forth in the Community Character and Design Guidelines Chapter (Chapter 4).

LU 3.2.b Parallel with the adoption of the "Mixed Use" zoning text and development standards, the County will modify the Zone District Maps consistent with the Mixed Use land use designation of the Delhi Community Plan Land Use Diagram. (Figure 3.1)

LU 3.2.c Prior to the designation of new zone districts for the Mixed Use land use designation, the existing zoning shall be considered consistent with the Mixed Use designation for projects developed consistent with the Delhi Downtown Guidelines and the standards set forth in the Community Character and Design Guidelines Chapter. Other than the new zoning designation of "Mixed Use" no other re-zonings shall be permitted except through use of the Special Planning Zone (SPZ) or Planned Development (PD) zone designations.

Policy:

LU 3.3 Establish a planning zone for the Business Park land use designation.

Implementation Measure:

LU 3.3.a Merced County shall amend the Merced County Zoning Code (Title 18) by establishing a "Business Park" zoning designation. Allowable land uses, permit requirements and development standards shall be consistent with the Employment Centers Guidelines and standards set forth in the Community Character and Design Guidelines Chapter (Chapter 4).

LU 3.3.b Parallel with the adoption of the "Business Park" zoning text and development standards, the County will modify the Zone District Maps consistent with the Business Park land use designation of the Delhi Community Plan Land Use Diagram. (Figure 3.1)

LU 3.3.c Prior to the designation of new zone districts for the Business Park land use designation, the existing zoning shall be considered consistent with the Business Park designation for projects developed consistent with the Employment Center Guidelines and the standards set forth in the Community Character and Design Guidelines Chapter. Other than the new zoning designation of "Business Park" no other re-zonings shall be permitted except through use of the Special Planning Zone (SPZ) or Planned Development (PD) zone designations.

Policy:

LU 3.4 Amendments to the Community Plan shall be consistent with Plan's goals, objectives, policies and implementation measures and the design guidelines and standards.

Implementation Measures:

LU 3.4.a Amendments to land use designations will be reviewed for appropriateness based on Community and regional needs, and compatibility with adjacent land uses.

LU 3.4.b Applicants shall submit a General Plan amendment and supporting documentation deemed necessary in order to convert Urban Reserve designated land to an active urban land use designation.

LU 3.4.c A General Plan Amendment to convert Urban Reserve designation to active urban land use designations shall only occur when adequate provisions have been made for obtaining public services, including community sewer and water,



and the current supply of vacant land already designated for development cannot accommodate 10 years of historic or reasonably projected growth in accordance with the Merced County General Plan.

LU 3.4.d Additional consideration for the urbanization of areas designated Urban Reserve is the removal of the commercial airstrip / airport located east of Palm Street, north of North Avenue.

Policy:

LU 3.5 Establish plan areas within the Community through preparation of "Master Plans" to ensure coordinated development of a neighborhood with related public facilities and infrastructure.

Implementation Measures:

- LU 3.5.a Master Plans shall be used to implement the Community Plan and the County General Plan as indicated in Figure 3.2, Master Plan and Special Plan Areas.
- LU 3.5.b Prior to development of the any major project (e.g., residential subdivision or commercial shopping center, etc.), a master plan, conforming to the boundaries identified in Figure 3.2, Master Plan and Special Plan Areas, shall be submitted for approval to the County of Merced, and shall become a component of the Delhi Community Plan.
- LU 3.5.c The Flower Street, Bradbury and South Avenue Master Plan Areas shall focus on residential development that creates neighborhood continuity compatible with the land uses within the respective Master Plan Area with adequate public services and facilities.
- LU 3.5.d The North Delhi Gateway Master Plan Area and the Shanks - Vincent Commercial Center Plan Area shall focus on business, industrial and commercial development. Plan Area site design shall be compatible with surrounding neighborhoods and

follow the guidelines and standards set forth of in Chapter 4 (Community Character and Design Guidelines).

- LU 3.5.e The Downtown Delhi Plan Area should be considered for a redevelopment area. Development in the Downtown Plan Area shall following the guidelines and standards set forth in Chapter 4 (Community Character and Design Guidelines).
- LU 3.5.f Placement of schools and parks within plan areas are generalized, however those facilities shall meet the minimum size requirements for neighborhood and community parks, and the type of school facility, the latter as defined by the school district.
- LU 3.5.g Recognizing some efficiency for joint use facilities, where joint school and park sites are proposed, smaller acreage requirements may be considered acceptable.
- LU 3.5.h Within master plan areas, land use designation may be dispersed throughout the plan area. However, the total acreage for each land designation use shall not be lower than the minimum acreages specified in Table 3.2 (Master and Special Plan Areas Land Use Summary).
- LU 3.5.i Master plans, at minimum, shall identify and address the elements outlined in Table 3.6 (Master Plan Elements).

Objective:

LU 4.0 Achieve a harmonious relationship between the urban environment and surrounding agricultural setting.

Policy:

LU 4.1 Establish a land use pattern that promotes compatible land uses and provides an effective transition between the built environment and agricultural uses along the periphery of the Community. (EM)



Table 3.6 Master Plan Elements

Land Use							
A	Land Use diagram for all land use categories, including public facilities						
В	Land Use table identifying use by acreage, intensity, and density						
С	A zoning map identifying designation for each land use identified						
D	Typical lotting plan for housing types proposed, indicating range of lot sizes and unit sizes						
Е	Discussion of any special design consideration for buffers and residential / commercial land use interface						
Community Character and Design							
A	Identify proposed architectural themes						
В	Identify site development standards proposed not otherwise consistent with the Community Plan						
С	Provide typicals of planned streetscape improvements including location and examples of walls lighting, neighborhood signage and entry or gateway features						
D	Provide general landscape details for public areas including types of vegetation such as turf, shrub, tree or groundcover use						
Circulation							
A	Conceptual circulation diagram identifying access points and street hierarchy including arterials, major and/or minor collectors and local streets						
В	Identification of street standards to be utilized in the Plan Area						
Parks and Open S	Space						
A	Diagram of proposed bicycle paths and pedestrian trails and classification or standard of development						
В	Diagram identifying the size and location of any community, neighborhood or pocket park						
С	Conceptual park layout indicating passive and active use areas, general improvements and areas dedicated to drainage purposes						
Noise							
A	Diagram of noise contours that meet or exceed 65 ldN						
В	Identification of alternative mitigation measures to reduce noise impacts for sensitive land uses						



Table 3.6 (cont'd) Master Plan Elements

ALLEGOTA A LEAT ENGINEER						
Public Services						
A	Identify the location and provision of any proposed public facilities, such as police or fire stations					
В	Discussion of the provision of public services for the area					
С	Identification of school facilities, when included within the Master Plan boundary					
D	Provide conceptual layout of major infrastructure including sewer, water and storm drain trunk lines and storm drainage ponds					
Е	Describe the proposed sewer, water and storm drainage improvements and their consistency with District master plans and/or County standard					
Implementation						
A	Describe applicable implementation measures for the development within the Plan Area					
В	Generally describe the phasing of the Plan Area; the funding/financing of the infrastructure; and the mechanisms for maintenance and operations of the public services and facilities					



Implementation Measure:

LU 4.1.a Utilize Low Density Residential, Agricultural Residential and nonresidential designations along the edge of the Community that provide for transitions between urban to rural land uses. (EM)

Policy:

LU 4.2 Establish distinct physical buffers such as streets or canals along the edge of the community to define urban and agricultural edges. (EM)

Implementation Measures:

LU 4.2.a Land use designations / zoning that share a boundary with agricultural designated lands outside the Community Plan Boundary shall provide a building setback to agriculture. Setbacks may include an open space corridor or physical improvements such as a road or canal. (EM)

LU 4.2.b Utilize the "Right-to-Farm" notification provision and other measures to inform future residents of the inconvenience of living near active farms. (EM)

Policy:

LU 4.3 Provide adequate setbacks and / or physical landscape screens between non-residential development and adjacent agricultural land uses outside the Community Plan boundary. (EM)

Implementation Measure:

LU 4.3.a Non-residential development shall be buffered from adjacent agricultural land uses outside the Community Plan boundary by landscaping and similar improvements imposed during the development permit approval process. (EM)

4.0 Community Character & Design Guidelines

Guidelines and standards for development assist public officials during project review and approval of development applications. They also guide developers, builders, planners, architects, landscape architects and civil engineers in project design. Through master plans and the project permit process, these guidelines and standards may be refined to fit a particular neighborhood.

These development guidelines and standards are not intended to be rigid in their application, rather they encourage diverse architectural opportunities while maintaining an overall design character and quality. Exceptions to these guidelines are based on demonstrable benefits to the Community or where not otherwise possible to adhere to these guidelines and standards.

4.1. Community Architecture

To maintain an overall design character, quality and diversity in Delhi, development should have consistent architectural theme throughout a planning area or project. Architectural themes should follow historical elements already present in Delhi (refer to Table 4.1, Architectural Themes) and include but are not limited to:

- ☐ California Craftsman—one-story or two story wood structures found throughout California, especially in the late 1800s and early 1900s. These buildings consist of wood exterior with either a stone or brick base.
- ☐ California Bungalow-one-story or one-and-ahalf-story wood framed structure found in the western United States especially during the early 20th Century. These buildings typically have a wood exterior and a low-pitched gabled roof.
- ☐ California Ranch—designed to emphasize a low pitched roof, with eaves hanging, and exterior walls consisting of stucco, brick, wood or a combination of those elements.
- ☐ California Mediterranean—consist of a mix of Spanish Colonial and Italian Villa Style housing either one or two stories, tile roofs, stucco walls and rounded or arched windows.

Table 4.1 Architectural Themes

Arch Style	California Craftsman	California Bungalow	California Mediterranean	Spanish Colonial	California Ranch
Building Material		Wood with a painted or stained brown or dark color that applies to the a neighborhood.		Stucco or plastered walls.	Stucco, wood, brick or combination of these materials.
Roof	Low-moderate pitched gabled roof.	Low- pitched gabled roof.	Low to moderate pitched hipped or gabled tile roof.	A low to moderate pitched hipped or gabled roof with red mission tiles. Occasional flat roof.	Low pitched roof, hipped crossed- gables, or side-gabled roof.
Windows	Double hung windows each with multiples panes.	Casement windows.	Rounded or arched windows.	Round arches over most prominent window, rectangular on others.	Ribbon windows with shutters in residential neighborhoods.
Trim	Wide overhang eaves, exposed roof rafters, beams, false beams or triangular braces.	Wide overhead eaves, dark wood paneling.	Heavy wooden decorative doors, sometimes double doors.	Heavy wood doors, some wood or wrought iron.	Eaves hanging, moderate to exposed rafters.
Public Spaces	Recessed porch, side porch may also be present.	"Sleeping porch" recessed and attached.	Long covered porch or enclosed courtyard.	Long covered porch or arcade.	Porches, large patio.



DESIGN GUIDELINES

Spanish Colonial—features include low pitched roofs, uneven clay tiles, and detail on beams, posts, gates and windows. Long narrow porches and courtyards are common features.

4.2. Residential Neighborhoods

The Community Plan encourages the development of a range of housing types and densities. The Plan encourages the development of detached and attached single family homes, duplexes, garden homes, townhomes, and multifamily complexes. With an increased diversity comes a significant opportunity and greater responsibility for outlining neighborhoods features that will make Delhi an attractive place to live. The following design elements are to be incorporated in new residential neighborhoods.

4.2.1. Neighborhood Design

- a. Create a pedestrian friendly environment by encouraging porches, windows, entries and living spaces that are oriented towards the street.
- b. Building heights and setbacks shall be varied to avoid streetscape and neighborhood monotony.
- c. A variety of custom homes designs should be constructed in the Agricultural Residential (AR) designated areas.
- d. Designs that make a neighborhood appear to have developed over time, rather than all at once are encouraged.
- e. New development shall compliment existing neighborhoods in comparable design and architectural elements.
- f. While not required for all local streets, the neighborhood streetscape should include planting strips between the curb and sidewalk throughout the residential subdivisions. (refer to Figure 5.5, Local Street). Streetscape details shall be submitted with all master plans or subdivision maps prior to approval of plans or maps.
- g. Each residential lot shall provide a minimum of one shade tree if a planting strip is not implemented, with additional trees strongly encouraged.



Planting strips separate sidewalks from the street.



An example of a porch oriented towards the street.



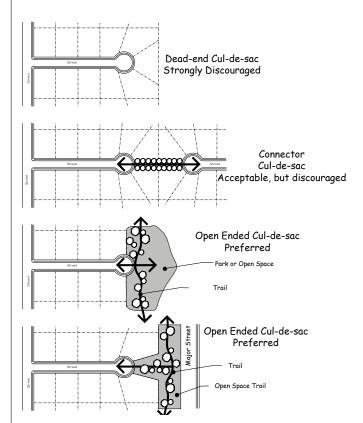
High density housing that is similar a single-family residential neighborhood.



- h. High Density Residential housing shall be designed to have a street presence and create a similar scale to that of a Lower Density Residential neighborhood.
- i. Multi-family building heights should be a maximum of two stories or 35 feet, whichever is greater, directly adjacent to a single family residential neighborhood but may be up to 1,000 feet if intersected (i.e., "T-intersection) with another street or significantly altered in direction.
- j. Multi-family development should include courtyards and gathering areas that contain seating and barbecues that are accessed by landscaped pedestrian walkways and paths.
- k. Multi-family parking lots should be located to the rear to minimize visual impacts. Where parking lots cannot be completely hidden, they should be landscaped, or behind a landscaped berm. If a berm is not feasible, than a low fence no taller than three feet with landscaping shall be used.
- l. The maximum length of a neighborhood block shall be no more than 600 feet but may be up to 1000 feet if intersection (i.e. T-intersection) with another street or significantly altered in directions.
- m. Neighborhoods should be designed to interconnect with surrounding neighborhoods. This may be accomplished through the use of a grid or modified grid network or the use of interconnected cul-de-sacs. Dead end cul-de-sacs that do not provide for pedestrian movement are strongly discouraged except where necessary to provide for the development of odd shaped parcels.
- n. All housing should be designed with defensible space to enhance the personal safety of residents.
- o. Duplexes may be allowed on corner lots in all residential designations including Mixed Use.



Varying building heights and architecture within a block.



Cul-de-sacs should open to collectors or community wide trails.



DESIGN GUIDELINES

4.2.2. Neighborhood Character

- a. Provide elevation variations in the design of structures to create visual interest. Avoid creating abrupt changes in the overall character of the neighborhood. Generally no more than 25 percent of the homes on a block should have the same architectural elevations.
- b. Residential units shall be designed to have a pedestrian scale.
- c. Porches are strongly encouraged, especially in single family developments, and where provided should have usable covered seating areas.
- d. Details should be designed to compliment overall neighborhood character including materials and colors.
- e. Building heights shall be varied to provide visual breaks in the neighborhood skyline.

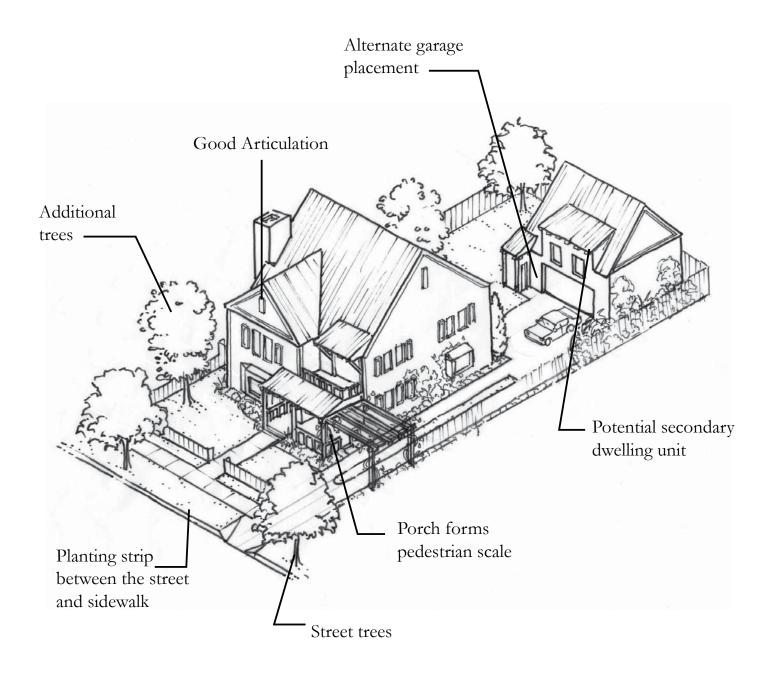


Exterior walls, trim, and other details are well articulated.



Example of alternative garage location.





A prototypical single family home.



DESIGN GUIDELINES

- f. Exterior wall materials, trim and architectural details need to be applied equally to all sides visible from adjacent public use areas i.e. public roads, parks, dedicated open space such as gateways or public plazas.
- g. Utility and mechanical equipment (e.g., roof mounted air conditioners, coolers, antennas and satellites) should be avoided or screened from public view.
- h. Garages should not exceed 40 percent of the first floor front elevation on lots greater than 6,000 square feet but may be exceeded on smaller lots with concomitant design enhancements including one or more of the following: staggered garage doors, separated garage doors, trellises attached to garage walls, second story recess, and variation of first floor roof elements. No single garage design should predominate on any specific street with a mix of styles strongly encouraged so as to avoid garage dominant streets and monolithic streetscapes.

4.3. Commercial Centers

Commercial centers are critical to Delhi's economic vitality. Commercial centers may provide small retail shops, cafés, restaurants, to large "big box" retail, and highway oriented services. Community residents and visitors alike are attracted and inclined to shop at centers that reflect quality architecture and design. Thus, it is important that commercial centers incorporate quality architecture and landscaping features to provide an attractive and positive atmosphere. The architecture style should be similar to its surrounding neighborhood.

4.3.1. Commercial Center Design

- a. Architectural themes shall be consistent within a commercial center. Special themes shall be applied within the Shanks Vincent Commercial Center (refer to Figure 3.2)
- b. Buildings should promote and enhance a pedestrian-oriented atmosphere while being adaptable to accommodate changes.
- c. Buildings shall be located adjacent to streets to define the edge of the commercial center and encourage pedestrian activity and access.



Consistent architecture theme and pedestrian friendly arcade and landscaping.



An example of a landscaped parking lot.



An example of a pedestrian oriented parking lot.



- d. Buildings should be clustered to retain a pedestrian scale and walkable environment.
- e. To encourage pedestrian activity, sidewalks shall connect to adjacent residential neighborhoods or community trail.
- f. Provide bicycle racks to promote the use of bicycles.
- g. Streetscape shall include landscape elements (e.g., turf, shrubs, and trees that complement the centers architecture and surroundings.
- h. Off-street parking shall be internalized (behind buildings), compartmentalized and shielded from residential neighborhoods and public spaces.
- i. Parking areas shall be thoroughly landscaped and shaded with trees that provide shade for 60 percent of the parking lot at full maturity.
- j. Loading areas shall be avoided in highly visible areas in and adjacent to commercial centers.

4.3.2. Commercial Center Character

- a. Undulating facades and varying heights are required to provide visual interest and pedestrian scale.
- b. Corporate tenant architecture should be designed to fit the scale and character of Delhi. The use of corporate architecture should compliment the individual character of a commercial center.
- c. Buildings shall be designed for viewing from all areas.
- d. Parapets and similar elements shall shield roof mounted equipment and be designed so as not to appear tacked on and to convey a sense of permanence.
- e. Provide variable roof forms throughout individual tenant buildings.
- f. Large reflective surfaces, polished metallic architectural features and reflective glass shall be prohibited on surfaces of multi-story buildings facing streets, open space or residential neighborhoods. (EM)

COMMUNITY PLAN



An example of corporate architecture that compliments a commercial center design.



An example of variable roof forms and other elements that shield roof mounted equipment.

DESIGN GUIDELINES

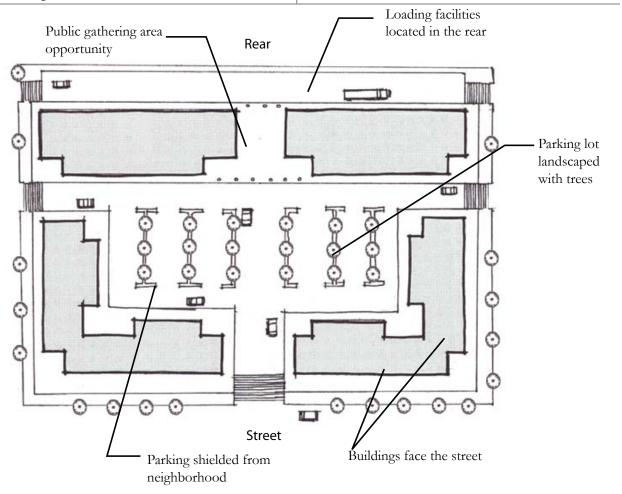
- g. Exterior light fixtures shall employ lower intensity lighting that is directed towards the ground. No more than 10 percent of light shall cast above the horizontal plane of lighting fixtures. (EM)
- h. Building heights shall not exceed 75 feet in accordance to the Merced County Zoning Code.
- i. Windows and entryways, at the street level, shall have a pedestrian scale and detail.

4.3.3. Commercial Center Signage

- a. A single development with multiple tenants shall provide a unifying sign theme consistent throughout the center.
- b. For multiple tenants, monument signs should be well articulated, proportioned, and consistent with the exterior's building architecture and design.



An example of a monument sign for multiple tenants.



A prototypical commercial center layout.



- c. Highway oriented monument signs are strongly encouraged for centers adjacent to Highway 99. These signs should be consistent with the overall architecture and design of the center.
- d. Pole signs shall be limited to commercial land uses along Highway 99, where a monument sign is otherwise not visible.

4.4. Downtown Delhi

Downtown Delhi will be a destination for not only residents, but visitors as well. Downtown Delhi should be pedestrian oriented with a small town atmosphere and provide a full range of commercial services, including markets, apparel stores, restaurants, specialty shops, real estate, entertainment center, financial, medical and professional offices and other general office or government services. Residential development is also allowed.

Downtown Delhi is divided into two sub districts (refer to Figure 3.2). West of Highway 99, the emphasis is on a traditional "downtown setting" with an encouragement of retail commercial services and higher density housing opportunities. East of Highway 99, the emphasis is on supportive existing residential and encouraging compatible light industrial and service uses. The following guidelines and standards provide a framework for future development in both sub districts. An example of implementing the standards and guidelines for Downtown Delhi is illustrated in Figure 4.1, Downtown Delhi Concept.

4.4.1. Downtown Architecture

- a. The architecture style throughout the downtown area shall be consistent and attractive.
- b. The use of wood, brick, stone trimming and other architectural elements consistent with an architectural style is strongly encouraged.
- c. Banners should be installed on the outer edge and throughout the downtown to give greater significance to the area. The Delhi Downtown Association or similar organization would be responsible for the installation and maintenance of the banners.

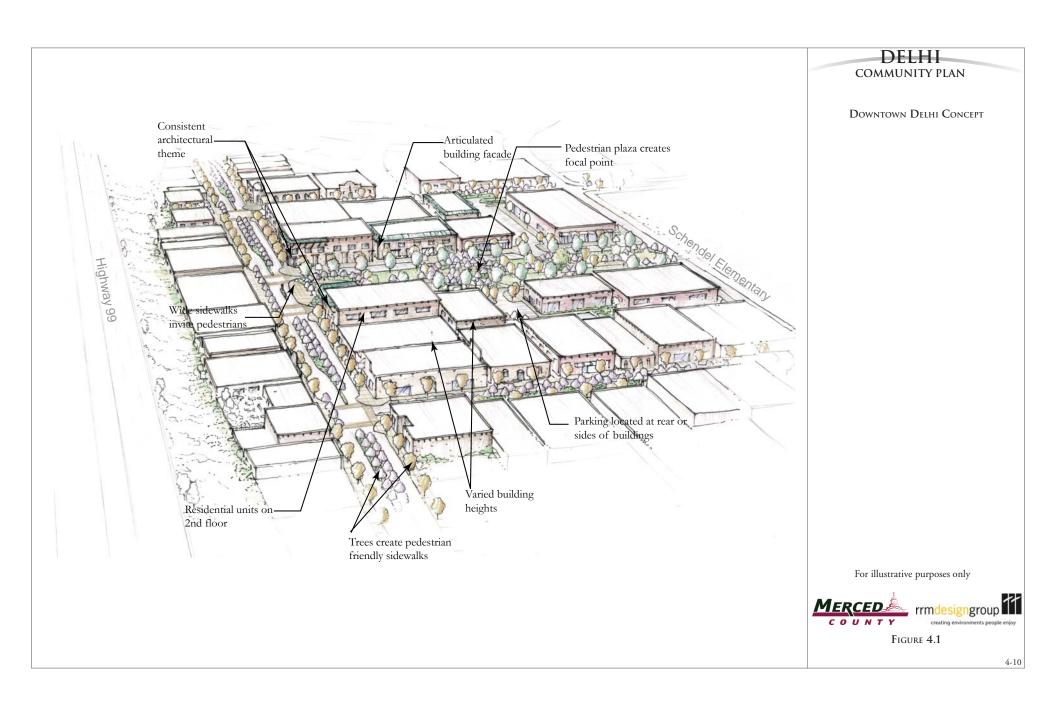


An example of a zero setback for buildings.



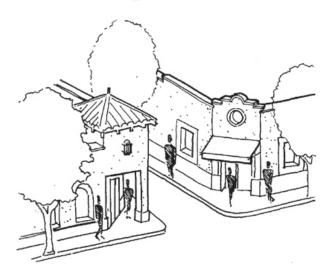
A wide sidewalk with planters invites pedestrians.



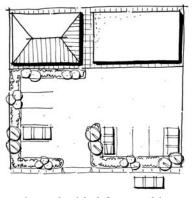


4.4.2. Downtown Design

- a. Residential development is permitted in this area as a stand alone building placed between, or above commercial developments. The minimum residential density permitted along Stephens Street is 15 dwelling units / acre.
- b. A zero building setback is required along the sidewalk. The exception to this are pedestrian plazas and seating areas along the sidewalk, and new single family detached homes, where the setback will be a minimum of 10 feet and a maximum of 20 feet. The intent of the zero building setback is to invite pedestrian activity along storefronts and establish a sense of place.
- c. Buildings visible from Highway 99 shall be consistent with the Caltrans Highway 99 "Main Street" theme.
- d. Buildings shall be located along the perimeter and corners of the blocks adjacent to the sidewalk.
- e. Service areas and loading functions shall be located to the back of buildings.
- f. Street trees are to be planted adjacent to the curb at regular intervals of 30 feet on center.
- g. On-street parking shall be provided throughout the downtown area.
- h. Off-street parking lots, when possible, should be internalized (behind buildings) and shielded from public views. Access to these lots shall be provided by side streets or connecting driveways.
- i. Street signage should direct motorists to off-street parking facilities.
- j. Parking lots shall be landscaped with shade trees and properly lighted.
- k. Shared parking facilities serving multiple businesses and residences are encouraged throughout the downtown area. Where appropriate, residents should have designated parking spaces within these facilities, but residents parking shall not exceed 40 percent of the parking facility allocation.



Building articulation at a street intersection.



Off-street parking shielded from public view.





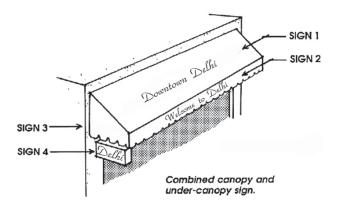
Ornamental banner and directional parking sign that add character.



DESIGN GUIDELINES

4.4.3. Downtown Character

- a. The street level component of a building should consist primarily of store fronts with a minimum of 50 percent transparency (i.e. windows).
- b. Downtown building abutting Highway 99 should not exceed two stories so as not to block the viewscape into the center of Delhi from the freeway. Commercial buildings are otherwise encouraged to be at least two stories in the Downtown to preserve and create a small town central business district.
- c. Second stories or higher shall have a minimum of 25 percent window area.
- d. Blank wall areas shall not adjoin sidewalks or the Highway 99 corridor.
- e. Coordinated awnings and shade canopies may be installed; however, they should be coordinated with all store front awnings on the block and be consistent with any awning plan established for the downtown area. The shape of the awning should be compatible with the shape of the window or door.
- f. Sidewalks should be a minimum of 10 feet wide, with landscaping and appropriate street furniture.
- g. Benches and other street furniture shall have a style consistent with the chosen architectural elements for the downtown area.



A example of coordinated awning and sign placement.

h. Proper street lighting shall occur within the area at a pedestrian scale and safety.

4.4.4. Downtown Signage

- a. Signs shall coordinate with the building design, material, color, size and placement.
- b. Sign variety is encouraged among different uses.
- c. Buildings with awnings shall have the store name on the center of the awning to provide visibility to motorists.
- d. A second sign should be smaller and oriented to passing pedestrians and extended out horizontally from the building façade.

4.5. Employment Centers

Both the Business Park and Industrial land use designations provide for economic and job opportunities in Delhi, and are collectively referred to as "Employment Centers." Uses include office centers, research and development facilities, warehouse and distribution centers, light manufacturing and fabrication (limited to indoor activity) and other similar uses. Guidelines are critical since these centers are often located adjacent or close to existing or proposed residential areas.

4.5.1. Employment Center Design

- a. Employment centers adjacent to residential uses shall incorporate a 30 foot wide landscape setback or combination of berms, walls, setbacks, and landscaping.
- b. Buildings should be placed at the perimeter of the site and provide a street presence consistent with Section 4.5.2 Employment Center Character.
- c. Provide strong pedestrian and bicycle connections throughout the center that link to sidewalks or bicycle lanes serving the Community.
- d. Parking facilities should be located internally and / or screened from public view with extensive landscaping.
- Parking facilities shall be thoroughly landscaped and shaded, with trees that provide shade for 60
 percent of the parking lot at full maturity.



- f. Parking facilities shall be lighted and minimize provide safety of users both while in a vehicle and on foot.
- g. Loading and storage areas shall be screened from public view.
- h. Mini-storage facilities when located in Business Parks or adjacent to residential uses shall provide solid exterior walls along their perimeter and consistent with other implementation measures.

4.5.2. Employment Center Character

- a. Provide buildings with visibly interesting forms and consistent architectural theme.
- b. A variety of building designs is encouraged so that business parks appear to have been developed over time.
- c. Buildings shall maintain a minimum window transparency of 30 percent along public use areas i.e. public roads, parks, dedicated open space such as gateways or public plazas.
- d. Large reflective surfaces, polished metallic architectural features and reflective glass shall be prohibited on surfaces of multi-story buildings facing streets, open space, or residential neighborhoods. (EM)
- e. Exterior light fixtures shall employ lower intensity lighting that is directed towards the ground. No more than 10 percent of light shall cast above the horizontal plane of lighting fixtures. (EM)
- f. Roof mounted equipment (e.g., air conditioners, fans, vents, etc.) shall be screened by architectural elements such as parapets.

4.5.3. Employment Center Signage

- a. Tenant signs should incorporate the architectural theme of the center.
- Freestanding signs shall consist of monument type signs that compliment the overall design of buildings.

4.6. Walls and Fences

Walls and fences are an integral part of the streetscape. Walls and fences are necessary elements that provide safety, security, privacy, property definition, and noise attenuation. When poorly designed or maintained, walls and fences become targets for vandalism and an eyesore that detracts from the overall quality and character of a neighborhood. Walls and fences will be regulated to ensure they contribute to the quality of Delhi through design standards. Master plans and new projects outside of the master plan areas shall use these standards to articulate walls and fences to better fit neighborhoods.

Walls should be used sparingly and only constructed when existing or projected ambient noise level mandate attenuation and no other physical land use design alternative is considered a viable option. The following guidelines shall be applied in the design of walls and fences within public right of ways or public viewscapes.

4.6.1. Wall and Fence Design Guidelines

- a. When a wall is needed, its scale and design should be compatible with the surrounding landscape and architectural materials found within the neighborhood.
- b. Subdivisions perimeter walls along arterials or major collectors shall be of masonry construction or similar material with a likely life expectancy of 50 years.



A well landscaped wall constructed of masonry material.



DESIGN GUIDELINES

- c. Walls and fences shall incorporate materials, colors and shapes that appropriately reflect the character of a chosen architectural element within the neighborhood.
- d. Wherever feasible, walls and fences should be integrated with grade changes, comprised of overlapping segments, designed with intentional pattern(s) and / or incorporated with berms or planting areas to blend into the landscape.
- e. Walls should not exceed seven (7) feet in height, except for noise attenuation along the Highway 99 or railroad corridors. Sound walls shall consist of masonry materials and landscaped to maintain community aesthetics and avoid being a target for graffiti.
- f. Walls shall be landscaped with plants and vines that are tolerant to Delhi's climate with relatively low maintenance costs.

4.7. Landscape

Landscape can improve the overall appearance of Delhi by creating a sense of open space, shade and overall quality to a neighborhood. Landscaping can positively change neighborhoods and visual perception. Landscaping should be identified for all master plan areas and during the permitting process for projects outside of master plan areas. The following guidelines shall be applied.

4.7.1. Landscape Design Guidelines

- a. Selected tree species should be tolerant to Delhi's general climate features.
- b. Accent trees species shall be incorporated as part of any major gateway feature.
- c. A variety of tree species shall be planted in prominent community areas such as the downtown, schools and community parks.





A conceptual before and after of landscaping improvements along a roadway corridor.



5.0 Circulation

An orderly network of streets, and pedestrian and bicycle facilities are essential for the health and welfare of a community. It is the intent that Delhi becomes a walkable community with easy access to neighborhoods, commercial centers, and public amenities. This Chapter defines circulation concepts that provide for safe and convenient movement of people and goods throughout Delhi.

5.1. Street Classifications

The Delhi Community Plan's Circulation Diagram shown in Figure 5.1 illustrates an orderly network of roads in the Plan Area. The Circulation Diagram classifies a hierarchy of roads based on their intended function and projected traffic levels they are to support. The following street classifications are recognized within the Plan Area.

5.1.1. Arterial

Arterials function as high volume thoroughfares with heavy truck traffic and with intersection intervals between ½ mile and ½ mile apart. Arterials are typically 4 to 6 lanes. Bradbury Road, the only arterial in the Plan Area, is a regional east-west route extending from Crows Landing Road in the west to just past Santa Fe Drive in the east. Bradbury Road forms a portion of the northern Community Plan Area Boundary.

Given the importance of Bradbury Road as a major regional transportation corridor, the Community Plan establishes adequate right of way to allow for the expansion, if necessary, to meet future regional transportation needs. Bradbury Road improvements will be phased in, as needed, to accommodate planned and long range traffic demands (refer to Figure 5.2).

Initial improvements (phase 1) consist of 4 lanes within the bounds of the Plan Area (between Early Dawn Road and Vincent Road). Phase 1 includes an adjacent community trail to the south and a landscaped median with turning pockets to accommodate left-turns into the residential neighborhood and commercial centers. Phase 2 includes an additional travel lane in each direction, and will only be constructed when traffic conditions warrant additional improvements.

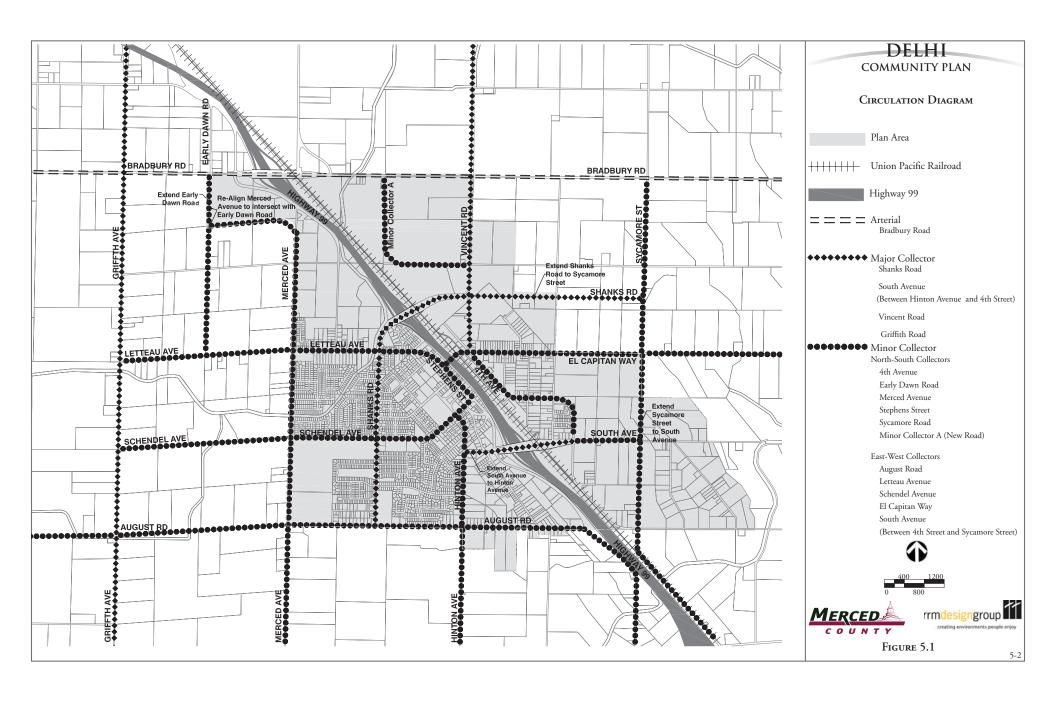
5.1.2. Major Collector

Major collectors serve as primary circulation corridors. Similar to arterials, major collectors are high capacity roadways that are typically 2 to 4 lanes. As illustrated in Figure 5.3, a major collector consists of two, 12 to 14 foot wide travel lanes with a 5 foot sidewalk and landscaped planting strip in each direction, separated by a median. The median will be 16 feet wide and will narrow down to as small as 2 feet wide to accommodate left-turn pockets at intersections.

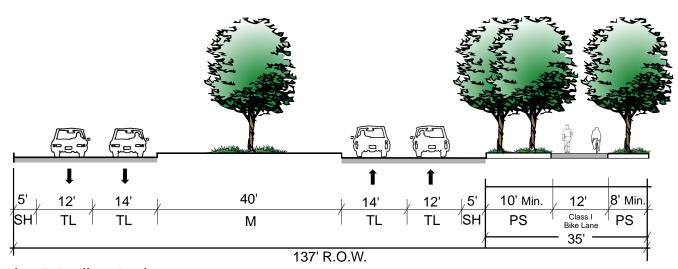
The following roadways are designated major collectors:

- Shanks Road between August Avenue and planned extension of Sycamore Street. Shanks Road between August Avenue and Letteau Avenue will be a two lane road with a continuous left turn lane and Class II bike lanes. Shanks road between Letteau Avenue and the edge of the Plan Area boundary will be a four lane roadway.
- South Avenue between Sycamore Street and planned extension of Hinton Avenue. South Avenue will serve as the Community's southern gateway and link to downtown Delhi.
- ☐ Vincent Road between El Capitan Way and Bradbury Road serves as a major north-south thoroughfare east of Highway 99. Vincent Road between El Capitan Way and Bradbury Road will be a four-lane road (refer to Figure 5.3).

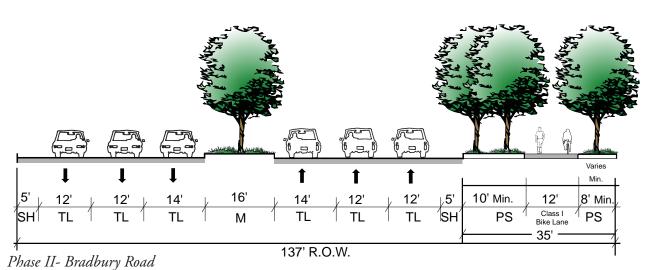




CIRCULATION



Phase I- Bradbury Road



•

Right-of-Way (R.O.W)

Class I Bike Path

Median (M)

Planting Strip (PS)

Shoulder (SH)

Travel Lane (TL)

137 feet wide

12 feet wide

16 feet wide

Minimum 10 feet back of curb and Class I path

Minimum 8 feet between Class I path and edge of R.O.W.

5 foot wide

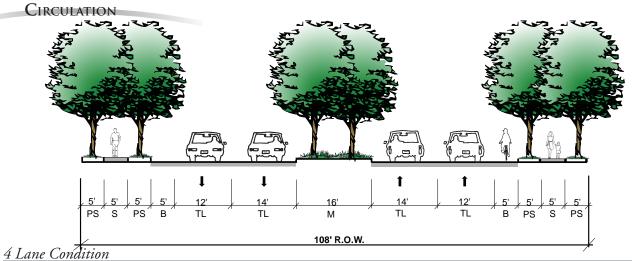
12-14 feet wide as noted

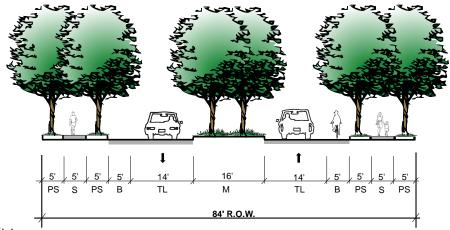
FIGURE 5.2 - ARTERIAL



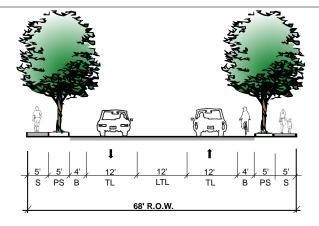
^{***}Parking Prohibited

^{***}Design and posted speed 45-55 m.p.h.





2 Lane Condition



Shanks Road South of Letteau Avenue

Right-of-Way (R.O.W)

Class II Bike Lane (B)

Median (M)

Planting Strip (PS)

Sidewalk (S)

Travel Lane (TL)

68/84 / 108 feet wide

4-5 feet wide

16 feet wide

5 feet between back of curb and sidewalk

5 feet wide

12-14 feet wide as noted

FIGURE 5.3 - MAJOR COLLECTOR



^{***}Design and posted speed limit 35-45 m.p.h.

^{***}Parking Prohibited

5.1.3. Minor Collector

Minor collectors serve as connective links to and from residential neighborhoods, commercial and employment centers, and to and from public activity areas. Minor collectors are typically 2 lanes and as illustrated in Figure 5.4, a minor collector consists of a 12 foot wide travel lane with a 10 foot wide shoulder for parking and a Class II bicycle lane in each direction. In addition, a 5 foot wide planting strip and sidewalk is also provided on both sides. Table 5.1 outlines the minor collectors within the Community Plan.

The following street segments may deviate from the typical minor collector street section.

El Capitan Way

El Capitan Way serves as an entrance for residents of surrounding communities to the east. It is currently a segment of Merced County's bikeway system. El Capitan will serve as a primary entrance for residents on the east side of the Community and as a gateway to downtown Delhi. Special attention should be given to El Capitan Way in order to preserve the aesthetic value present on El Capitan Way (refer to Figure 5.4)

Merced Avenue

Merced Avenue is a minor collector from August Road to the planned extension of Early Dawn Road. Merced Avenue between Schendel Avenue and Letteau Avenue, will not be subject to the improvements shown in Figure 5.4 due to existing residential development along this segment of Merced Avenue.

Stephens Street-Letteau Avenue

Letteau Avenue - Stephens Street from Shanks Road to South Avenue serves as the "main street" and thoroughfare for downtown Delhi. Special details should be outlined for Stephens Street during any redevelopment planning of downtown. Although street improvements standards shall follow those of a minor collector, further studies are recommended for downtown street improvements such as placement of monuments, parking opportunities and use of special paving, etc.

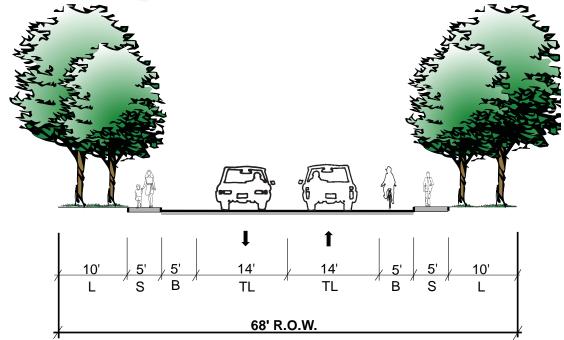
5.1.4. Local Street

Local streets make up the bulk of the circulation network in the Community. Local streets provide direct access to adjacent properties. Local street sections vary depending on the type of land use the local streets directly serves – residential neighborhoods; commercial centers or

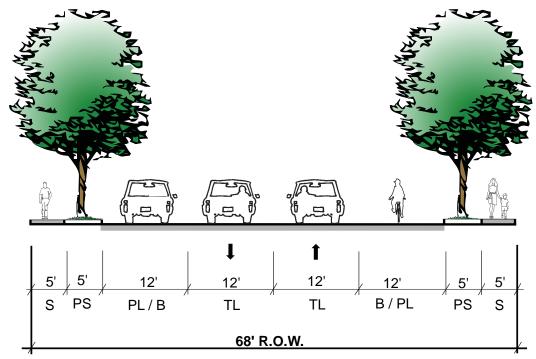
		Table 5.1						
Minor Collectors Delhi Community Plan								
Street Name Beginning of Designation Ending of Designation Comments								
North-South Collectors	Beginning of Besignation	Litaling of Designation	Comments					
New Collector A	Bradbury Road North Avenue		New Collector					
Early Dawn Road	Bradbury Road	Merced Avenue	Planned extension to Merced Avenue					
4th Avenue	South Avenue	El Capitan Way						
Hinton Avenue	August Road	Schendel Avenue						
Merced Avenue	August Road	Early Dawn Road	Re-align to intersect with Early Dawn Road north of Swanson Road					
Stephens Street	Letteau Avenue	Schendel Avenue						
Sycamore Street	Bradbury Road	Plan Area Boundary	Extends beyond the Plan Area Boundary					
East-West Collectors								
August Road	Merced Avenue	Stephens Street						
Letteau Avenue	Merced Avenue	Shanks Road						
Schendel Avenue	Merced Avenue	Stephens Street						
North Avenue	Vincent Road	New Collector A						
El Capitan Way	Sycamore Street	Stephens Street	Refer to Figure 5.4 for special street section					



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El Capitan Way - Minor Collector



Right-of-Way (R.O.W.)

Parking / Class II Bike Lane (PL/B)

Planting Strip (PS)

Sidewalk (S)

Travel Lane (TL)

68 feet wide

12 feet wide

5 feet between back of curb and sidewalk

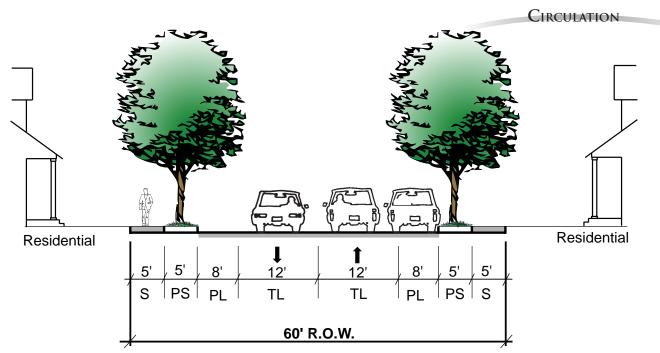
5 feet wide

12 feet wide

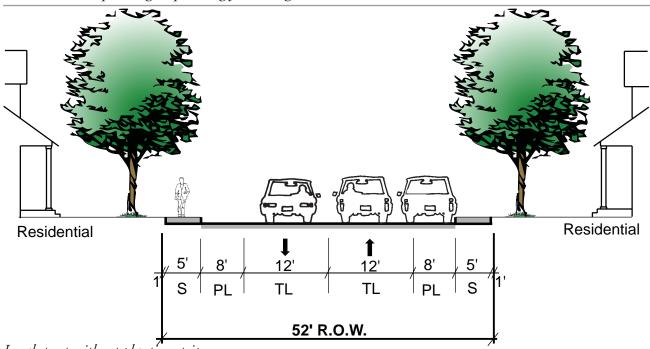
FIGURE 5.4 - MINOR COLLECTOR



^{***}Parallel on-street parking permitted. Curb and gutter included in parking lane.



Local street with planting strip- strongly encouraged



Local street without planting strip

Right-of-Way (R/W) 52 / 60 feet wide Parking Lane (PL) 8 feet wide

Planting Strip (PS)

5 feet between back of curb and sidewalk

Sidewalk (S) 5 feet wide Travel Lane (TL) 12 feet wide

Figure 5.5 - Local Street



^{***}Parallel on-street parking permitted. Curb and gutter included in parking lane.

^{***}Planting strip required on all entry and through local roads within subdivisions.

^{***} I foot easement back of sidewalk on local street without planting strip.

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business parks. Typically, local residential streets support low traffic volumes and do not require wide lanes. Future local streets should strive to create a more pedestrian friendly, walkable community, while still accommodating local traffic.

Residential local street standards reflected in Figure 5.5 adequately accommodates two-way traffic, including public transit service (full-size bus), emergency service vehicles (e.g., fire engines), and parking along both sides of the street. A landscape planting strip between the street and sidewalk creates a safe environment for pedestrians to walk. Local residential street shall consist of a 12 foot wide travel lane with an 8 foot wide parking lane, and a 5 foot wide sidewalk and optional planting strip in each direction.

Cul-De-Sacs

The proper design of cul-de-sac streets is fundamental to successful pedestrian and bicycle circulation (refer to Community Character and Design Guidelines: Neighborhood Design 4.2.1). Open-ended cul-de-sacs

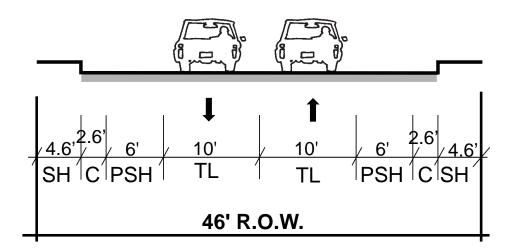
can provide pedestrian and bicycle access to major and minor collectors, while restricting through automobile traffic. The use of cul-de-sacs is only discouraged within the neighborhoods that do not promote walkable neighborhoods.

5.1.5. Agricultural Residential Street

Agricultural residential streets serve Agricultural Residential neighborhoods and represent a transition between urban and rural agrarian character outside of the Community. Agricultural residential streets should have a characteristic similar to the neighborhoods they serve. Agricultural residential streets shall have a 10 foot wide travel lane, with a 6 foot wide paved shoulder in each direction (refer to Figure 5.6).

5.2. Bicycle and Pedestrian Facilities

In older neighborhoods, sidewalks may not be available, making it difficult for residents to walk safely. Newer residential neighborhoods provide sidewalks as a requirement of subdivision approval.



Right-of-Way (R.O.W.)
Rolled Curb (C)
Paved Shoulder (PSH)
Should (SH)
Travel Lane (TL)

46 feet wide
2.6 feet wide
6 feet wide
4.6 feet back of curb
10 feet wide

FIGURE 5.6 - AGRICULTURAL RESIDENTIAL



A number of residents walk or ride bicycles, thus an important emphasis of the Plan is to provide nonvehicular circulation opportunities. The Community Plan provides a network of bicycle and pedestrian facilities. The designated trails and streets will serve as the Community's network of bicycle and pedestrian facilities with Class I bike path along or adjacent to Turlock Irrigation District Lateral #6. Bicycle and pedestrian facilities branching from this corridor will ensure non-motorized access throughout the Community. If designed and implemented correctly, bicycle and pedestrian facilities can provide safe and efficient movement of residents to and from neighborhoods, commercial centers and public areas such as schools and parks. Figure 5.7, Pedestrian and Bicycle Facilities, illustrates the Community circulation elements and bicycle network.

The Merced County Regional Bicycle Plan, completed by the Merced County Association of Governments (MCAG) and adopted by Merced County in June 2003, provides a comprehensive long-range vision for the development of a regional bikeway system. In the vicinity of Delhi, El Capitan Way, Stephens Street and Schendel Avenue are identified as components of the Regional Bicycle Plan. In accordance with the Regional Bicycle Plan, Class II bike lanes are to be implemented within these corridors. As the Community grows, bicycle and pedestrian facilities should connect with the County's regional bikeway network.

While a variety of local, state, and federal sources may be available for funding bikeway and pedestrian improvements, new development shall finance these facilities required of their projects. Different types of assessment districts may also be used to fund the construction and maintenance of bicycle and pedestrian facilities. Examples include community facilities district, infrastructure financing district, open space district, or lighting and landscape district. These types of districts have specific requirements relating to their establishments and use of funds.

5.3. Public Transit

Public transit service ("The Bus") is provided by Merced County Transit. The Bus is managed by the Transportation Division of the Merced County Department of Public Works. Delhi is served by the North County Shuttle. This route provides a connection between communities in northern Merced County. Fix route services is supplemented by para-transit or "dial-a-ride" service.

This bus route serves as a public transit link between the City of Turlock in Stanislaus County, and the communities of Hilmar, Delhi and Livingston in Merced County. This route provides several bus stops within Delhi and connects to other bus routes that serve the City of Merced.

Delhi's relative small size and short commuting distances result in a small role for public transit. While a larger population base and higher development intensities have proven to increase levels of transit service, communities such as Delhi must rely on other methods to improve public transit utilization.

Providing convenient, reliable, and ample access to public transit within the Community will improve public transit ridership. In residential neighborhoods, residents should be within the proximity of bus stops. Public transit routes and bus stops should be planned in the areas of high public activity in the Community. The more important the designation, the closer the bus stop should be. For example, a government center or retail center would not be well served at the very edge of the one-quarter mile service area and probably should have a stop at the center. Bus stops should be well marked and provide adequate shade, seating and shelter.

5.4. Rail Service

The Union Pacific Railroad which extends through Delhi adjacent to Highway 99 provides only freight rail service. The nearest Amtrak passenger station is in the City of Merced.

5.5. Goals, Objectives, Policies and Implementation Measures

Goal:

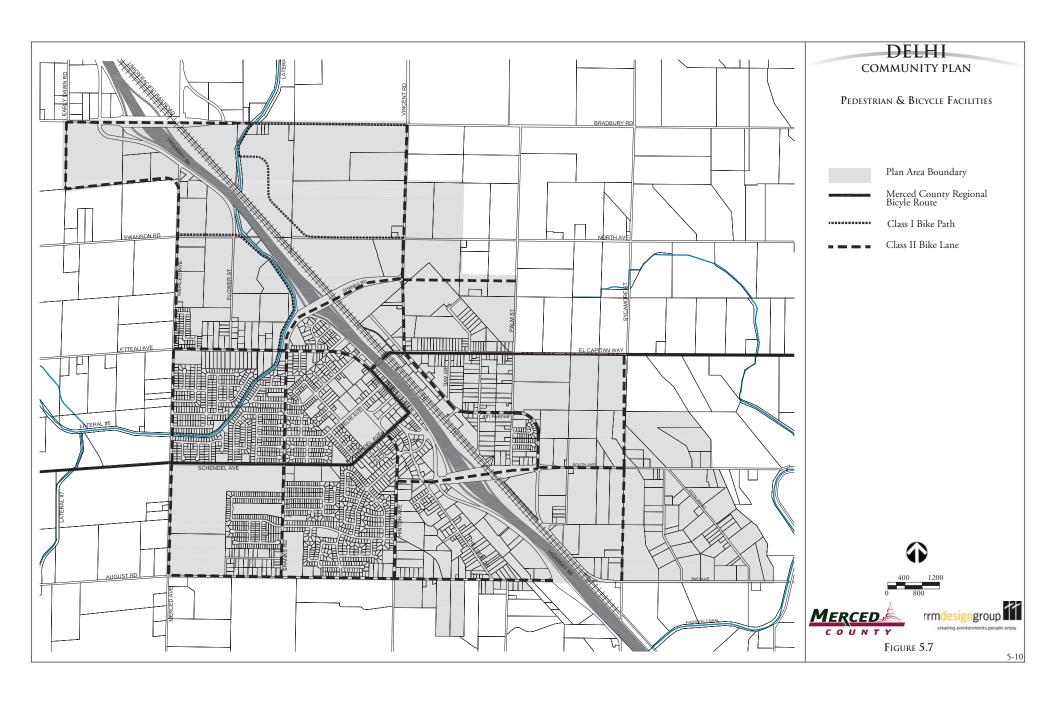
A balanced, safe, and efficient circulation system for Delhi.

Objective:

CI 1.0

Establish a hierarchical circulation system to serve the Community and regional traffic needs.





Policy:

CI 1.1 Provide and maintain a circulation system of arterials, major and minor collectors, local streets, agricultural residential streets, and bicycle and pedestrian facilities.

Implementation Measures:

- CI 1.1.a Designate a hierarchical circulation system consistent with the Delhi Community Circulation Diagram (refer to Figure 5.1) and the Pedestrian and Bicycle Facilities Map (refer to Figure 5.7).
- CI 1.1.b All street improvements shall be consistent with the appropriate street sections (refer to Figures 5.2 through 5.6) and Merced County Improvement Standards Specifications Manual (Title 16).
- CI 1.1.c Merced County shall require dedication and improvements of necessary on- and off-site right of way at the time of development in accordance with the street classification shown in the Delhi Community Plan Circulation Diagram (Figure 5.1) and the street standards specified in the Delhi Community Plan (Figures 5.2 through 5.6). The developer shall be responsible for required on- and off-site improvements. (EM)
- CI 1.1.d Driveways on major collectors should be at least 50 feet from an intersection for non-residential uses. Driveways on Major Collectors and within 300 feet of each other or an intersection, should be restricted to right hand turns only.
- CI 1.1.e Medium to large canopy shade trees shall be planted at intervals of 30-40 feet apart within the planting strip on major collectors. A single row of smaller trees shall be planted to add color and canopy across the entire width of the median.

- CI 1.1.f Major collector median and planting strip landscape should consist of turf areas with drifts of temperate evergreen, flower shrubs, and ground cover. Streetscape on abutting property along the corridor should reflect and complement the median and parkway landscaping.
- CI 1.1.g Driveways shall not be closer than 50 feet to the intersection on minor collectors. Class II bike lanes and on-street parking is permitted.
- CI 1.1.h Medium to large canopy shade trees shall be planted at intervals of 30-40 feet within the planting strip along minor collectors. Streetscape on abutting property along the corridor should reflect and compliment the planting strip landscaping.
- CI 1.1.i A preservation and maintenance program shall be adopted as part of future circulation improvements determined by the County Public Works Department along El Capitan Way.
- CI 1.1.j Intersections shall include improvements that are deemed necessary to mitigate traffic impacts. (EM)
- CI 1.1.k Design of local streets should employ traffic calming devices, such as chokers, "bulb outs", medians, etc., where necessary to manage traffic speeds and create a more pedestrian friendly environment.
- CI 1.1.1 Neighborhoods should be designed to interconnect with surrounding neighborhoods. This may be accomplished through the use of a grid or modified grid network or the use of interconnected cul-de-sacs. Dead end cul-de-sacs that do not provide for pedestrian movement are strongly discouraged except where necessary to provide for the development of odd shaped parcels. (EM)



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- CI.1.1.m A pedestrian connection shall be provided at the end of the cul-de-sac for access to adjoining pathways, open spaces, or streets. In conditions where arterials or major collectors with walls adjoin residential areas, open ended cul-de-sacs shall be used to create wall openings with pathway connections, unless a continuous noise wall is demonstrated to be required based on a noise study. (EM)
- CI 1.1.n Subdivisions shall be designed to maximize connectivity between adjacent land / parcels designated for an urban use. (EM)
- CI 1.1.0 To the extent practical, subdivisions shall maximize the number of connections to surrounding minor collectors through either open-ended cul-de-sacs or connecting local streets. (EM)

Objective:

CI 2.0 Provide timely and effective means of programming street improvements to maintain a peak hour level of service as established by the Merced County General Plan.

Policy:

CI 2.1 Prioritize street improvements based on current and forecasted service levels. Street sections and intersections experiencing or forecasted to experience a level of service (LOS) lower than a LOS D shall require improvements. (EM)

Implementation Measures:

- CI 2.1.a Merced County Public Works Department shall monitor traffic volumes of roads within the Community. (EM)
- CI 2.1.b Merced County Public Works Department shall be responsible for developing action plans to improve the Community's roadway system, as necessary, for build-out of the Community Plan. (EM)

Objective:

CI 3.0 Provide for a comprehensive pedestrian and bicycle travel in Delhi.

Policy:

CI 3.1 Establish a pedestrian and bicycle friendly environment that includes both on- and off-street pedestrian and bicycle facilities to encourage non-vehicular travel in the Community. (EM)

Implementation Measures:

- CI 3.1.a Where possible, the minimum required street widths should be used to reduce vehicular speed and encourage a pedestrian oriented streetscape. (EM)
- CI 3.1.b Sidewalks should be separated from the street by landscaped planting strips to improve the overall pedestrian scale and character.
- CI 3.1.c Sidewalks shall be a minimum of five (5) foot wide unless otherwise allowed in the Delhi Community Plan's street sections (Figures 5.2 through 5.6) or in the Community Character and Design Chapter (Chapter 4). Sidewalks shall comply with the Americans with Disabilities Act.
- CI 3.1.d Class I separated bike paths shall be established along or adjacent to Turlock Irrigation District Lateral #6, and along Bradbury Road, Vincent Road, Flower Street and Swanson Road. (EM)
- CI 3.1.e Class II bike lanes shall be provided along all major and minor collectors, except for those planned to have Class I bicycle lanes as indicated in CI 3.1.d. (EM)
- CI 3.1.f Bike lanes shall be striped in accordance with Caltrans specifications. (EM)
- CI 3.1.g Include planned bikeways as part of roadway projects involving widening, overlays or other street improvements. (EM)



Objective:

CI 4.0 Link parks, schools, residential neighborhoods, commercial and employment centers with bicycle and pedestrian facilities.

Policy:

CI 4.1 Integrate a multi-purpose bicycle / pedestrian trail system within the Community's Circulation system. (EM)

Implementation Measures:

- CI 4.1.a A bicycle / pedestrian trail on or adjacent to Turlock Irrigation District Lateral #6 is required and Merced County shall coordinate with TID to secure right-of-way and / or easements. For those areas as yet undeveloped, land shall be provided by adjacent developments, if not otherwise acquired. (EM)
- CI 4.1.b Integrate the Community bicycle and pedestrian trail system with the County-wide regional bicycle system. (EM)
- CI 4.1.c Recreational trails should be constructed of concrete or similar material acceptable to the Merced County Public Works Department..

Objective:

CI 5.0 Provide an efficient and attractive public transit system to serve Delhi. (EM)

Policy:

CI 5.1 Development adjacent to minor and major collectors shall coordinate with Merced County Transit to identify appropriate locations for public transit improvements (i.e., bus pullouts, seating shelters) to encourage public transit use. (EM)

Implementation Measures:

- CI 5.1.a Public transit stops shall be provided throughout Delhi as recommended by the County Transit Authority to ensure residents are within the proximity of a public transit stop. (EM)
- CI 5.1.b Public transit stops shall include bus shelter as recommended by the Merced County Transit Authoritys. (EM)
- CI 5.1.c Public transit routes and stops shall be planned in the areas of high public activity throughout Delhi. (EM)
- CI 5.1.d Street designs for major and minor collectors shall include provisions for fixed route transit system. (EM)



6.0 Open Space / Conservation

The Open Space / Conservation Chapter provides direction regarding the conservation of agricultural, biological, and historical resources. Delhi's setting, north of the Merced River includes natural resources that are important, not only for aesthetic value, but also for environmental quality, habitat protection, recreation and agriculture. A goal of the Community Plan is to preserve the viability of agriculture production outside of the Plan Area, while accommodating projected growth within the Plan Area. The preservation of Delhi's historical character and resources is also an important goal of the Community Plan.

This Chapter also provides direction for the development and maintenance of parks and recreational facilities. As described in Chapter 3 and 4, these facilities are viewed as fundamental building blocks of neighborhoods and overall community appearance. This Chapter establishes standards for neighborhood and community parks to ensure adequate open space will be available as Delhi grows.

6.1. Parks and Recreation

Park and recreation facilities ranked high on the list of Delhi's needs. By the year 2020, Delhi is expected to have approximately 20,000 residents. Meeting Community growth projections and the Community's park and recreation opportunities will require a wide range of actions including parkland acquisition, facility design, construction, and management of on-going facility maintenance. Figure 6.1, Parks and Recreation, illustrates the location of existing and planned recreational facilities, except for mini parks. The location of planned recreational facilities are generalized, but appropriately sized in the vicinity where recreational facilities are required to support community growth.

6.1.1. Mini-Parks

Mini-parks are small open space areas that serve residential neighborhoods. These parks, up to an acre in size and strategically located in neighborhoods provide recreational opportunities within a short walking distance from homes. Mini-parks provide open space areas for passive recreation, including an area for small children, seating areas, and picnic areas. Mini-parks may also include civic monument sites, public art sites and beautification areas in Delhi's downtown or other areas in the Community.

Neighborhood mini-parks are generally permitted only when developed or improved with private funds and accompanied with a landscape maintenance benefit district, or other identified funding source due to the inefficiency inherent in the development and maintenance of such small areas. Developers will be given credit against their park fees for developing mini-parks.

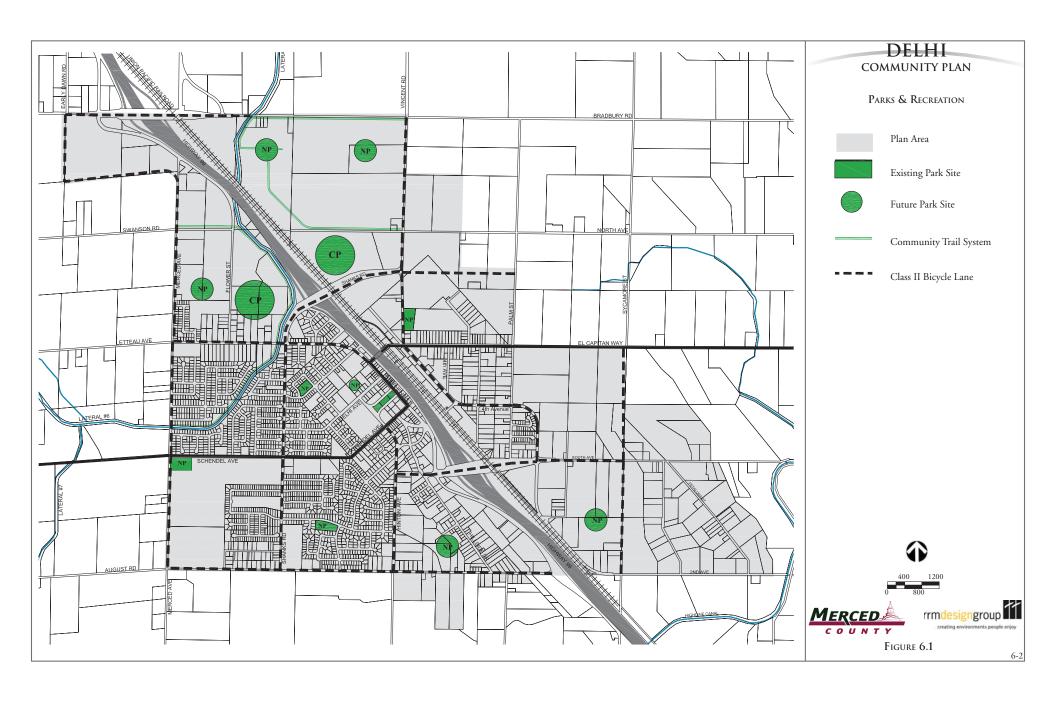
Mini-parks located in the downtown area may be developed or improved by Merced County under agreements with local civic organizations responsible for funding the maintenance costs and / or installation.

6.1.2. Neighborhood Parks

Neighborhood parks serve as neighborhood focal points, and as the hub for both physical and social activities in a recreational setting that should be primarily passive. Appropriately designed, neighborhood parks act as "pulse points" within neighborhoods. These spaces develop a sense of place, while at the same time, reflect the neighborhood they represent. They also serve as critical nodes and access points in a community-wide trail network. Neighborhood parks act as critical building blocks of Delhi's image, and assist in developing an overall sense of community and security.

A neighborhood park should be at least three acres in size, developed primarily to serve the recreational needs of residents living within a one half (1/2) mile radius of the park. The standard for this type of park is 3.0 acres per 1,000 residents of the Delhi. This standard may be accomplished by a single or multiple park sites.





Neighborhood parks may also serve a dual purpose by providing both recreation and storm drainage capacity. Details regarding the use of neighborhood parks as storm drainage detention or retention basins are described in Chapter 8, Public Services.

Neighborhood park standards described in this Chapter serve as the framework for site selection and design. Each future neighborhood park site should contain design elements that give it a special sense of place. Other design elements and refinements may be specified by subsequent master plans.

6.1.3. Community Parks

Community parks are larger in size than neighborhood parks and serve to fulfill the active and passive recreational needs of multiple neighborhoods. A community park serves the needs of multiple neighborhoods by providing a site for more active recreation that is not typically suitable or physically possible in a neighborhood park (e.g., sports fields and courts).

A community park is where most organized activities and league sports are intended to occur. To allow for organized activities and league sports, a community park should be based on a "focus" sport (e.g., soccer, baseball, etc.) where at least one-third of the active sports fields are for a focus sport. However, maintaining a diversity of activities is still necessary, thus in addition to accommodating a focus sport, the parks should reflect the needs and desires of the immediate neighbors.

In addition to supporting localized activity and passive sports, community parks should act as hubs in the community-wide trail network. To accomplish this, they should have a direct connection to a community-wide trail. Community park standards described in this Chapter will serve as the framework for site selection and design.

A community park should be at least 15-20 acres in size and meet the park and recreational needs of those living or working within a 2 mile radius of the park. The standard for this type of park is 2.0 acres per 1,000 residents of the Community.

Currently, there are no community parks within Delhi. Two proposed community park sites are needed for Delhi, one to satisfy the needs of new development, and another to serve the needs of the existing community. The former community park is proposed within the



An example of neighborhood park in a residential neighborhood.



An example of a trail adjacent to a Turlock Irrigation District canal.



Delhi is committed to the preservation of agriculture.



Bradbury Master Plan Area, while the latter location is within the Flower Street Master Plan. An alternative location for the latter may be considered based on suitability to serve the existing community and the availability of funding and land. The community parks will be further defined by the master plan using design elements as standards found in this chapter.

6.1.4. Community Trail System

Trails are essential to community character, activities, and to providing open space. Community trails are planned along or adjacent to Turlock Irrigation District Lateral #6, and Bradbury Road. A network of trails is also recommended for the Bradbury and Flower Street Master Plan Areas. The overall community trail system links residential neighborhoods to public amenities such as parks, schools, commercial centers, and employment areas.

6.2. Agriculture

Agriculture is a significant component of Merced County's economy. Agriculture is also an important contributor to Delhi's economy. As such, the Delhi Community strives to minimize the effects on farmland in the Delhi area.

As land is developed to urban uses, conflicts between agriculture and urban uses allowed by the Delhi Community Plan may occur. Establishing buffers between residential uses and agriculture by designating new development adjacent to agriculture Agricultural Residential (AR) provides a transition between agriculture and urban development. Farm owners have utilized other means of protecting their land from urban encroachment such as the Williamson Act or agriculture conservation easements to continue agriculture operations near the urban fringe.

6.2.1. Williamson Act

The Williamson Act is a California law that protects agriculture and open space from converting to urban uses. Landowners enter in a contract with Merced County to restrict their lands to agriculture and open space uses in exchange for a lower tax assessment on their parcels. This tax assessment is consistent with the actual use of the land, rather than the potential market value. Landowners enter these contracts for a period of 10 years with an automatic annual renewal unless Merced County or the landowner files a "notice of non-renewal." Predominant use of the

Williamson Act occurs to the north and east of the Plan Area (refer to Figure 6.2, Agriculture). Other parcels under Williamson Act Contract outside of the Plan Area include clusters near Swanson Road to the west.

6.2.2. Agriculture Conservation Easement

Agriculture conservation easements are a method by which a landowner may place restrictions to keep their land in agriculture uses in perpetuity. These easements are permanent and apply to all successive property owners. Landowners north of Bradbury Road, east of Highway 99 and properties south of August Road have agriculture conservation easements (refer to Figure 6.2). Potential easements are pending east of Palm Street.

6.2.3. Community Agriculture Buffer

Williamson Act lands and in particular, agriculture conservation easements are beginning to create an agriculture buffer around Delhi. The Williamson Act and agriculture conservation easements are providing, agriculture buffers that not only provide an economic base, but establish a boundary for a compact community, and a sense of place and small town atmosphere for Delhi.

6.3. Biological Resources

Existing agriculture and underdeveloped areas in the Plan Area provide potential known habitat for species identified as sensitive or special status by the California Department of Fish and Game (DFG) and the U.S. Fish and Wildlife Service (USFWS). The following special status species habitats or potential habitat may be present in the Community Plan Area.

6.3.1. Swainson's Hawk

The Plan Area includes potential Swainson's hawk foraging habitat. Swainson's hawk foraging habitat includes alfalfa, fallow fields and row crops. Swainson's hawks are known to forage within 10 miles of its nest. While no Swainson's hawk nests have been recorded in Delhi, there have been recorded nests sites along the Merced River, which is less than 10 miles from the Plan Area.

6.3.2. Burrowing Owl

The Plan Area includes potential burrowing owl habitat. Potential burrowing owl habitat includes low, open vegetation with minimal canopy coverage and ground



squirrel dens. While no borrowing owls have been recorded in the Plan Area, potential habitat in the form of vacant and under utilized parcels can be found in the Plan Area.

6.3.3. California Horned Lizard and California Legless Lizard

Neither the California horned lizard nor the California legless lizard has been recorded within the Plan Area. Both species can occasionally persist as small, isolated populations in areas that have not been subject to regular ground disturbances such as vegetated road right-of-ways, vacant lots, dry portions of ranchettes, or along canals.

6.3.4. Loggerhead Shrike

There is a potential for loggerhead shrike to nest in the Plan Area. The species typically nest in densely-foliaged shrubs or trees located adjacent to suitable habitat. Loggerhead shrikes nest March through June, and if nests are present in the Plan Area, construction activities may harm nests, eggs, or young.

6.4. Historical and Cultural Resources

Fifteen structures in the Plan Area have been identified as having potential historical significance. These structures have been recorded by the California Historical Resources Information Center. During the re-construction of Highway 99, the State identified and recorded potential historical structures within proximity of the Highway 99 corridor. A majority of these structures are located within the Downtown Delhi Plan Area (refer to Figure 3.2). Given the magnitude of structures identified within the immediate area of Highway 99, it is very likely other potential historical structures may exist in other parts of the Community. As such, the Plan Area should identify and evaluate structures as to their historical significance, as well as, possible eligibility for listing on the National Historical or State register. Based on the outcome of the inventory of historical structures, it may be appropriate to map and establish a "Delhi Historical District."

Additionally, Native American burial remains and artifacts have been recorded within one mile of the Plan Area. Construction within the Plan Area (e.g., grading and excavation, etc.) may potentially disturb or destroy subsurface cultural artifacts or remains. Policies and implementation measures found in this Chapter will minimize the risk of destroying subsurface cultural resources.

6.5. Goals, Objectives, Policies and Implementation Measures

Goal:

Adequate park and recreational facilities are provided to meet the varied needs of the Community.

Objective:

OS 1.0 Provide suitable locations for parks and open space elements within the Community.

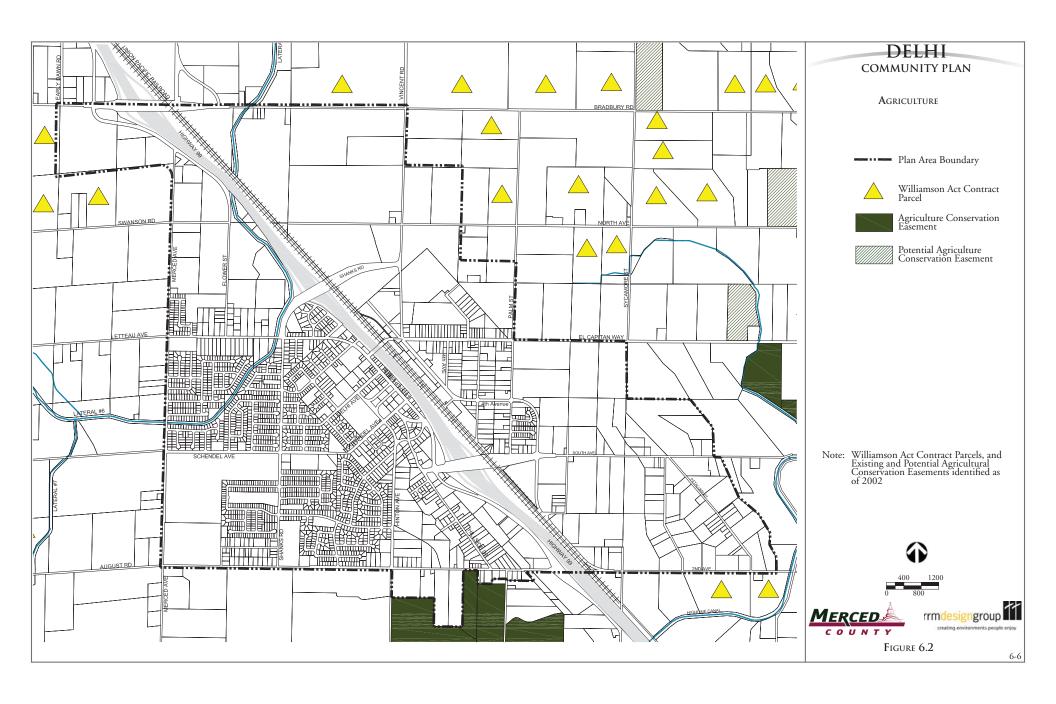
Policy:

OS 1.1 Provisions for parks and recreational facilities shall be consistent and within the general location and size of the parks and open space corridors identified in Figure 6.1, Parks and Recreation.

Implementation Measures:

- OS 1.1.a Master plans shall address the recreational needs of plan area's residents. The master plans shall design, in detail, applicable neighborhood or community park sites.
- OS 1.1.b The County of Merced shall develop a Community Parks and Recreation Master Plan, coordinated with the Delhi Unified School District, which details site specific plans of new parks and mechanisms to pay for the construction and maintenance of new and existing parks.
- OS 1.1.c Develop community trails that encourages pedestrian / bicycle use.
- OS 1.1.d Require parkland dedication to meet the standard of 3.0 acres of neighborhood parkland per 1,000 residents and provide an additional 2.0 acres of community parkland per 1,000 residents. Trails and / or open space corridors should be provided with new development. (EM)





OS 1.1.e Merced County shall explore financial mechanisms such as assessment districts, grants, and state bond monies to fund additional public park and recreation projects.

Policy:

OS 1.2 Mini-parks, neighborhood and community parks shall satisfy standards that serve as a framework for site selection and design of future parks.

Implementation Measures:

OS 1.2.a Mini-parks shall include the design elements identified in Table 6.1.

- OS 1.2.b Mini-parks shall be a minimum of onequarter (0.25) net acres of usable land.
- OS 1.2.c Mini-parks shall have a general service area of approximately one-quarter (1/4) mile radius and located to avoid patrons having to cross an arterial, major or minor collectors, railroad or irrigation canal to gain access.
- OS 1.2.d Parkland dedication and construction of mini-parks are eligible for park land dedication credits.

Table 6.1 Mini-Parks

Park Size	□ 0.25 to 1.0 net acres.	
Location	Located within residential neighborhoods with a maximum service area of 0.25 mile radius.	
	☐ May be located in Delhi's downtown area or other areas in the Community.	
	☐ A mini-park should be fronting on at least one (1) street.	
Basic Design Elements	☐ Small open turf areas for non-organized field play.	
	☐ Play area and equipment for children 2 to 5 years of age.	
	☐ Seating areas adjacent to play area(s).	
	☐ Picnic table(s) to accommodate 6-8 people.	
	☐ Trees and / or shade structure, appropriate in play area(s) and seating area(s).	
	☐ Drinking fountains.	
	☐ Trash receptacles.	



OS 1.2.e	Neighborhood parks shall include the design elements identified in Table 6.2.	OS 1.2.h	Community parks shall include the design elements identified in Table 6.3.
OS 1.2.f	Neighborhood parks shall be a minimum of three (3) net acres of usable land. (EM)	OS 1.2.i	Community parks shall be a minimum of 15 net acres of usable land. (EM)
OS 1.2.g	Neighborhood parks should be located to service an area of approximately one-half (½) mile and avoid the need for children to cross arterials, major collectors, railroad or irrigation canals.	OS 1.2.j	Community parks should have a general service area of approximately one (1) mile radius and located to provide adequate access to an arterial, major collector or minor collector.

Table 6.2 Neighborhood Parks

Neighborhood Parks				
Park Size	Approximately 3 to 6 net acres.			
Location	☐ Centrally located within a neighborhood and if possible within easy walking distance of a community wide open space corridor. A neighborhood park should be fronted on all sides by local streets. A neighborhood park may be located adjacent to a school site or an open space corridor.			
Basic Design Elements	Separate play areas and equipment for children 2 to 5 years of age and for children 6 to 12 years of age.			
	Seating areas shall be adjacent to play areas individual and small group picnic areas with tables and individual barbeques.			
	☐ Multi-purpose courts.			
	Trees and structures to provide shade as appropriate in play areas, picnic areas and seating areas.			
	Permanent restroom structures shall be located on site for neighborhood parks greater than five (5) acres in size.			
	☐ Drinking fountains.			
	☐ Trash receptacles.			



Table 6.3 Community Parks

Park Size	10 to 25 net acres.	
Service Area	Up to a 2 mile radius serving approximately 5,000 to 12,000 people.	
Location	Park should be located such that access from the surrounding neighborhoods its maximized, yet the impacts of the high activity level of the neighborhood is minimized.	
	Community parks should be located to provide adequate access to an arterial, major collector, or minor collector. A community park may be located adjacent to, but not within a school site or an open space corridor.	
Basic Design Elements	A multi-purpose building/community center for social gatherings, recreation programs, etc should be located on site.	
	Larger group picnic areas with barbecue pits with large shade structures.	
	Seating areas shall be adjacent to play areas.	
	Individual and small group picnic areas with tables and individual barbeques.	
	Trees and structures to provide shade as appropriate in play areas, picnic areas and seating areas.	
	Multipurpose courts.	
	Lighted and unlit sports fields as appropriate for various sports.	
	Open-air amphitheater/outdoor performance or suitable gathering area.	
	Public art where appropriate.	
	Separate play areas and equipment for children 2 to 5 years of age and children 6 to 12 years of age.	
	Permanent restroom structures shall be located on site.	
	Drinking fountains.	
	Trash receptacles.	



OS 1.2.k Storm drainage basins shall be developed pursuant to those standards adopted by Merced County Public Works Department to include, but not limited to design, construction, parkland credits if applicable, and dual use criteria.

Objective:

OS 2.0 Establish a community trail system to provide open space, circulation and recreational opportunities.

Policy:

OS 2.1 Residential neighborhoods, commercial, and employment centers should be linked by a pedestrian / bicycle trail system to promote local non-vehicular travel.

Implementation Measures:

- OS 2.1.a A bicycle / pedestrian trail on or adjacent to the Turlock Irrigation District Lateral #6 is required and Merced County shall coordinate with TID to secure right-of-way and / or easements. For those areas as yet undeveloped, land shall be provided by adjacent developments, if not otherwise obtained from TID. (EM)
- OS 2.1b The community trail along or adjacent to Turlock Irrigation District Lateral #6 should connect to existing residential culde-sacs abutting the Lateral.
- OS 2.1.c Bike trails (Class I & II), pedestrian trails, and sidewalks shall be provided along arterials, major and minor collectors as shown in Figure 5.2, 5.3, and 5.4.
- OS 2.1.d Bike / pedestrian trails shall connect to public facilities such as neighborhood and community parks, and schools.
- OS 2.1.e Community trails shall be landscaped and provide access opportunities from adjoining residential neighborhoods.

- OS 2.1.f At intersections with streets, community trails should incorporate appropriate hardscaping such as pavers, concrete, or similar material and other pedestrian / bicyclist safety features that are acceptable to the Merced County Public Works Department..
- OS 2.1.g Appropriate lighting should occur along the community trails to enhance safety and add aesthetic value.

Goal:

Preserve agriculture, and biological and historical resources around the Community while accommodating growth.

Objective:

OS 3.0 Establish a transition area between the urban environment and agriculture environment to support and preserve viable farming and ranching operations outside the Plan Area.

Policy:

OS 3.1 Establish land use designations that provide a transition between the urban and surrounding agricultural environment on the fringes of the Community.

Implementation Measures:

- OS 3.1.a Land adjacent to agricultural conservation easements shall be limited to only non-residential uses, or Agricultural Residential (AR) with no more than one lot per acre, consistent with the Chapter 3 land use policies, and be served with public water and sewer if adjacent to or within the Delhi County Water District, when available. (Director Clarificiation 2006-01, September 11, 2006) (EM)
- OS 3.1.b Residential land use designations / zoning other than Agricultural Residential (AR) adjacent to agricultural designated lands outside of the Community Plan should provide some type of buffer, such as a 200 foot habitable building setback to



agriculture, an open space corridor or physical improvements such as a collector or greater road or a canal. (EM)

OS 3.1.c The County of Merced shall continue to support and assist in the establishment of agricultural conservation easements in the Delhi Area to separate urban land uses and agriculture from each other. (EM)

Policy:

OS 3.2 Mitigation shall be required to preserve important farmland around Delhi and protect agriculture within Merced County. (EM)

Implementation Measure:

OS 3.2.a Prior to the subdivision / development of a master plan area or the subdivision / development of vacant or underutilized land identified as Important Farmland, as defined by the California Department of Conservation, that is equal to or greater than 20 acres, the applicant shall place farmland of equal or higher quality into a permanent agriculture conservation easement and / or provide equivalent in-lieu fees at a ratio of one (1) acre of agriculture conservation easement for every acre of Important Farmland subdivided / developed. (EM)

In the event the County of Merced adopts an agriculture impact mitigation program / ordinance that defines the method(s) by which the loss of Important Farmland shall be mitigated (e.g., in-lieu fee, agriculture conservation easement replacement ratio criteria, timing of dedication, etc.), projects which meet the agriculture impact mitigation program / ordinance criteria will be subject to the requirements of the program / ordinance.

Objective:

OS 4.0 Protect special status species in accordance with federal and state regulatory requirements.

Policy:

OS 4.1 Survey and identify potential special status species habitat prior to development activity.

Implementation Measure:

OS 4.1.a Any project that is within master plan area or is more than one (1) acre in size, the applicant shall submit a pre-construction survey on all lands proposed for clearing or development for Swainson's hawk nests, burrowing owl habitat, California horned lizard habitat, California legless lizard habitat, and for loggerhead shrike nesting habitat or provide documentation of compliance from the California Department of Fish and Wildlife Service. (EM)

Objective:

OS 5.0 Preserve archaeological, palontological and historical resources found in the Delhi Community Plan Area.

Policy:

OS 5.1 Identify, inventory and register historic sites, buildings and structures. (EM)

Implementation Measures:

OS 5.1.a The County of Merced shall undertake an inventory of historic resources in the Delhi Community Plan Area to determine and map sites, buildings or structures of federal, state or local significance. The County of Merced is encouraged to seek the assistance of the Delhi Historical Society to undertake the inventory of historic resources.

The State Office of Historic Preservation has determined that buildings or structures 45 years or older have the potential to be historically significant. Sections 5020-5029 of the State Public Resources Code address historic resource assessment and protection procedures and requirements. The inventory of historic resources shall be conducted in accordance with Sections 5020-5029 of the State Public Resources Code. (EM)



- OS 5.1.b The County of Merced shall promote, assist and / or facilitate the registration of qualified historic sites, buildings or structures in the National Register of Historic Places, State Register of Historic Properties and / or inclusion in the California Inventory of Historic Resources. (EM)
- OS 5.1.c Prior to completion of an historic resource inventory, master plans and projects that may involve the remodeling (other than routine maintenance) or demolition of buildings or structures 45 years or older prior to the County of Merced undertaking an inventory of historic resources in the Delhi Community Plan Area shall be evaluated by a qualified professional for historic significance and recommendations prior to approval.

Buildings and structures determined to be of historic significance shall be avoided to the extent feasible. The property shall be preserved in situ, and recommendations of an architectural historian regarding treatment of property to ensure that it maintains its historic significance shall be implemented. If preservation is not feasible, the property shall be recorded consistent with Secretary of Interior standards prior to demolition. (EM)

Policy:

OS 5.2 Identify, inventory and preserve archaeological and palontological resources.

Implementation Measures:

OS 5.2.a The County of Merced shall consult with the local Native American community in cases where new development may result in disturbance to Native American sites. (EM)

- OS 5.2.b Master Plans and projects one (1) acre or greater requiring grading or excavation shall prepare an archaeological survey, including testing, if recommended by a qualified archaeologist, prior to approval. Areas that are found to contain or be likely to contain archaeological resources shall be fully surveyed, including excavation to the extent needed, to characterize and record the archaeological site. If a sensitive site cannot be fully surveyed prior to construction (due to the presence of pavement or other reasons), a qualified archaeologist shall be present to monitor all grading and excavation activity. Any artifacts that are uncovered shall be recorded and preserved in situ or donated to an appropriate organization or archive. (EM)
- OS 5.2.c If human remains are discovered, California Health and Safety Code Section 7050.5 states that no further disturbance shall occur until the County Coroner has made the necessary findings as to their origin and disposition pursuant to Public Resource code Section 5097.98. If the Corner determines that no investigation of the cause of death is required, and if the remains are of Native American origin, the Coroner will notify the Native American Heritage Commission, which in turn will inform a most likely descendent. The descendent will then recommended to the landowner appropriate disposition of the remains and any grave goods. (EM)



7.0 Noise

The purpose of the Noise Chapter is to set forth policies that regulate the ambient noise environment and to protect residents from exposure to excessive noise. Noise is primarily a concern with respect to noise sensitive land uses such as residences and schools. Community centers, places of worship and meeting facilities are noise sensitive with respect to interior conditions, but are generally not sensitive to exterior noise levels.

Noises vary widely in their source and volume ranging from individual occurrences such as a "warning horn blast" from a passing train, to intermittent disturbances of overhead aircraft, to fairly constant noise generated by vehicular traffic on highways.

The principle noise source in Delhi is related to vehicular traffic on Highway 99. Other noise sources include railroad activity, overflights from a private airstrip located at the northeast corner of Palm Street and North Avenue, and agricultural operations in and around the Plan Area. Noise produced by industrial activity has a negligible effect on the Community's ambient noise environment.

7.1. Noise Measurement

Noise is commonly defined as undesirable or unwanted sound. Three aspects of community noise are used in assessing the noise environment:

Level – Sound levels (e.g., magnitude or loudness) of sound are measured and expressed in decibels (dB) with 10 dB roughly equal to the threshold of hearing.

Frequency – Frequency (composition or spectrum of the sound) is a measure of the pressure fluctuations per second, measured in units of hertz (Hz). The characterization of sound level magnitude with respect to frequency is the sound spectrum, often described in octave bands, which divide the audible human frequency range (e.g., from 20 to 20,000 Hz) into ten segments.

Variation – Variation in sound level with time is measured as noise exposure. Most community noise is produced by many noise sources that change gradually throughout the day and produce a relatively steady background noise having no identifiable source. Identifiable events of brief duration, such as aircraft overflights, passing trains, result in the community noise level to vary from instant to instant. A single number called the equivalent sound level or L_{eq} describes the average noise exposure level over a period of time.

Transient noise events may be described by their maximum A-weighted noise level (dBA) or by their sound exposure level (SEL). SEL values may be summed on an energy basis to compute $L_{\rm eq}$ values over a period of time. Hourly $L_{\rm eq}$ values are called hourly noise levels.

Measuring and reporting noise levels involves factoring in variations in sensitivity to noise during the daytime versus nighttime hours. Noise descriptors used to account for human sensitivity to nighttime noise when background noise levels are generally lower than in the daytime and outside noise intrusions are more noticeable. Common descriptors include Community Noise Equivalent Level (CNEL) and the Day-Night Level (L_{dn}). Both reflect noise exposure over an average day with weighting to reflect the increased sensitivity to noise during the evening and night. The two descriptors are roughly equivalent. The L_{dn} descriptor is used in relationship to major continuous noise sources, such as traffic, and is the reference level for the Merced County General Plan and Delhi Community Plan.

The Merced County General Plan Noise Element establishes the following noise standards:

New residential land uses shall not be approved were existing or projected noise levels will exceed 65 dBA L_{do}.



Noise

Residential	subdivisions	should	incorporate					
measures to	reduce exterior	noise ex	posure from					
ground sources to less than 65 dBA L_{dn} .								

- Design standards and construction measures should be used to achieve interior noise levels that do not exceed 45 dBA L_{dn}.
- Proposed schools and hospitals shall not be approved in locations where existing or projected traffic noise levels exceed 70 dBA L_{dn}.
- Noise levels from properties adjacent to residential development are not allowed to exceed 65 dBA L_{dn} or 75 dBA L_{max} at the property line.
- Noise adjacent to non-residential development land uses is not to exceed 70 dBA $L_{\rm dn}$ or 80 dBA $L_{\rm max}$.

The State Building Code (Title 24) requires acoustical studies be conducted prior to construction at residential building locations where the exterior noise levels exceed 60 dBA $L_{\rm dn}$. The studies must include measures that would limit the noise levels in any habitable room to 45 dBA $L_{\rm dn}$.

Knowledge of the following relationships is helpful in understanding how changes in noise and noise exposure are perceived:

- Except under special conditions, a change in sound level of 1 dB cannot be perceived;
- A 3 dB change is considered a just-noticeable difference;
- A 5 dB change is required before any noticeable change in community response would be expected. A 5 dB increase is often considered significant; and
- ☐ A 10 dB increase is subjectively heard as an approximate doubling in loudness and almost causes an adverse community response.

7.2. Railroad Noise

Rail operations on the Union Pacific Railroad line contributes to the ambient noise level in Delhi. To quantify railroad noise exposure in the Community, continuous (24 hour) noise level measurements were conducted in November 2004. The purpose of the noise measurements were to determine the approximate number of daily railroad operations on these tracks, to quantify typical sound exposure levels (SEL) for railroad passage, and to calculate railroad noise levels in terms of day / night average levels ($L_{\rm dr}$).

The study found that on average, 30 freight trains per day passed through Delhi. The passing of trains were randomly distributed throughout the day and nighttime hours and that the maximum speed of these freight trains averaged 65 miles / hour (mph). The approximate distance to the 60 and 65 dB $\rm L_{dn}$ railroad noise contours was computed from the measurement results as being 1,000 feet and 470 feet, respectively from the centerline of the railroad track. Railroad noise levels increase approximately 300 feet north and south of El Capitan Way at grade crossings due to the use of the warning horn.

Existing and planned residential development, and other noise sensitive land use development within 1,000 feet of the Union Pacific Railroad corridor could be potentially exposed to unacceptable noise levels associated with passing freight trains. Exposure to unacceptable noise levels can be counteracted by the implementation of land use designs that factor in noise concerns. Noise barriers, site planning, improvements to building facades or other effective measures may be used to achieve required interior and exterior noise level standards. Policies in this Chapter establish criteria for noise sensitive land uses to ensure that exposure to noise levels will not exceed acceptable levels.

7.3 Traffic Noise

Vehicular traffic on Highway 99 is a major contributor to the ambient noise environment in Delhi. Traffic noise depends primarily on the speed of traffic and the percentage of truck traffic. The primary source of noise from automobiles is high frequency tire noise, which increases with speed. In addition, trucks and automobiles produce engine and exhaust noise. Trucks also generate wind noise. While tire noise from autos is generally located at ground level, truck noise sources can be located as high as 10 to 15 feet above the roadbed due to tall exhaust stacks and higher engine placement.

Noise level measurements and subsequent modeling were conducted in November 2004 to quantify existing and projected traffic noise exposure in the Community. With the exception of development in the immediate area of



Highway 99, the Community's residential neighborhoods or other noise-sensitive land uses are not exposed to moderate or excessive noise levels from the Community's traffic corridors.

The approximate distance to present 60 and 65 dB $L_{\rm dn}$ Highway 99 noise contours was computed from the measurement results as being 2,113 feet and 981 feet, respectively from the centerline of the Highway, north of Shanks Road. South of Shanks Road, the traffic noise levels decreased. The approximate distance to the 60 and 65 dB $L_{\rm dn}$ Highway 99 noise contours computed was 1,864 feet and 865 feet respectively from the centerline of the Highway south of Shanks Road.

Future development within the Plan Area will result in new roads and increased traffic volumes, increasing the noise levels in some areas. Noise levels along Highway 99 are expected to increase. The approximate distance to projected 60 and 65 dB L_{dn} Highway 99 noise contours will be 2,769 feet and 1,285 feet, respectively from the centerline of the Highway, north of Shanks Road. South of Shanks Road, the traffic noise levels will also increase. The approximate distance to the 60 and 65 dB L_{dn} Highway 99 noise contours, south of Shanks Road will be 2,443 feet and 1,134 feet respectively from the centerline of the Highway (refer to Figure 7.1, Noise Contours)

7.4 Industrial Noise Sources

Noise is inherent in many industrial processes, even when the best available acoustical technology available is applied. The noise levels within an industrial facility are controlled by Federal and State employee health regulations (OSHA and Cal-OCHA). Exterior noise levels, as noted above are govern by local ordinances. Noise produced by industrial facilities in the Delhi area has a negligible effect to the Community's ambient noise environment.

With newly designated industrial areas within Delhi, noise issues could potentially arise from new industrial uses. Design project review and site specific mitigation should be identified and follow the guidelines set forth in this Chapter.

7.5 Agriculture Noise Sources

There are active agricultural uses both within and adjacent to the Delhi Plan Area. Due to the wide array of equipment types and conditions under which that equipment is used in the agriculture industry, noise generated by agricultural processes varies substantially.

Noise measurements were conducted at a crop-dusting airstrip located at the northeast corner of Palm Street and North Avenue that has now ceased operations. Noise generated by crop-dusting activities varies, depending on the type of aircraft used and the proximity of the receiver to the operating aircraft. The approximate locations of the 65 dB Ldn noise contours associated with aircraft activity were plotted (refer to Figure 7.1).

The approximate distance to 65 dB $L_{\rm dn}$ noise contour is 3,000 feet from the centerline of the runway. For the most part, the 65 dB $L_{\rm dn}$ contour is located in areas designated Urban Reserve (UR). A small portion of existing residential development is located with the 65 dB $L_{\rm dn}$ contour (refer to Figure 7.1).

7.6 Goals, Objectives, Policies and Implementation Measures

Goal

To keep residents free from harmful and annoying affects of excessive noise.

Objective:

N 1.0

Protect noise sensitive land uses (e.g., residences, schools) from sources that generate noise levels beyond acceptable levels (over 65 dBA L_{dn} for sensitive uses).

Policy:

N 1.1

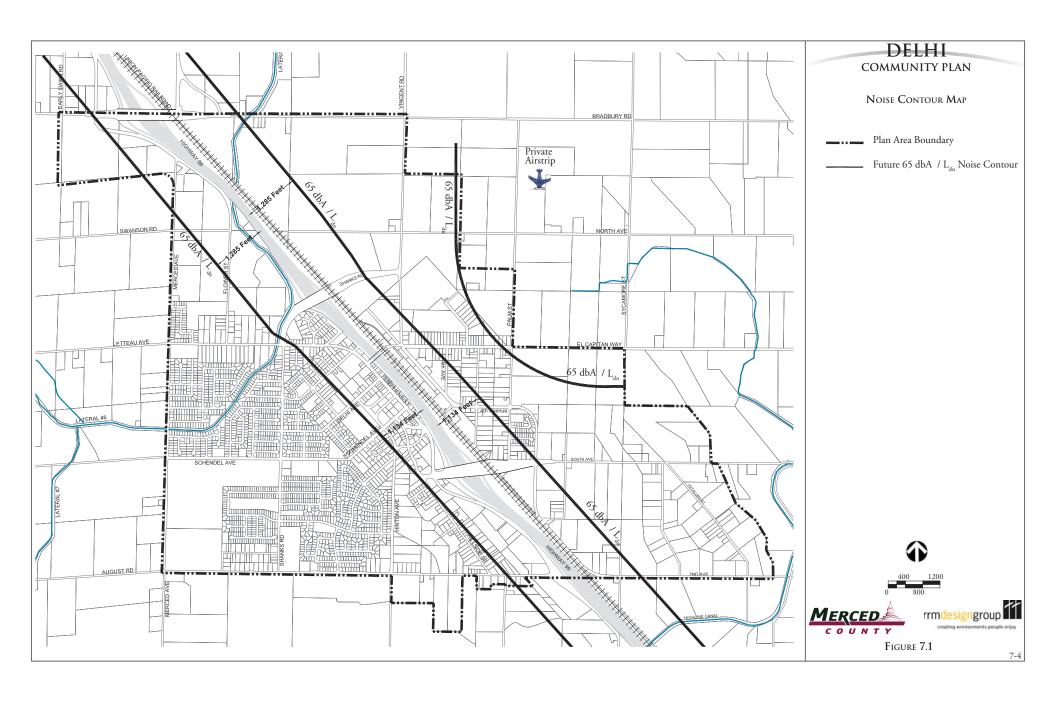
Residential development shall incorporate means to reduce noise levels to 65 dBA L_{dn} as mandated by the Merced County General Plan Noise Chapter policies. (EM)

Implementation Measures:

N.1.1.a During the master plans tentative subdivision map, or building permit process, proponents shall submit noise studies that are needed to comply with Policy N1.1 and identify appropriate measures to screen new noise sensitive

development. (EM)





N 1.1.b Any new noise sensitive development that occurs within the identified 65 dBA $L_{\rm dn}$ contour shall first provide a study of noise levels and mitigation efforts to reduce noise levels within the project site. This study shall be reviewed by the County before project approval. (EM)

Policy:

N 1.2 Noise reduction measures and buffers shall be attractively designed consistent with the Community Character and Design Chapter (Chapter 4).

Implementation Measures:

N 1.2.a Landscape setbacks and berms shall be the preferred method of noise abatement for noise sensitive development. Noise attenuations walls are strongly discouraged along minor collector corridors or designated Community gateways. If the setback required to attenuate noise levels to an acceptable level exceeds 50 feet, a noise attenuation wall may be constructed with appropriate landscaping.

N 1.2.b Walls shall not exceed seven (7) feet in height, except for noise attenuation along the Highway 99 or railroad corridors. Sound walls shall consist of masonry materials, and landscaped to maintain community aesthetics and avoid being a target for graffiti.



8.0 Public Services

This Chapter addresses public infrastructure and services requirements necessary to support the Delhi Community Plan. Public infrastructure facilities include water, sewer, and storm drainage. Public services addressed in this Chapter include schools, fire protection and law enforcement. One of the basic principle of the Delhi Community Plan is to ensure adequate public infrastructure and services will be provided with the level of entitlement associated with new and existing development.

8.1. Water Services

The Delhi County Water District (Delhi CWD) provides municipal water services to a majority of the Delhi Community Plan Area (refer to Figure 8.1, Conceptual Water Diagram). Delhi residents that do not receive municipal water from the District are supported by individual water wells. As land within the Community Plan Area is developed which is not presently within the Delhi CWD, it will need to be annexed into the District to receive municipal water services. Land designated Agricultural Residential (AR) will not be provided municipal water service from the Delhi CWD. Existing and new development on land designated AR will rely on individual groundwater wells. Land designated AR is not expected to annex into the Delhi CWD.

Municipal water is supplied by groundwater wells. Water is pumped, chlorinated, and conveyed through a pressurized water system with hydrants available for fire protection. The southeast section of Delhi experiences the lowest water pressure because of its distance from current well sites. Nevertheless, Delhi CWD's water pressure meets the California Department of Health's minimum standard of 20 pounds / square inch. The overall water quality of existing wells meets State of California Title 22 requirements. Existing water mains and wells are adequate to meet present demands and fire flows in Delhi. However, the current system is not capable of supporting projected water supply demands.

To support the Delhi Community Plan's projected water supply demand, additional wells and water mains will be needed. Additional wells connected to a system of 12 inch pipes spaced approximately a one-half (1/2) mile apart will be needed (refer to Figure 8.1). New well sites will be located in the northern section of the Plan Area. It is possible that a well site may be constructed in the southeast section of the Plan Area, depending on further

studies and well tests. Past groundwater test results in the southeast section of the Plan Area have not met California Title 22 requirements.

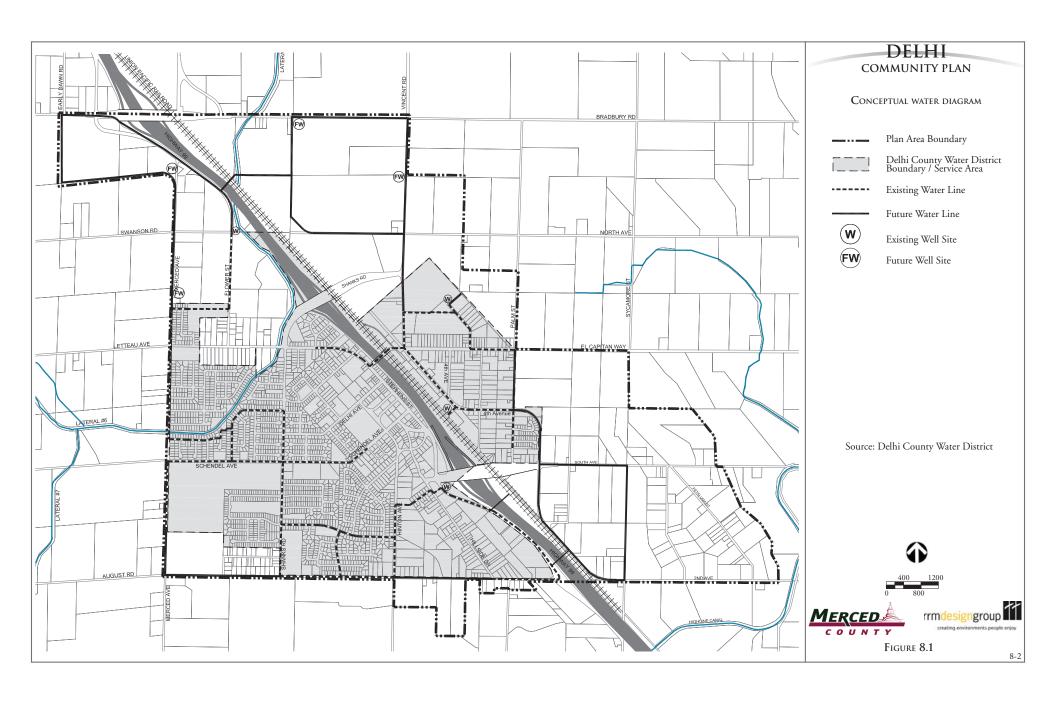
8.2. Wastewater Services

The Delhi CWD provides wastewater service to a majority of the Plan Area (refer to Figure 8.2, Conceptual Wastewater Diagram). Delhi residents that are outside of the Delhi CWD and do not receive wastewater service from the District rely on individual septic systems for wastewater disposal and treatment. Similar to municipal water service, land will need to annex to the District in order to receive wastewater services. However, the District will not be providing wastewater service to properties designated Agricultural Residential (AR). Existing and new development on land designated AR will rely on individual septic systems for wastewater disposal and treatment. Land designated AR is not expected to annex into the Delhi CWD.

The Delhi CWD uses gravity collection supported by lift stations to convey sewage to the District's wastewater treatment plant located southeast of the Community Plan Area. Currently, the Delhi CWD wastewater system is comprised of 7 trunk lines and 6 lift stations. The existing wastewater collection system and treatment plant is adequate to meet existing demands However, existing lines, lift stations and the wastewater treatment plant are not capable of handling projected demands.

To support the Delhi Community Plan's projected wastewater demands, additional improvements will be needed. Existing sewer trunk lines, force mains, lift stations and the wastewater treatment plant will need to be upgraded and expanded. New sewer lines, force mains, and lift stations will also be needed (refer to Figure 8.2, Conceptual Wastewater Diagram). Conceptual trunk line improvements include:





- ☐ Trunk Line "J"—consists of 8 to 15 inch gravity lines north of Letteau Avenue, a lift station in the vicinity of Flower Street and a force main. The force main will convey effluent flow from the lift station to Line "L". Trunk Line "J" will provide sewer service north of Letteau Avenue and west of Highway 99.
- ☐ Trunk Line "K"—consists of 8 to 15 inch gravity lines north of North Avenue and lift station near the intersection of North Avenue and Vincent Road. This line will tie into Line "L" and provide for sewer service north of Shanks Road, east of Highway 99.
- ☐ Trunk Line "L"—consists of 18 to 21 inch gravity lines from the intersection of North Avenue and Vincent Road to Lift Station 1. Trunk Line "L" supports Line "K" and trunk lines west of Highway 99.
- ☐ Trunk Line "M"—consists of 10 and 15 inch gravity lines south of South Avenue to Lift Station 1. The trunk line will provide sewer service for development east of Highway 99, north of 2nd Avenue, excluding Agricultural Residential (AR) designated parcels.

In addition to the trunk line improvements, new lift stations and expansion of existing lift stations will be necessary for additional sewage service.

Proposed improvements to existing lift stations include the following:

- ☐ **Lift Station 1**—upgrade the pump to allow for 2,900 gallons per minute (gpm) to accommodate existing and future flows. A new force main from this lift station to the wastewater treatment plant would be included in the upgrade.
- ☐ **Lift Station 4**—upgrade the pumping capacity from 180 gpm to 350 gpm.
- □ **Lift Station 5**—upgrade the pumping capacity to 500 gpm. The existing force main from this station will be re-routed to accommodate the upgrade of the facility.

The following new lift stations will need to be provided:

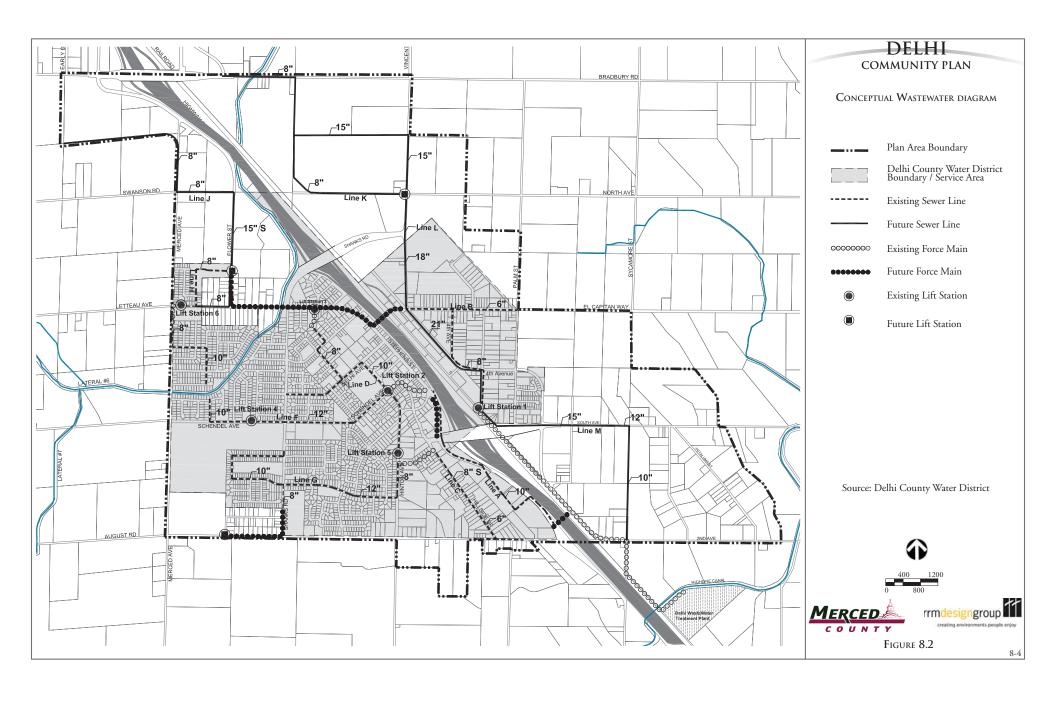
- A new lift station east of Flower Street, north of Letteau Avenue will replace Lift Station 6. Flows from Line H will be re-routed to this new Lift Station. This new lift station will also serve Line "J".
- Due to low ground elevation, a lift station will be needed in the vicinity of Vincent Road and North Avenue. The lift station will connect to Trunk Lines "K" and "L".
- Due to low ground elevation and shallow inverts in the southwest portion of the Plan Area, a lift station will be needed in the vicinity of August Road and Corner Street. This lift station will serve existing residential units development in the vicinity of August Road and Merced Avenue.

The wastewater treatment plant has the capacity to process 0.8 million gallons per day (mgd). The plant currently process approximately 0.56 mgd. The current plant operation has the acreage to expand its operation up to 1.2 mgd. It is estimated that the existing demand plus projected demand would require a treatment plant that can process approximately 2.0 mgd. A new wastewater treatment plant will be necessary to accommodate projected demand. Since the preparation of the plan, the DCWD announced their plant is nearly at capacity with only approximately 150 residential hook-ups remaining, most of which will be use for in-fill within their existing service boundaries.

8.3. Storm Drainage

Existing storm drainage facilities within Delhi is fragmented between new development and old development. Delhi is prone to seasonal flooding due to an inadequate drainage system. The Delhi Community Plan proposes a comprehensive drainage plan to address storm drainage on an area wide basis, rather than by a project by project basis. New development within the Community will address storm drainage during the permit process or with the public facilities plan if a proposed development is located in a master plan area. Storm drainage basins are strongly encouraged to be incorporated with neighborhood or community parks.





8.4. Schools

Delhi is served by the Delhi Unified School District for primary (grades K-5), secondary (grades 6-8), and high school (grades 9-12) (refer to Figure 8.3, Public School Districts). The Merced Community College District provides continuing education for residents of the area from facilities located in Delhi, and in the cities of Merced and Los Banos. (Director Clarification 2007-01, January 23, 2007)

All existing school facilities within the Community are currently operated by the Delhi Unified School District. They include three elementary schools (Schendel, Harmony Ranch, and El Capitan) serving grades K-5, a middle school (Delhi Middle school) serving grades 6-8, and a high school (Delhi High School) serving grades 9-12. Both the middle school and high school are located at the Delhi Educational Park. Merced College continuing education programs are also provided at the Delhi Educational Park. The District also operates one continuation high school, Shattuck Continuation High School.

Even with the addition of the recently opened Harmony Ranch Elementary, Delhi Unified School District is experiencing overcrowding conditions. Additional elementary schools and middle schools will be necessary to adequately serve the Community Plan's projected student enrollment. Four elementary and two middle schools have been sited in areas projected to yield the largest student populations (refer to Figures 2.3 and 8.3).

One of the two future middle school sites will replace the present middle school now housed at the Delhi Educational Park. The relocation of the Delhi Educational Park middle school students is necessary if the Delhi Educational Park is to provide additional classrooms facilities to serve the Community Plan's projected high school enrollment.

The Community Plan recognizes that schools are a high activity area generating vehicle, as well as, pedestrian traffic. Schools will be sited within neighborhoods to promote walkable neighborhoods and create a neighborhood focal point. Criteria for siting new schools in the Community should be based on the following guidelines:

☐ Elementary School—should be planned as an integral part of neighborhoods, located away from arterials, and major and minor collectors. Elementary schools should be located within walking distance (1/2 mile) of the student

population and should be linked to the community trail system serving residential neighborhoods to reduce the need for student transportation services.

- ☐ Middle school—should be located with direct access to at least one minor or major collector. Middle schools should be located within walking distance of the majority of the student population it is to serve and be linked to a community trail system to reduce the need for student transportation services.
- ☐ **High School**—should be located within direct access to at least one major collector.

8.5. Fire Protection

Under Contract with Merced County, the California Department of Forestry (CDF) provides fire, rescue, and emergency medical services in Delhi. Merced County Fire Station 91 is located near the corner of Acacia Street and Hinton Avenue in Downtown Delhi. CDF staffs the fire station with a full-time fire captain, and emergency response is augmented with paid call firefighters. Emergency response time is currently 1 to 5 minutes.

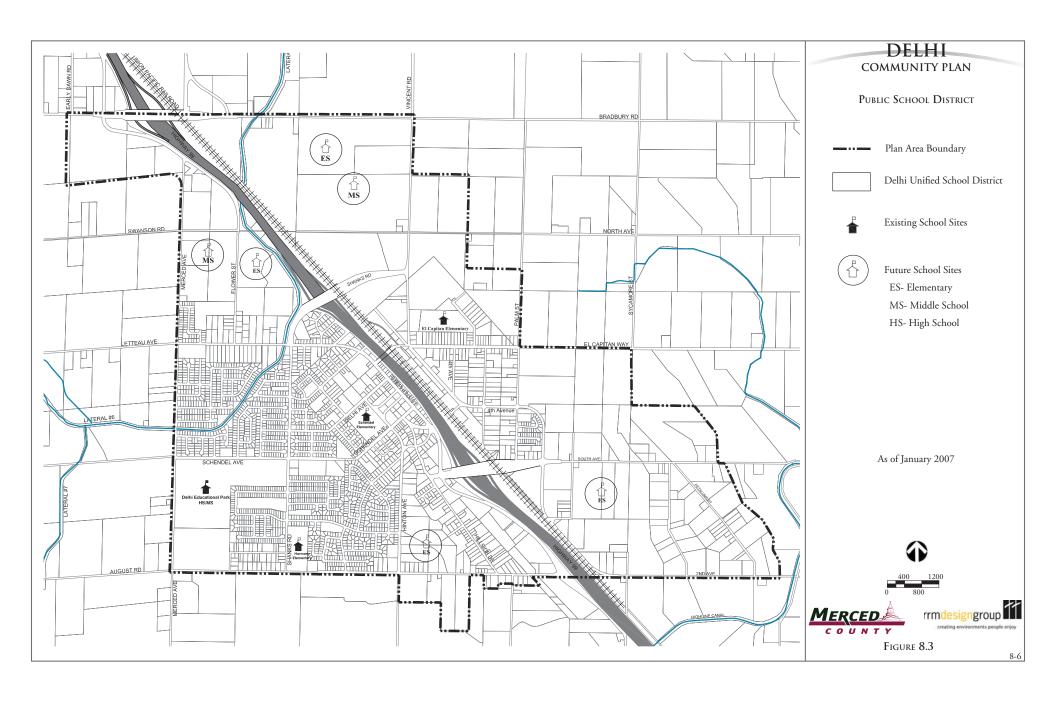
To provide adequate fire protection, the Merced County Fire Department proposes to relocate Station 91 from its current location to a location near August Road and Hinton Avenue. In addition to the relocation of Station 91, a second fire station will be located in the Community to serve the eastern section of the Community (refer to Figure 8.4, Public Safety). Both fire stations will be staffed and equipped to appropriate levels that ensure timely responses to fire and other emergency situations.

8.6. Law Enforcement

Law enforcement is provided by the Merced County Sheriff's Department. Within the County, there are 3 primary stations (Merced, Santa Nella and Livingston) with a total of 7 to 8 patrols on duty at any given time. Other deputies serve in schools and other community functions.

The Merced County Sheriff's North Station is located within the community services building at the corner of Schendel Avenue and Shanks Road. This station serves the needs of community residents (refer to Figure 8.4).





8.7. Goals, Objectives, Policies and Implementation Measures

Goal:

Provide adequate infrastructure (water, sewer, storm drainage) systems to serve Delhi.

Objective:

PS 1.0 Provide water, sewer and storm drainage facilities to serve residents and businesses within the Community.

Policy:

PS 1.1 As development occurs ensure that adequate public services are provided. (EM)

Implementation Measures:

- PS 1.1.a Master plans shall identify the placement and general type of necessary infrastructure (e.g., water, sewer, drainage) improvements. (EM)
- PS 1.1.b Master plans shall include an infrastructure phasing plan to ensure public services are in place as development occurs. (EM)
- PS 1.1.c Development projects shall provide its proportionate share of urban level public services. (EM)
- PS 1.1.d Require preliminary "can and will serve" letter from the Delhi County Water District prior to processing a discretionary application (tentative map, conditional use application or administrative application. (EM)
- PS 1.1.e A comprehensive storm drainage plan shall be developed during the Master Plan stage of each master plan area that identifies the location of a master storm drainage pond, storm drainage trunk lines and any discharge location. (EM)

Objective:

PS 2.0 Ensure public infrastructure is adequately funded.

Policy:

PS 2.1 Development shall finance public facilities if development impacts existing public services. (EM)

Implementation Measures:

- PS 2.1.a Master plans shall include a financial plan to fund necessary public improvements. This plan shall include fees or special assessment district in order to finance all infrastructure improvements. (EM)
- PS 2.1.b Require financing of infrastructure improvements to be the responsibility of development of vacant and undeveloped lands. (EM)

Objective:

PS 3.0 Provide for the educational needs of the Community.

Policy:

PS 3.1 Merced County will work with the Delhi Unified School District and the Turlock Joint Unified School District to help meet the educational demands resulting from growth in the Community Plan. (EM)

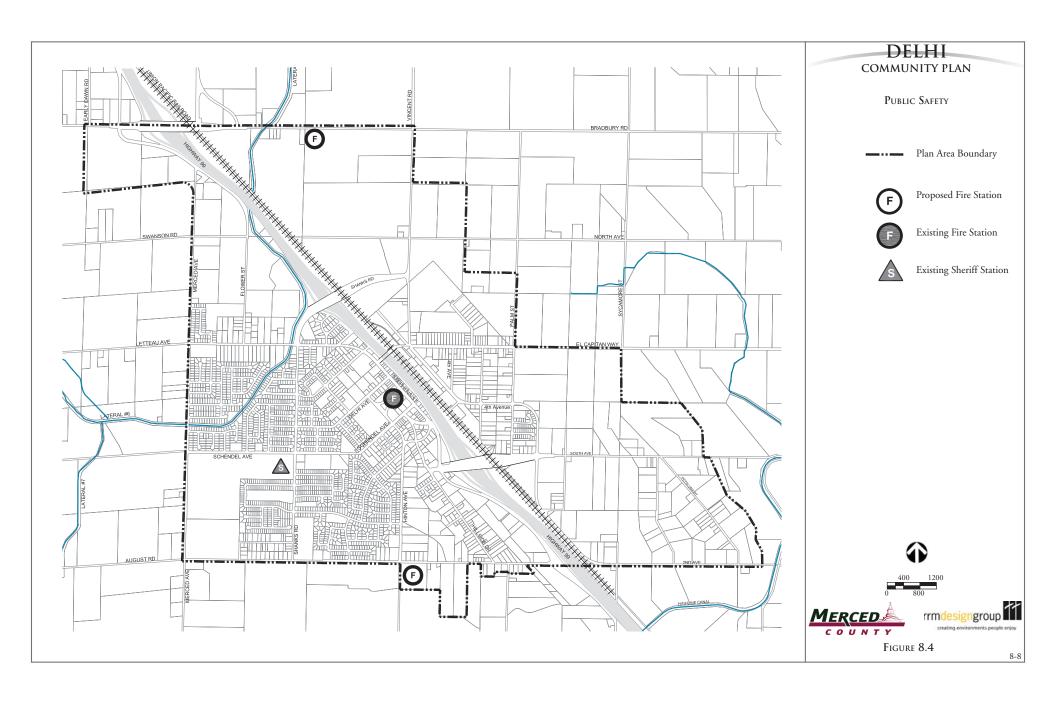
Implementation Measures:

- PS 3.1.a The County will continue to ensure all legally adopted school impact fees are collected at the building permit stage of development. (EM)
- PS 3.1.b The County will coordinate with the school districts and project proponents in siting new school facilities necessary to accommodate increases in student enrollment. (EM)
- PS 3.1.c Delhi Unified School District and Turlock Joint Union School District should work together to realign district boundaries so that residents of Delhi are able to attend the local schools. (EM)

Policy:

PS 3.2 Ensure that adequate new school sites are made available within the Community. (EM)





Implementation Measures:

- PS 3.2.a Prepare development agreements to establish the pro rata share of development impacts within the Delhi Unified School District. (EM)
- PS 3.2.b Coordinate planning, siting, and construction of new schools with the appropriate school district to ensure that facilities are constructed in a timely fashion relative to the phasing of residential development. (EM)
- PS 3.2.c Locate new elementary schools within one-half (½) mile walking distances from residences. (EM)
- PS 3.2.d New elementary schools should always be located on local streets so as to provide access and separation from other uses. However, they should never be adjacent to an arterial, or major or minor collector when student safety has been thoroughly addressed. (EM)
- PS 3.2.e Locate new middle schools to serve the needs of the residents. The new middle schools facilities should be located adjacent to one or more major or minor collector. (EM)
- PS 3.2.f New school sites are encouraged to be built jointly with park sites.

Goal:

Ensure that the public safety needs of residents in Delhi are met.

Objective:

PS 4.0 Protect the residents of Delhi from criminal activity and fire dangers.

Policy:

PS 4.1 The Community should maintain and improve County levels of standards for fire and police protection. (EM)

Implementation Measures:

- PS 4.1.a Establish a fire facilities and law enforcement impact fee to ensure appropriate levels of service are maintained. (EM)
- PS 4.1.b Through the use of the fire facilities and law enforcement impact fee, establish a new fire station within the northeastern quadrant of the Plan Area, or other appropriate location determined by the Merced County Fire Department. (EM)
- PS 4.1.c Through the use of the fire facilities and law enforcement impact fee, ensure that equipment and personal meets standards set by both Merced County Fire Department and Merced County Sheriffs Department to serve Delhi residents. (EM)
- PS 4.1.d Development shall include appropriate security design measures such as increased lighting, low-level landscaping, and easily identifiable access. (EM)



9.0 Implementation

The Delhi Community Plan is an integral chapter of the Merced County General Plan. The Delhi Community Plan is the primary tool for the review and approval process of precise development proposal (e.g., master plans, tentative subdivision maps, site plans, and improvement plans) within the Delhi Community Plan Area Boundary (refer to Figure 1.2).

Prior to the approval of any of the Master Plans; a study shall be prepared to evaluate the fiscal and financial implications of development within the overall Delhi Community Plan. Based on this study, the County may amend the land uses within the Master Plans that may result in subsequent CEQA documentation.

The Planning Director may make revisions to correct typographical errors, misspellings, and format, along with minor word changes for the purpose of clarification that does not alter the intent or purpose of the plan. The Planning Director shall prepare a summary to inform the Planning Commission and Delhi MAC on an annual basis of any changes made by the director.

9.1. Administration

The Merced County Planning and Community Development Department responsible is interpretation of the goals, objectives, policies and implementation measures of the Delhi Community Plan. The goals, objectives, policies and implementation measures shall be utilized in conjunction with the County's General Plan and Zoning Code during the project approval process. Flexibility is permissible only if, the overall goals and vision of the Community Plan are met. Other agencies shall use this Community Plan to approve aspects of projects (e.g., public works to approve street sections, intersection geometrics etc.).

9.2. Community Plan Amendments

A community plan may only be amended four times per calendar year (refer to Chapter 1). Each amendment shall include all sections or elements of the Community Plan that are affected by the change. Amendments shall be processed as a General Plan amendment subject to the same restrictions imposed by state law. Amendments to the Community Plan that create expansion of the Plan Area Boundary that is over 150 acres, or if land use designations are amended by a total of 10 percent of the total acreage of the Community, the County shall complete a comprehensive update of the Community Plan.

9.3. Land Use and Zoning

So as to be consistent with the adopted Delhi Community Plan, there will be a need to amend the Merced County Zoning Code (Title 18) and zoning maps. The County shall review for consistency between the Delhi Community Plan, the Merced County General Plan, and Zoning Code and update the General Plan and Zoning Code to reflect the changes made by adoption of the Community Plan. Specific zoning ordinances reflecting the addition of Mixed Use and Business Park designations shall be developed to allow for those uses (refer to Chapter 3).

9.4. Master Plan Areas

Master Plan areas have been created to facilitate orderly and timely development (refer to Figure 3.2). These plan areas shall require a master plans to be completed prior to any major project approval. Master plans shall be consistent with the Delhi Community Plan's goals, objectives, policies and implementation measures, and contain the elements that are required for a master plan (refer to Table 3.6). Any additional environmental documents deemed necessary by the Merced County Community Planning and Development Department to satisfy the California Environmental Quality Act shall be completed and approved prior to any project approval by the Merced County Planning Commission and Board of Supervisors.



9.5. Projects Outside of Master Plan Areas

With a few exceptions (e.g., master plan areas, non-residential projects), the Community Plan will be developed through the process of subdividing land. Within this process, streets, wastewater lines, water lines and other public facilities improvements are installed, and parks and other amenities are provided for the welfare and enjoyment of residents. The subdivision approval process is the principal means through which Merced County and private developers implement development outside of master plan areas.

9.6. Urban Reserve

At the time the Urban Reserve locations are developed, land use designations shall be specified and a master plan developed (refer to Chapter 3) consistent with the policies of the Delhi Community Plan.

9.7. Public Infrastructure and Services

Merced County shall work with developers to ensure that improvements to infrastructure be made prior to the finalization of permits. Improvements to infrastructure shall be the responsibility of the developer (refer to Chapter 8).

9.8. Fees

New fees shall be established by Merced County to mitigate costs associated with new growth. Other fees shall be updated in order to implement the necessary infrastructure improvements to road right-of-ways and other improvements. The following fees shall be established or updated:

☐ The Delhi Bridge and Thoroughfare Plan Fee to reflect community wide projects that are necessary to implement the Delhi Community Plan.

- ☐ Establish a fire facilities and law enforcement impact fee to ensure appropriate levels of service are maintained.
- ☐ Establish a landscape and lighting district, or other comparable district or financing mechanism to finance services.

9.9. Maintenance

A landscaping and lighting district, or other comparable district approved to by Merced County, will be responsible for the maintenance costs of its internal facilities (e.g., streets, street lights, landscaping, sound walls, storm drainage) plus a share of the maintenance costs of the common facilities, (e.g., neighborhood parks, community parks and trails).

9.10. Comprehensive Community Plan Update

The Delhi Community Plan has a planning horizon of Year 2020. Over time, the Community Plan will need to be updated to prepare for and address growth beyond the Year 2020 and to provide a quality of life, while maintaining a small town atmosphere. The Delhi Community Plan should be updated if:

- ☐ Delhi reaches 18,000 residents;
- 80 percent of the Plan has been built out;
- ☐ The Plan Area expands by over 150 acres; or
- ☐ Land use designation areas have been amended by 10 percent of total acreage.

A comprehensive update should begin by the Year 2015 to ensure that the Delhi Community Plan update is in place by year 2020.

