

SECTION 19

Specific Plan





SECTION 19

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**Prepared for:****CITY OF RANCHO MIRAGE**

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SECTION 19

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1. Summary Statement

1-1

1.1 OVERVIEW

The Section 19 Specific Plan is designed to foster the creation of a new mixed-use destination for the City of Rancho Mirage and the broader Coachella Valley. Situated at the northern edge of the City, adjacent to the Agua Caliente Casino · Resort · Spa and Interstate 10, Section 19 represents an opportunity to bring a different style of living, working, and shopping to the desert. Departing from the low-scale resort-oriented development, the Section 19 Specific Plan will introduce a high density mix of commercial, office, entertainment, hotel, and residential uses in the context of a master-planned Town Center.

The unique blend of land uses proposed under this Specific Plan will allow for development of up to approximately 3,096,500 square feet of commercial, retail, office, restaurant, and entertainment uses, as well as up to 1,899 residential units and 580 hotel rooms. The creation of a mixed-use development enables residents to live within walking distance of stores, restaurants, and recreational areas, while visitors can walk and shop along the esplanade and enjoy an assortment of public plazas and parks. The comprehensively planned commercial and entertainment development will generate a new tax base, a regional destination, and a City landmark.

The increased intensity of development allowed under this Specific Plan will enable developments to incorporate additional open space areas and recreational amenities. Public plazas and pocket parks will be linked with pedestrian walkways through retail and residential areas, connecting open space areas and allowing future residents and visitors to travel within the site without relying upon their cars. Land has also been set aside for a future multi-modal station to offer long-term expansion of transportation choices for residents and employees.

This specific plan will provide the guidance necessary for a cohesive Section 19 project, so that development will be able to proceed for individual properties while allowing for other parts of the site to grow and develop in an organized manner. Coordinated planning efforts have brought together the City and property owners, including the Agua Caliente Band of Cahuilla Indians and

the Coachella Valley Association of Governments, to produce an integrated and unified vision for the Section 19 area.

1.2 SPECIFIC PLAN OBJECTIVES

The overall purpose of this Specific Plan is to generate momentum and provide comprehensive direction for the development of the Section 19 project site, while implementing the goals and policies of the City's General Plan. The project is guided by several major objectives:

- 1) Establish a unified vision for the project site to guide a cohesive, complementary mix of uses structured around a comprehensive set of circulation and infrastructure systems.
- 2) Create a new mixed-use regional destination development that stimulates a major new source of tax base for the City of Rancho Mirage.
- 3) Plan for an appropriate mix of commercial, office, entertainment, hotel and residential uses, within the context of a master-planned Town Center, to meet the trade area's growing demand and build in the flexibility to respond to changes in the market.
- 4) Apply innovative planning and design solutions to create a sense of place at multiple scales.
- 5) Provide new housing concepts for the community, encouraging high quality, high density residential units that appeal to residents seeking shorter commutes to jobs, restaurants, and entertainment opportunities.
- 6) Take advantage of the area's location and exposure by establishing a welcoming gateway to the City, characterized by distinct and attractive signage, architecture, and landscaping, both on-site and in the public right-of-way.
- 7) Implement a circulation concept that optimizes circulation for both vehicular and pedestrian traffic, internalizes pedestrian activity to buffer it from the vehicular traffic along perimeter roadways, and establishes connectivity between uses infused with pedestrian-friendly and walkable spaces.



2. Introduction

2.1 AUTHORITY

The City of Rancho Mirage initiated and prepared the Section 19 Specific Plan pursuant to the provisions of California Government Code, Title 7, Division 1, Chapter 3, Article 8 (Sections 65450 through 65457). The law allows for the preparation of specific plans as may be required for the implementation of the general plan. Specific plans act as a bridge between the general plan and individual development proposals. They combine development standards and guidelines, capital improvement programs, and financing methods into a single document that is tailored to meet the needs of a specific area. Jurisdictions may adopt specific plans by resolution or ordinance.

Upon adoption, this Specific Plan will serve as zoning for the properties involved. It establishes the necessary plans, development standards, regulations, infrastructure requirements, design guidelines, and implementation programs on which subsequent project-related development activities are to be founded. It is intended that local public works projects, design review plans, detailed site plans, grading and building permits, or any other action requiring ministerial or discretionary approval applicable to this area be consistent with this Specific Plan.

2.1.1 AGUA CALIENTE BAND OF CAHUILLA INDIANS

The Section 19 Specific Plan contains approximately 19 acres of land under control of the Agua Caliente Band of Cahuilla Indians (Tribe) as tribal trust land (see Figure 4 in Section 3). Tribal trust land, unlike allotted trust land, is not subject to the land use contract forged between the City of Rancho Mirage and the Tribe on June 22, 1998. This land use contract identifies the powers and authority of the City and Tribe over allotted trust lands within the Agua Caliente Indian Reservation boundaries. No lands within the Specific Plan are identified as allotted trust lands. Accordingly, the Section 19 Specific Plan does not legally regulate tribal trust land; however, in practice, the Tribe and the City coordinate planning efforts on all lands to assure successful development for both entities. The Specific Plan and associated Environmental Impact Report have been designed to accommodate



the development plans provided by the Tribe on tribal trust land within the Specific Plan (see Planning Area 1.06 in Section 4).

An additional three acres (beyond the Tribe's 19 acres) is identified as fee land. This land is former allotted trust land that has been sold to a buyer, who holds fee title to the land. Although the fee land remains within the boundaries of the reservation, it can be owned by anyone and is subject to the same development standards and requirements as non-Indian land. The Specific Plan will serve as zoning for properties identified as fee land.

2.2 PURPOSE AND INTENT

The Section 19 Specific Plan provides a new vision for the northern area of Rancho Mirage known as Section 19. The City created the Specific Plan to transform this 268.6-acre area into a special place for the residents and property owners of Rancho Mirage. What is now a collection of vacant parcels can grow into an oasis of commercial, resort hotel, and mixed-use residential activity. The provisions of this Specific Plan will foster the creation of a recognizable, landmark development that appeals to the City's population, expands the City's employment opportunities and tax base, stands out from other development along Interstate 10 (I-10), and provides unique gathering spaces for the community.

The creation of a Specific Plan establishes a unified vision for the project site with the goal of applying innovative planning and design solutions to create a sense of place at all scales. The Specific Plan will create land uses, circulation design, site development standards, and design guidelines that promote the site's long-term success and enhance the character of the site and adjacent land uses. This document will provide for a scale and mix of uses for the most efficient use of the existing parcels, taking into consideration size and shape, proximity to existing and adjacent uses, and the potential for incremental development on the site.

The Section 19 Specific Plan provides for a potential mix of 3,096,500 square feet of commercial, retail, office, restaurant, and entertainment uses as well as up to 1,899 residential units and 580 hotel rooms.

2.3 SITE LOCATION

2.3.1 REGIONAL SETTING

The City of Rancho Mirage is in the heart of the Coachella Valley in Riverside County, nestled at the base of the Santa Rosa Mountains and conveniently located to utilize the southern California freeway system via I-10. Incorporated in 1973, Rancho Mirage offers an abundant amount of sunshine, great climate, and resort lifestyle. Famous political leaders, corporate executives, and top entertainers all call Rancho Mirage home because of its renowned quality of life. Primarily a residential community from the beginning, later development saw the addition of world-class destination resorts and prime retail, restaurant, and office development along Highway 111. The majority of future development, including that under this Specific Plan, is expected to occur near I-10. The location of the project site in its regional setting is illustrated in Figure 1.



2.3.2 LOCAL SETTING

The proposed project consists of the development of a specific plan for an approximately 269-acre area of the City of Rancho Mirage located in sections 19 and 24. The Section 19 Specific Plan is located at the northeast edge of the City of Rancho Mirage and is bordered by Bob Hope Drive to the west, Dinah Shore Drive to the south, the I-10 and Union Pacific Railroad (UPRR) corridor to the north, and undeveloped land in the City of Palm Desert to the east. With the majority of land located in section 19 (247 acres), the specific plan adopted the name Section 19 Specific Plan. The location of the project site within the City boundaries is illustrated in Figure 1.

Approximately 19 acres of land on the northwest side of the project area are tribal lands owned in trust by the Tribe. In geographic terms, the Section 19 Specific Plan area is much like a “peninsula” to the remainder of the city to the south, and is expected to become the northern primary gateway to the community of Rancho Mirage.

An aerial photograph of the project site vicinity is provided in Figure 2 and a public land survey showing the boundaries of the survey section areas is illustrated in Figure 3. Residential and commercial development is present in the immediate vicinity of the project area, but large areas of undeveloped land remain, including lands that adjoin the site to the east and west. The I-10 and UPRR corridor border the site to the north. Limited commercial development has occurred directly north of this corridor and east from the site, with low density residential housing emerging along the southern site boundary.

The Agua Caliente Casino · Resort · Spa occupies a 36-acre parcel that adjoins the 19 acres of land within the northwest corner of the project area owned in trust by the Tribe. The facility, which originally opened in 2001 and underwent expansion in 2006, includes a 340-room hotel; 70,000 square feet of gaming floor; a 2,000-seat showroom; 13,000 square feet of flexible meeting space; six distinct dining venues; a resort pool, spa, and fitness center; and 2,500 parking spaces.

Figure 2 also shows future projects surrounding the project area, such as office and commercial development along Bob Hope Drive, south of Dinah Shore Drive. Additional development on tribal trust land and allotted trust lands include a low density resort residential community and a 400,000-square-foot lifestyle retail center west of Bob Hope Drive and south of Ramon Road.

2.4 PROJECT BACKGROUND

The Section 19 area was identified within the 2005 Rancho Mirage General Plan Update as a significant opportunity site for new economic development within a largely built-out community. The initial development objectives for this area included an emphasis on attracting large retailers to the community within a planned commercial district context.

The preliminary work on the Section 19 Specific Plan responded to this direction and included a one-day interactive developers’ forum involving seven major private developers active throughout southern California and notable commercial realtors in the Coachella Valley. A quantitative market analysis of the site was presented at the meeting and the participants identified the most feasible alternatives for viable economic development within Section 19. The input from developers, realtors



and market analysts consistently reinforced the following strategic recommendations for structuring the Section 19 Specific Plan:

- 1) The project’s approach should emphasize a creative mixed-use development program for a “town center” that would include a dynamic combination of community retail shops and boutiques, high-end thematic restaurants, professional office space, mid- and high-density residential neighborhoods, resort and business hotels, furniture and furnishings, designer outlets, and resort recreational uses.
- 2) The adjacent Agua Caliente Casino · Resort · Spa development should be viewed as an economic engine for Section 19 and could function as a non-retail anchor for a “Lifestyle Center” development strategy for the area and could stimulate other complementary uses such as other destination resort and recreational facilities.
- 3) The project should consider the two significant challenges for the successful development of Section 19: 1) the nearby Monterey Avenue corridor in adjacent Palm Desert, which has absorbed nearly all of the big box retail uses feasible for this location; and 2) the lack of nearby residential rooftops around Section 19, which may not be sufficient for the first phase of retail uses, indicating a need to develop residential uses within Section 19 as part of the overall mix.

Accordingly, the consensus derived from major developers, realtors, and the City provided the basis for a refined economic development strategy for Section 19. This strategy is reflected throughout this Specific Plan and endows Section 19 with a diversified mix of high quality land use activities and building types that will generate market-based economic success while creating an attractive northern gateway for Rancho Mirage.

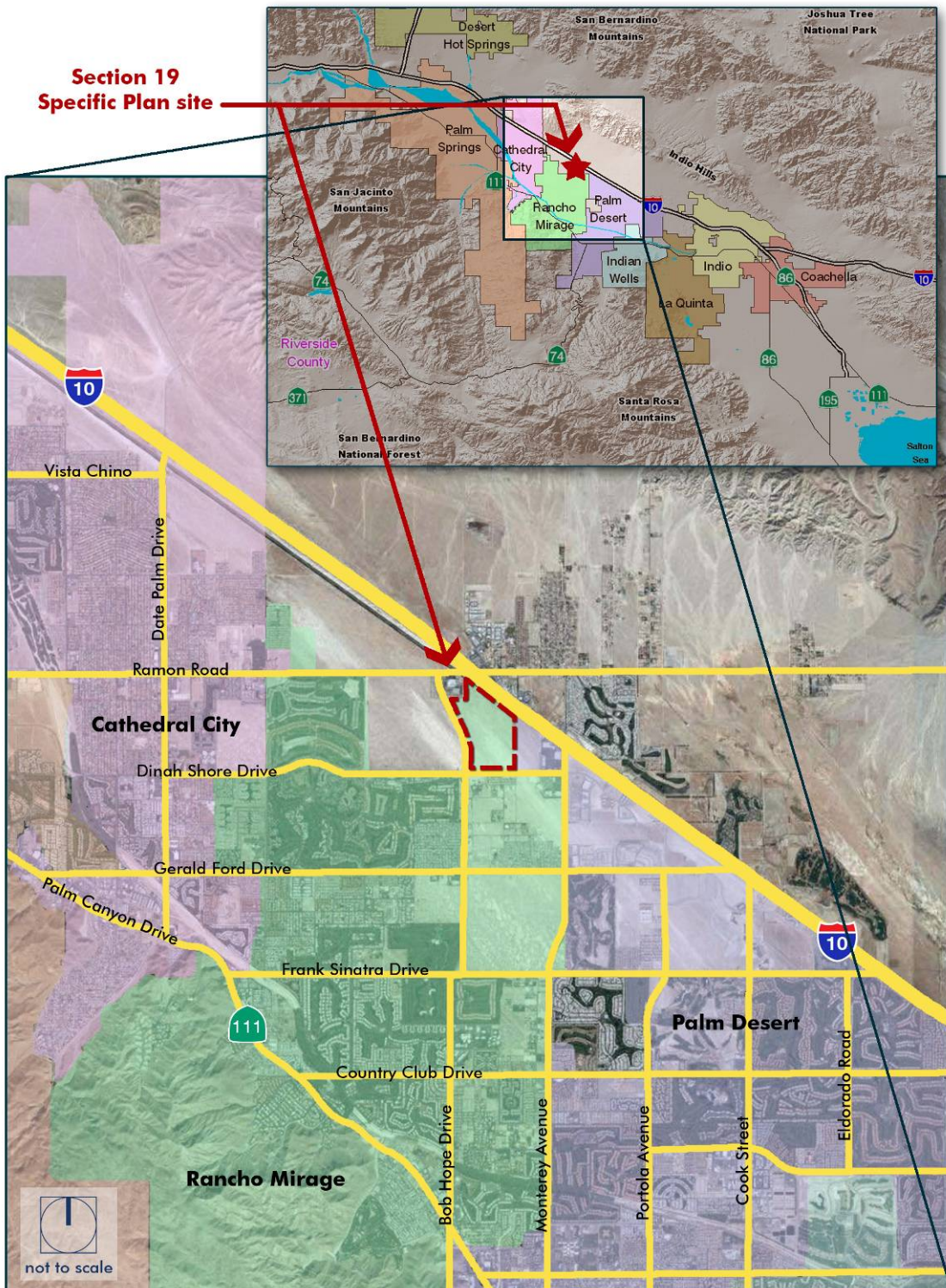


Figure 1 Regional and Local Setting

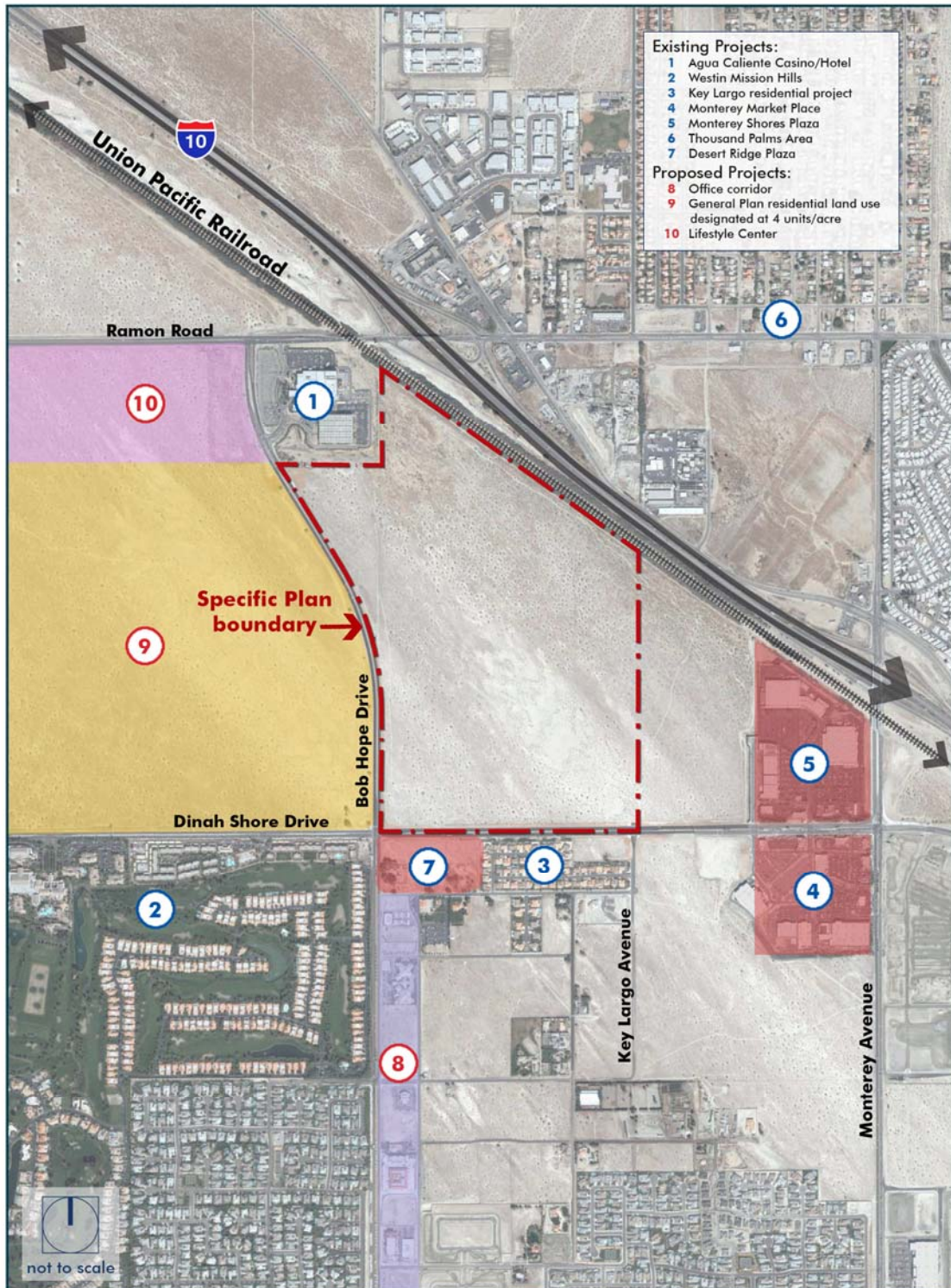


Figure 2 Aerial Photograph and Project Vicinity

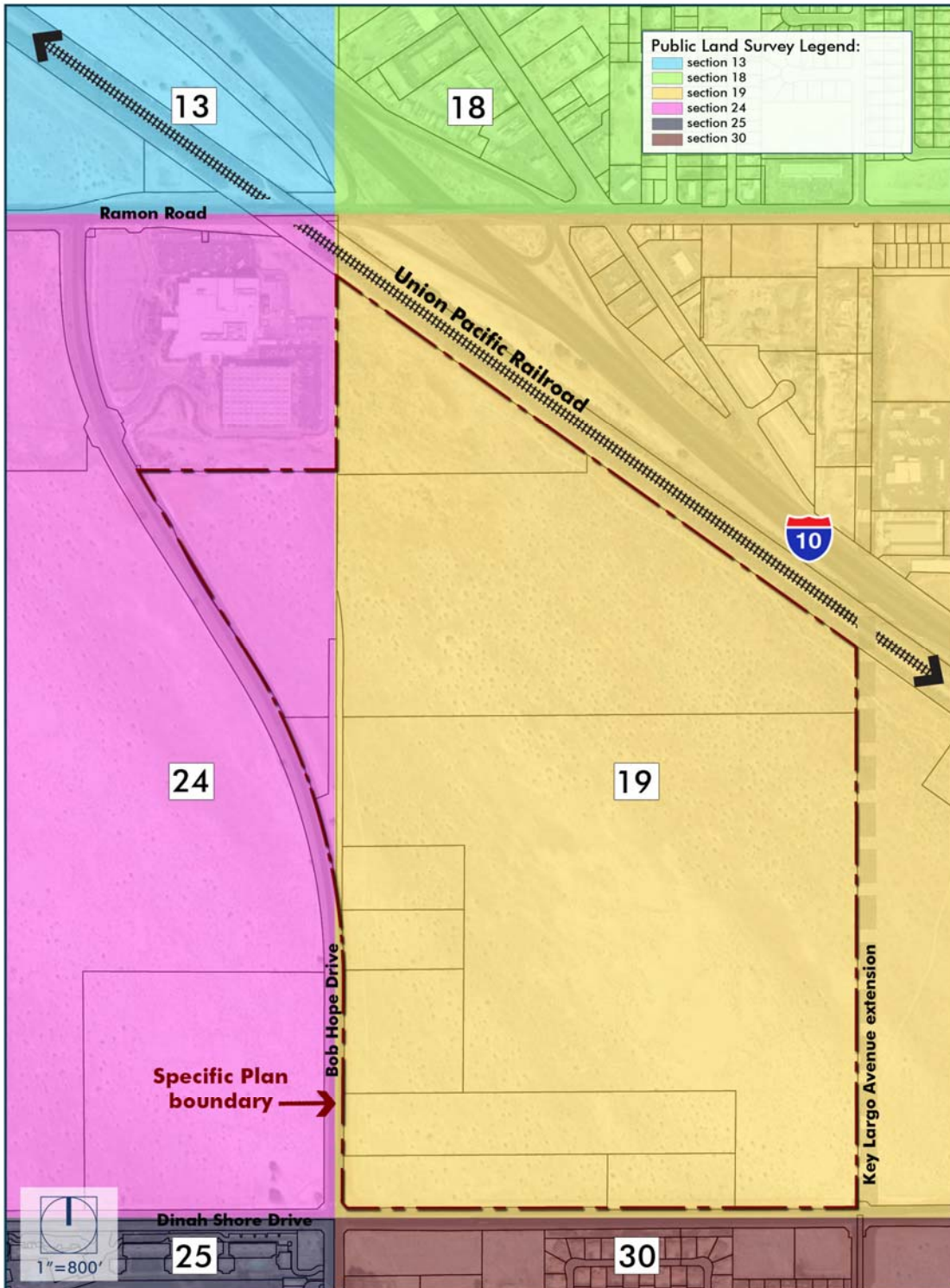


Figure 3 Public Land Survey



2.5 ENVIRONMENTAL ASSESSMENT

The Environmental Impact Report (EIR) serves primarily as a source of environmental information for the City of Rancho Mirage, the lead agency for the project. The EIR describes the potential impacts of the adoption of the Section 19 Specific Plan. Subsequent development projects within the Section 19 Specific Plan are anticipated as the Specific Plan builds out. The EIR has been prepared as a program EIR, as defined by Section 15168 of the CEQA Guidelines, and subsequent projects that are within the scope of this EIR may be subject to a more limited environmental review process, as determined by the Director of Community Development of the City of Rancho Mirage.

Tribes are federally connected entities within the territory of states and are not generally subject to state jurisdiction. Federal Public Law 280, adopted in 1953, gave California, four other states, and the territory of Alaska jurisdiction over criminal and some civil matters on Tribal lands. However, the delegation of authority did not include regulatory power over environmental, land use, or most other governmental matters. Thus, state environmental laws and regulations, including CEQA, do not apply to the portion of the project site that is tribal trust land. Nevertheless, potential environmental impacts associated with tribal trust lands are addressed in the EIR. Future project-specific development on tribal trust lands will be subject to an environmental review process administered by the Tribe and will not be subject to discretionary approvals or permitting by the City of Rancho Mirage.



3. Context and Conditions

3.1 EXISTING SITE CONDITIONS

3.1.1 PROPERTY CONFIGURATION AND EXISTING USES

The Section 19 Plan project site is currently divided into 11 parcels, ranging from 1.4 to 112.8 acres. The majority of parcels are owned by separate individuals, with one parcel owned by the Coachella Valley Association of Governments (CVAG) and one parcel owned by the Agua Caliente Band of Cahuilla Indians (Tribe). Additionally, excess right-of-way land left over from the abandoned Rio Del Sol Road totals approximately 2.4 acres and remains under the City's control.

A map of the existing parcels is provided in Figure 4. Historical aerials are provided in Figure 5 to illustrate the abandonment of Rio Del Sol Road, the transition to Bob Hope Drive, and the excess right-of-way area. As both figures show, the project site has never been developed and currently consists of vacant land with native desert vegetation.

3.1.2 CIRCULATION

The project site is surrounded by the Union Pacific Railroad (UPRR) and Interstate 10 (I-10) to the north; major arterials Dinah Shore Drive and Bob Hope Drive to the south and west, respectively; and Key Largo Avenue to the east, as a future minor arterial and flyover across I-10. Freeway access is currently obtained through the Ramon Road interchange. With no development east or west of the site, the existing circulation system consists of only three perimeter roadways and access roadways into the Agua Caliente Casino · Resort · Spa.

Bob Hope Drive has recently been improved to its full configuration as a six-lane divided Major Arterial with a 120-foot right-of-way. As no development, with the exception of the Agua Caliente Casino · Resort · Spa, currently exists on either side of Bob Hope Drive north of Dinah Shore Drive, sidewalks and parkways will need to be constructed concurrently with new development.



Dinah Shore Drive is also designated a Major Arterial and has already been fully improved to operate as a six-lane divided roadway. As Section 19 builds out, the sidewalk and parkway will be fully constructed on the north side of Dinah Shore Drive. Key Largo Avenue currently exists only south of Dinah Shore Drive as a two-lane road. Key Largo Avenue is designated as a Minor Arterial and will eventually expand to a four-lane divided roadway that consumes a 110-foot right-of-way and travels north of Dinah Shore Drive to the northern boundary of the project. Figure 4 displays the existing roadway configuration surrounding the site. A possible extension of Key Largo Avenue over I-10 and a new freeway interchange at Bob Hope Drive are discussed in Section 3.3.

3.1.3 TOPOGRAPHY

Topographically, the site generally slopes downward to the northeast. Surface elevation of the subject property ranges between approximately 210 to 340 feet above mean sea level, with the highest point located at the southwest corner of the project site. Figure 6 illustrates the existing topography of the site.

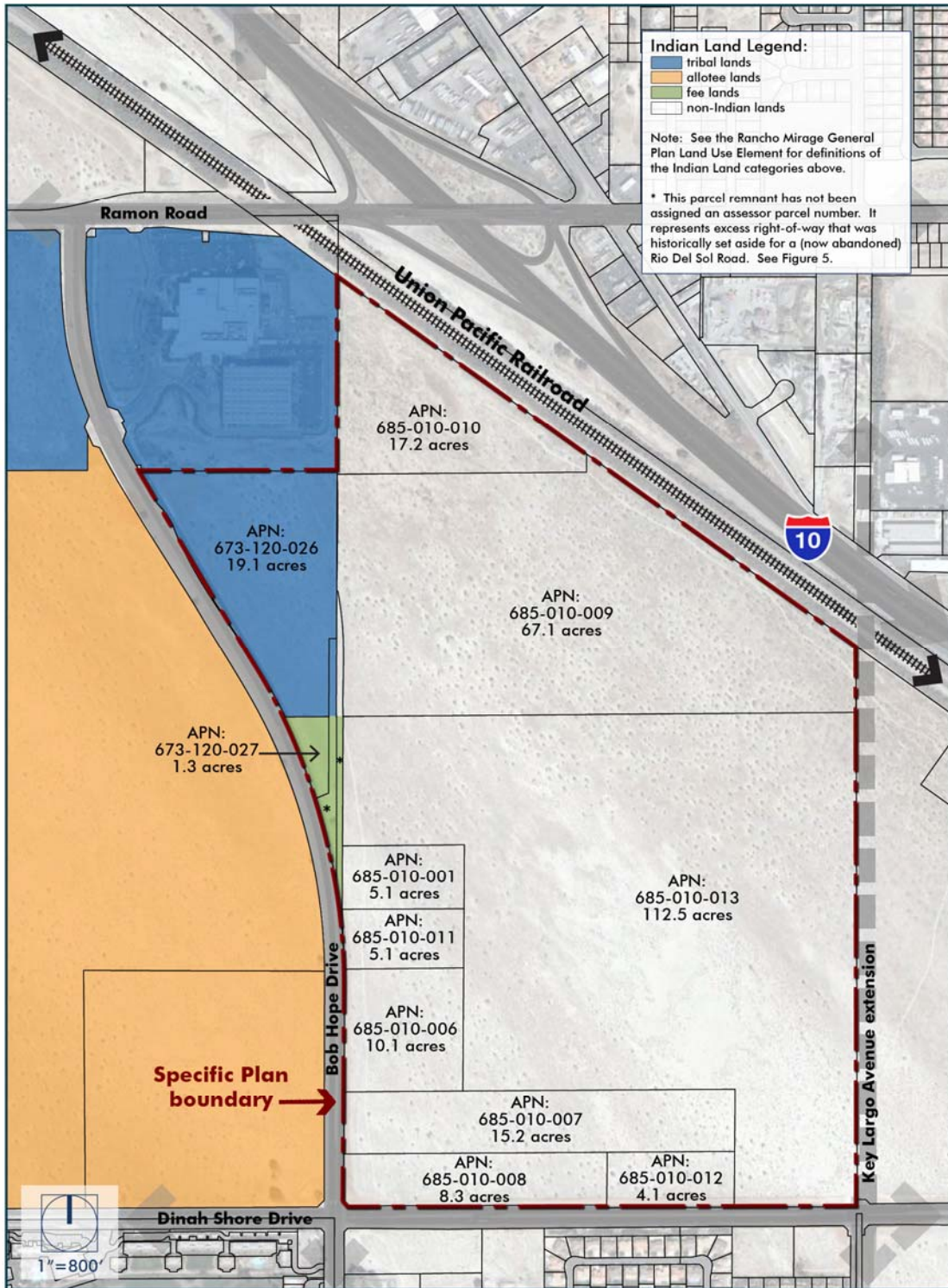


Figure 4 Property Configuration

Note: Acreage figures shown are approximate.

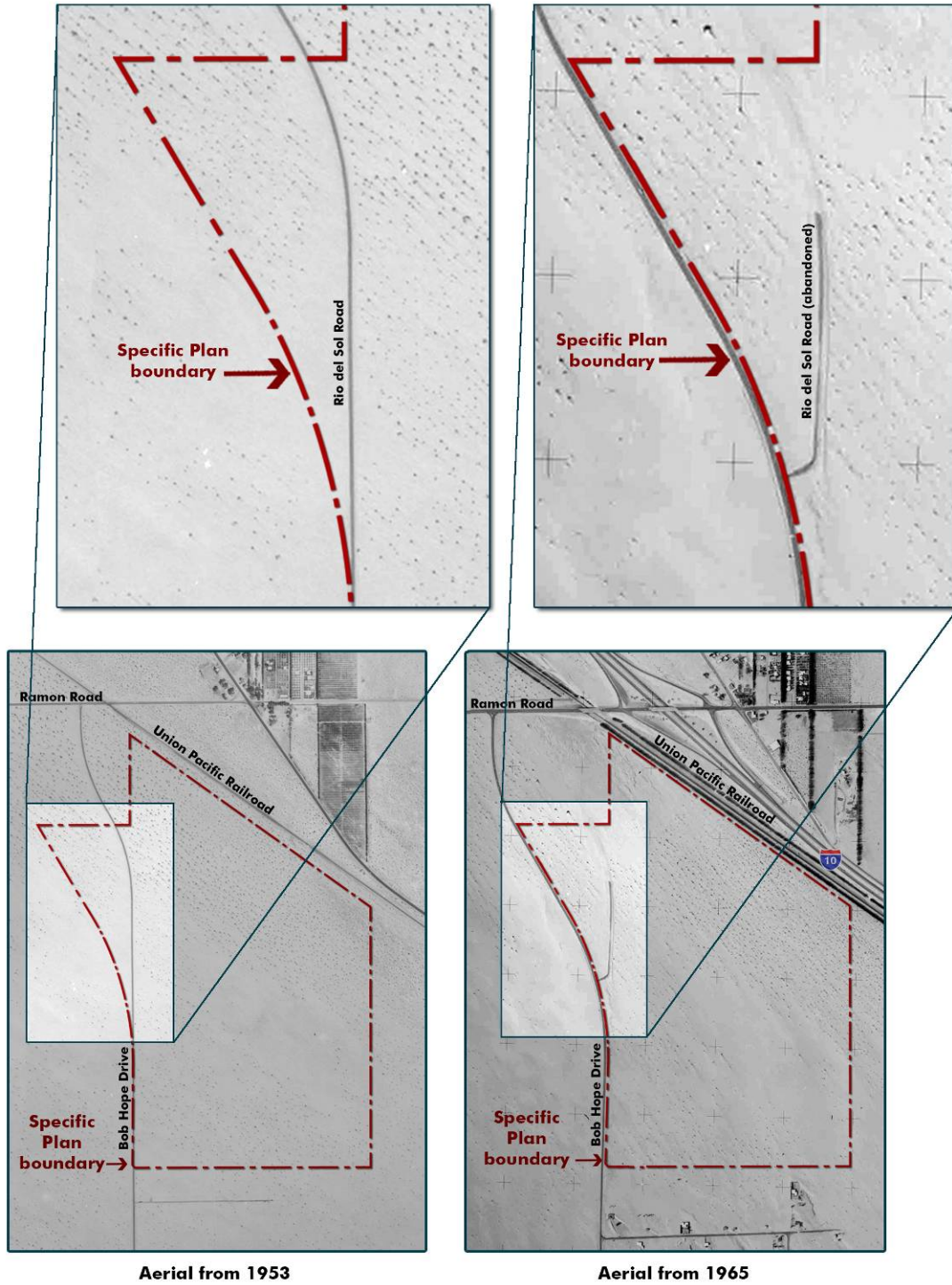


Figure 5 Historical Aerials Illustrating Abandonment of Rio Del Sol Road

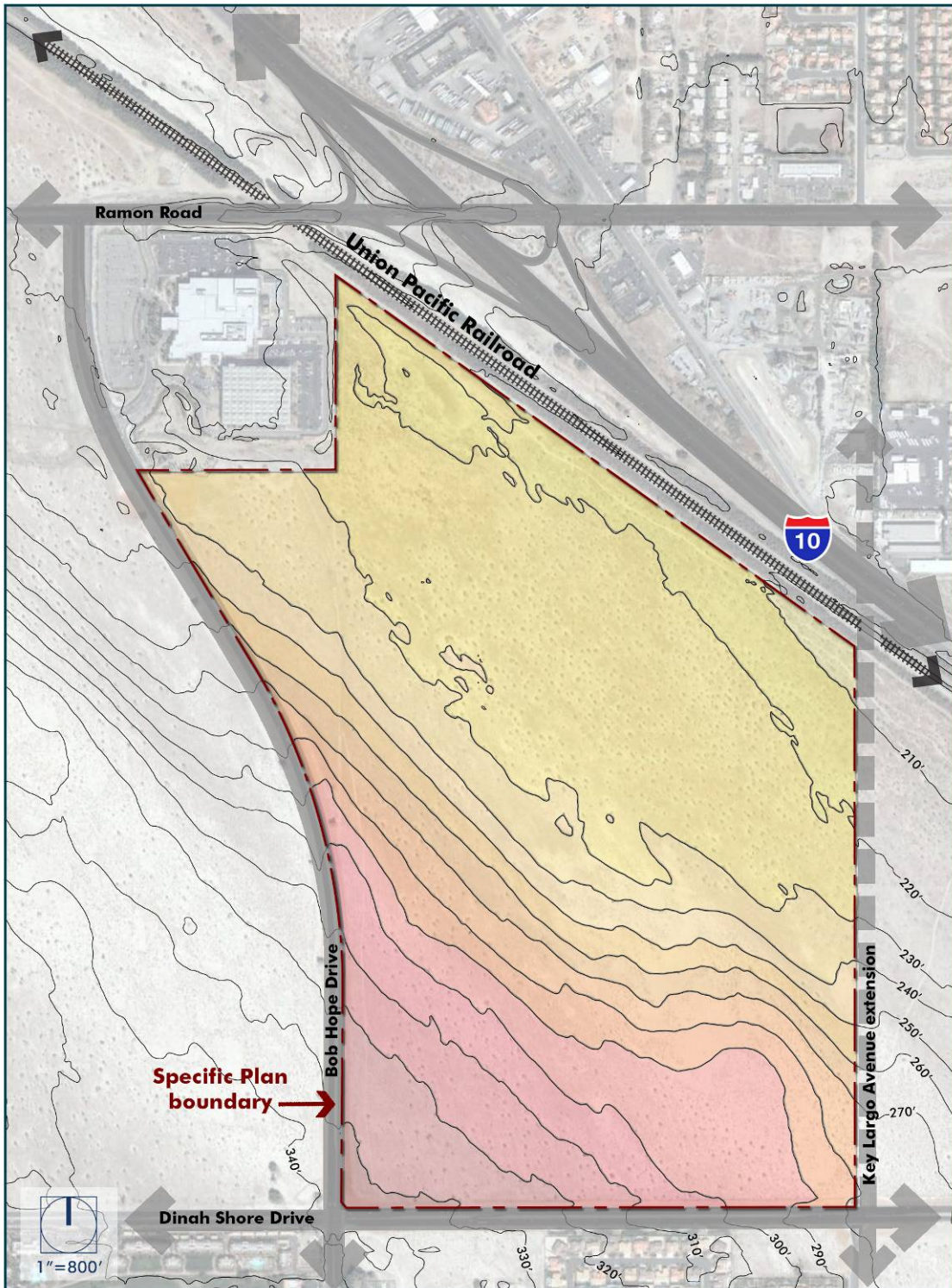


Figure 6 Existing Topography



3.2 GENERAL PLAN AND ZONING DESIGNATIONS

The City of Rancho Mirage employs a “single map” system of land uses. This means that the City’s General Plan land use designations are the same as its zoning designations. Also, the density and intensity standards expressed in the General Plan are the same as those expressed in the City’s Zoning Ordinance. Existing General Plan land use designations are shown in Figure 7. Most of the 268.6-acre project site is designated for Community Commercial (C-C) use in the Land Use Element of the City’s General Plan.

This designation provides for regional or community-scale shopping centers and malls. These centers may be anchored by several department stores or other large-scale anchors as well as a variety of retail outlets and restaurant and entertainment uses. Hotels and motels may also be appropriate on these lands. The community commercial center is intended to serve the entire community as well as the surrounding market area.

The portion of the site on tribal trust lands is designated Resort Hotel (Rs-H), reflecting its proximity to the Agua Caliente Casino · Resort · Spa directly off-site to the north. This land use designation allows for the development of hotels and destination resorts with limited ancillary commercial uses, such as spas, recreational facilities, restaurants, lounges, and small retail shops that directly support the primary use.

The project area has been designated the Section 19 Specific Plan Overlay in the Rancho Mirage General Plan since the City annexed this area in 1994. Additionally, the General Plan applies a Special Corner Overlay zone on approximately 40 acres of the site at the southwest corner of the project, at the intersection of Dinah Shore Drive and Bob Hope Drive, where aesthetics and land use compatibility are of primary importance.

Zoning and general plan amendments are proposed as part of the Section 19 Specific Plan. The change requested is from Community Commercial and Resort Hotel to Specific Plan to allow for the more intense development envisioned by the Specific Plan. Additionally, the development regulations and design standards of the Specific Plan will supplant the Special Corner Overlay zone requirements.

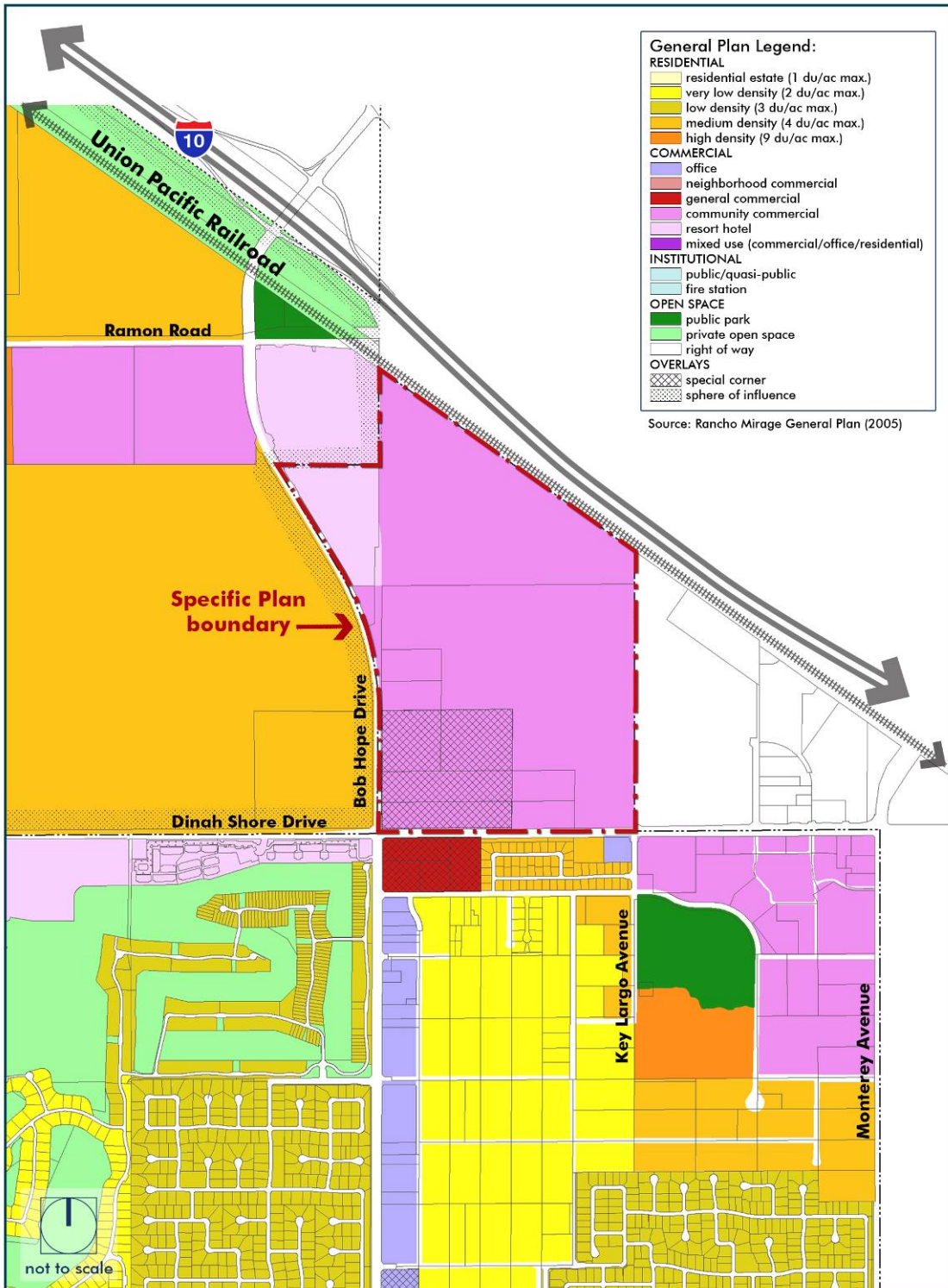


Figure 7 Existing General Plan Designations



3.3 OPPORTUNITIES AND CONSTRAINTS

A variety of factors must be considered to maximize the development potential of the project. The significant opportunities and constraints that affect the Section 19 Specific Plan relate to the regional context and location of the project, as well as the site's direct relationship to the adjacent uses, surrounding streets, the channel to the northeast, and the railroad easement along the northeastern edge. Additional issues include the appropriate amount and types of commercial and residential development, the number of property owners on the site, existing circulation patterns, and environmental issues.

3.3.1 TOPOGRAPHY

The topography of Section 19 presents a significant challenge to the access and circulation of the site and yet provides the opportunity to accommodate the higher densities proposed in this Specific Plan without disrupting the character of the City. As shown in Figure 6, the site slopes downward from the southwestern corner to the northeastern corner, with a cross-fall of approximately 140 feet. The slope is greatest in the center of the site while the northern portion is relatively flat. The direction and gradient of the site's slope creates potential problems with external and internal circulation and access to central portions of the site. A roadway system that follows the natural contours would limit access to parcels in the center of the site, divide parcels into multiple pieces, and make efficient intersections with the perimeter roadways difficult. This could compromise the potential of the site for economic development. Thus, to minimize these potential problems, a continuation of the existing grid was utilized in the creation of the land use plan. The grid roadway pattern also enhances the urban character of the project and facilitates the creation of valuable views as a result of stepped development pads.

The topography also creates the opportunity to utilize higher buildings and greater densities than typically found in the City. Section 19 is in the northeastern portion of the City, with the majority of existing development to the south and west. The downward slope of the site would reduce the visual impact of taller buildings because they would appear shorter when viewed from other parts of the City. Thus, the site's slope will allow for the construction of up to eight-story buildings in the northern portion of the site without changing the visual appearance and character of the rest of the City.

3.3.2 PROPERTY OWNERSHIP

The Specific Plan area comprises 11 parcels owned by multiple entities, including CVAG, the Tribe, the City of Rancho Mirage, and private landowners. Without a unified vision and development plan for the area, the full development potential of the area may not be reached. Thus, one of the primary challenges of this project is to create a plan that will result in a cohesive identity and sense of place for the project area while allowing each owner to maximize the potential of individual parcels. The plan attempts to do so by providing a set of development standards and design guidelines to set standards for the area and allow for relative flexibility in the uses of the parcels. In addition, the plan maximizes the development potential of the area by anticipating and accommodating the phasing of specific parcels (see the Phasing Plan in Section 8.5).



One of the primary focal points of the project will be the parcel owned by CVAG. This northernmost parcel adjacent to I-10 has been set aside for future use as a potential multimodal transit center. However, the transit center, if developed, may not be constructed for another 10 to 20 years. The site may be used for other interim or permanent uses such as parking or renewable energy generation. Thus, the plan must provide adjacent uses that would facilitate the future development of such uses but not rely upon them in the immediate future. Therefore, the plan locates mixed-use core and regional mixed uses adjacent to the parcel. These uses can intensify over time, eventually providing the population density appropriate for a multimodal transit stop. Prior to the development of the parcel, the CVAG parcel could be used for overflow parking for the casino located adjacent to the northwestern corner of the project area.

Another focal point of the Specific Plan area is the parcel south of the casino. Due to its location and ownership status, this parcel is intended for resort and casino-related uses. The casino and related resort uses have the potential to be a major regional draw and present the opportunity for synergies with other uses within the Specific Plan area. Specifically, this parcel could tie the casino together with the mixed-use core of the Specific Plan area to create a vibrant regional node.

The parcel is tribal trust land, which remains under the sovereign authority of the Tribe. Unlike allotted trust lands, which under the June 22, 1998, land use contract between the City of Rancho Mirage and the Tribe are subject to the City's development standards and land use requirements, tribal trust lands are not subject to the City's regulations. Thus, the Tribe has final say over the uses and standards of tribal trust lands within the City. Nonetheless, the Tribe has been involved in the creation of this Specific Plan and is interested in working with the City to create continuity and connectivity between the parcel and other portions of the Specific Plan area.

A one-acre fee land parcel is located along Bob Hope Drive, south of the tribal trust land parcel. This fee land was first allocated to an individual member of the Tribe (allotted trust land) and then sold to a buyer who holds fee title to the land. A large portion of the parcel was dedicated to the City for the historical right-of-way of Rio Del Sol Road. Vacation of the easement by the City would allow the entire parcel to be developed.

The remaining parcels within the Specific Plan area are owned by private landowners, developers, and the City of Rancho Mirage. These parcels vary in size and shape, with smaller parcels typically found along Bob Hope Drive and Dinah Shore Drive. The land use plan was designed to respect the ownership lines to the greatest extent possible to facilitate the development of the properties.

3.3.3 CIRCULATION

Interstate 10 Interchange

I-10 is a major east–west freeway serving interregional traffic movements between metropolitan areas, including the rapidly growing Coachella Valley desert communities. Commercial and residential development plans for the Coachella Valley have projected that additional growth over the next decade will result in a substantial increase in intraregional, interregional, and commuter traffic, with a corresponding increase in congestion.



Due to the projected growth and congestion of the local area, the County of Riverside, CVAG, and the California Department of Transportation have identified several interchange improvement projects within the I-10 corridor that would improve operating conditions, enhance safety, and increase the capacity of these facilities. The interchange improvements are an integral part of the overall transportation improvement program for the Coachella Valley.

In the vicinity of the proposed Bob Hope Drive interchange, I-10 is an eight-lane divided freeway. The proposed project would construct a new spread diamond interchange with Bob Hope Drive, 0.4 mile west of the existing Ramon Road interchange. This would be a new six-lane overpass above the UPRR and an eight-lane overpass above I-10. The existing Ramon Road eastbound on-ramp would remain operational while the other four ramps at Ramon Road would be removed. As a “Gateway Interchange” to Rancho Mirage, all improvements, including aesthetic treatment, landscaping, and restoration of natural areas will be coordinated with the Tribe. This project was programmed in the 2006 State Transportation Improvement Program (STIP) for \$30,373,000 in construction and \$8,629,000 in right-of-way cost for delivery in FY 2007–08. Additional funding needs for the project will be proposed as an amendment to the 2006 STIP. The current project cost for construction capital and right-of-way capital is estimated at \$66.7 million.

Key Largo Overcrossing

The General Plan Circulation Element includes conceptual improvements to maintain desired levels of service in the City at buildout. The Key Largo overcrossing over I-10 between Monterey Avenue and Bob Hope Drive is proposed to connect with Varner Road and/or Ramon Road to relieve potential congestion at the intersection of Monterey Avenue at Dinah Shore Drive and in the vicinity of the Monterey Avenue/I-10 interchange.

The Key Largo overcrossing is mostly outside the City’s jurisdictional boundary and, therefore, implementation is not wholly within the City’s control. Through coordination with the County of Riverside and the City of Palm Desert, the City of Rancho Mirage will determine when and if a Key Largo overcrossing over I-10 is required.

3.3.4 MARKET

The Specific Plan aims to identify the most appropriate mix of land uses to create a unique, high quality place with successful residential and non-residential components. Two focused studies were performed to assess the demand for retail and office development within the Section 19 Specific Plan area. Both of these studies support long-term potential for retail and office development. This, along with the demand for residential uses, calls for flexibility in accommodating market demands for both commercial and residential uses at this location. The strategic integration and positioning of these different uses contribute to the establishment of a unique destination for residents and visitors.

Retail Analysis

According to these studies, the Coachella Valley currently has approximately 400,000 residents, with a seasonal influx of approximately 100,000 people. The Valley’s population is expected to grow in areas of the Valley with convenient access to Section 19. While a number of established shopping



opportunities exist in the Valley, market research shows that the anticipated growth would support the development of an additional regional shopping destination within Section 19.

Currently, five primary retail districts serve the bulk of residents within the Coachella Valley: the Palm Springs Downtown and Airport area, the El Paseo/Westfield Palm Desert area in Palm Desert, the Monterey Avenue/I-10 District serving Rancho Mirage and Palm Desert, the Highway 111/Washington area in La Quinta, and Desert Hot Springs. An emerging district in the Indio/Coachella area will provide needed goods and services to the growing eastern portion of the Valley. However, significant major quality retailers oriented to affluent residents and visitors will seek more central locations.

The El Paseo/Westfield Palm Desert district currently serves as the primary quality lifestyle and luxury destination in the Coachella Valley. Additional quality lifestyle and luxury retailers not currently located in the Valley will most likely gravitate in the future to the El Paseo/Westfield Palm Desert area because the collective performance of quality retailers is enhanced due to the synergies created by strong co tenancy. While the El Paseo/Westfield Palm Desert shopping district will remain the dominant quality lifestyle and luxury shopping location in the Valley, it will not be able to conveniently serve the anticipated population growth. An additional regional center will be needed in a location that is convenient to a wide area. The Section 19 Specific Plan site is well situated to take advantage of this opportunity.

The new center would most likely need to be a “hybrid” mixed-use center comprised of promotional retailers, lifestyle retailers, restaurants, entertainment, hotels, office commercial, and residential. The entire site would need to be planned and phased so that any single element would not preclude the opportunity to support various other important elements over time. For example, lifestyle retailers would likely not be attracted to the site until it was established as a legitimate retail/dining destination.

The Section 19 Specific Plan land use plan was designed to accommodate the potential uses identified above and the phasing plan developed to facilitate the desired mix of uses in the interim and at buildout. A potential CVAG multimodal transit center would provide convenient access from other areas of the Valley and beyond. In addition, the existing casino would provide the opportunity for synergies beyond those typically found in a traditional regional retail center.

Office Analysis

The market for office space was analyzed in two categories: medical office space and general office space. Population growth drives the demand for medical offices, whereas general economic and employment growth drives demand for general office space.

High population growth is forecast for the Coachella Valley for the next five years. This population growth suggests strong demand for medical office space. The market study found that the Valley’s entire five-year demand for 300,000 to 350,000 square feet of medical office space will be met by the 330,000 square feet of new medical office and condominium development already approved and/or under development in the next 24 to 36 months.

Continued high population growth beyond 2010 suggests that market demand could induce medical office development in the Section 19 Specific Plan project site. Most medical offices tend to locate in



proximity to population cores and medical centers. With the nearby Eisenhower Medical Center and associated medical services, the Section 19 Specific Plan should be well positioned to attract a portion of the Valley's long-term medical office growth.

The forecast for continued economic and employment growth in the Coachella Valley suggests continued demand for new general office space. The market study found that economic growth would annually generate demand for approximately 65,000 square feet of new office space in the primary market area. The study also found, however, that office space under construction and planned office space would meet the demand for new offices through 2012. By permitting general office development in a wide range of locations and land use categories, the Specific Plan builds in the flexibility to respond to demand for new office space beyond 2012.



4. Specific Plan Concepts

4-1

4.1 SPECIFIC PLAN OBJECTIVES

The overall purpose of this Specific Plan is to generate momentum and provide comprehensive direction for the development of the Section 19 project site, while implementing the goals and policies of the City's General Plan. The project is guided by several major objectives:

- 1) Establish a unified vision for the project site to guide a cohesive, complementary mix of uses structured around a comprehensive set of circulation and infrastructure systems.
- 2) Create a new mixed-use regional destination development that stimulates a major new source of tax base for the City of Rancho Mirage.
- 3) Plan for an appropriate mix of commercial, office, entertainment, hotel, and residential uses, within the context of a master-planned Town Center, to meet the trade area's growing demand and build in the flexibility to respond to changes in the market.
- 4) Apply innovative planning and design solutions to create a sense of place at multiple scales.
- 5) Provide new housing concepts for the community, encouraging high quality, high density residential units that appeal to residents seeking shorter commutes to jobs, restaurants, and entertainment opportunities.
- 6) Take advantage of the area's location and exposure by establishing a welcoming gateway to the City, characterized by distinct and attractive signage, architecture, and landscaping, both on-site and in the public right-of-way.
- 7) Implement a circulation concept that optimizes circulation for both vehicular and pedestrian traffic, internalizes pedestrian activity to buffer it from the vehicular traffic along perimeter roadways, and establishes connectivity between uses infused with pedestrian-friendly and walkable spaces.



4.2 DEVELOPMENT PLAN

The Section 19 Specific Plan provides for a potential mix of approximately 3,096,500 square feet of commercial, retail, office, restaurant, and entertainment uses, as well as up to 1,899 residential units and 580 hotel rooms. The Development Plan is designed to accommodate these uses through the creation of 26 planning areas and 8 land use categories that cover approximately 226 acres. These planning areas, in addition to 43 acres for rights-of-way, comprise a total of 268.6 acres for the Specific Plan area. The planning areas and land use categories allow for a greater variety of land uses and customized development standards. Figure 8 illustrates the land use plan and planning areas for the Section 19 Specific Plan.

4.2.1 LAND USE CATEGORIES

Mixed-Use Core

4-2

The Mixed-Use Core land use category is the most intense and compact part of the Section 19 Specific Plan, supporting the vertical or horizontal integration of housing with smaller commercial services in the center of Section 19. Spread throughout 85 acres of the project, the mix of uses is intended to produce a unique and walkable shopping, working, and living experience that creates a sense of place within the project and the City. The greater intensity of development combines with the site's downward (northeasterly) sloping topography to support mixed-use multiple-story buildings that gain visibility from surrounding roadways and provide expansive views without dominating the remaining portions of the Specific Plan.

The Mixed-Use Core category permits the widest range of uses, including community retail, restaurants, regional lifestyle mixed center, professional office, live/work, attached single-family residential, and attached multifamily residential. Although this category allows for various land uses, it does not require a specified mix of uses.

Regional Mixed-Use (RMU)

Located in the northernmost area of the specific plan, the Regional Mixed-Use land use category serves as the window to the City of Rancho Mirage from Interstate 10 (I-10). To take advantage of this freeway exposure, the RMU land use category supports a variety of commercial, office, hotel, and entertainment uses on approximately 16 acres of land. The proximity to the railway and highway, however, limit the viability of residential uses.

Resort Flex

Embracing the resort lifestyle of Rancho Mirage, the Resort Flex land use category permits uses such as retail, restaurants, resort hotels, and regional entertainment to create a lively and comfortable atmosphere for business and leisure travelers. The Resort category also supports the development plans for an expansion of the Agua Caliente Casino · Resort · Spa and the creation of a lifestyle or event center in Planning Area 1.06 by the Agua Caliente Band of Cahuilla Indians (Tribe). The Resort Flex category accounts for approximately 29 acres of the project; however, a little over 18 of these acres are under the control of the Tribe.



Retail

The Retail land use category provides the project's primary shopping destination and offers a range of commercial, service, hotel, entertainment, and eating establishments on approximately 51 acres. These uses are best located at the perimeter of the site, exposed to the high volumes of traffic along Bob Hope Drive, Dinah Shore Drive, and Key Largo Avenue. A limited number of residential and interval ownership units may be introduced into identified planning areas to invigorate the retail environment and provide a transition to the internal Mixed-Use Core planning areas.

Residential (RES)

The Residential land use category provides a more urbanized style of residential living on approximately 22 acres across the street from the project's mixed-use core. Buildings up to four stories in height could contain dwelling units at densities reaching 28 units per acre (greater intensity can be reached with the inclusion of affordable housing). The residential uses will also serve as a transition between the Mixed-Use Core and the future residences across Key Largo Avenue in the City of Palm Desert. The residential uses will also offer higher density housing options for Rancho Mirage residents who may want to move out of single family detached housing but wish to remain in the City.

Public Facility (PF)

Approximately 11 acres have been set aside in the northwest corner of the project for a possible multimodal transit station that could be developed by CVAG. As construction of the transit station is not anticipated within the next 10–15 years, the site may temporarily serve as overflow parking or open space. Once under development, however, small commercial and food establishments may be permitted in conjunction with the transit station to serve transit riders. The site may also be used for permanent parking solutions, renewable energy generation, or a combination of the two.

Open Space (OS)

The Open Space land use category reflects the project's need to accommodate a system of parks and recreation features as well as adequate infrastructure systems, including retention basins. Within the 5 acres of open space, a 94-foot-wide esplanade unrolls along Center Street and serves as the project's spine, supporting active recreation space and a visual relief from the surrounding urban spaces. Additional discussion is provided in Sections 4.4 and 4.7.

Drainage Channel (DC)

To support the Section 19 Specific Plan and surrounding development, a drainage channel is planned for the northern border of the project, consuming approximately 6 acres of land. Initially, this area will function as a drainage basin until such time as the Mid-Valley Channel is fully improved.

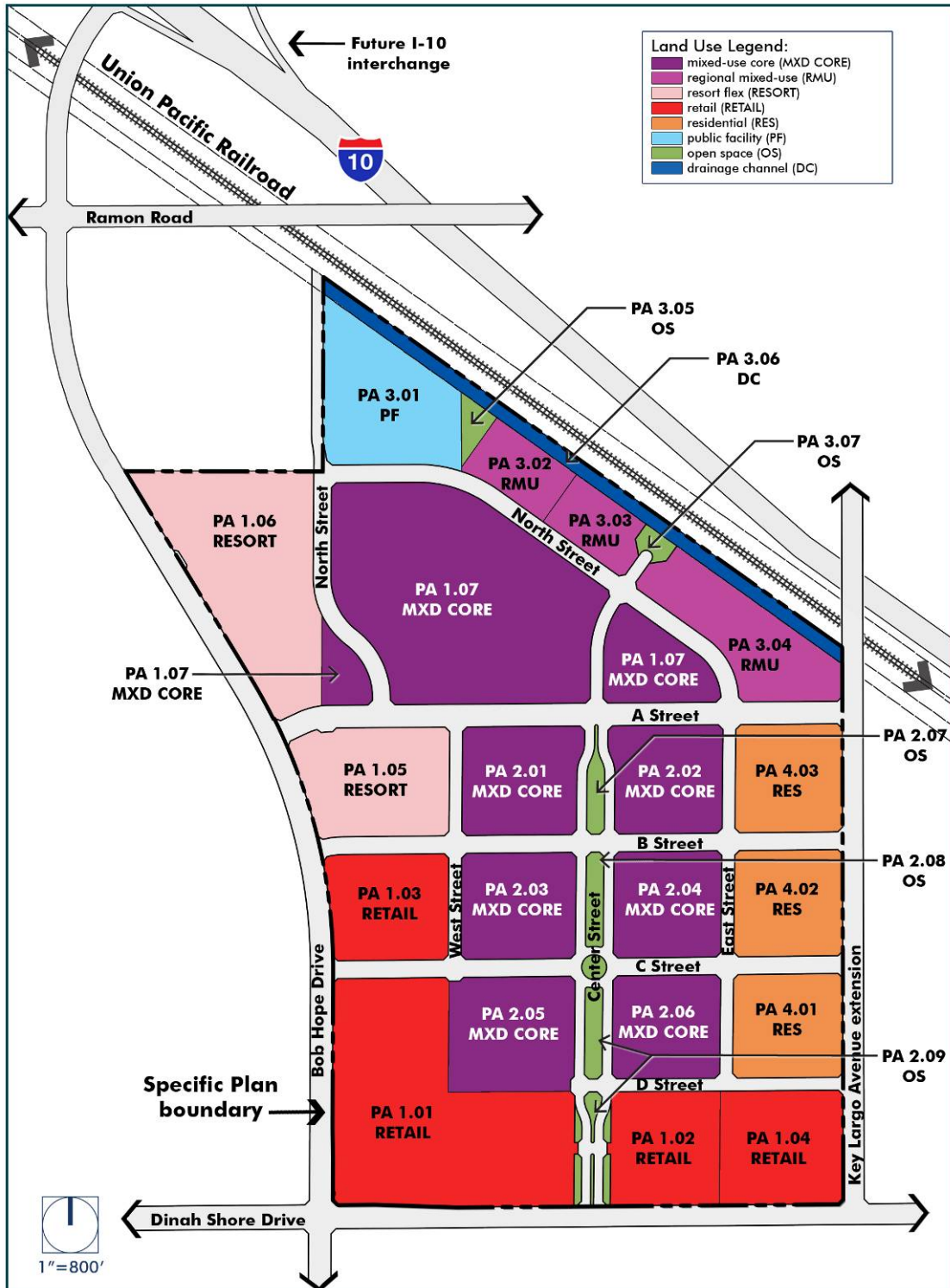


Figure 8 Land Use Plan and Planning Areas



4.2.2 PLANNING AREAS

A total of 26 planning areas have been delineated to describe the amount, type, and distribution of development throughout the project site. The planning areas have also been constructed to reflect the ownership patterns and enable the project to be constructed in an incremental fashion while still achieving a unified, comprehensive plan.

Each planning area is subject to a distinct list of allowed uses and development standards. Additionally, the numbering of each planning area relates to the estimated phase of development. Accordingly, Planning Area (PA) 1.01 is estimated to develop prior to PA 1.04 in the first phase and before PA 3.05, which would occur in the third phase. Additional detail on the project's phasing plan can be found in Chapter 8. The following is a brief description of the conceptual uses and overall purpose proposed for each planning area.

Planning Areas 1.01–1.04

Situated at the project's southern corners and three entrance points, this group of planning areas receives the greatest exposure from locations inside the City. Permitted development will take advantage of this exposure to provide a range of commercial and unique residential uses in a lifestyle center atmosphere. A boutique hotel with up to 150 rooms would be integrated into the lifestyle center.

The planning areas offer a view corridor into the site to expose the more intense mixed-use core, with project signage and the low scale retail buildings as foreground architecture, taller buildings transitioning into the center of the site as midground architecture, and scenic views of the mountains as background.

With their southern location and elevated topography, PAs 1.01 through 1.04 are the least intense planning areas, permitting a maximum floor-area ratio (FAR) of 0.35–0.40. Explicit height regulations are provided in Chapter 5 to ensure proposed development complements the surrounding Rancho Mirage development pattern.

Planning Areas 1.05–1.06

The resort lifestyle of Rancho Mirage is reproduced in PAs 1.05 and 1.06, which are designated Resort Flex and may contain uses such as regional entertainment, retail, and hotels. As an evolution of the City's wonderfully expansive resort areas, PAs 1.05 and 1.06 envision resort development that is as luxurious, attractive, and relaxing, but more compact, to match the urban feel of the Section 19 Specific Plan.

PA 1.05 anticipates the construction of a resort hotel that could offer approximately 280 rooms in buildings reaching up to eight stories at a maximum FAR of 0.80. PA 1.06 is under the control of the Tribe and may be planned for an expansion of the Agua Caliente Casino · Resort · Spa and the creation of a lifestyle or event center, as well as a smaller hotel of approximately 100 rooms, at a maximum FAR of 0.80.



Planning Area 1.07

PA 1.07 is the largest planning area in the project and is designated Mixed-Use Core, which permits a wide range of uses, including community retail, restaurants, regional lifestyle mixed center, professional office, live/work, attached single-family residential, and multifamily residential. However, it is important to understand that while residential development is encouraged, it is not permitted to be the only type of development constructed in PA 1.07.

As the northern part of the project's mixed-use core, PA 1.07 is envisioned as a compact, walkable area to shop, work, and live. The interior location of the planning area allows for a relatively intense FAR of 1.0. The desired compact form is assisted by the low topography of the planning area to enable buildings to rise to up to six stories without impacting the City's views or overall character.

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PA 1.07 also acts as a transition between the RMU planning areas to the north and the Resort planning areas to the south and west. Accordingly, PA 1.07 permits uses allowed in the RMU and Resort land use categories, but not in the project's other Mixed-Use Core planning areas, such as furniture showrooms and business campus.

Planning Areas 2.01–2.06

An additional six planning areas of Mixed-Use Core are provided in PAs 2.01 through 2.06 to generate a series of mixed-use buildings that could support residential units constructed next to or above retail stores, restaurants, offices, or other commercial uses. These planning areas present the opportunity to create a unique town center corridor that offers activities currently unavailable in Rancho Mirage. For example, gatherings such as farmers' markets and holiday festivals could be organized within and along the central esplanade provided in PAs 2.07 through 2.09 by closing off Center Street from through traffic.

As the planning areas transition southward, their relative elevation increases, granting them greater visibility from the rest of the city. To ensure compatibility with surrounding development, the allowable intensity of development for each planning area is inversely related to elevation; i.e., as elevation increases, intensity of development decreases. For example, PAs 2.01 and 2.02 are at some of the lowest elevations on the site and are permitted to develop taller, more intense development at an FAR of 1.0. Comparatively, PA 2.05 is permitted to construct development at an FAR of 0.70, reflecting its proximity to the low-scale retail development of PA 1.01. PA 2.06, located across the street from the residential planning areas, is permitted to develop at a slightly higher FAR of 0.80. PAs 2.03 and 2.04 are slightly elevated and are permitted to develop at an FAR of 0.90.

Each planning area should consist of a combination of residential and non-residential uses; residential development is not permitted to be the only type of development constructed throughout PAs 2.01 through 2.06. A mix of uses is encouraged, with a strong focus on orienting activity toward the central esplanade.



Planning Areas 2.07–2.09

PAs 2.07 through 2.09 provide a string of linear parks that play a key role in creating the project's unique identity. In addition to supplying a highly attractive visual amenity and recreational resource, the parks are permitted to contain small food or retail establishments to encourage activity throughout the central corridor.

Planning Areas 3.01–3.07

The northern edge of the Section 19 Specific Plan serves many purposes and is divided into seven planning areas to set aside land for public facilities, including a drainage channel and open space, and RMU development. This collection of planning areas must also consider the need to both buffer the project from the Union Pacific Railroad corridor and take advantage of the freeway exposure to advertise the site's commercial land uses.

PA 3.01 is set aside for a possible multimodal transit station that could be developed by the Coachella Valley Association of Governments (CVAG). Coordination with CVAG indicates that construction of the transit station is not anticipated within the next 10–15 years, until the region's growth could justify the development and operation of a transit station. The site may also be used for temporary or permanent parking, renewable energy generation, a water well site, or any combination thereof.

PAs 3.02 through 3.04 are designated for RMU and are intended to support a range of larger regional commercial, entertainment, hotel, and business uses that can benefit from significant freeway exposure. The combination of the proximity to the freeway and low elevation of the planning areas allows development to be constructed at a maximum FAR of 0.70, slightly higher than traditional regional commercial uses.

PAs 3.05 and 3.07 are designated Open Space and will provide the project with parkland that can be used for both recreation and water retention. At the lowest elevation point in the project, PA 3.06 is designated for a drainage channel to serve the project and surrounding uses.

Planning Areas 4.01–4.03

PAs 4.01 through 4.03 are designated Residential and will serve as a transition between the mixed-use core and the future residences across Key Largo Avenue in the City of Palm Desert. The higher density residential development would also provide new housing opportunities previously unavailable in the City, attracting residents seeking shorter commutes to jobs, restaurants, and entertainment opportunities. A maximum of 588 units could be constructed at densities reaching 28 units per acre in buildings ranging from three to four stories in height. The inclusion of affordable housing is highly encouraged and could increase allowable density to approximately 36 units per acre.

4.2.3 CONCEPTUAL LAND USE PLAN

The development regulations and guidelines within the Section 19 Specific Plan permit a range of uses and intensities. To gain an understanding of what could be developed under the Specific Plan, and to understand the project's potential needs and impacts, Table 1 presents a breakdown and distribution of the potential mix of residential and non-residential uses.

The amount of development shown in the land use summary also serves as the basis for the Specific Plan's Environmental Impact Report (EIR). The EIR determines the maximum needs and potential impacts of the project to ensure the City and developers provide the appropriate level of infrastructure. The exact type, location, and amount of uses within each planning area will be determined during the tract map and development review process. The final development plan may exceed the square footage figures in the land use summary, so long as the overall maximum impact thresholds allowed by the project's EIR are not exceeded. Chapter 5, *Development Regulations*, addresses the project's development standards such as maximum intensity and permitted uses.



**Table 1
Land Use Summary**

Planning Area	Land Use Category	Typical Permitted Uses												Net Land Area (Acres)	Non-Residential Square Footage	Dwelling Units	Hotel Rooms
		Community Retail	Restaurants	Office/Service	Resort Hotel	Furniture / Business Campus	Single-Family Attached Residential	Multifamily Residential	Live/Work	Mixed-Use	Open Space/ Recreation	Public Facility	Drainage Channel				
1.01	Retail													26.3	415,600	50	150
1.02	Retail													8.0	110,400	–	–
1.03	Retail													8.1	112,200	–	–
1.04	Retail													9.0	119,100	–	–
1.05	Resort Flex													10.2	329,000	–	280
1.06	Resort Flex													18.5	489,200	–	100
1.07	Mixed-Use Core													37.7	542,400	750	–
2.01	Mixed-Use Core													8.2	115,800	107	–
2.02	Mixed-Use Core													7.7	108,400	100	–
2.03	Mixed-Use Core													7.8	86,500	87	–
2.04	Mixed-Use Core													7.4	81,500	82	–
2.05	Mixed-Use Core													9.3	70,900	71	–
2.06	Mixed-Use Core													7.1	63,500	64	–
2.07	Open Space													0.9	2,700	–	–
2.08	Open Space													1.1	2,300	–	–
2.09	Open Space													2.0	2,200	–	–
3.01	Public Facility													10.9	–	–	–
3.02	Regional Mixed-Use													3.7	98,200	–	–
3.03	Regional Mixed-Use													3.5	90,400	–	–
3.04	Regional Mixed-Use													9.0	256,200	–	50
3.05	Open Space													0.9	–	–	–
3.06	Drainage Channel													5.7	–	–	–
3.07	Open Space													0.6	–	–	–
4.01	Residential													7.3	–	183	–
4.02	Residential													7.4	–	205	–
4.03	Residential													7.4	–	200	–
–	Right-of-Way													43.0	–	–	–
TOTAL														268.6	3,096,500	1,899	580



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4.3 CIRCULATION PLAN

The ability to easily access and travel within the project site by multiple modes of transportation is paramount to the project's success. As shown on Figure 9, the vehicular circulation system for the Section 19 Specific Plan includes both regional and local roadways. This system of roadways generally forms a grid pattern to maximize access to each planning area and enhance the walkability of the site. The grid system also allows for the overall project to be developed in a phased approach without disrupting continuity or access for existing or developing projects.

The Specific Plan's circulation system contains a hierarchy of access points and roadways to dictate the function and character of each intersection and roadway. Access points in the Specific Plan are described as Primary or Right In/Out. Roadways are classified as Major Arterial, Minor Arterial, Minor Collector, Local, Esplanade, Alleys, and Service Roads. The following are brief descriptions of each street and access point classification, with illustrations of each street section provided on the following pages, beginning with Figure 11.

4.3.1 ACCESS POINTS

The Section 19 Specific Plan can be accessed at 12 points, each of which are attractively landscaped and signed for vehicles and pedestrians. Five of the twelve access points are signalized and placed approximately a quarter mile between each other or existing signalized intersections. These signalized access points are identified as Primary access points and serve as the main access routes into the project. Seven secondary access points are strategically placed 600 to 700 feet from the signalized access points to facilitate access into and out of the project. The secondary access points are classified as Right In/Out entries. These points are identified in Figure 10.

Primary Access

The five Primary access points are distributed along all three sides of the project site. The first two are along Bob Hope Drive, at the intersections of A Street and C Street, and the third is at the intersection of Dinah Shore Drive and Center Street. The remaining two are along Key Largo Avenue at the intersections of A Street and C Street. As these entry points will carry the bulk of traffic entering the project and serve as gateways to the site, the landscape and streetscape at these entry points should be distinctive and more prominent in character than other access points.

Right In/Out Access

Seven Right In/Out access points are also provided, with three along Bob Hope Drive, two along Dinah Shore Drive, and two along Key Largo Avenue. The purpose of the Right In/Out entry is to provide secondary access routes from the perimeter roadways to the interior of the site. The seven access points are generally evenly placed in conjunction with Main Entries. The Right In/Out access points will be used primarily for the movement of vehicles to and from residential and commercial uses on the site, and are not intended for pedestrian activity.

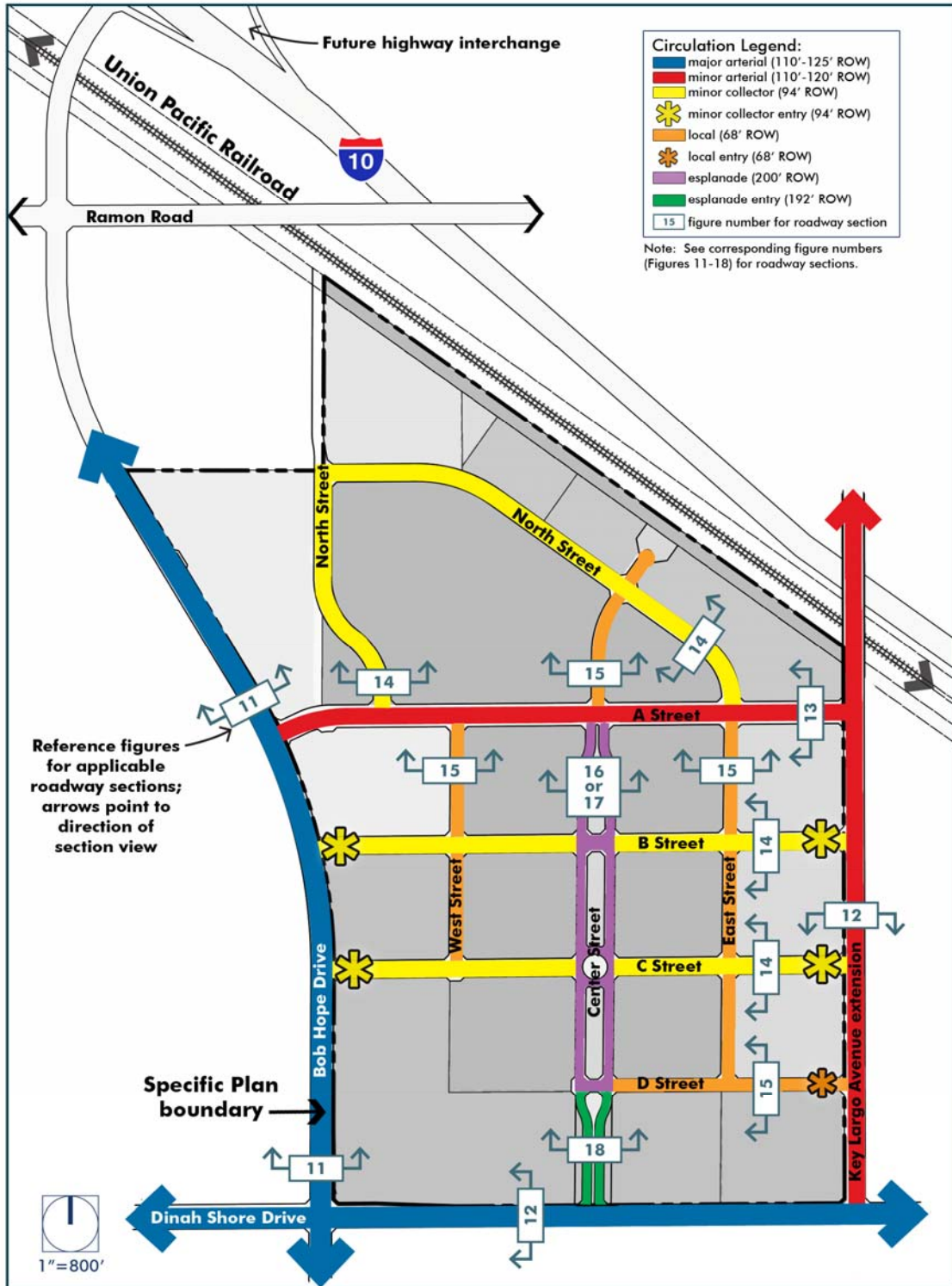


Figure 9 Circulation Plan

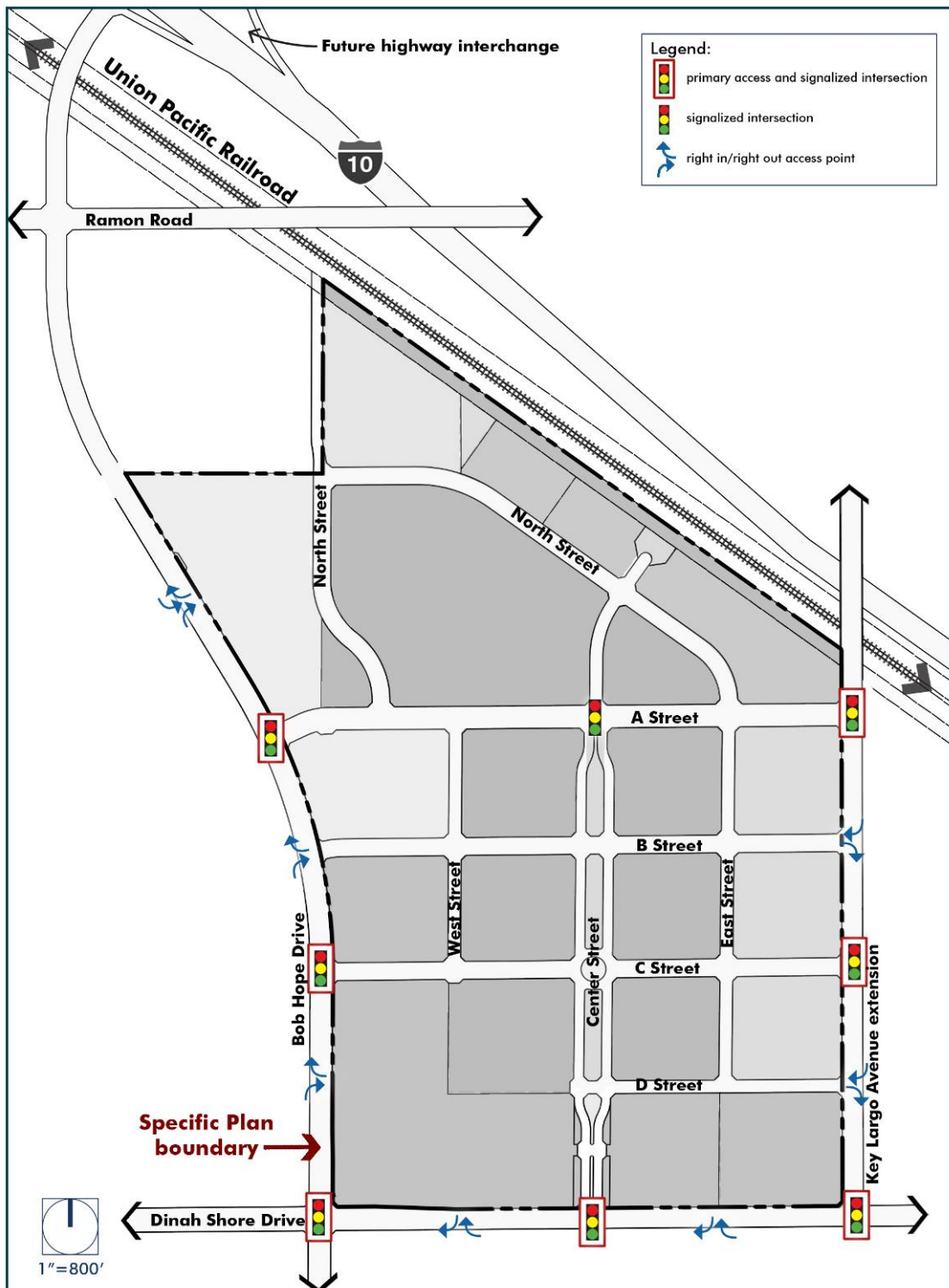


Figure 10 Access and Intersection Control



4.3.2 ROADWAYS

The Specific Plan street pattern is organized to provide efficient circulation and access to each of the planning areas within the Section 19 project. In addition to addressing vehicular circulation, the Circulation Plan allows for the movement of pedestrians, bicyclists, and golf carts, and also accounts for future public transit opportunities.

All roadways within the project area will be constructed according to the minimum standards and guidelines set forth in the Specific Plan. On-street drainage is not portrayed on the following roadway section illustrations, and should be constructed per City engineering standards. All roadways shown on the Circulation Plan (Figure 9) are public. Roadways that are within the project area, but are not illustrated on the Circulation Plan are private and will be maintained through a property owners' association.

Major Arterial

The Major Arterial roadways are Bob Hope Drive and Dinah Shore Drive, along the western and southern borders of the project site. These are the major transportation corridors for the City of Rancho Mirage and will provide the regional access to the Section 19 Specific Plan. This Specific Plan addresses the edge of the Major Arterial roadway that falls within the Specific Plan boundary.

The midblock right-of-way for these two Major Arterials varies depending upon the presence of above-ground power poles. Along Bob Hope, south of A Street, the power poles dictate that the City's right-of-way extends 12 feet from the curb; in all other instances the right-of-way extends 7 feet from the curb.

The Specific Plan's edge condition incorporates an additional building setback in addition to the City's Major Arterial right-of-way requirement, as shown in the street sections in Figures 11 and 12. The property owner will be responsible for maintaining this setback area as well as the parkway area within the public right-of-way.

This buffer allows adequate space for an 8-foot sidewalk to permit golf carts in addition to pedestrians and cyclists. The proposed Class I golf cart paths would connect into the existing Rancho Mirage golf cart circulation system, which provides paths along the south side of Dinah Shore Drive west of Bob Hope Drive. The pathway would also provide enhanced access to the Agua Caliente Casino · Resort · Spa and a future park north of Ramon Road.



The 8-foot-wide sidewalk accommodates pedestrians, cyclists, and golf carts.



Minor Arterial roadways include a landscaped median to divide traffic.



Section 4.3.4 describes the golf cart, bicycle, and pedestrian routes within the Specific Plan area in more detail.

The right-of-way dimensions expand to 123 feet for Bob Hope Drive and 128 feet for Dinah Shore Drive at the intersection to accommodate turning lanes. Along Bob Hope Drive, the expansion of the right-of-way begins approximately 380 feet north of the intersection, reaching the fully expanded dimension at approximately 230 feet north of the intersection. Along Dinah Shore Drive, the expansion of the right-of-way begins approximately 630 feet east of the intersection, reaching the fully expanded dimension at approximately 200 feet east of the intersection.

Minor Arterial

The Minor Arterial roadways within the Section 19 Specific Plan consist of the extension of Key Largo Avenue, which runs along the eastern boundary of the project, and A Street, which bisects the project and connects Bob Hope Drive and Key Largo Avenue. These roadways will filter regional traffic from the Major Arterial roadways and provide access to the Section 19 planning areas.

The Minor Arterial street section contains four travel lanes bisected by a 16-foot median, as shown in Figures 12 and 13. The street section also includes sidewalks and Class II bikeways and golf cart paths to accommodate pedestrians, cyclists, and golf carts within the Section 19 project. The 17-foot parkway included within the Minor Arterial right-of-way may be reduced for projects that propose buildings that front onto the street, in which case the building may encroach up to 6 feet into the parkway to provide a more urban streetscape (this encroachment cannot project into the 5-foot sidewalk area).

Minor Collector

The Minor Collector streets are intended to accommodate local internal circulation and provide access to Major and Minor Arterials within the Section 19 project. The Minor Collectors are North Street, B Street, C Street, and the portion of Center Street between the intersection of Dinah Shore Drive and the beginning of the Esplanade roadway. On-street parking is provided within the street section as shown in Figure 14, which will be replaced with landscaped bulb-outs at intersections. This will narrow the roadway for pedestrians to cross at these intersections, creating a more pedestrian-friendly environment. Similar to the Minor Arterial roadway, the Minor Collector street section also allows buildings to encroach up to 4 feet into the parkway to create a more urban edge condition (this encroachment cannot project into the 5-foot sidewalk area).

The Minor Collector Entry street section applies to the first 30 feet of each Minor Collector street from the Major and Minor Arterial intersections at the boundaries of the Section 19 Specific Plan. This modified street section, shown in Figure 14, replaces the on-street parking with a landscaped median to enhance the entryway treatment.



Local

The Local streets within the Section 19 Specific Plan primarily serve internal circulation within the project and are not intended for through traffic. The Local roadways include West Street, East Street, and D Street, supplementing the grid system established by the major roadways. The parallel on-street parking illustrated within the street section shown in Figure 15 will be replaced with landscaped bulb-outs at intersections, similar to the Minor Collector roadway. However, buildings will not be allowed to encroach into the parkway identified in the Local street right-of-way.



Local streets are designed for local resident circulation.

The Local Entry street section applies to the first 30 feet of D Street, from the intersection at Key Largo Avenue at the eastern boundary edge of the Specific Plan. Comparable to the Minor Collector Entry street section, the Local Entry street section replaces on-street parking with a landscaped median to provide additional landscape treatment at the entryway.

Esplanade

The Esplanade roadway comprises the portion of Center Street that is bisected by the linear park. The right-of-way incorporates park areas and creates 2 one-way streets as shown in Figure 16. This portion of Center Street provides a unique setting for residents and visitors to gather, live, and shop, distinguishing the Section 19 Specific Plan from other developments in Rancho Mirage.



The Esplanade roadway includes 2 one-way streets bisected by a linear park.

The right-of-way for the esplanade accommodates landscaped sidewalks alongside the park area and development edge to facilitate pedestrians and bicycle access. The parkways along the development edges of the Esplanade allow buildings to encroach up to 8 feet to bring them closer to the street and park and create a more engaging streetscape for pedestrians (this encroachment can project into the 5-foot sidewalk area, so long as a clear 3-foot walkway is preserved and not obstructed by any objects such as chairs, fencing, or signage).

The Esplanade street section may incorporate either diagonal or parallel on-street parking, as shown in Figures 16 and 17. The on-street parking shall be replaced with landscaped bulb-outs at intersections to narrow the width of the roadway and facilitate pedestrian



Traffic entering the roundabout will turn right to enter the circle.



crossings at these points. Incorporating landscaped bulb-outs at mid-block locations is also encouraged to increase access to the linear park area and the shops and buildings located across the street.

At the intersection of Center Street and C Street, the Esplanade roadway incorporates a traffic circle to help maintain traffic flow and serve as a unique design and circulation feature. This roundabout will reduce capital and operational costs for the City, and promote safer vehicular travel within the Section 19 Specific Plan. In addition to slowing down traffic, roundabouts reduce the potential for head-on collisions because all traffic moves in the same direction, and air quality improves because fewer cars are stopped at red lights, idling needlessly.

Roundabouts require entering motorists to yield at the entry, and once there is a gap in traffic, motorists turn right into the circle and follow around counterclockwise until they reach the street they want and turn right again, exiting the roundabout. The circulating traffic constantly moves, though more slowly than through a traditional four-way intersection. The large round open space placed in the middle of the roundabout should incorporate a tower or signature feature that reflects the character of the development. The size of the traffic circle should be designed to match anticipated traffic volumes, according to the Federal Highway Administration standards.

The Esplanade Entry street section provides a grand entrance to the project and applies to Center Street from Dinah Shore Drive to D Street. Large setbacks of decorative and edible landscaping, plaza spaces, and pedestrian pathways serve as a pair of smaller open spaces that transition into the larger central esplanade at D Street. In addition to providing an attractive view corridor, this configuration facilitates safe and efficient movement of vehicular, bicycle, and pedestrian access into and out of the site. The street section for the Esplanade Entry Roadway is illustrated in Figure 18.



Alleys allow buildings to place garages away from local streets.

Alleys and Service Roads

The objective of the Alleys and Service Roads classification is to provide access to areas within the project site, typically between buildings to allow access to parking areas or service businesses. Alleys and service roads must provide a 24-foot travel area to accommodate two passing vehicles or allow cars to back out of parking spaces or garages. Alleys must also provide a 2-foot apron on both sides of the travel area to support landscaping along the building edges and soften building facade. Service roads must provide a 4-foot apron on either side to support larger landscape elements such as trees. If required by the Riverside County Fire

Department, the travel area may need to be expanded to 26 feet to maintain a “clear to sky” width of 26 feet. The street sections for both the Service and Alley Roadways are illustrated in Figure 19.

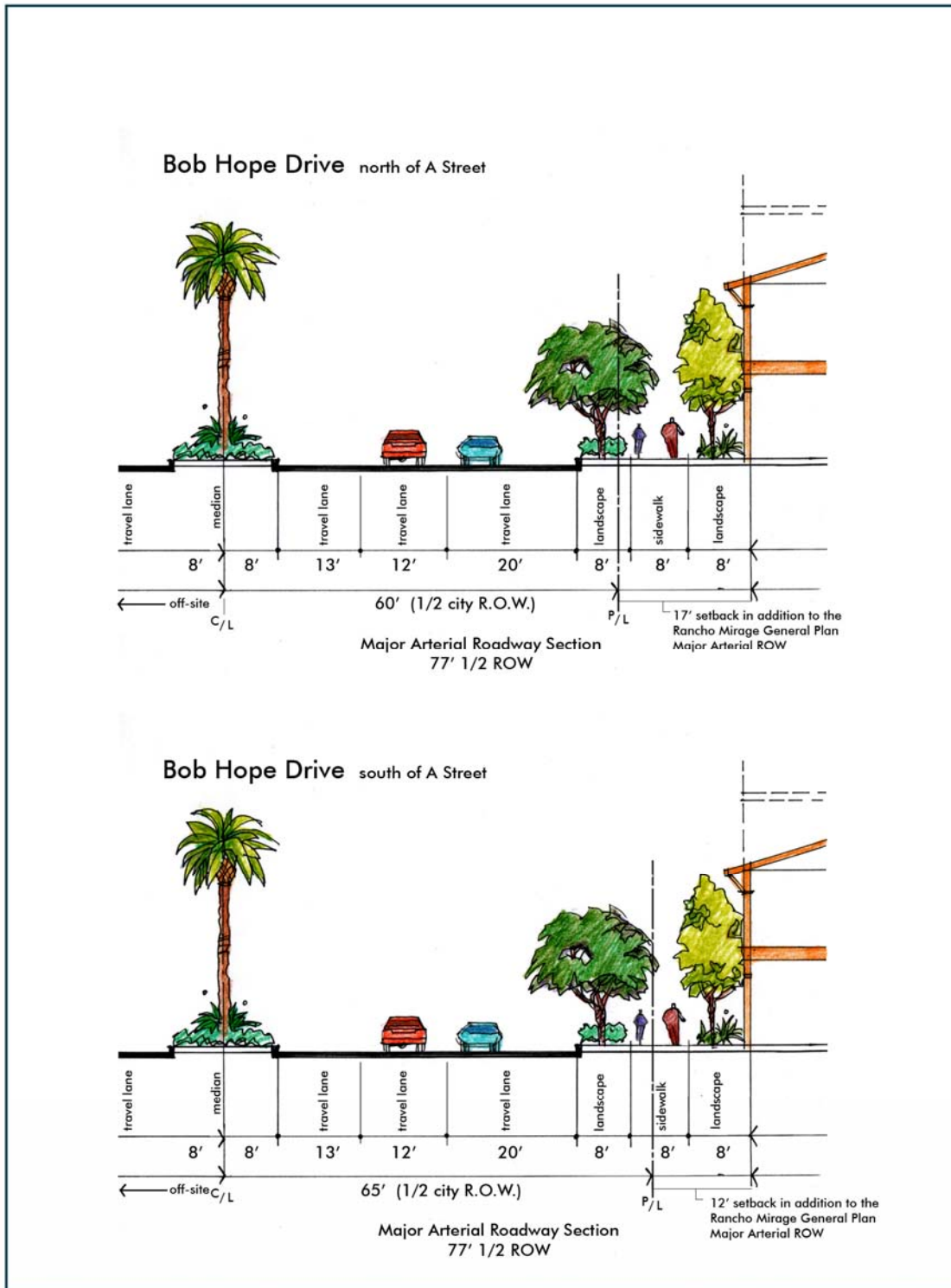


Figure 11 Major Arterial Roadway Sections: Bob Hope Drive

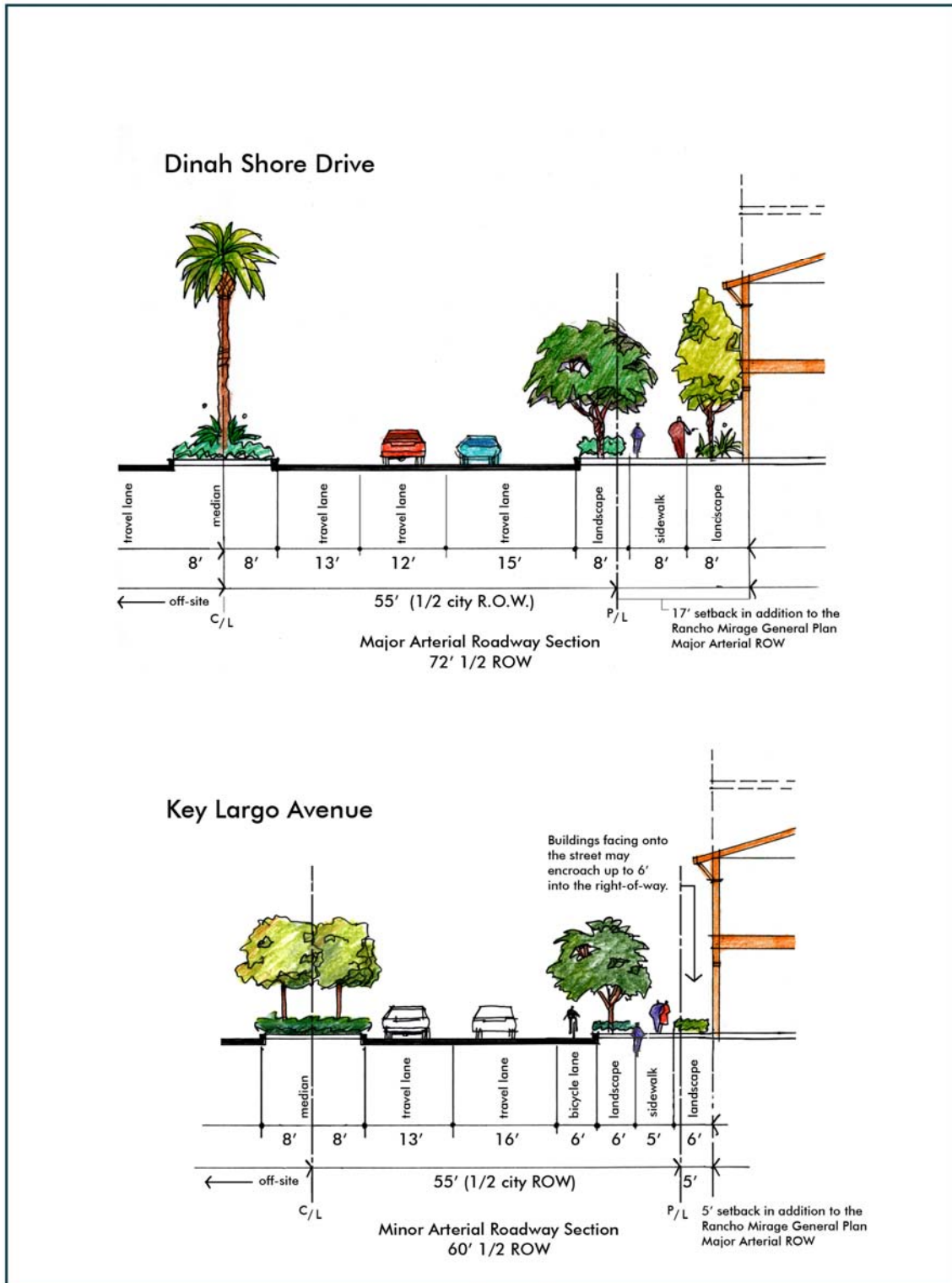


Figure 12 Major and Minor Arterial Roadway Sections: Dinah Shore Drive and Key Largo Avenue

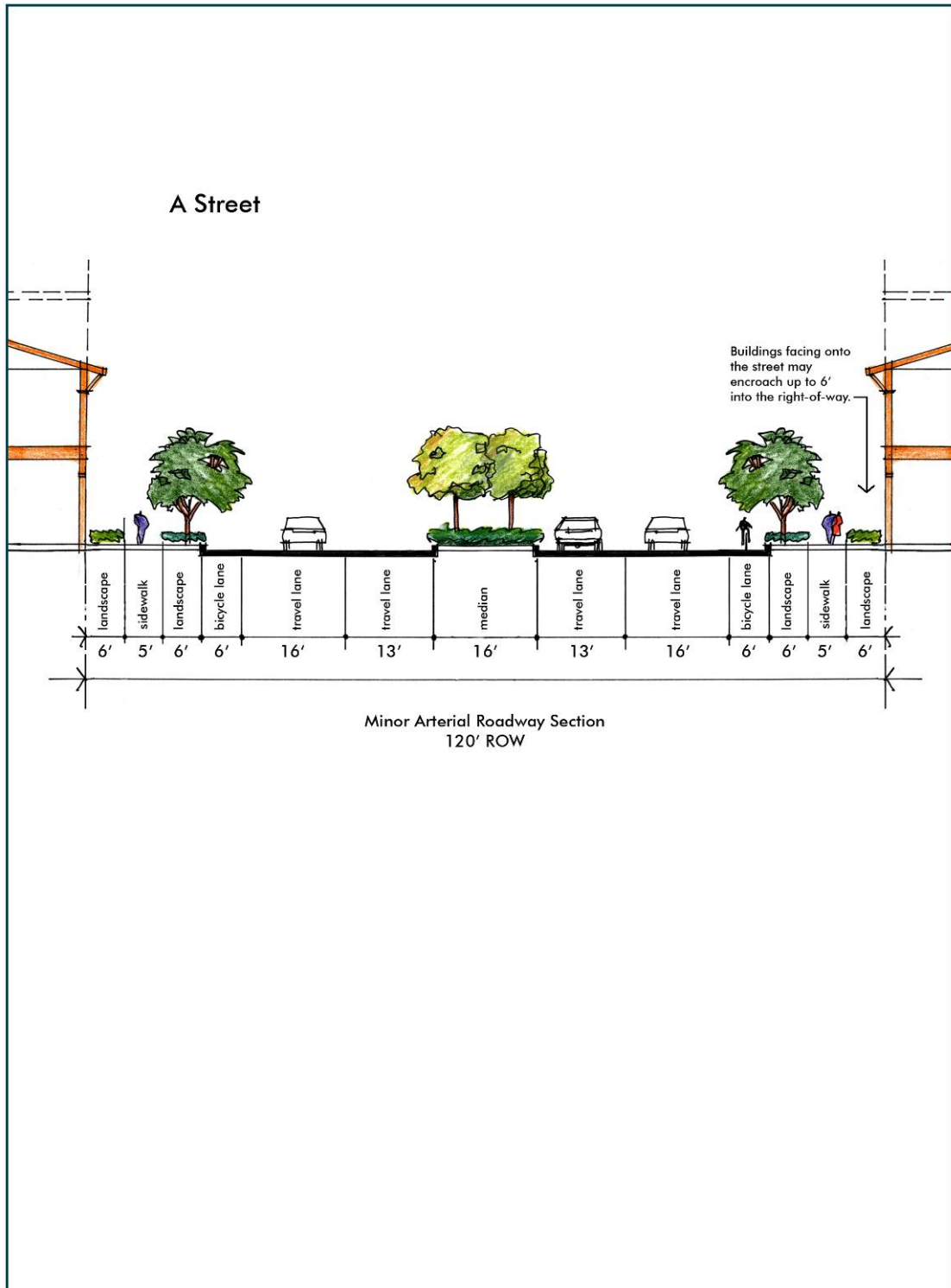


Figure 13 Minor Arterial Roadway Section: A Street

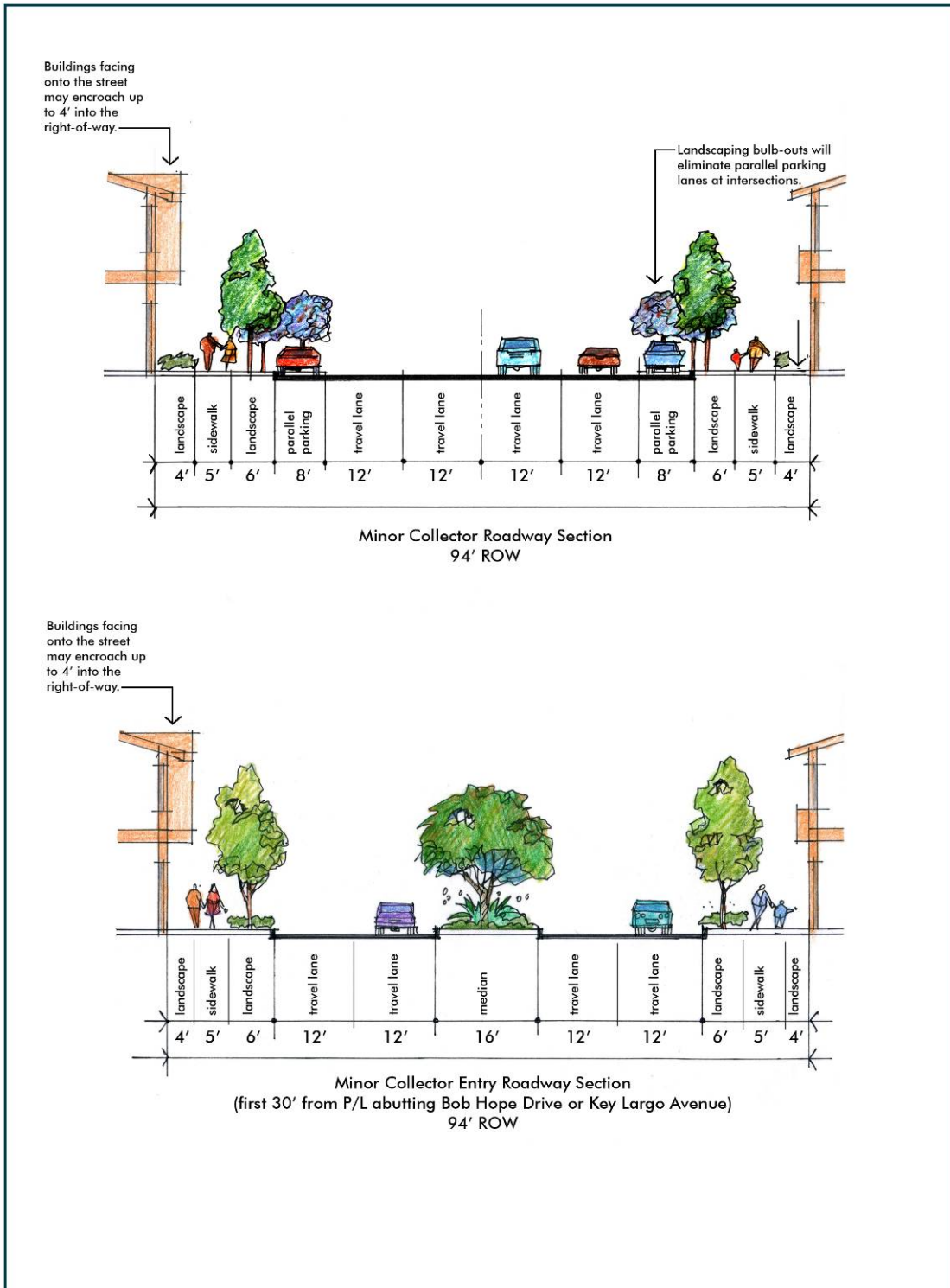


Figure 14 Minor Collector Roadway Sections

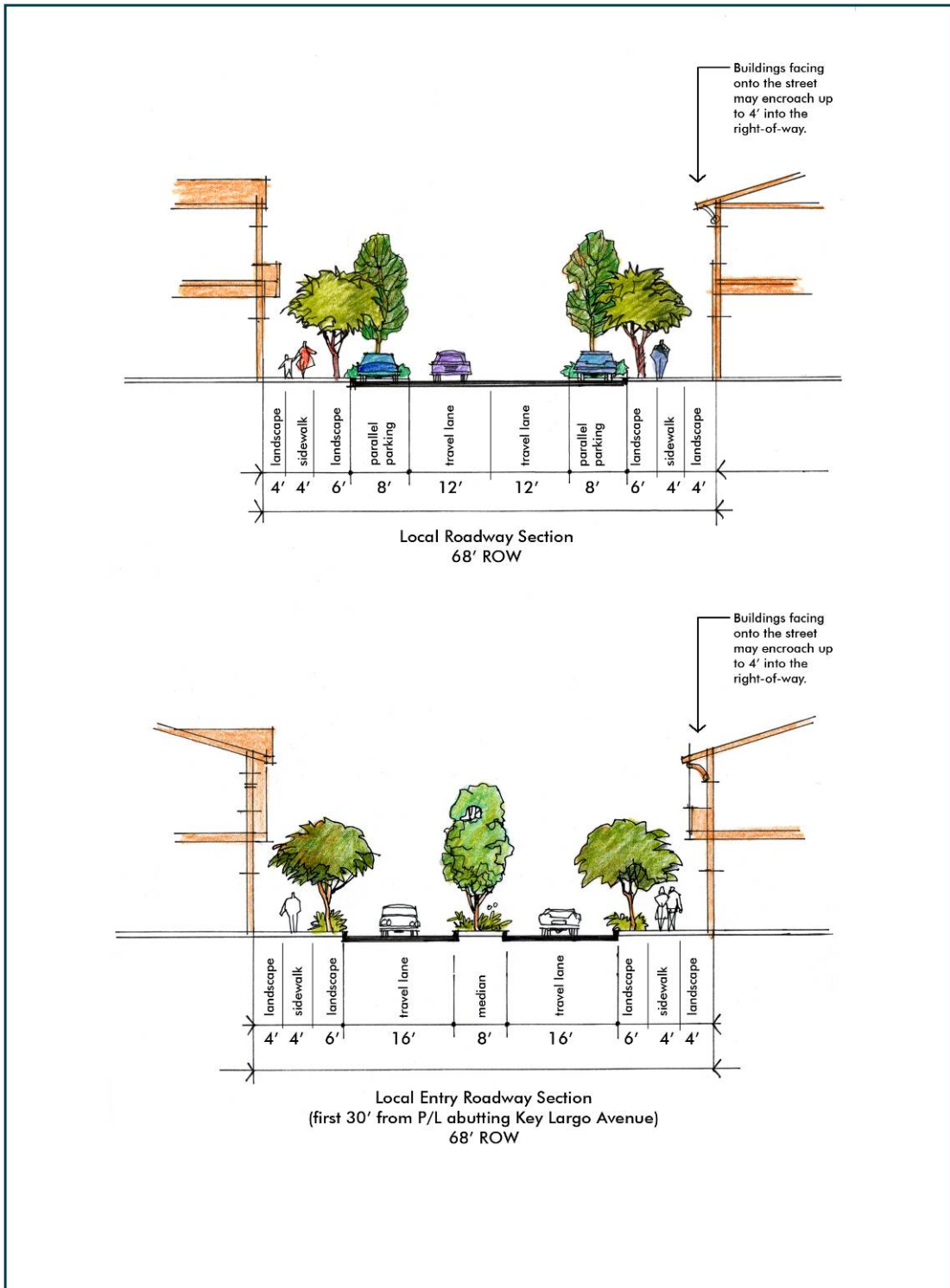


Figure 15 Local Roadway Sections

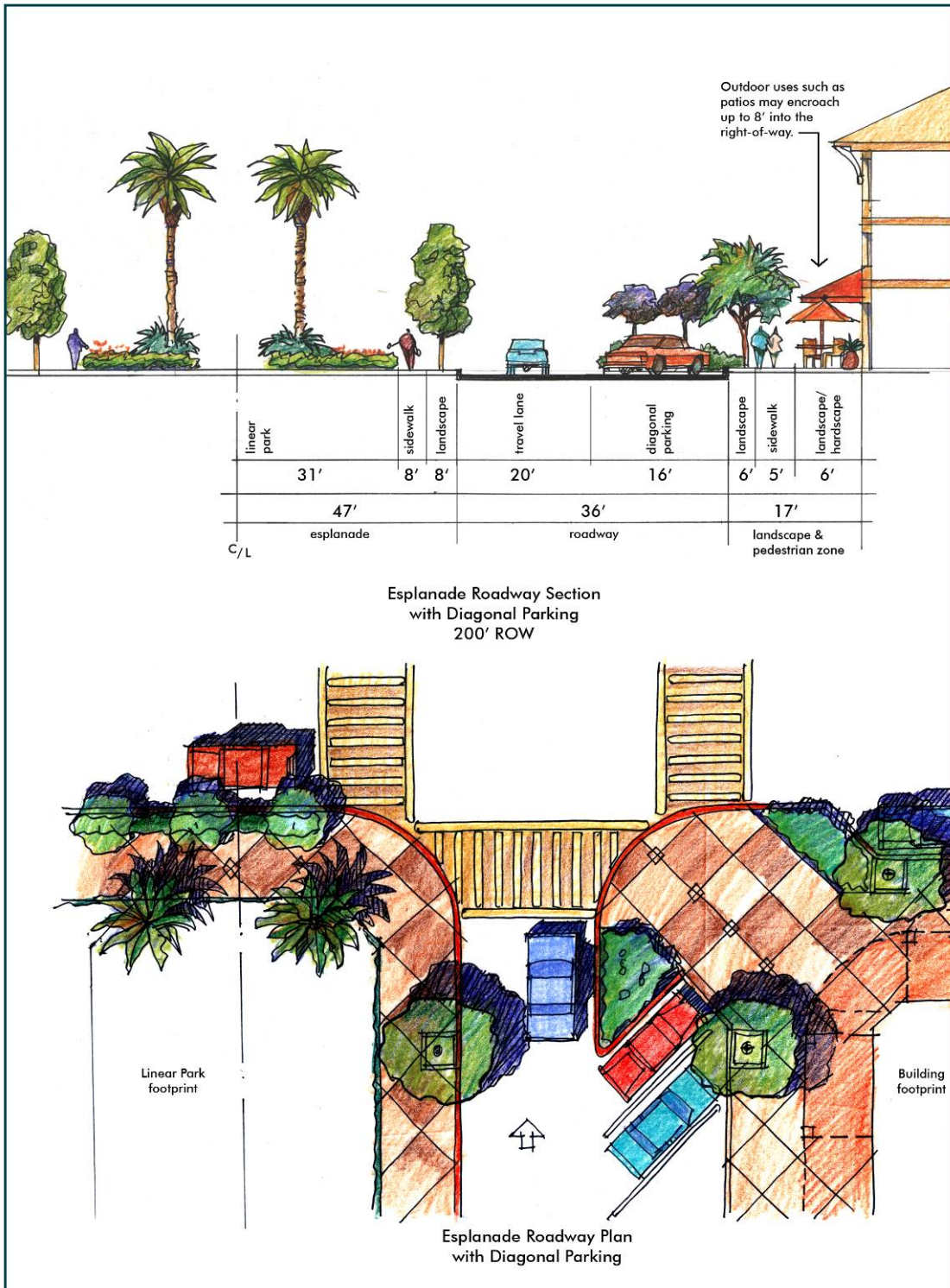


Figure 16 Esplanade Roadway Section: Diagonal Parking

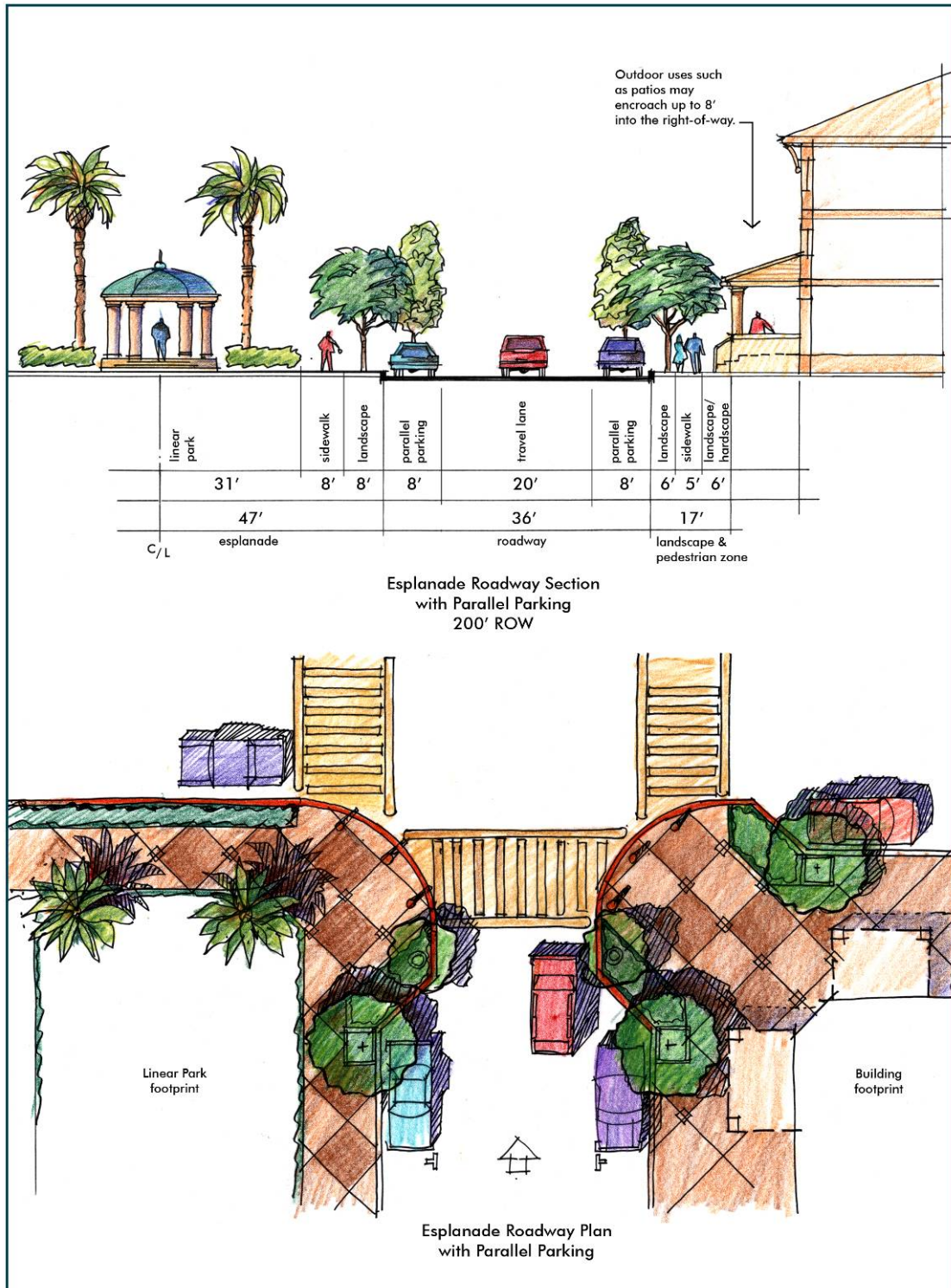


Figure 17 Esplanade Roadway Section: Parallel Parking

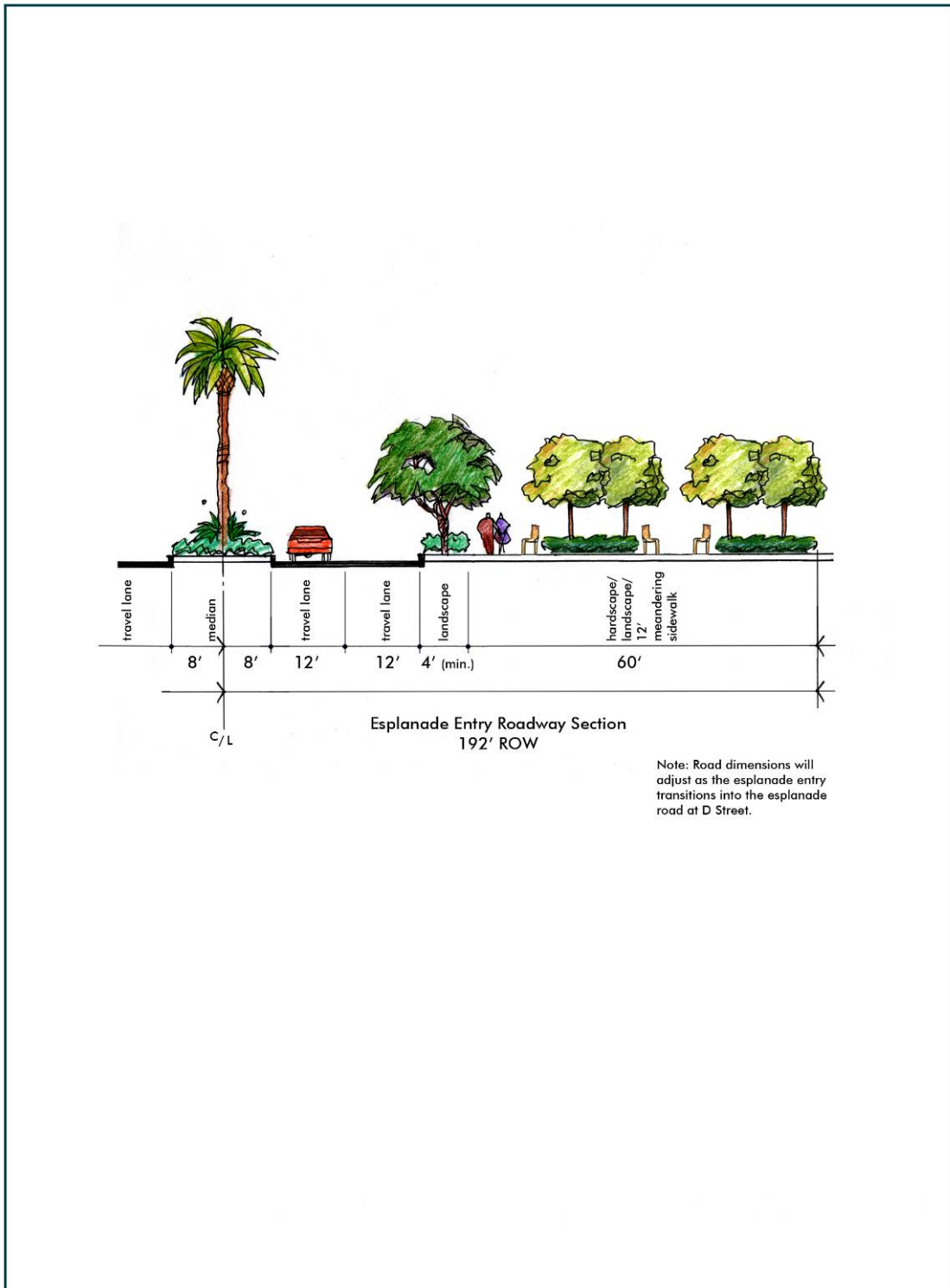


Figure 18 Esplanade Entry Roadway Section

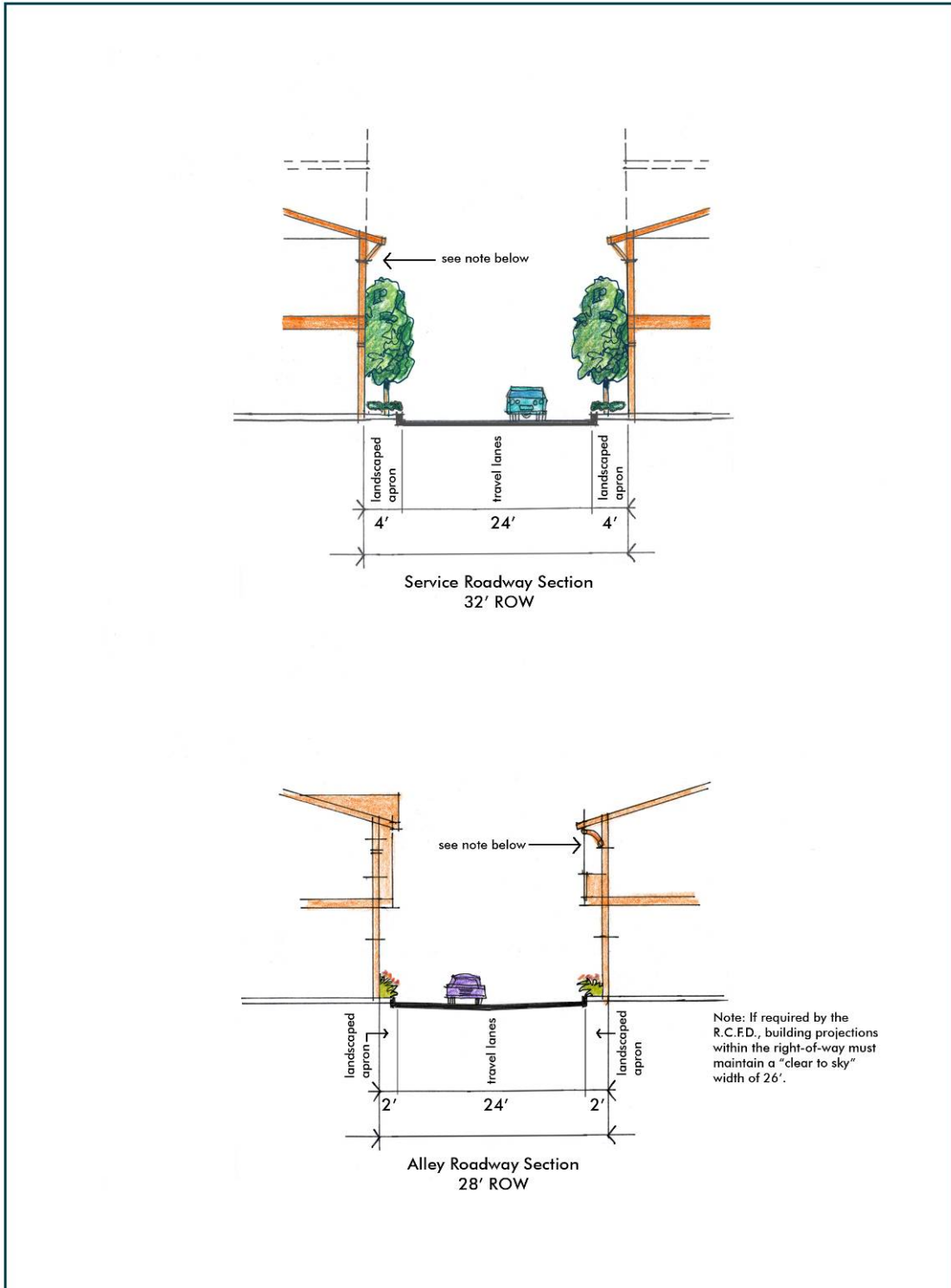


Figure 19 Service and Alley Roadway Sections



4.3.3 PUBLIC TRANSPORTATION

The SunLine Transit Authority is the provider of public transit service within the City of Rancho Mirage and the Coachella Valley. The Section 19 Specific Plan area is served by Route 32 of the SunLine fleet, which travels between Rancho Mirage, Thousand Palms, and Cathedral City via Bob Hope Drive, Dinah Shore Drive, Monterey Avenue, and Ramon Road, as shown on Figure 20. The buses are equipped with wheelchair lifts and bike racks, facilitating mass-transit travel for a wide variety of riders.

As development matures within Section 19, sufficient demand may be generated to support additional bus lines or a change in routes to stop at two or three locations within the Mixed-Use Core or Residential planning areas. Potential SunLine routes with conceptual stop locations are shown on Figure 20. The potential routes and stops shown could represent deviations from the existing route, an additional route option along an existing route during peak service times, or a completely new route. The ultimate route alignment and stop location will be determined by the SunLine Transit Agency (in coordination with the City of Rancho Mirage) as development of the project proceeds and needs and resources can be assessed.

Access could be taken from Bob Hope Drive or Dinah Shore Drive into the project site without deviating significantly from the remainder of the current route. The design of the Esplanade roadway, with its one-way street system, supports the introduction of a bus stop while maintaining safe and efficient vehicle and pedestrian circulation. A portion of the parkway between B Street and C Street along the northward traveling lane should be designed so that a future bus turnout (12 feet by 70 feet) could be introduced without disrupting the overall design and functionality of the Esplanade and any internal uses. The landscape treatment and hardscape elements should be able to function with or without a bus turnout.

The potential for a future multimodal transit station in PA 3.01 could provide a significant long-term opportunity for residents and commuters within the Coachella Valley. Close coordination will be required between the City of Rancho Mirage, the CVAG, the SunLine Transit Authority, and adjacent property owners to identify appropriate short- and long-term uses of the transit station property. Such uses could temporary or permanent parking, renewable energy generation, or a combination of the two.

Nearby planning areas should also develop in a manner that recognizes the site's potential future use as a transit station and avoid building orientation or other site designs that isolate or turn their backs to the station property. To encourage orientation and connectivity to the station property, PA 3.01 could also be initially developed as interim open space to be used by residents, employees, and visitors in the mixed-use PAs 1.07 and 3.02. Once a station is developed, the resulting design will retain the desired look and feel of coordinated, complementary development.

4.3.4 PEDESTRIAN AND ALTERNATIVE VEHICLE CIRCULATION

The overall intent and design of the Specific Plan is geared toward generating an environment that maximizes connectivity for pedestrians between the diverse uses within the Plan. At nearly 270 acres, however, the Section 19 Specific Plan is by no measure a small development. While pedestrian access



will be incorporated into all development, many internal and external trips cannot or will not be undertaken by foot. Accordingly, the Specific Plan facilitates golf cart and bicycle travel throughout the site. Golf carts and bicycles present viable means of transportation for residents to travel within the site and into the surrounding community.

Pedestrian Circulation

Pedestrian circulation is provided by the 4- to 8-foot-wide sidewalks provided along all internal and perimeter roadways, as shown on Figure 21. Pedestrian circulation within each planning area will not be determined until site plans are developed by each property owner as the project builds out. All development, however, must be designed to facilitate pedestrian access to surrounding planning areas. Particular emphasis should be placed on providing enhanced pedestrian access to the Center Street esplanade, which will function as a primary pedestrian corridor within the site. The conceptual illustratives provided in Chapter 6, *Design Standards and Guidelines*, illustrates how pedestrian connections within planning areas and along street roadways would connect open space areas throughout the Section 19 project.

Additionally, strong pedestrian connections to a possible future multimodal transit station should be incorporated into the design of PAs 1.06 and 1.07. Although the facility may not be constructed for many years, it is important to incorporate pedestrian connections as development occurs to ensure the public transportation facility is accessible when ultimately developed. Chapter 6 provides direction on how to implement pedestrian-oriented development.

Alternative Vehicle Circulation

Class I bikeways and golf cart paths are provided along Bob Hope and Dinah Shore Drive as off-street pathways that allow bicyclists, golf carts, and pedestrians to travel along the same route. These routes, also shown on Figure 21, will connect to the City's golf cart circulation system, which provides paths along the south side of Dinah Shore Drive west of Bob Hope Drive. Class I bikeways and golf cart paths are also provided along the Center Street entryway to facilitate entry into the project for all residents and visitors.

Class I bikeways should also be constructed along both sides of the drainage channel to facilitate connection to a future Class I bikeway identified in the City's General Plan Conservation and Open Space Element along land just south of I-10, starting near the future Bob Hope Drive freeway interchange and traveling northwest to 30th Avenue. A bikeway constructed along the drainage channel would also enable access to a future Class I trail (Mid-Valley Bikepath Project) proposed by the City of Palm Desert, to be located along land just south of I-10 starting from the Palm Desert city boundary and traveling southeast to Washington Street.

Class II bikeways and golf cart routes provide a striped lane for one-way golf cart and bicycle travel on Key Largo Avenue and A Street. The Class II facilities extend from the Class I pathways to provide dedicated access to the project's residential and mixed-use interior. A connection between the Class II bikeway along Key Largo Avenue extension and the Class I bikeway along the drainage channel is desirable if it is feasible to incorporate a connection into the future overpass design for Key Largo Avenue.



One Class III bikeway is provided along North Street to identify a route for bicyclists to access a possible future multimodal transit station. This route also provides an additional or alternative path for connecting to the Class I bikeway along the drainage channel. Class III bikeways are designated but unmarked bike routes on the street within vehicular travel lanes. Golf carts are expected to gain access to the majority of the site's uses by traveling along Class I and II facilities and through parking lots and smaller access roads within planning areas. Additional internal access may be provided to golf carts on streets with designated speed limits no higher than 25 miles per hour. Figure 21 identifies potential roadways that may be designed with lower posted speed limits.

The Specific Plan also envisions that the residents of Section 19 may purchase Neighborhood Electric Vehicles (NEVs) to make short trips to run errands, visit recreation facilities, or meet with friends. NEVs are public street-approved vehicles that have no emissions and can travel at a maximum speed of 25 mph. In contrast with golf carts, NEVs are able to travel on city streets with posted speed limits of 35 mph or less, and can cross intersections of roadways with higher posted speed limits (per California Vehicle Code Section 385.5).

Commuter information boards should be placed at appropriate locations in each planning area identifying paths, routes, and schedules for alternative vehicles and public transit within Section 19 and throughout Rancho Mirage. This does not apply to Planning Areas 3.05–30.7 and only one commuter information board is necessary for Planning Areas 2.07–2.09. Together, the system of pedestrian and alternative vehicle pathways would provide multiple forms of access to development within the project site, including the future multimodal transit station, as well to off-site landmarks such as the Agua Caliente Casino · Resort · Spa; future residential neighborhoods; and other commercial nodes, tourist-oriented facilities, and employment centers such as resort hotels.

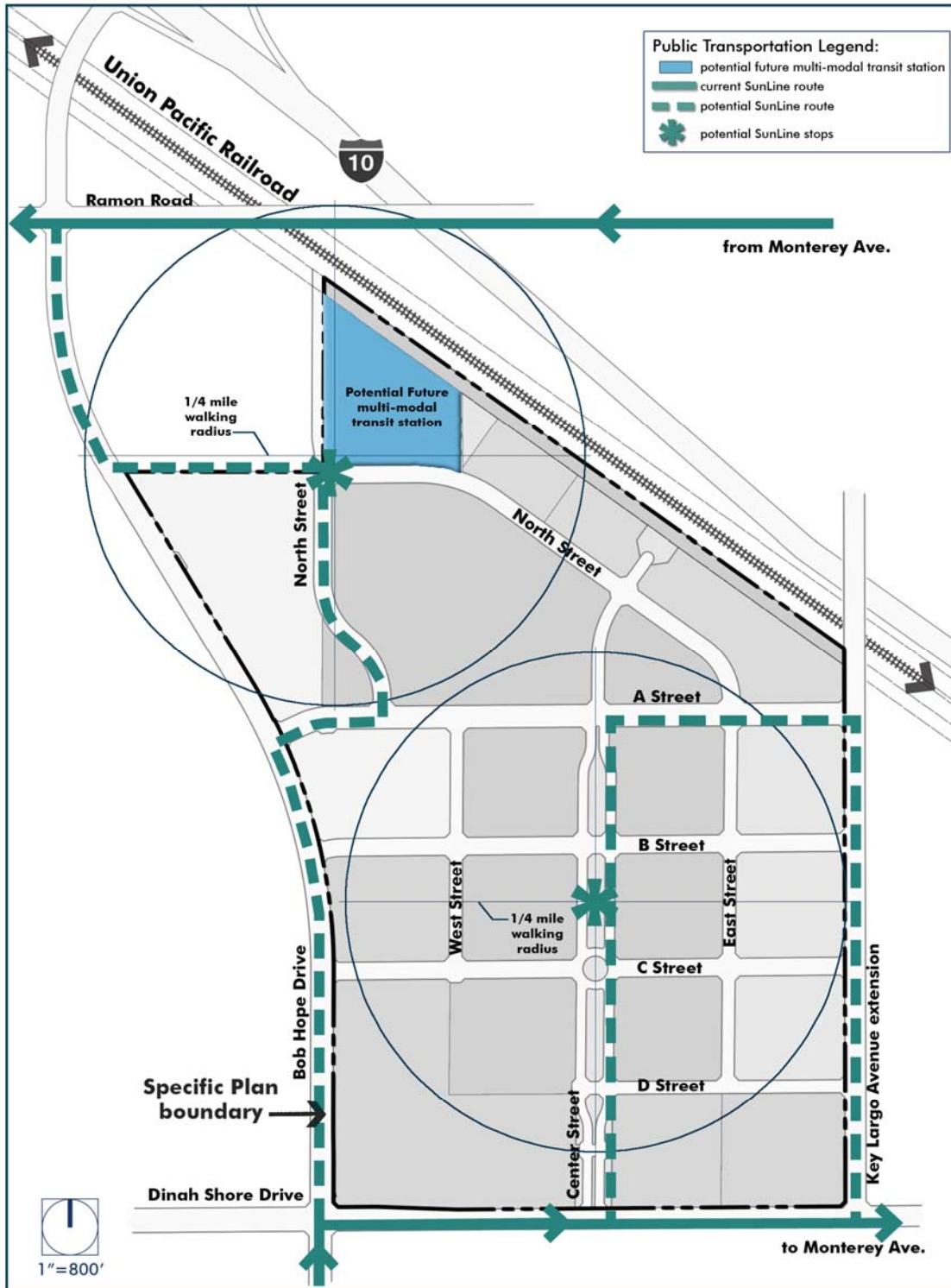


Figure 20 Public Transportation

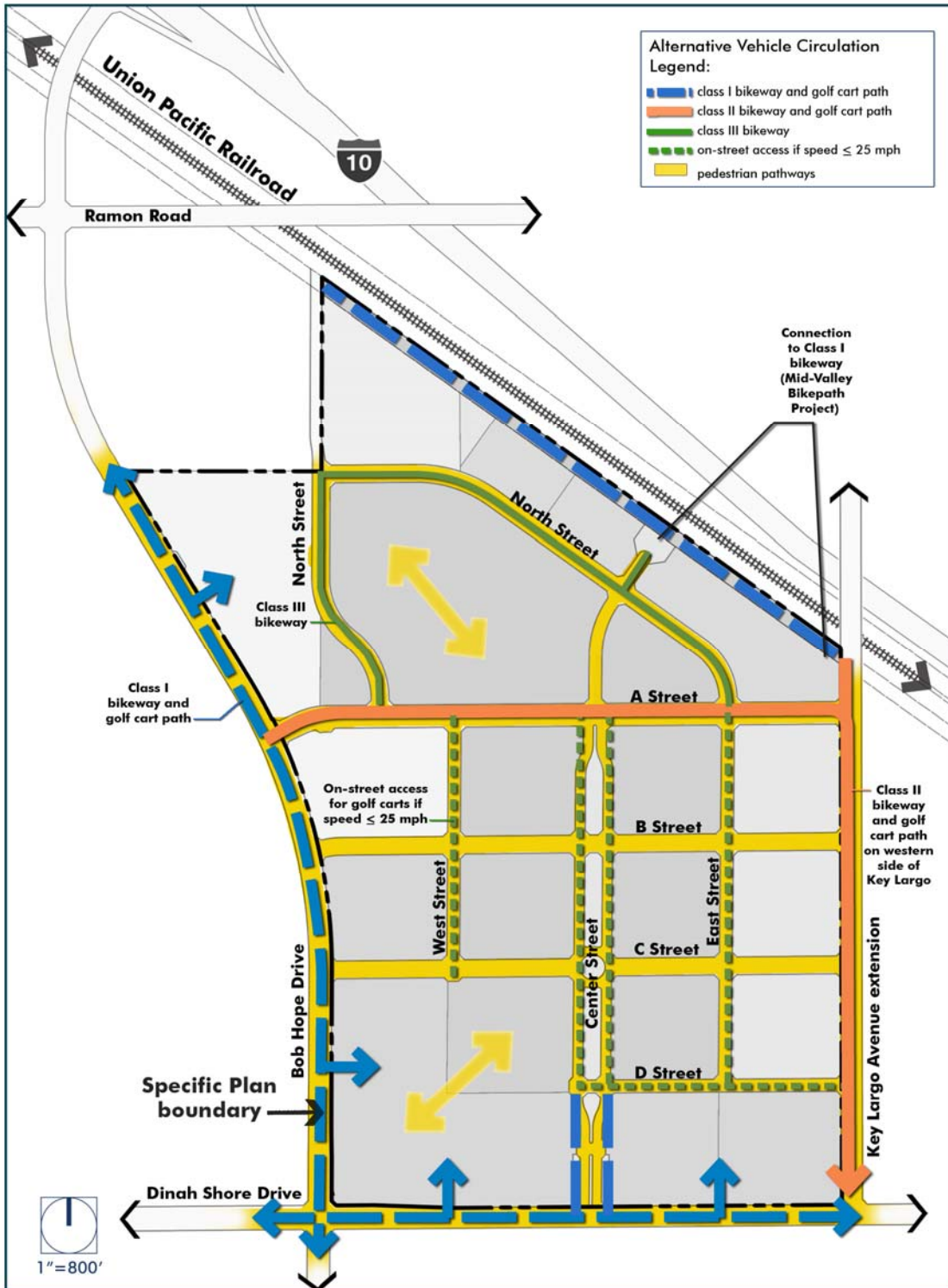


Figure 21 Pedestrian and Alternative Vehicle Circulation



4.4 OPEN SPACE PLAN

The Section 19 Specific Plan supports development that is more compact and urban than the low-scale, resort-oriented patterns currently found in Rancho Mirage. With the clustering of buildings, the Section 19 project has an opportunity to provide smaller, more intimate plazas and elaborate streetscapes, while also offering larger parkland settings for future residents and visitors.

The Conceptual Open Space Plan shown in Figure 22 illustrates the different types of open space envisioned by planning area. The Plan identifies the precise location of only a portion of the open space, to enable each planning area to design and accommodate open space areas in the manner that best serves individual projects. The tailored open spaces of each project and planning area should, however, connect to adjacent open spaces through means such as greenbelts or landscaped pedestrian walkways.

4.4.1 PARKS

Parks should reflect the natural desert context of Rancho Mirage, but may use landscaping such as turf to provide places for people to sit, relax, and gather. Parks should incorporate public art and water features combined with enhanced vegetation to serve as small oases within the development. The dual use of parkland as both recreation areas and retention basins is highly encouraged to maximize developable land and satisfy on-site retention requirements.

The City park requirement for residential development of three acres of park per 1,000 residents will be met through a combination of land dedication, improvements, private recreation, and in-lieu fees in accordance with Section 16.18.060 of the Rancho Mirage Municipal Code (RMMC). Buildout of the Specific Plan will result in the addition of approximately 3,703 residents to the City of Rancho Mirage, which translates into a total of 11.1 acres of required parkland.

The Specific Plan sets aside approximately 7 acres of usable parkland, shown as esplanade/park space in Figure 22. The remaining 4.1 acres of parkland as described below (5.6 acres if the drainage channel cannot be developed as trails) should be developed within each planning area proposing residential land uses, although the payment of in-lieu fees or dedication of land is permitted. The provision of parkland on-site is preferred and developments may satisfy parkland requirements by consolidating parkland into one or more locations. Planning Areas 3.05 and 3.07 should be developed as dual usage retention basin facilities that can also be safely used as park facilities. If they cannot be used as park facilities, the parkland requirements shall increase accordingly.

Approximately 10 feet on both sides of the drainage channel in PA 3.07 could be used for pedestrian or bicycle trails. Traveling nearly 3,400 linear feet along the northern edge of the Specific Plan boundary, these trails could generate approximately 1.5 acres of recreational open space. They should ultimately connect to a future Class I bikeways and trails in Rancho Mirage and Palm Desert (see Section 4.3.4). Table 2 identifies the amount of parkland required, programmed into planning areas, and the balance to be provided in other planning areas or satisfied through the payment of in-lieu fees.



Table 2
Provision of Park Space

Projected Park Requirement	11.1 Acres
Programmed Parkland	7.0 Acres
Planning Area 2.07	0.9 Acres
Planning Area 2.08	1.1 Acres
Planning Area 2.09	2.0 Acres
Planning Area 3.05	0.9 Acres
Planning Area 3.06	1.5 Acres
Planning Area 3.07	0.6 Acres
Balance to be Provided in other Planning Areas or In-Lieu Fees	4.1 Acres

Esplanade

The 4.0-acre esplanade is one of the defining features of the Section 19 Specific Plan. Bisecting Center Street, the esplanade is the project's spine, supporting active recreation space and providing visual relief from the surrounding urban spaces. At 94 feet in width, the esplanade is also large enough to accommodate small commercial uses and community gathering spaces. It is envisioned that the esplanade will include a variety of spaces and features, each section emphasizing a different aspect of the desert environment. A conceptual illustration of the esplanade is shown in Figures 23 and 24. Three desert themes that should be reflected throughout the esplanade are explained in greater detail below.

The Tranquil Desert. The esplanade should include elements and spaces that epitomize the quiet, tranquil aspects of the desert environment. This may include a desert flower garden, shaded amphitheater area, and natural gas fireplaces where people can wander, sit, and relax day and night. The esplanade may also include a large-scale water feature that consists of a series of ponds, streams, and a recirculated cascade that winds down Center Street, possibly transitioning into additional open space areas in PA 1.07. Additional water features can be incorporated at gathering spaces or be used as landmarks.

The Artistic Desert. A large variety and number of public art pieces should be woven into the esplanade fabric. Functional art is highly encouraged, such as a grouping of wind microturbines that generate energy (e.g., to power lighting or water features), or a grouping of rock features that can be used as a rest area. Materials should reflect the desert environment and be able to withstand the effects of strong winds and blowsand. Public art installed in the esplanade shall contribute toward the public art requirement of adjacent buildings (as well as other buildings as permitted by Section 5.3.5 of this specific plan).

The Edible Desert. The esplanade should incorporate landscaping that is attractive and functional. Native fruit trees should be planted in groves or interspersed with decorative trees that provide shade for pedestrians walking along the esplanade. While the produce from the edible trees could not be expected to be substantial, it would speak to the Coachella Valley's rich agricultural history and could encourage themes for annual festivals or farmers' markets. A small organic desert garden could also



be introduced in conjunction with a small culinary store or restaurant (as an independent business or an extension of a restaurant across the street) to serve freshly grown native food in a unique setting. A sampling of edible landscaping can be found in Table 3.

Residential and Resort Open Space

In addition to parks, residents living in Section 19 will enjoy a combination of common and private open and recreation spaces within the project boundaries. Amenities such as pools, clubhouses, plazas, courtyards, lawn areas, and jogging paths are just some of the features that could be provided. Many of the open spaces in residential locations may be private to ensure secure, unfettered access for residents. Resort projects such as hotels will incorporate many of the same features as residential projects, although they may be combined with or placed alongside ancillary commercial uses such as day spas. Residential and resort developments are also encouraged to provide spaces that are accessible to the general public, such as paths or greenbelts that connect to open spaces in adjacent planning areas.

Commercial and Mixed-Use Open Space

Public open spaces are also important components for the commercial uses in Section 19. Spaces such as walkways, multipurpose paths, enhanced streetscapes, and plazas provide gathering spaces for people shopping, eating, or just enjoying the atmosphere. These spaces are an especially important feature of the mixed-use core area, where spaces serve residential, retail, and office users throughout the day and night. The more compact form of mixed-use projects attracts tenants and residents who enjoy smaller, more urban open spaces. These open spaces favor landscaping dominated by potted plants and ornamental trees, combined with intricate hardscape elements.

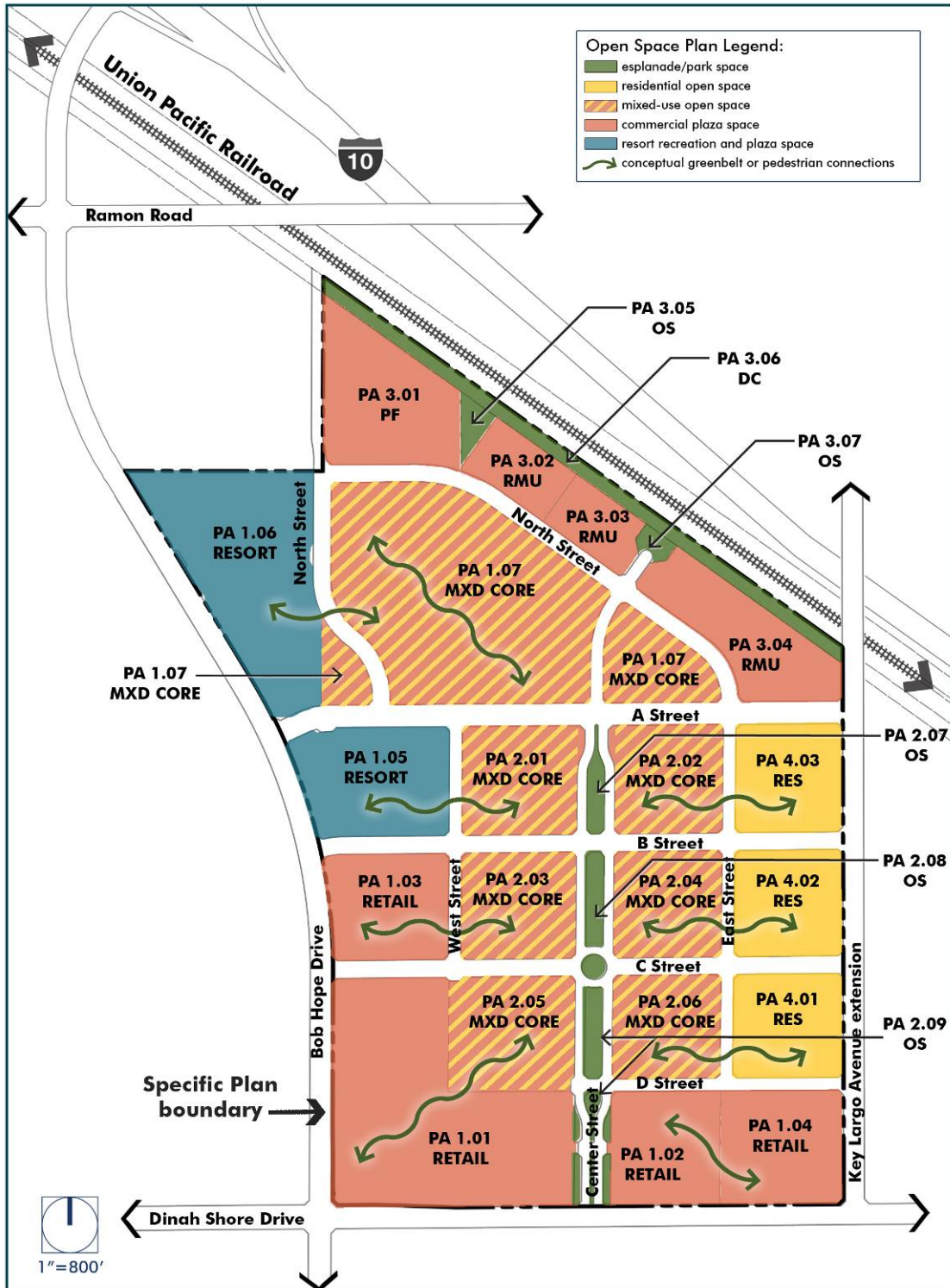


Figure 22 Conceptual Open Space Plan



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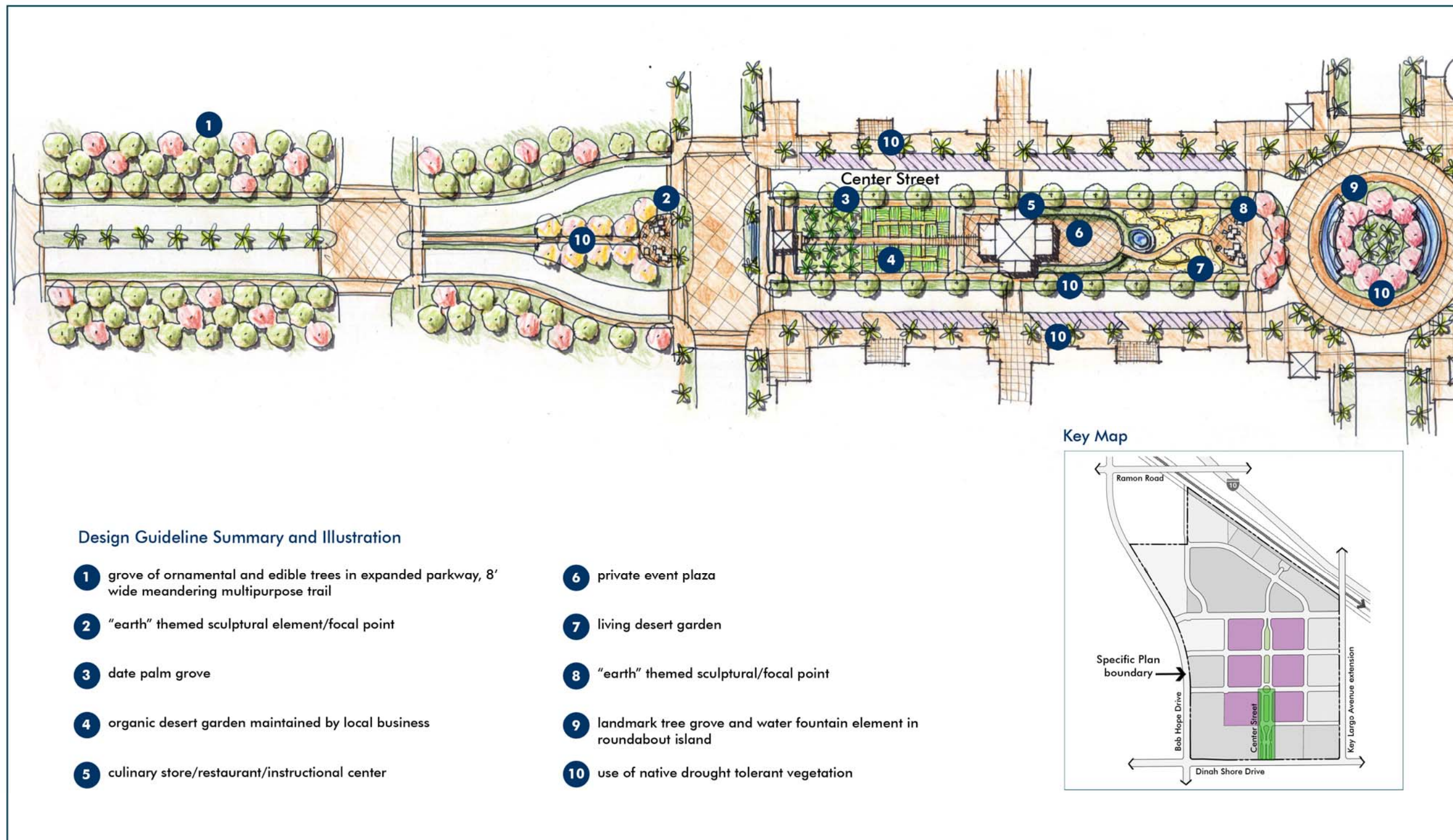


Figure 23 Conceptual Esplanade Illustrative (south of C Street)



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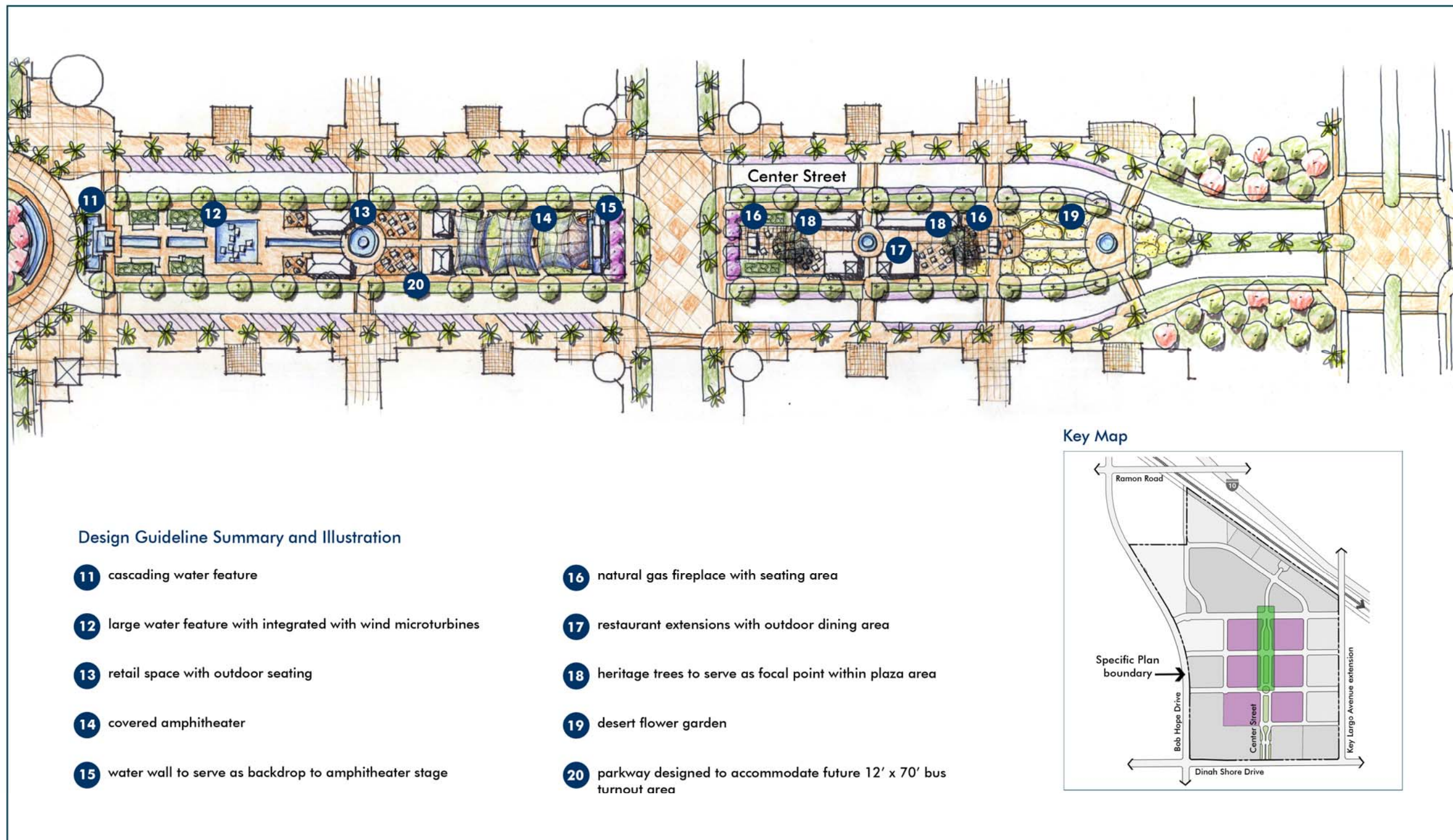


Figure 24 Conceptual Esplanade Illustrative (north of C Street)



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4.5 LANDSCAPE PLAN

The landscape design will establish an identity and theme for the project and will be an overall unifying element, transcending parcel boundaries and defining open space areas. The Landscape Plan provides guidelines for the treatment of areas within the Section 19 Specific Plan, including the surrounding streets, parkways, development edges, project entries, and open space areas. The landscaping theme is influenced by the climate of the Coachella Valley, where native and drought-resistant plants are emphasized. The Plant Palette presented in Table 3 provides a selection of desert-friendly trees and landscaping for the project. Landscaping is not limited to this list, but should fulfill the intent of the Section 19 Landscape Plan by selecting similar plantings that respect native plant species and are compatible with the Coachella Valley climate.

The Landscape Plan implements the applicable Community Design Element policies of the Rancho Mirage General Plan by distinguishing a hierarchy of roadways and identifying key intersections surrounding the Specific Plan area. Accordingly, each landscape treatment should incorporate a distinct theme tree to further define the different areas and roadways within the Specific Plan. Rock gardens and water features may also be used to enhance the landscape elements.

In addition to providing landscape guidelines for roadways and intersections, the Landscape Plan provides direction for landscaping within public open spaces, including the landscape buffer between the project area and the Union Pacific Railroad. The landscape treatments for these areas within the Section 19 project are delineated in Figure 25.



The Landscape Plan includes native and other plants that are compatible with the Coachella Valley climate.

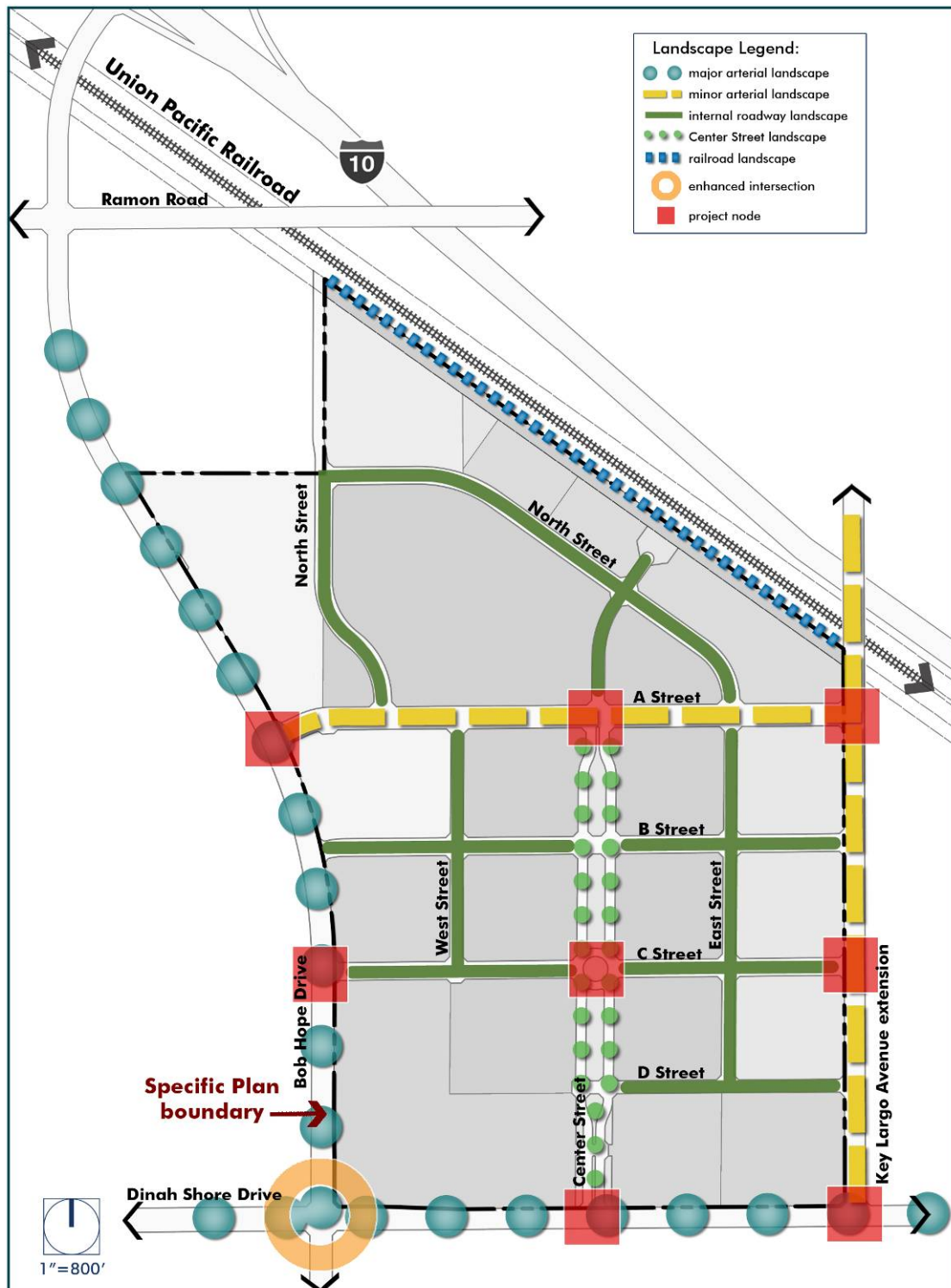


Figure 25 Landscape Plan



**Table 3
Plant Palette**

Common Name	Scientific Name	Common Name	Scientific Name
Major Arterial Landscape		Wind Resistant Landscaping: Flowers	
Date palm	<i>Phoenix dactylifera</i>	Desert marigold	<i>Baileya multiradiata</i>
Chilean mesquite	<i>Prosopis chilensis</i>	Blue flax	<i>Linum perenne</i>
Minor Arterial Landscape		Desert lupine	<i>Lupinus sparsiflorus</i>
Palo verde	<i>Cercidium floridum</i>	Desert poppy	<i>Eschscholzia parishii</i>
Southern live oak	<i>Quercus virginiana</i>	Wind Resistant Landscaping: Grasses	
Ironwood	<i>Olneya tesota</i>	Deer grass	<i>Muhlenbergia rigens</i>
Internal Roadway Landscape		Mexican thread grass	<i>Stipa tenuissima</i>
Tipu tree	<i>Tipuana tipu</i>	Bull grass	<i>Muhlenbergia emersleyi</i>
Railroad Landscape		Red fountain grass	<i>Pennisetum setaceum</i>
Pepper tree	<i>Schinus molle</i>	Wind Resistant Landscaping: Groundcover	
Africa sumac	<i>Rhus lancea</i>	Damianita	<i>Chrysactinia mexicana</i>
Intersection Treatment: Trees		Trailing lantana	<i>Lantana montevidensis</i>
Sweet acacia	<i>Acacia smallii</i>	Dwarf ruellia	<i>Ruellia sp.</i>
California fan palm	<i>Washingtonia filifera</i>	Periwinkle	<i>Vinca major</i>
Pindo palm	<i>Butia capitata</i>	Windbreak Trees	
Mediterranean fan palm	<i>Chaemaerops humilis</i>	Mondel pine	<i>Pinus eldarica</i>
Sissoo	<i>Dalbergia sissoo</i>	Arizona cypress	<i>Cupressus arizonica</i>
Aleppo pine	<i>Pinus halepensis</i>	Swamp malee	<i>Eucalyptus spathulata</i>
Intersection Treatment: Plants		African sumac	<i>Rhus lancea</i>
Agave	<i>Agave desmettiana</i>	Edible Landscaping	
Mexican grass tree	<i>Dasyllirion longissimum</i>	Almond tree	<i>Prunus dulcis</i>
Red yucca	<i>Hesperaloe parvifolia</i>	Apricot tree	<i>Prunus armeniaca</i>
Yucca pendula	<i>Yucca recurvifolia</i>	California amaranth	<i>Amaranthus californicus</i>
General Use: Shrubs		Date palm	<i>Phoenix dactylifera</i>
Desert cassia	<i>Cassia nemophila</i>	Pinyon pine	<i>Pinus edulis</i>
Mexican bird of paradise	<i>Caesalpinia pulcherrima</i>	Prickly pear cactus	<i>Opuntia spp.</i>
Texas ranger	<i>Leucophyllum spp.</i>	Ruby red grapefruit tree	<i>Citrus Paradisi</i>
General Use: Groundcover			
Bougainvillea	<i>Bougainvillea spp.</i>		
Green carpet	<i>Carrisa grandiflora</i>		
Purple lantana	<i>Lantana montevidensis</i>		



4-44



Date palm (in rows)



Date palm (fruit)



California fan palm



Aleppo pine



Chilean mesquite



Palo verde



Ironwood



Tipu tree



Pepper tree



Sissoo



Red yucca



Green carpet



Texas ranger



Rosemary

*Almond tree**Apricot tree**Citrus varieties**California amaranth**Pinyon pine**Prickly pear cactus*

Major Arterial landscaping should be highly visible, such as these date palms, and distinct from adjacent landscaping.

4.5.1 MAJOR ARTERIAL LANDSCAPE

The Major Arterial roadways along the western and southern edges of the Section 19 Specific Plan area should incorporate a landscaping theme that reflects their principal status and distinguishes these streets from the surroundings. The use of tall and decorative primary street trees should create a dramatic background for residents and visitors along these major corridors. Tree plantings should be formal and consistent along the length of the street, creating a rhythmic streetscape for residents and visitors. Landscape enhancements shall be completed as City projects.



Example of informal plantings of shrubs and groundcover.

4-46



Shrubs and groundcover should complement the primary street trees.

Rancho Mirage General Plan, Dinah Shore Drive is delineated with Special Arterial Landscaping. Similar to Bob Hope Drive, the landscape theme for Dinah Shore Drive should incorporate these objectives from the General Plan.

4.5.2 MINOR ARTERIAL LANDSCAPE

The Minor Arterial Landscape treatment highlights the secondary spine roads for the Section 19 Specific Plan. Landscaping for these thoroughways should reflect the Plant Palette in Table 3 and complement the adjoining Section 19 development. Primary street trees should be planted in a formal manner, with more informal plantings of shrubs and groundcover allowed underneath.

Median treatment should include hardy plant specimens that complement the primary street tree, also consistently planted to create an interesting visual pattern along the roadway. Landscaping along these edges should preserve and enhance strategic view corridors into the adjacent project areas to take advantage of frontage along these major traffic corridors.

Bob Hope Drive

The major street tree for Bob Hope Drive should be the California fan palm, interspersed with smaller drought-resistant specimens. Additional shrub and groundcover landscaping should complement the primary street tree and adhere to the Major Arterial Landscape theme.

The Rancho Mirage General Plan designates Bob Hope Drive as a part of the Boulevard Streetscape plan under the Community Design Element. Bob Hope Drive is also demarcated with Gateway Landscaping. These categories highlight Bob Hope Drive as a major entry to the City and one of the City's primary commercial corridors. The intent of these designations should be integrated within the final landscape theme for Bob Hope Drive.

Dinah Shore Drive

The Mesquite, Palo Verde, or similar tree should be the primary street tree for Dinah Shore Drive. Under the



4.5.3 INTERNAL ROADWAY LANDSCAPE

The minor collector and local streets within the Section 19 Specific Plan should integrate a landscape theme that creates a distinct identity for each planning area. Canopy trees should be used as the primary street trees, to provide shade and help reduce the deflection of heat into nearby buildings. Layered landscaping that integrates shrubs and groundcover should complement the primary street trees. The type and intensity of development envisioned along Center Street justifies a specialized landscaping treatment.

Center Street

The development along Center Street and the Esplanade will reflect the most intense character of any development in the Specific Plan, with multistory buildings facing directly onto this central corridor. The landscape theme should be more urban, integrating the use of potted plants and tree grates along pedestrian walkways. Primary tree plantings should include a mix of shade and accent trees strategically placed to generate a consistent streetscape and clustered to highlight a special area or provide an abundance of shade.

The roundabout at the intersection of Center Street and C Street will be one of the most unique components of the project, highly visible to both vehicles and pedestrians. The roundabout should incorporate a creative blend of public art, hardscape elements, and landscaping to celebrate the project and provide a key landmark for the project's residents, visitors, and employees.

4.5.4 INTERSECTION TREATMENT

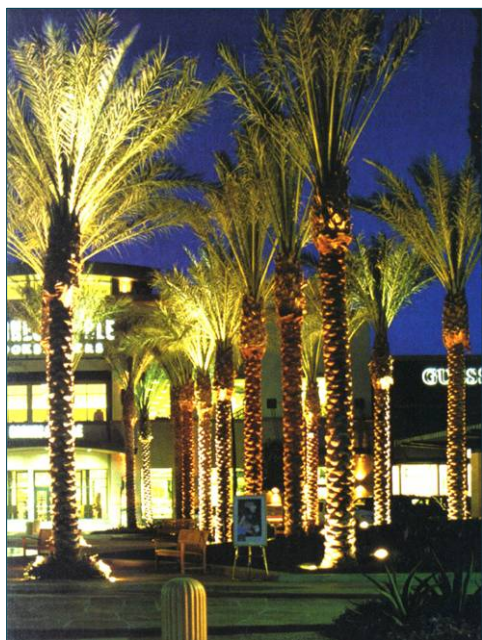
The landscape treatment for intersections demarcated on the Landscape Plan should incorporate a higher quality of landscape design than the adjoining roadway treatments to highlight these areas within the Section 19 Specific Plan. Landscaping should be more intense and incorporate taller plantings at these intersections to create an oasis effect. The vertical plant elements and intensification of landscaping draws the attention of passing residents and visitors and creates a dramatic sense of arrival. The landscape treatments should



Canopy trees should be the signature tree for the Internal Roadway Landscape treatment.



Landscaping for the Center Street should complement and enhance landscape for the system of linear parks.



Groupings of vertical plants create the oasis effect characteristic of Intersection Treatments.



complement and accent the themed signage, lighting, and hardscape elements within these areas to produce an organized visual appearance.

Enhanced Intersection

The intersection of Bob Hope Drive and Dinah Shore Drive has been cited as an Enhanced Intersection under the Community Design Element, and also demarcated with a Special Overlay Land Use under the Rancho Mirage General Plan. Together, these policies call for widened setbacks, special paving, and coordinated landscape treatment at all four corners. The Special Overlay District standards in Chapter 17.12 of the City's Zoning Ordinance include the setback requirement that structures be a minimum of 122 feet from the corner curb (see Section 5.1.2, *General Development Standards*, for more information regarding the Special Overlay District policies). The landscaping for this intersection shall be consistent with these standards, in addition to adhering to the landscape theme depicted in the previous paragraph for the Intersection Treatment, to provide a focal point at this location. Tall trees such as the date palm, queen palm, should be used to distinguish the intersection from the surrounding streetscape.



Example of an accent tree, Sweet Acacia, for Project Node landscaping.



Dense landscaping should buffer Section 19 development from the adjacent railroad easement.

Project Node

The landscape design for project nodes should highlight the entry points and primary intersections within the project. Although the landscaping at these locations will integrate taller trees and intense plantings, these elements should not overwhelm the surrounding development and should be smaller than those at the Enhanced Intersection. Landscape for project nodes should incorporate more colorful and floral plantings for emphasis.

4.5.5 RAILROAD AND CHANNEL BUFFER

This landscape buffer encompasses the drainage channel in PA 3.06 and the UPRR easement. This buffer area should integrate informal plantings, with a concentration of low shrubs and hedges to screen potential walls or fences and railway activity along the railroad easement. The tamarisk trees currently landscaping the railroad easement are not compatible with the desert environment and should be replaced by desert-friendly trees and shrubs. This includes multi-trunk trees that shall be allowed to branch to the ground to maximize the buffering effect. Low shrubs should also be used to deliberately screen undesirable views.

The drainage channel should be similarly landscaped and buffered if access is restricted and the channel is used solely for drainage purposes. If the channel edges are opened up for use as



recreational trails, the landscape treatment should integrate informal plantings of low shrubs and groundcover interspersed with native canopy trees.

4.5.6 OTHER INTERNAL LANDSCAPING

Parking Areas

Each parking area within the Section 19 project should incorporate a specific primary tree that is distinct from other parking areas in the development. These trees may be clustered at key locations in the parking areas or spread throughout the parking areas. The placement of trees and other landscaping should emphasize the creation of shaded parking spaces at full maturity. Where there is a pedestrian passageway through a parking area, formal landscaping should be used to highlight these areas and direct pedestrians towards their destinations.



Landscaping within parking areas may be clustered at strategic locations.

Parks

Landscaping for the park areas should pull from the identified plant palette; however, additional native plants may be used to promote variety. Plantings should be clustered together for a natural feel, with designated open areas to encourage more active recreational activities. Dense plantings along park walkways should provide a rhythm of concealing and revealing views in and out of open space areas. Clusters of canopy trees should be strategically located to provide shade for areas and protect them from the sun for extended periods of the day. Setback and sidewalk dimensions along the edge of parks are addressed in the Esplanade roadway section in Figures 16 and 17.



Park area landscaping should provide open areas for recreation.

Pedestrian Plazas

Pedestrian plazas should provide landscaping that is attractive and highly functional. For example, large planters may include wide edges that provide additional seating areas, in addition to accommodating canopy trees to offer shade for pedestrians. These areas should integrate an urban landscape treatment, with the use of planters and tree grates that complement the possible seating areas, fountains, and public art pieces in each plaza. Section 6.6 contains additional guidelines relating to the hardscape components within pedestrian plazas.

Well Areas

The Section 19 project area will contain well sites that should be appropriately screened with the use of landscaping and fencing. See Section 4.7.1 for a description of the capacity and location of these well sites. The design of the well sites is subject to standards imposed by the Coachella Valley Water



District, which generally require the sites to be secure areas. As a result, a system of fences or gates (possibly including razorwire or similar fencing alternatives) and security cameras is used to discourage trespassers. A combination of groundcover, shrubs, and trees should screen the enclosing fence arrangement from adjacent development. The intense planting of hedges and shrubs should conceal undesirable views, giving off the appearance of a grove of trees to passing residents and visitors, rather than the appearance of a security-enforced well site.

4.6 GRADING CONCEPT

Grading for the Section 19 Specific Plan is tailored to take advantage of the views and changes in elevation presented by the existing topography of the project site. The primary objectives of the grading concept are to (a) create a series of stepped development pads that create valuable views; (b) obscure the more intense development of the mixed-use core from the surrounding development; (c) ensure efficient access between the perimeter roadways and all planning areas, particularly those in the center of the site; and (d) allow cut and fill to be balanced within each planning area, precluding the need to import or export significant amounts of existing soil across planning areas or individual property boundary lines.

The grading analysis considered the general slope of the site from southwest to northeast and the inclusion of two proposed basins at the north end of the site to receive and retain surface stormwater runoff. The proposed concept grading plan shows the general locations, sizes and elevations of the various pads and their perimeter slopes. Access to these pads from the adjacent roadways would be determined when more details regarding the future developments are better known.

The preliminary earthwork analysis done as part of this conceptual grading determined that the cut and fill quantities each would be on the order of approximately 1.4–1.5 million cubic yards. This earthwork quantity was calculated without consideration of potential shrinking or bulking of the quantities or remedial work associated with the specific on-site soil conditions determined from a geotechnical investigation of the site. Also, very preliminary analysis by others was used to size the detention basins shown. A more thorough drainage analysis is recommended to establish the actual size of the detention basin requirements for the site.

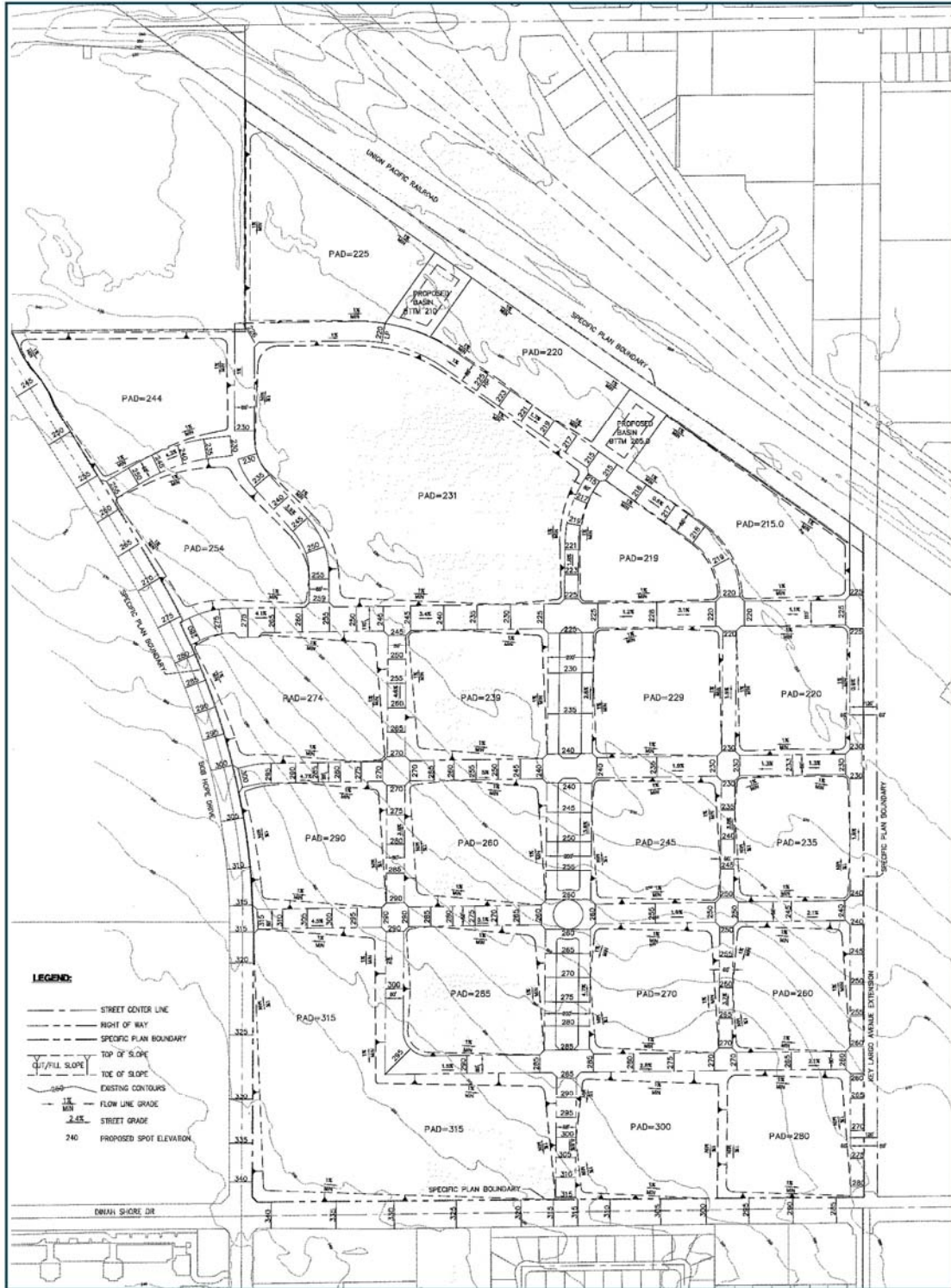


Figure 26 Conceptual Grading Plan



4.7 PUBLIC FACILITIES AND UTILITIES PLAN

4.7.1 WATER

The Coachella Valley Water District (CVWD) will provide water service for the Section 19 Specific Plan. At buildout, the Specific Plan will need about 600 acre-feet of water per year (0.54 million gallons per day or about 8,000 gallons per minute). The potable water will be provided by five on-site groundwater wells. Each well, along with an associated pump station, will be capable of producing about 1,800 to 2,000 gallons per minute. The proposed conceptual water plan, designed to accommodate the Specific Plan's domestic water demand, is depicted in Figure 27. The location of each well site is conceptual and is subject to standards and requirements imposed by the CVWD. This includes a prohibition against the location of a domestic water well site within 500 feet of existing petroleum transmission mains located at the northern edge of the project boundary. Accordingly, well site 1 is proposed to be located on the southwest corner of Planning Area 3.01, which is beyond the minimum horizontal separation distance of 500 feet from the petroleum transmission mains.

Storage capacity of 6 to 7 million gallons will be provided off-site from the proposed reservoir 4603. The 30-inch transmission main from the proposed reservoir will be connected to an existing 36-inch water main in Bob Hope Drive at Ramon Road. The on-site water distribution system will include pipes ranging from 8 to 18 inches in diameter.

A Water Supply Assessment and Water Supply Verification Report (WSA) for the Specific Plan were prepared based on a Coachella Valley Management Plan, Urban Water Management Plan, and Water System Backup Facilities charge study dated July 2006. The proposed conceptual potable water master plan is consistent with the WSA.

4.7.2 SEWER

The CVWD will provide sewer service for the Section 19 Specific Plan. The sewage collection system has been designed using design criteria from the CVWD's wastewater master plan. The sewage collection system will have gravity sewer mains ranging from 8 to 12 inches. The sewer flows from the Specific Plan site will be collected through these sewer mains located in the street rights-of-way and will discharge into the existing 15-inch sewer in Ramon Road. The sewer flows in the existing 15-inch sewer will gravity flow across the Union Pacific Railroad and I-10 into CVWD's Wastewater Reclamation Plant No. 7 for treatment and eventual disposal. The proposed conceptual sewer plan is shown on Figure 28.

The sewer generation rate for the project is based on the number of residential Equivalent Dwelling Units (EDU), using building square footage and frontage to determine EDUs. All sewer facilities were sized using the CVWD's peak demand. The project is expected to generate 859 acre-feet per year (0.77 million gallons per day or 535 gallons per minute) of sewer flows.



4.7.3 DRAINAGE

As shown on Figure 29, two retention basins would be developed in Planning Areas 3.05 and 3.07 and storm drains would be provided along Bob Hope Drive, the Key Largo Avenue extension, and the various roads proposed onsite. In addition to servings as basins for retaining water, the retention basins would be designed with earthen slopes to allow for the percolation of water and therefore allow for the treatment of the retained water as it infiltrates into the ground. The Specific Plan also establishes Planning Area 3.06 for the future development of the Mid Valley Drainage Channel. At these three planning areas, the drainage would flow into catch basins to be dropped into dry wells, and an easement would be provided so that they are maintained in perpetuity. Per City requirements, the on-site retention basins would be sized to retain the 100-year flood level. Gravity would direct the drainage flow from the southern portion of the project site to the northern portion. Precipitation, nuisance water, or storm-drain flows that fall onto streets would flow to the low points on the northern end of project site and eventually be deposited into the proposed retention basins.

A hydrology study conducted for the EIR indicated that the retention basins would need to be constructed to retain approximately 68-acre feet during a 100-year, 24-hour storm event. As constructed, the retention basins would provide for on-site retention for the entire Specific Plan project area and would not require each project or planning area to provide on-site retention as is currently required by Section 13.05.010 (Required on-site retention) of the RMMC.

In the event that it is not feasible to implement the construction of a project-wide assessment district and retention basin system to retain the stormwater runoff volumes from the entire project site, as analyzed in the EIR, on-site retention of each development project shall be provided in accordance with the requirements outlined in Section 13.05.010 of the RMMC.

Prior to the issuance of grading permits for any development in the Section 19 Specific Plan, the project applicant shall submit to the City Engineer a site-specific drainage plan and hydrology report that demonstrates that the proposed on-site retention system(s) will be able to retain the stormwater runoff volumes associated with the proposed development in accordance with requirements outlined in Section 13.05.010 of the RMMC.

4.7.4 SOLID WASTE

Burrtec Waste Industries provides solid waste management and disposal services for the development within the Section 19 Specific Plan. The majority of trash destined for disposal at landfills is taken to the Edom Hill Landfill, which is managed by Riverside County.

Waste Management also provides a resource-recovery/recycling service to the community. This includes provision of crates or other containers for the separation of cans, glass, and newsprint by residents for once-a-week curbside pickup. The City's recycling program is run in conformance with State of California Assembly Bill 939, which requires that every city and county implement programs to recycle, reduce at the source, and compost 50 percent of its solid waste. The Section 19 Specific Plan project shall use these recycling measures to divert a minimum of 50 percent of solid waste to assist the City and county in reaching and moving beyond the 50 percent diversion mandate.



4.7.5 ELECTRICITY

The Imperial Irrigation District (IID) supplies electrical power to the properties within the Section 19 Specific Plan. IID's service area lies north and south of I-10, mainly east of the Cities of Palm Desert and Indian Wells. Most of Rancho Mirage is not covered in the IID service area; however Section 19 lies just within the boundary of IID. There is an existing IID-owned substation near the project site, in the community of Thousand Palms, across I-10 at the intersection of Monterrey Avenue and Broadmoor Drive.

The increase in population and building space created by the Specific Plan would require IID to expand their existing service facilities. IID indicates that they would be able to provide service to the first phase of the proposed project using the existing substation. Development beyond the first phase, however, would require the construction of a new electrical substation to service the proposed project. As further indicated by IID, this new substation would not only be necessary to serve the proposed project, but also provide electricity to development in the overall Sections 19 (consists of land in Rancho Mirage and Palm Desert) and 30 (entirely within Rancho Mirage) areas. The location of this new substation has yet to be determined.

Southern California Edison (SCE) maintains electrical transmission lines (115 kilovolts) along Bob Hope Drive, beginning at Dinah Shore Drive and traveling north to "A" Street. Past "A" Street, the power lines are underground. As shown in Figure 30, the location of the power poles do not overlap or conflict with the access points for internal roadways that access Bob Hope Drive.

4.7.6 PUBLIC SERVICES

The surrounding area is largely developed and afforded all municipal services. Fire protection is provided to the City by the Riverside County Fire Department under contract to the California Department of Forestry. This includes fire fighters and certified paramedics. Fire protection and emergency medical response services will likely be provided by Rancho Mirage Station No. 69, which is on Gerald Ford Drive approximately one mile from the project site. This station is staffed with two paid personnel with one engine.

Police protection in the City is provided on a service contract basis by the Riverside County Sheriff's Department, which operates out of the Palm Desert Station. This station, located at 73520 Fred Waring Drive, would likely dispatch the personnel that would serve the project site. Police response times can vary significantly, depending on the location of patrol cars at the time of a call. The average emergency response time in Rancho Mirage is four minutes or less to any location.

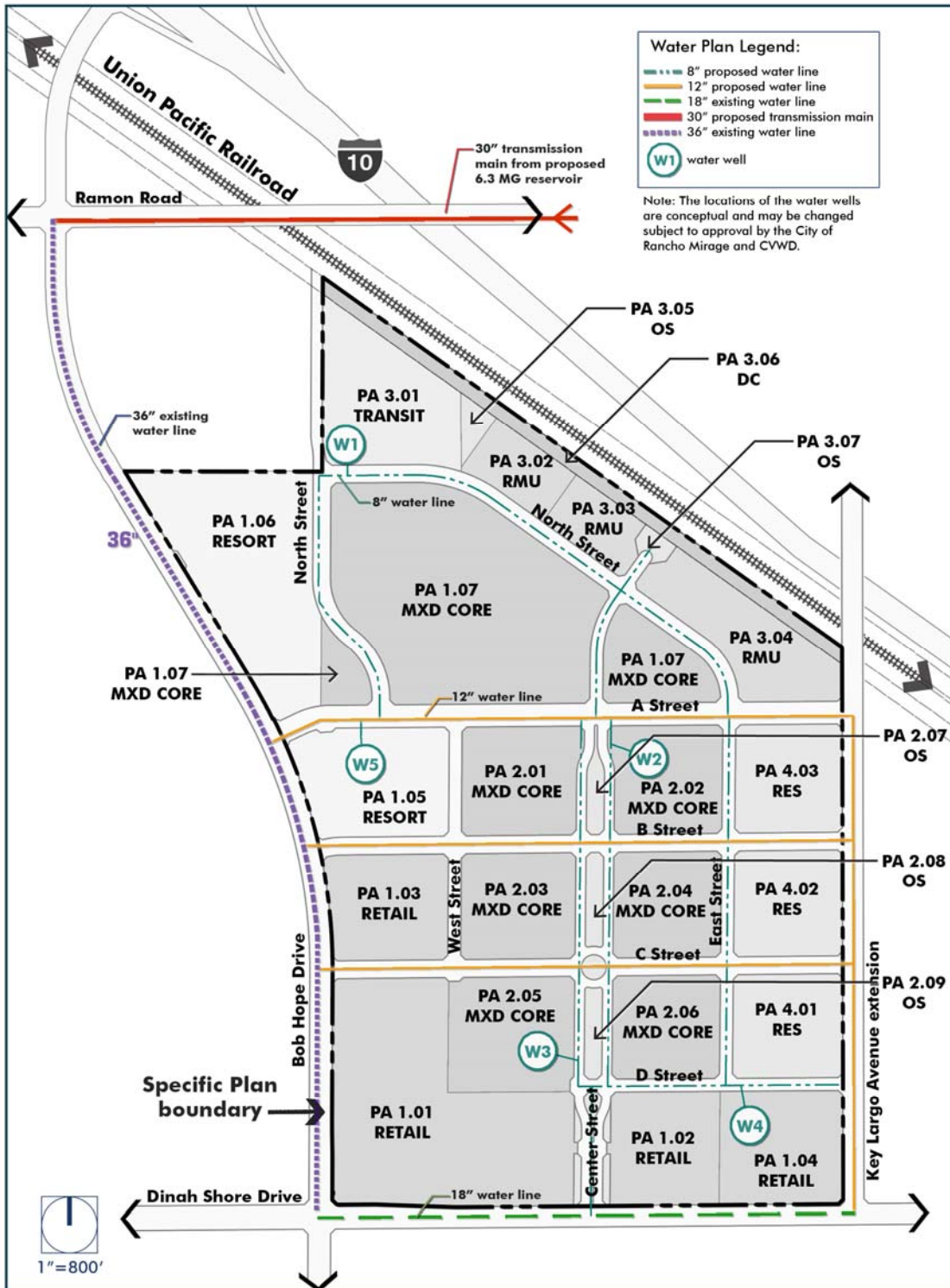


Figure 27 Conceptual Water Plan

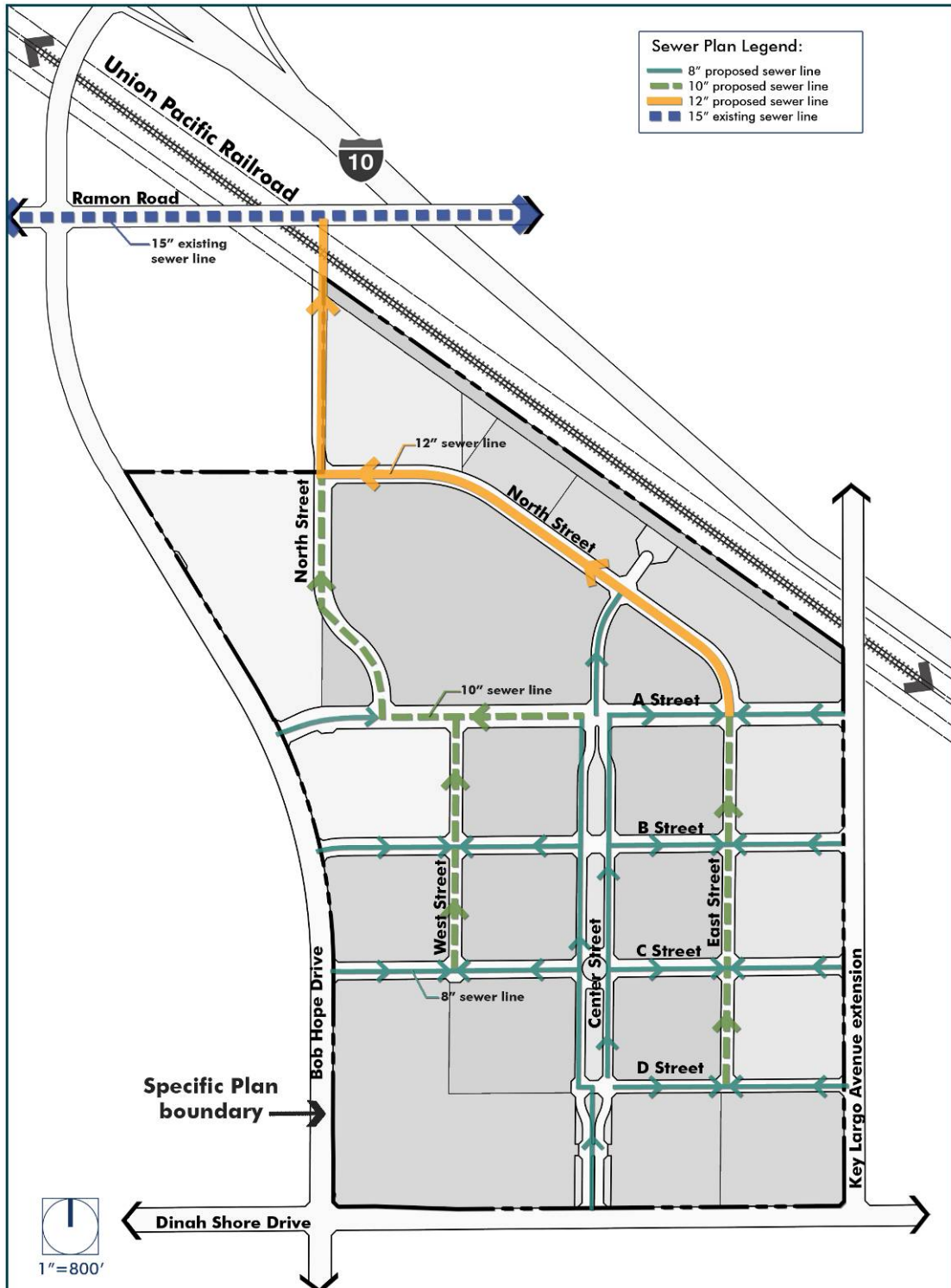


Figure 28 Conceptual Sewer Plan

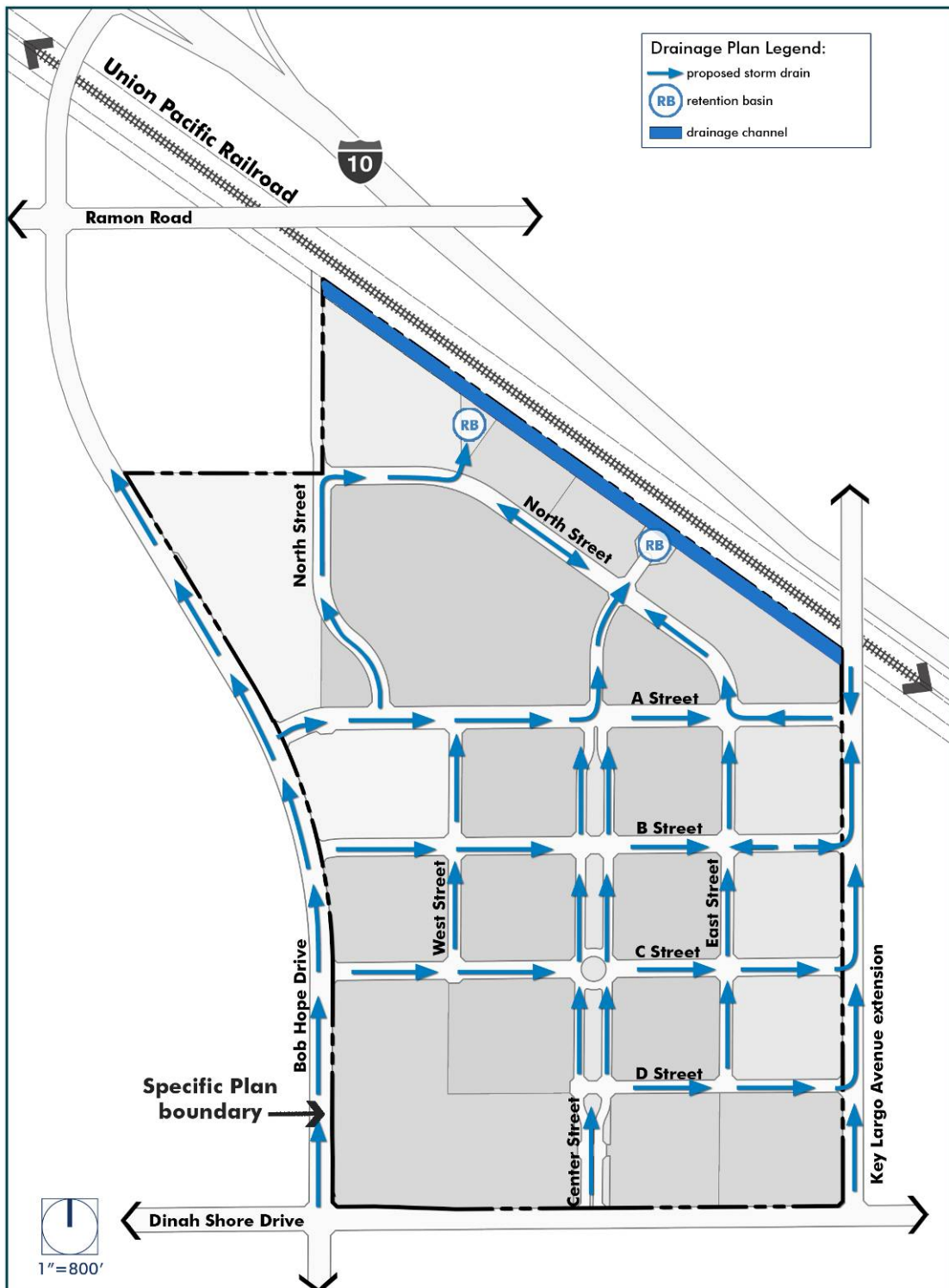


Figure 29 Conceptual Drainage Plan

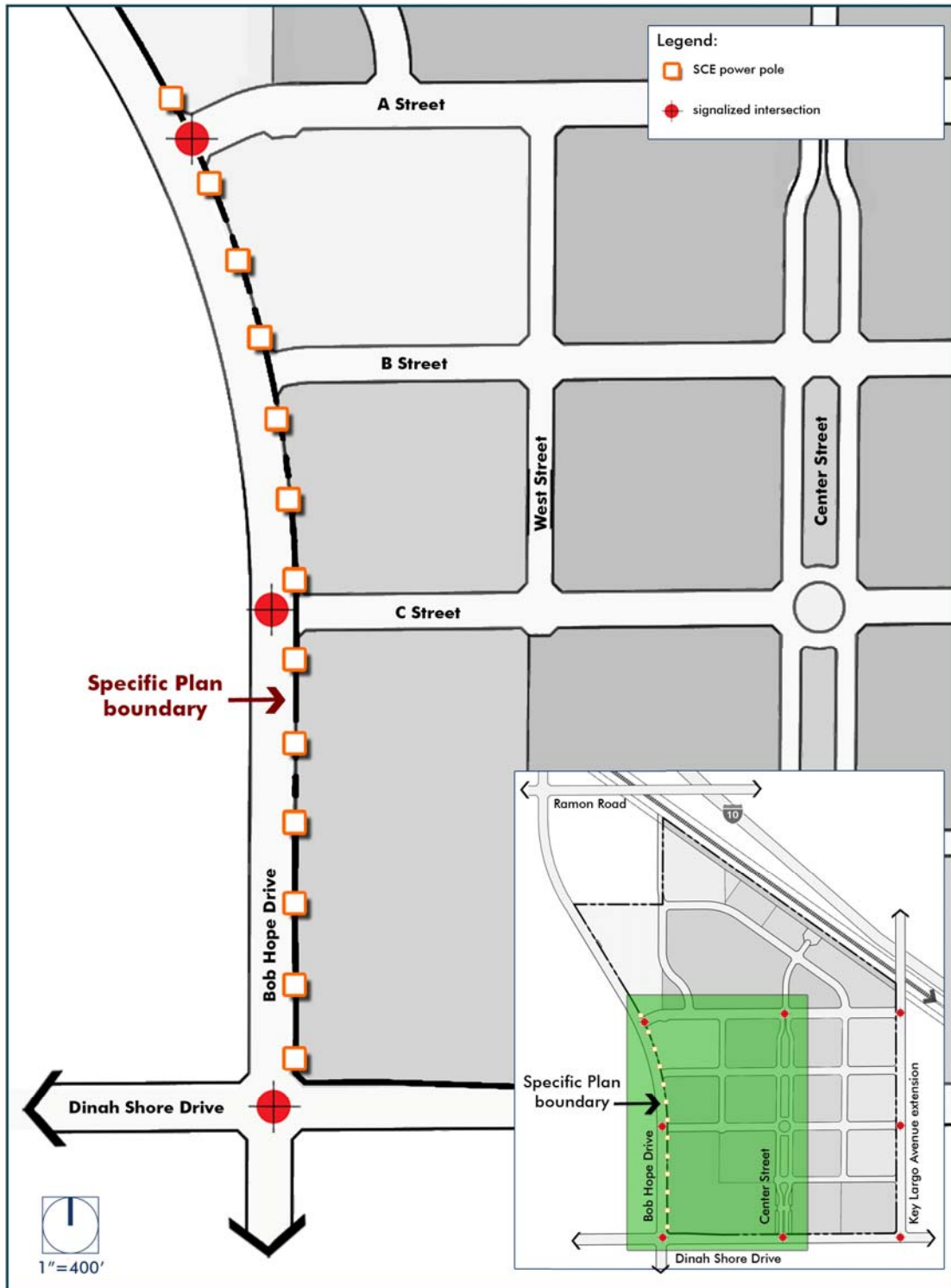


Figure 30 Locations of SCE Power Poles



5. Development Regulations

This chapter contains the regulations, requirements, and by-laws by which development must abide, indicated by the use of the word “shall.” These regulations are mandatory and cover general development standards, open space requirements, parking standards, nonconformities, lighting standards, sign programs, maintenance standards, and other standards for accessory structures. Provisions within these standards may also use the word “should,” in which case the standard is encouraged but not mandatory.

5.1 DEVELOPMENT STANDARDS

The development standards set forth for commercial, residential, and mixed-use development are intended to provide flexibility in site design and methods to integrate commercial and residential uses within the Section 19 Specific Plan area. These development standards prescribe the minimum standards for development and typically concern topics such as permitted uses, density, building and property dimensions, and the quantity of parking and landscaping.

5.1.1 PERMITTED USES

Each land use category is defined in precise terms to ensure that the range of permitted and conditionally permitted uses respects the intent of the plan and the conditions encountered in each portion of the site. Tables 4 and 5 outline the permitted uses (P) and conditionally permitted uses (C) that are allowed to develop within the Section 19 Specific Plan by land use designation. This list was derived from existing zoning regulations to allow greater or lesser flexibility in some areas of use and to customize land use requirements in response to site characteristics, potentials, and limitations. This tailored use list is specifically aimed at stimulating investment on this site to generate a land use pattern of higher value and quality than may be possible under conventional zoning. The table also lists prohibited uses to avoid potential conflicts between uses on and adjacent to the site.

Those uses not specifically listed in the table are subject to review based on the consistency within the purpose and intent of the land use categories and planning areas and are subject to the approval

of the Director of Community Development. The Director may refer uses or interpretation of permitted uses to the Planning Commission.

**Table 4
Permitted Uses**

(P) Permitted; (C) Conditional Use Permit; ■ Not Permitted

Use Category	Typically Permitted Uses (1)	Mixed-Use Core	Regional Mixed Use	Resort Flex	Retail	Residential	Public Facility	Open Space	Drainage Channel
Retail	Accessory uses and structures (e.g., storage)	P	P	P	P	C	P	C	
	Art, antiques, collectibles, gifts	P	P	P	P				
	Bars, alcoholic beverage drinking places (on-site consumption)	C	C	C	C				
	Building material stores >5,000 square feet		C						
	Building material stores ≤5,000 square feet		C		C				
	Convenience stores	C	C	C	C		C		
	Furniture, furnishings, equipment stores	C	P		P				
	Grocery stores	P	P		P				
	Health/fitness centers	C	P	P	P				
	Indoor amusement/entertainment centers	C	C	P	C				
	Liquor stores (off-site consumption)	C	C		C				
	Nightclubs, with or without food service	C	C	C					
	Pet stores	P	P		P				
	Plant nurseries		P					C	
	Restaurants (fast food)		C		C		C		
	Restaurants (specialty)	P	P	P	P		P	C	
	Restaurants (standard)	P	P	P	P		P		
	Retail stores (general merchandise)	P	P		P		P	C	
	Warehouse retail stores		C						
Office/Service	Accessory uses and structures	P	P	P	P	P	P		
	Automated teller machines (ATM)	P	P	P	P	C	P		
	Banks and financial services	P	P		P				
	Bed and breakfast inns	C		C					
	Broadcast and recording studios	P	P		C				
	Business support services	P	P	P	P				
	Car (motor vehicle) washes		C		C				
	Dry cleaning (drop-off only)	P	P	C	P				
	Hotels (2)	P	P	P	C				
	Medical services, clinics, laboratories	C	P		P				
	Offices (professional)	P	P		P				
	Personal services		P	P	P				
	Repair and maintenance (consumer products)	C	P		P				
	Service stations (3)		C		C				



**Table 4
Permitted Uses**

(P) Permitted; (C) Conditional Use Permit; ■ Not Permitted									
Use Category	Typically Permitted Uses (1)	Mixed-Use Core	Regional Mixed Use	Resort Flex	Retail	Residential	Public Facility	Open Space	Drainage Channel
Residential	Spa facilities	P	P	P	P				
	Veterinarian clinics and animal hospitals		C		C				
	Accessory uses and structures	P				P			
	Child care centers (up to 14 children)	P	P	P	P	P			
	Child care centers (15 to 30 children)		C		C	C			
	Community apartments and condominiums	P				P			
	Employee housing			C					
	Manufactured housing	C				C			
	Multifamily housing	P				P			
	Residential care homes (up to 8 clients)	C				P			
Residential care homes (9 or more clients)	C								
Timeshare/condo-hotel units (4)				C					
Mixed Use (5)	Vertically or horizontally attached buildings with two or more different uses (e.g., commercial and residential), but excluding live/work units unless attached to a separate commercial or residential building.	P	C	C			C		
Live/Work	<i>See Table 5 for permitted uses</i>								
Open Space and Recreation	Open space (private or public)	P	P	P	P	P	P	P	C
	Residential recreational facilities (private)	P				P			
Education, Public Assembly, Public Institutions	Community/cultural centers	C	C		C	C			
	Country clubs	C	C	C	C	C	C		
	Fire stations	C				P			
	Government offices	C	C		C	C			
	Libraries and museums	C	P		C		C		
	Membership organization facilities	P	P	P	P		C		
	Police stations	C	C	C	C				
	Post offices	C	C	C	C	C	C		
	Religious facilities/places of worship	P	C		C	C	C		
Schools (specialized education and training)	C	C							
Theatres, auditoriums, meeting halls	P	P	C						
Transportation/Communication Facilities	Alternative fuels and recharging facilities	P	P	P	P	C	P		
	Bus stations or taxi stands	P	P	P	P	P	P	C	
	Gas stations (6)	C							
	Public utility facilities	C	C	C	C	C	C	C	P
	Satellite antennae	C	C	C	C	P	C		
	Rail stations and terminals						P		

**Table 4
Permitted Uses**

(P) Permitted; (C) Conditional Use Permit; ■ Not Permitted

Use Category	Typically Permitted Uses (1)	Mixed-Use Core	Regional Mixed Use	Resort Flex	Retail	Residential	Public Facility	Open Space	Drainage Channel
	Wireless facilities	C	C	C	C	C	C	■	C
Manufacturing and Processing (6)	Furniture/fixtures/cabinet shops	C	P	■	■	■	■	■	■
	Handicraft industries, small-scale assembly	C	P	■	■	■	■	■	■
	Printing and publishing	C	P	■	■	■	■	■	■
	Recycling facilities – small collection facility	C	C	■	C	■	■	■	■
	Recycling facilities – reverse vending machines	C	C	■	C	■	■	■	■
Temporary Uses	Outdoor displays and sales of merchandise	See section 17.46 of the Rancho Mirage Municipal Code							
Other Uses	Businesses operating between 10:00 P.M. and 6:00 A.M.	C	C	C	C	■	C	C	■
	Stand-alone tower elements, such as obelisks, clock towers, and campaniles (7)	P	P	P	P	P	P	P	■
Prohibited Uses	Automobile sales (new or used)	■	■	■	■	■	■	■	■
	Auto parts and supplies	■	■	■	■	■	■	■	■
	Check-cashing, payday loan services	■	■	■	■	■	■	■	■
	Industrial uses	■	■	■	■	■	■	■	■
	Massage parlors, stand-alone	■	■	■	■	■	■	■	■
	Mobile home, recreational vehicle sales	■	■	■	■	■	■	■	■
	Off-site advertising, including but not limited to billboards (8)	■	■	■	■	■	■	■	■
	Sexually oriented business establishments	■	■	■	■	■	■	■	■
	Single-family detached housing	■	■	■	■	■	■	■	■
	Storage facilities (self service, personal storage)	■	■	■	■	■	■	■	■
	Uses that generate excessive vibrations, noise, heat, or smells	■	■	■	■	■	■	■	■
	Uses that use caustic chemicals on site (other than those used in copy machines); hazardous activities such as, but not limited to, welding, open flame, or storage of flammable liquids	■	■	■	■	■	■	■	■
Vehicle repair and maintenance	■	■	■	■	■	■	■	■	



Notes for Table 4:

- 1) For buildings that already exist and are to be occupied by a use that is permitted by right, only a certificate of occupancy and business license shall be required.
- 2) Hotels are not permitted in Planning Areas 1.02 or 1.04. In Planning Area 1.01, hotels shall not be permitted within 600 feet of the property line abutting Dinah Shore Drive.
- 3) Service stations may provide service and repairs to automobiles, but may not dispense fuel (gasoline or diesel) through individually accessed pumps. Any use that dispenses fuel in this manner (either as the sole service or as one of many services) is defined as a gas station and is regulated under the Transportation/ Communication Facilities category in Table 4.
- 4) See Chapter 8 for additional direction on the development procedures for timeshares/condo-hotel units. Such units are only permitted in the northern half of Planning Area 1.02. The northern half of PA 1.02 is defined by the property lines as of 2008 or, if that information is not available, the area of PA 1.02 that is more than 300 feet north of the property line abutting Dinah Shore Drive (approximate distance).
- 5) Mixed-use buildings are permitted by right in the Mixed-Use Core land use category, except when a use to be included in the mixed-use building requires a conditional use permit, in which case the entire mixed-use building shall require a conditional use permit.
- 6) These uses are only conditionally permitted within Planning Area 1.07. These uses are prohibited for the remaining Mixed Use Core Planning Areas. If a gas station is proposed, it should be located in the portion of PA 1.07 west of North Street.
- 7) Stand-alone tower elements, such as obelisks, clock towers, and campaniles, are permitted to reach up to 50 feet in height, so long as they do not have a footprint larger than 400 square feet. Taller tower elements and/or towers with a larger footprint are permitted with a CUP.
- 8) Prior to the start of construction on a site, all existing billboards must be removed.



**Table 5
Live/Work Permitted Uses**

(P) Permitted; (C) Conditional Use Permit; ■ Not Permitted

Use Category	Typically Permitted Uses	Mixed-Use Core	Regional Mixed Use	Resort Flex	Retail	Residential	Transit Station	Open Space	Drainage Channel
Food sales/service	Restaurants and eating establishments	C	■	■	■	■	■	■	■
Professional, administrative, and business uses	Architect/engineer/planners	P	■	■	■	■	■	■	■
	Attorneys	P	■	■	■	■	■	■	■
	Billing service providers	P	■	■	■	■	■	■	■
	Consulting and business services	P	■	■	■	■	■	■	■
	Dance teachers	P	■	■	■	■	■	■	■
	Estate planners	P	■	■	■	■	■	■	■
	Income tax service/accounting	P	■	■	■	■	■	■	■
	Interior decorators	P	■	■	■	■	■	■	■
	Internet or web-oriented businesses	P	■	■	■	■	■	■	■
	Kitchen and bath designers	P	■	■	■	■	■	■	■
	Music teachers	P	■	■	■	■	■	■	■
	Photography studio/portrait/bridal services	P	■	■	■	■	■	■	■
	Planning consultants	P	■	■	■	■	■	■	■
	Public relations consultants	P	■	■	■	■	■	■	■
Real estate developers/specialty contractors	P	■	■	■	■	■	■	■	
Secretary/communication service providers	P	■	■	■	■	■	■	■	
Retail sales/service	Building contractor offices	P	■	■	■	■	■	■	■
	Business consulting	P	■	■	■	■	■	■	■
	Catering services	C	■	■	■	■	■	■	■
	Child care facilities	C	■	■	■	■	■	■	■
	Electronic/computer equipment repair	P	■	■	■	■	■	■	■
	Flower shops	P	■	■	■	■	■	■	■
	Landscaping offices	P	■	■	■	■	■	■	■
	Pick-up and delivery services	C	■	■	■	■	■	■	■
	Printing and lithography facilities	C	■	■	■	■	■	■	■
Studios	Ceramic and pottery studios	P	■	■	■	■	■	■	■
	Copywriter studios	P	■	■	■	■	■	■	■
	Commercially operated professional studios	C	■	■	■	■	■	■	■
	Fine art studios, museums, galleries	P	■	■	■	■	■	■	■
	Photography studios	P	■	■	■	■	■	■	■
	Video producer studios	P	■	■	■	■	■	■	■



5.1.2 GENERAL DEVELOPMENT STANDARDS

Development standards control the building envelopes for the proposed commercial, residential, service, and entertainment uses. These regulations have been designed to provide flexibility in site design while ensuring a consistent and coordinated built environment. Tables 6 through 9 address the general development standards that will guide the Section 19 Specific Plan: density and intensity, building heights, and setbacks.

Density and Intensity

Table 6
Density and Intensity Standards

Planning Area	Net Land Area (Acres)	Maximum					Minimum		
		DU/AC (1)	Dwelling Units (2)	Hotel Rooms	FAR (3)	Lot Coverage	Project Size (4)	Unit Size (5)	Unit Width (6)
Retail									
Planning Area 1.01	26.2	–	50	150	0.40	40%	1 acre	600 SF	10' min. 15' avg.
Planning Area 1.02	8.0	–	–	–	0.40	40%		–	–
Planning Area 1.03	8.1	–	–	–	0.35	35%		–	–
Planning Area 1.04	9.0	–	–	–	0.35	35%		–	–
Resort Flex									
Planning Area 1.05	10.2	–	–	280	0.80	35%	2 acres	–	–
Planning Area 1.06	18.5	–	–	100	0.80	35%		–	–
Mixed-Use Core (7) (8)									
Planning Area 1.07	37.7	–	750	–	1.00	50%	2 acres	600 SF	10' min. 15' avg.
Planning Area 2.01	8.2	–	107	–	1.00	50%			
Planning Area 2.02	7.7	–	100	–	1.00	50%			
Planning Area 2.03	7.8	–	87	–	0.90	50%			
Planning Area 2.04	7.4	–	82	–	0.90	50%			
Planning Area 2.05	9.3	–	71	–	0.70	50%			
Planning Area 2.06	7.1	–	64	–	0.80	50%			
Open Space (9)									
Planning Area 2.07	0.9	–	–	–	0.05	5%	None	–	–
Planning Area 2.08	1.1	–	–	–				–	–
Planning Area 2.09	2.0	–	–	–				–	–
Regional Mixed-Use									
Planning Area 3.02	3.7	–	–	–	0.70	40%	1 acre	–	–
Planning Area 3.03	3.5	–	–	–	0.70	40%		–	–
Planning Area 3.04	9.0	–	–	50	0.70	40%		–	–
Medium Density Residential									
Planning Area 4.01	7.3	28	183	–	–	50%	2 acres	850 SF	10' min. 15' avg.
Planning Area 4.02	7.4	28	205	–	–	50%			
Planning Area 4.03	7.4	28	200	–	–	50%			
3.01, 3.05, 3.06, 3.07 – N/A									



Notes for Table 6:

- 1) Density is measured by dividing the number of dwelling units proposed in a planning area by the acreage of that planning area, resulting in a number of dwelling units per acre. Higher densities are permitted in compliance with Chapter 17.22 of the Rancho Mirage Municipal Code (RMMC), Affordable Housing Incentives/Density Bonus Provisions. A maximum density standard does not apply to residential development in a mixed-use setting (Planning Areas 1.01, 1.07, and 2.01–2.06).
- 2) The number of units may be transferred between planning areas subject to the Residential Development Transfer Program discussed in Section 8.3 of the Specific Plan. If units are transferred to a planning area, the maximum density may increase for that planning area to allow for the additional units, but shall not exceed 35 units per acre, except as provided for by Chapter 17.22 of the RMMC. In no case shall the total number of units constructed within the Specific Plan exceed 1,899 units, except as provided for by Chapter 17.22 of the RMMC.
- 3) Maximum floor-area ratios (FARs) incorporate only non-residential square footage and exclude all areas used for parking. An FAR is calculated by dividing the sum of all non-residential development square footage proposed for a planning area by the total square footage of that planning area. A project may not necessarily be able to reach the maximum FAR if its environmental impacts (e.g., traffic) exceed the thresholds stated in the EIR.
- 4) Developments are subject to a minimum project size; however, there is no minimum parcel size for individual parcels that are part of a larger project.
- 5) Smaller units are allowed with a conditional use permit, but in no case shall a unit smaller than 450 square feet be approved. Studio units smaller than 850 square feet must only provide 1.5 covered parking spaces per unit.
- 6) Residential and live/work units are permitted to be as narrow as 10 feet (as measured from the interior walls) for no more than 20 percent of the unit length. Unit widths must average at least 15 feet for the overall unit length.
- 7) For live/work units, the maximum ratio of office square footage to residential square footage is 0.40:1. This ratio ensures that the live/work space does not transition to a predominately office use.
- 8) The combined developments of Planning Areas 2.01 through 2.06 shall not consist solely of residential development. One or more individual planning areas, however, may consist solely of residential development.
- 9) The maximum amount of development square footage in Planning Areas 2.07–2.09 may not exceed 5 percent (FAR 0.05) of the combined square footage of the three planning areas. The development square footage may be distributed among the three planning areas in any proportion, provided no individual planning area hosts more than 3,000 square feet of development.



Increasing building height allows more open space for residents to enjoy.

Building Height

Table 7 provides the maximum building heights for development within the Section 19 Specific Plan. The overall intent is to apply innovative planning and design solutions to create a sense of place at multiple scales, while also blending with the surrounding community. Buildings are generally permitted to increase in height as development proceeds into the lower elevations of the project site and into the mixed-use core planning areas.

The downward sloping nature of the site's topography, particularly the area surrounding Center Street, offers the greatest potential to construct taller residential and mixed-use buildings. Taller buildings allow for a more efficient use of land, increased open space areas, and the creation of landmark structures. Accordingly, the Specific Plan incorporates a process by which buildings can achieve building heights greater than the General Standards shown in Table 7, provided additional amenities and features are incorporated.

These features and amenities shall be granted credit in the form of points. To achieve increased building heights, a project must achieve a score of 125 points. The features must be above and beyond those already required by the project. Table 8 lists eligible features and associated points. Figure 31 provides an illustration of how the maximum height standards are measured.

The following standards apply to all planning areas within the specific plan, except for portions of Planning Areas 1.01 and 1.02. In these areas, base building heights are limited to 25 feet, regardless of use or the inclusion of additional features. The portions are defined as the area within 200 feet of the southern property line abutting Dinah Shore Drive, 500 feet to the west of the centerline of Center Street, and 700 feet to the east of the centerline of Center Street (see Figure 32).



**Table 7
Building Height Standards**

Land Use	Maximum Base Building Height (1)		Additional Height above Max for Projections (3)
	General Standards	With Additional Features (2)	
Single-Use Buildings			
Retail or office/service	18 feet	36 feet	5 feet
Residential	36 feet	60 feet	5 feet
Hotel	42 feet	80 feet	5 feet
Parking structures	30 feet	50 feet	5 feet
All other uses	18 feet	36 feet	5 feet
Mixed-Use Buildings			
Retail and office/service	32 feet	60 feet	5 feet
Retail and residential	42 feet	66 feet	5 feet
Office/service and residential	38 feet	62 feet	5 feet
Live/work	30 feet	45 feet	5 feet
Other Structures			
Tower elements (4)	50 feet	N/A	N/A

Notes for Table 7:

- 1) The base building height shall be determined from the highest elevation point of the finished grade of the parcel (at a point within five feet of the exterior wall[s] of the structure) to the highest ridgeline of the structure, excluding chimneys and similar architectural projections. Portions of Planning Areas 1.01 and 1.02 are limited to a base building height of 25 feet, regardless of use or the inclusion of additional features. See Figure 31 for an illustration of maximum building heights.
- 2) Maximum building heights may be increased pursuant to the provision of additional features as listed in Table 8.
- 3) Architectural projections are building elements (e.g., towers, cupolas) that are added to building faces to provide architectural interest without adding interior floor area. The maximum height of any architectural projection is 10 feet above the proposed base building height, unless otherwise approved by the Director of Community Development. In no case, however, shall architectural projections be allowed to exceed 5 feet above the maximum base building height.
- 4) Stand-alone tower elements, such as obelisks, clock towers, and campaniles, are permitted to reach up to 50 feet in height, so long as they do not have a footprint larger than 400 square feet. Taller tower elements and/or towers with a larger footprint are permitted with a CUP.



**Table 8
Features to Obtain Increased Building Height**

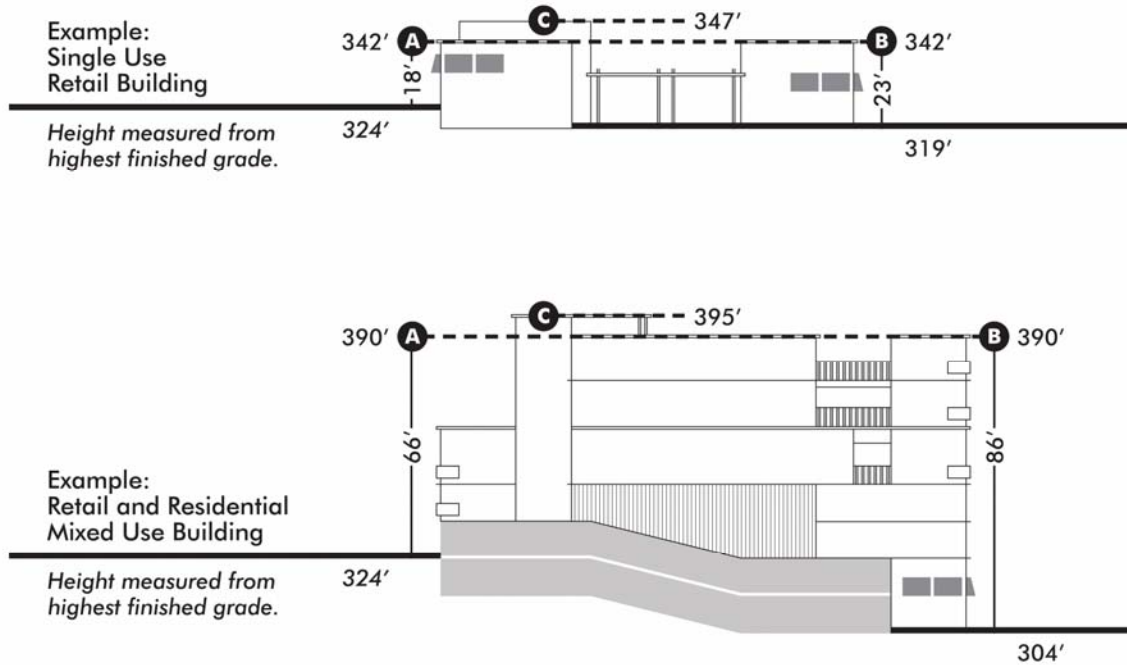
Features provided in addition to those already required (1)	Maximum Points (2)	
	per Feature	per Building
Semi-subterranean or below grade parking	75	75
Mixed-use building with residential above retail	75	75
Affordable housing: 10 percent low income (3)	75	75
LEED Certification: "Silver" or higher	75	75
Affordable housing: 5 percent very low income (3)	50	50
LEED Certification: "Certified"	50	50
Green roof feature (4)	50	50
Common open space or public plaza area along Center Street (minimum 20 x 20 feet)	50	50
Common open space or public plaza area (minimum 20 x 20 feet)	25	50
Organic garden (minimum 20 x 20 feet)	25	50
Cool roof building method (4)	25	25
Enhanced paving treatment for at least 50 percent of publicly accessible walkways and plazas	25	25
Public art equal to one-half of 1 percent (0.50%) of total building costs (5)	20	20
Formal garden space (minimum 20 feet x 20 feet) (e.g., rock garden, Japanese garden)	15	30
Landscape terraces visible from the public realm	15	15
Public art equal to one-quarter of 1 percent (0.25%) of total building costs (5)	10	20
Large water feature (6)	10	20
Small water feature (6)	5	15
Other amenities as approved by the Director of Community Development	As determined by the Director of Community Development	

Notes for Table 8:

- 1) The features must apply to or be accessible to the building seeking increased building height. An accessible feature is one that is located within 100 feet of a building's structural edge, with an exception for common open space or public plaza area provided along Center Street. Points earned by one building cannot be transferred to or shared by another building, with the following exceptions:
 - a) Buildings that share a parking structure may each receive 75 points towards the increased building height standards.
 - b) A feature within 100 feet of more than one building shall only be worth the maximum value of one feature in total. This point value can be divided amongst the buildings or given to one building.
- 2) A project can receive a maximum number of points for each feature, up to the stated maximum points per building. For example, a building can incorporate two organic garden areas worth 25 points each for a total of 50 points. If this building incorporates three organic garden areas, it will still receive a maximum of 50 points for these three features.
- 3) In compliance with Chapter 17.22 of the RMMC.



- 4) See Section 6.7.2, *Building Design and Materials*, for guidance on green and cool roof building methods.
- 5) See Section 5.3.5, *Public Art*, for guidance on how to calculate value.
- 6) Water features are focal points or decorative elements that contain still or moving water. They can be at the ground level (e.g., pop-jets) or elevated (e.g., fountains). Large water features are at least 100 square feet in area if flat and at ground level, or at least 10 feet wide or 5 feet tall if elevated. Small water features are up to 100 square feet in area if flat and at ground level, or up to 10 feet wide or 5 feet tall if elevated.



- A** The maximum building height is determined by measuring the highest elevation point of the finished grade of the parcel (at a point within five feet of the exterior wall[s] of the structure) to the highest ridgeline of the structure, excluding chimneys and similar architectural projections.
- B** The height standards are designed to permit buildings to increase in height as development proceeds into the lower elevations of the project site. If the highest elevation point is, as shown in these examples, 324 feet, and the building is permitted to have a base building height of 66 feet, the maximum base building elevation would be 390 feet.

This 390 foot elevation then serves as the maximum building elevation for the entire structure. Accordingly, if the finished grade of a portion of the building is at a lower elevation, that part of the building may be allowed to be greater than 66 feet in height, so long as the base building height does not surpass the 390 foot elevation.

- C** Architectural projections are building elements (e.g., towers, cupolas) that are added to building faces to provide architectural interest without adding interior floor area. The maximum height of any architectural projection is 10 feet above the proposed base building height, unless otherwise approved by the Director of Community Development. In no case, however, shall architectural projections be allowed to exceed 5 feet above the maximum base building height.

Figure 31 Building Height Standards

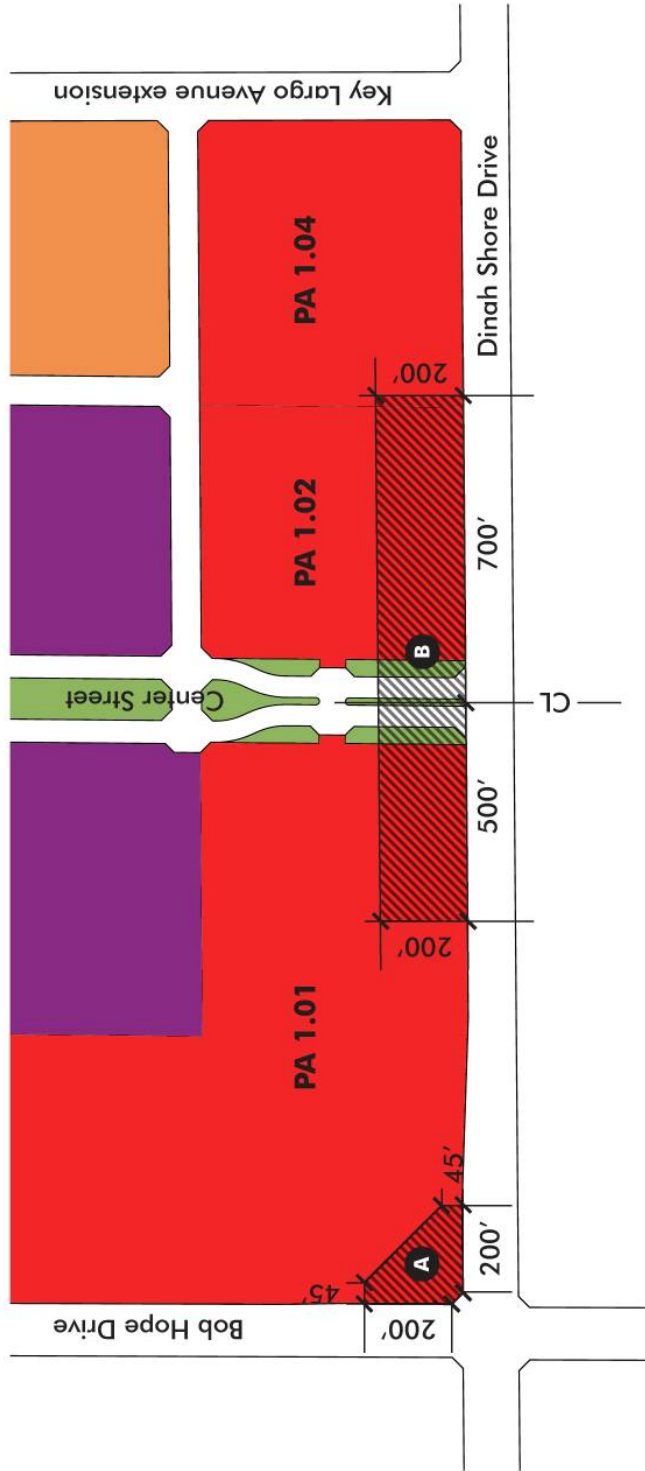


Figure 32 Special Height and Setback Standards

A The southwestern corner of Planning Area 1.01, as shown in the above figure, shall be subject to specific setback standards stated in Section 5.3.6 of this specific plan.

B The portions of Planning Areas 1.01, 1.02, and 1.04 shown in the above figure are limited to a base building height of 25 feet, regardless of use or the inclusion of additional features.



Setbacks

Table 9 provides the minimum building setbacks for development within the Section 19 Specific Plan. The overall intent is to allow for a more intense, urbanized building environment balanced by the luxury resort nature of Rancho Mirage. Buildings are generally permitted to be constructed close together to cluster development and create more vibrant and active spaces. Increased building setbacks are required for development constructed on the Specific Plan's perimeter roadways (Bob Hope Drive, Dinah Shore Drive, and Key Largo Avenue) to ensure development complements the surrounding community and scenic corridors are preserved.

Table 9
Building Setback Standards

Location	Minimum Setback (1)	Encroachments (2)
Building to Building (3)	15–20 feet for Planning Areas 1.01–1.04; 10–20 feet for development in all other planning areas	4-foot maximum
Building to Drainage Channel	20 feet from the property line	6-foot maximum
Building or Parking area to:		
Major Arterial	24 feet from the back of curb (4)	6-foot maximum
Minor Arterial	17 feet from the back of curb	6-foot maximum
Minor Collector (includes Entry)	14 feet from the back of curb	4-foot maximum
Local (includes Entry)	14 feet from the back of curb	4-foot maximum
Esplanade	17 feet from the back of curb	8-foot maximum
Esplanade Entry	64 feet from the back of curb	8-foot maximum
Building to Alley	2 feet to allow for a landscape apron	1-foot maximum
Building to Service Road	4 feet to allow for a landscape apron	2-foot maximum
Building to Parking Structure	15 feet (5)	3-foot maximum
Building to Parking Area/Drive Aisle	10 feet with walkway; 4 feet without walkway (must be landscaped)	2-foot maximum

Notes for Table 9:

- 1) All uses are allowed to be attached horizontally. Accordingly, the setback requirement for the building at the point of the shared wall is zero. Setbacks are measured from the closest point of a building. Stand-alone tower elements, such as obelisks, clock towers, and campaniles, are considered buildings and are subject to these building setback standards.
- 2) Encroachments/projections must maintain a minimum 3-foot walkway and are subject to the approval of the Director of Community Development. Encroachments include architectural features protruding from the building façade, cantilevered portions of the building, or outdoor uses such as porches and patios. All projections must allow for a minimum of 8 feet of headroom below. Awnings may project past the encroachment maximum into the setback area to provide additional shade coverage for pedestrians. If required by the Riverside County Fire Department, projections within Alley and Service Roadways must maintain a minimum clear-to-sky width of 26 feet within the right-of-way.



- 3) Commercial buildings greater than 20,000 square feet must be set back a minimum of 20 feet from another commercial building that is greater than 20,000 square feet or any residential, live/work, or mixed-use building. Commercial buildings greater than 20,000 square feet must be set back a minimum of 15 feet from a commercial building that is 20,000 square feet or less.
- 4) Buildings shall be set back at least one foot from the back of curb for every one foot of building height. For setback purposes only, the building height shall be determined by measuring the elevation point of the back of curb perpendicular to the highest ridgeline of the structure, including architectural projections. Those buildings in a specific portion of Planning Area 1.01 shall be subject to additional setback standards stated in Section 5.3.6 of this Specific Plan.
- 5) Buildings can reduce the building to parking structure setback to three feet if the adjacent wall is appropriately reinforced with sound-proof technology. Further reductions may be approved by the Director of Community Development.

5.2 LIVE/WORK STANDARDS

Live/work units are a unique type of mixed-use development that combines residential living space and commercial or office space within one structure for a single owner. They offer similar benefits to mixed-use development and eliminate the need to commute to work. The Specific Plan recognizes the changing patterns of work and technology that are leading to an increase in the number of home-based workers. Live/work uses enable the City to adapt to changes in the economy and meet the needs of special groups such as artists or new businesses that need affordable work and housing space.

Live/work uses are similar to home occupation uses currently permitted through a Home Occupation Permit in Chapter 17.44 of the RMMC. However, in the current (2009) RMMC, a home occupation use is intended to maintain a residential nature and prohibits alteration of appearance, advertising of the occupation, or sale of merchandise. In comparison, live/work uses are permitted to act and appear as functional businesses on the ground floor of the building. Accordingly, live/work units and buildings are subject to the following standards and shall not be subject to the requirements and procedures of Chapter 17.44 of the RMMC:

- 1) The work portion of the live/work unit shall not be initiated until a current Business License is obtained in compliance with RMMC Chapter 5.04, Business Licenses and Fees Generally. Immediately following the effective date of an approved Home Occupation Permit, when no appeal has been filed, the applicant shall obtain a Business License.
- 2) Work on the premises of a live/work unit shall be limited to persons who live in the live/work unit. Living and working spaces shall not be rented or sold separately. The owner/occupant of a live/work unit shall notify the City of any change in use or occupancy. Any change of use or occupancy shall comply with the uses identified in this Specific Plan and will require a new Certificate of Occupancy.



- 3) Off-street loading will be accomplished by the temporary use of planned parking spaces, or in parking spaces limiting a vehicle's permitted parking time (e.g., parking stalls designated with 20-minute parking limits).
- 4) Additions or enlargements of structures, modification of floor areas dedicated to living and working spaces, or any subsequent change in the approved live/work units shall require a specific plan amendment, administrative clearance, or conditional use permit, depending on the nature and scope of change being sought.
- 5) Live/work units and buildings must comply with any requirements imposed by the Rancho Mirage Public Works and Community Development Departments and Riverside County Sheriff and Fire Departments intended to protect the public health, safety, and welfare.
- 6) An administrative approval or conditional approval of the commercial/work component of the live/work units shall be granted to the owner of the unit. Approvals of commercial uses may not be transferred between units. A copy of all conditions of the approval of the project shall be provided to all future owners/occupants of the building prior to their execution of a lease or purchase agreement for the live/work unit. Project conditions are required to be recorded with the County Recorder's Office prior to exercise of entitlement.
- 7) Businesses using commercial vehicles are prohibited. Commercial vehicles are vehicles used for purposes other than transporting passengers, such as those used for the transportation of goods, wares, and merchandise.
- 8) Covenants, conditions, and restrictions for individual live/work projects may further restrict and prohibit uses, but shall not be more permissive than the requirements imposed by this Specific Plan.



Example of live/work units, with office space on the ground floor and residential space above.



More intimate open spaces may better serve residents in a mixed-use setting.



The building setback area may qualify for common open space with building articulation and landscaping.

5.3 OPEN SPACE STANDARDS

The unique character of the Section 19 Specific Plan requires special open space standards to ensure adequate



space for the residents and consumers while also recognizing the unique types of spaces that work best within a mixed-use setting. Residents living in mixed-use developments tend to consist of singles or young couples, who do not require the large private and common open spaces normally associated with families and traditional suburban and resort-style development. Instead, smaller, centrally located plazas and recreation areas are encouraged to provide a more intimate, urban lifestyle.

5.3.1 RESIDENTIAL STANDARDS

- 1) A minimum of 200 square feet of outdoor usable common and private open space shall be provided per dwelling unit.
 - a) A minimum of 70 square feet per unit shall be private with a minimum dimension of 7 feet in any direction. Private open space shall be accessible directly from the living area of the unit, in the form of a fenced yard, patio, deck, or balcony.
 - b) A minimum of 100 square feet per unit shall be usable common open space.
 - i) Usable common open space shall have a minimum level surface dimension of 20 feet in any direction and a minimum area of 400 square feet.
 - ii) The dimensions for usable common open space areas shall be measured from the outside of any private open space attached to a unit at ground level.
 - iii) The dimensions for usable common open space may include the building setback area if the buildings facing the open space area display a high degree of articulation and the building setback area is heavily landscaped. Landscaping may consist of in-ground or potted plantings.
 - iv) Usable common open space shall not include:
 - (1) Any area counted as private open space;
 - (2) Sidewalks and paved pathways;
 - (3) Any portion of open and enclosed parking areas, garages, streets, driveways, automobile turning aisles, or turnaround areas;
 - (4) Storage areas (refuse or otherwise) or any area fenced or otherwise inaccessible to the residents;
 - (5) Slope areas exceeding 7 percent; or
 - (6) Areas within public rights-of-way along roadways.



- c) A minimum of 30 square feet per unit shall be provided in the form of common or private open space. This space can also be referred to as “flex space” and can be added to the private or common open space areas.

5.3.2 LIVE/WORK AND MIXED-USE RESIDENTIAL STANDARDS

- 1) A minimum of 100 square feet of usable common open space shall be provided for each live/work unit or any residential unit in a mixed-use building.
 - a) Usable common open space shall have a minimum level surface dimension of 20 feet in any direction and a minimum area of 400 square feet.
 - b) The dimensions for usable common open space areas shall be measured from the outside of any private open space attached to a unit at ground level.
 - c) The dimensions for usable common open space may include the building setback area if the buildings facing the open space area display a high degree of articulation and the building setback area is heavily landscaped. Landscaping may consist of in-ground or potted plantings.
 - d) Usable common open space shall not include:
 - i) Any area counted as private open space;
 - ii) Sidewalks and paved pathways;
 - iii) Any portion of open and enclosed parking areas, garages, streets, driveways, automobile turning aisles, or turnaround areas;
 - iv) Storage areas (refuse or otherwise) or any area fenced or otherwise inaccessible to the residents;
 - v) Slope areas exceeding 7 percent; or
 - vi) Areas within public rights-of-way along roadways.
- 2) The residential open space requirements of mixed-use buildings must be met in addition to any public plaza space requirements generated by non-residential portions of the mixed-use building.



Usable open space may accommodate a slope of up to 7 percent.



5.3.3 COMMERCIAL AND OFFICE STANDARDS

- 1) Each building with a commercial, hotel, or office gross leasable area (GLA) of 15,000 square feet or less shall provide public plaza space equal to 10 percent of the total GLA of the building. Each building with a commercial or office GLA greater than 15,000 square feet shall provide or contribute towards public plaza space equal to 5 percent of the total GLA of building.
- 2) Public plazas shall have a minimum dimension 20 feet in any direction and a minimum area of 400 square feet.
- 3) Public plazas may consist of pedestrian-accessible spaces, including outdoor seating areas, open space, water features, and landscape areas.
- 4) Outdoor eating areas provided as part of private eating establishments cannot be counted toward the public plaza requirement, unless the eating areas are open and accessible to the public, with no fencing or other barriers or obstructions. If the eating areas are enclosed by fencing or landscaping no greater than 4 feet in height, these areas may count toward up to 20 percent of the total public plaza requirement.
- 5) Public plazas shall exclude parking areas, roadways and the first 5 feet surrounding all sides of the buildings.
- 6) Commercial or office space provided within a live/work unit is exempt from the public plaza requirement.



Open space requirements for commercial uses may accommodate eating areas when enclosed by fencing no higher than four feet.

5.3.4 SPECIAL OPEN SPACE PROVISIONS

- 1) Usable common open space or public plaza space requirements do not need to be located immediately next to their individual buildings, and are instead encouraged to coordinate with other uses to provide larger open spaces that are centrally located and serve multiple buildings.
 - a) The usable common open space and public plaza requirements of mixed-use buildings that contain residential and commercial uses can be combined into one or more large spaces to satisfy the



Common open space requirements should be combined to satisfy the requirements of multiple surrounding uses.



usable common open space requirements of all residential uses, so long as the space is centrally located along and directly accessed by those uses.

- b) The public plaza requirements of commercial or office buildings may be combined into one or more large spaces to satisfy the open space requirements of all buildings, so long as the space is centrally located along and directly accessed by those uses.
- 2) Usable common open space or public plaza requirements of a proposed project may be added to an existing open space or public plaza area, so long as the proposed open space or public plaza area is contiguous to the existing open space or public plaza area and permission is granted by the property owner.
- 3) For additions to existing, previously permitted commercial or office uses (or uses as part of a mixed-use building), the applicant shall only be required to provide plaza space at 5 percent of the new additional square footage of GLA. For example, if an existing 40,000-square foot commercial building adds 10,000 square feet of GLA, 500 square feet of additional public plaza space must be provided ($10,000 \times 5\%$).
- 4) The common open space or public plaza requirement shall be waived for additions to existing buildings or the construction of new buildings within the same planning area if existing open space provided by the existing building is of sufficient size to satisfy the total requirement of the existing and added square footage. For example, if an existing 40,000-square-foot commercial or office building adds 10,000 square feet of GLA, it would be considered a 50,000-square foot building. If the existing building already provides a 2,500-square foot public plaza ($50,000 \times 5\% = 2,500$ square feet), then no additional public plaza space shall be required.
- 5) A maximum of 33 percent of the area used to satisfy the public plaza requirement may be landscaped with vegetation or hardscape materials that cannot be walked or sat upon, such as in-ground flowerbeds.
- 6) For planning areas that front directly onto Center Street, the public plaza and common open space requirements can be reduced by an amount equal to 25 percent of the square footage provided in the central esplanade or Esplanade Entry building setback area. A planning area's public plaza and common open space requirements can only be reduced by the esplanade or Esplanade Entry building setback areas that are directly in front of that planning area.



5.3.5 PUBLIC ART

Public art is an instrumental feature that can be used to create a connection between the public and any particular project or space. Public art makes spaces more interesting, helps to distinguish one place from another by creating landmarks that are easily recognizable, and creates a unique shopping, working, or living environment. By enhancing the overall quality of a project and giving it a unique character, public art increases a project's value. The Specific Plan sets forth several public art requirements and guidelines:



Public art may become interactive elements within public plazas.

- 1) Public art within the Specific Plan project area shall be provided for:
 - a) New residential, commercial, live/work, or mixed-use development having total project costs of \$300,000 or more, as determined by the City's valuation of building permits issued for the development.
 - b) Expansion or remodeling of existing buildings when any such work has a building permit valuation of \$300,000 or more.
- 2) Public art provided shall have a value equal to one-half of 1 percent (0.50%) of the total building costs (as measured by building permit valuations), excluding land, site development, off-site requirements, and remediation costs. The value of the public art shall include the art piece itself and the cost of installation.
- 3) Public art may be installed concurrently with and adjacent to each building that triggers the public art requirement; or the value of the required public art for each building can be consolidated or banked and applied to the provision of larger installations that serve multiple buildings.
- 4) The public art requirements shall not apply to reconstruction of structures that have been damaged by fire, flood, wind, earthquake, or other calamity.

5.3.6 SPECIAL CORNER STANDARDS

The City applies specific development standards through a Special Corner Overlay Zone (SCOZ) to key intersections in Rancho Mirage where aesthetics and land use compatibility are of primary importance. The intersection of Bob Hope Drive and Dinah Shore Drive is a key gateway for Rancho Mirage, and the City's 2005 General Plan applied the SCOZ to the 40 acres of the Specific Plan that front the northeastern corner of this intersection.

The Section 19 Specific Plan proposes to replace the SCOZ development standards as they apply to these 40 acres within the Specific Plan with a customized set of setback and landscaping standards. These standards can be found in this chapter and in Chapter 6 of the Specific Plan.



The area within the first 200 feet of the intersection north along Bob Hope Drive and east along Dinah Shore Drive will receive the greatest exposure to passing vehicles and merits special corner standards. The following development standards provide direction for building and parking setbacks, landscaping and signage placement, and pedestrian connections. The area subject to these additional standards is illustrated in Figure 32.

- 1) The minimum distance from the curb at the main intersection inward to the parking area, measured at a forty- five degree angle, shall be 90 feet. The minimum distance to a building shall be one 122 feet.
- 2) The minimum landscape parkway width between curb and parking shall be 45 feet.
- 3) All buildings shall be a minimum of 85 feet from the front of arterial roadway curbs.
- 4) All parking and circulation areas shall be continuously screened from public streets to a height of 4 feet, by a combination of walls, berms, boulders and hedges. Where possible, placing parking areas lower than adjacent roadways is encouraged, with intent to reduce their visual impact and to screen the parking from the adjacent roadway.
- 5) Shade structures in the form of landscaped trellises are encouraged as an added method of providing parking lot and pedestrian area shade.
- 6) No signs, neon or otherwise, shall be visible through windows.
- 7) Stand-alone tower elements, such as obelisks, clock towers, and campaniles, are exempt from these standards.

5.3.7 PARKS

The City parks requirement for residential development of three acres of parkland per 1,000 residents per Section 16.18.060 of the RMMC (2009) shall be met through a combination of land dedication, improvements, private recreation, and in-lieu fees.

5.4 PARKING STANDARDS

Parking is an important component of development for the Section 19 Specific Plan. Due to the size of the project area and the mix of land uses proposed, the amount of parking required to satisfactorily park the project will take up a substantial portion of the site. The various uses will be required to meet the parking standards specified in Table 10. The amount of parking will vary depending on the number of units proposed and square footage of the non-residential uses. The parking may be provided through a combination of



The opportunity to share parking between different uses would allow for more efficient development.



surface parking and parking structures, with the structures having a maximum height limit of 45 feet.

There is a significant opportunity to share parking among different land uses, which could considerably reduce the number of parking spaces needed in order to adequately park the project. Shared parking can reduce the amount of land needed for parking, creating opportunities for more compact development, more space for pedestrian circulation, and more open space and landscaping.

Many city codes require parking spaces to accommodate the peak-hour demand by any land use at any time. Shared parking analyses establish that as different land uses peak at different times, together these land uses do not need their maximum parking supply at the same time. Planning for each land use individually would result in an over-supply of parking in a location where there is a combination of land uses. Therefore, for mixed-use areas, there is a potential to share a pool of parking that is smaller than the amount that would be required for each land use individually. However, because shared parking reductions depend on the specific type and size of land uses within a project, the specific parking demand ratio for each land use requires further study involving the specific type of land use and peak-hour demand.

The Rancho Mirage Municipal Code is unique in that the City provides minimum parking requirements based on the potential for different land uses to share parking requirements. However, a more specific shared parking analysis approved by the Director of Community Development and City Traffic Engineer may be completed for projects submitted under the Section 19 Specific Plan. As part of the study, the applicant will have to demonstrate that the proposed uses have differing peak hours of parking demand or that the total parking demand at any one time will be adequately served by the total number of parking spaces proposed.

Table 10
Parking Standards

Topic	Standard	Notes
Commercial and Office		
General retail	3.0 per 1,000 SF GLA	
Family restaurant	10.5 per 1,000 SF GLA	
Fast food restaurant	15.0 per 1,000 SF GLA	
Casual and fine dining	18.0 per 1,000 SF GLA	
General office	3.5 per 1,000 SF GLA for first 5,000 SF	Parking standard decreases to 2.5 per 1,000 SF GLA for additional SF
Medical office	4.0 per 1,000 SF GLA for first 5,000 square feet	
Hotel		
Business hotel	1.1 per room	Plus required spaces for ancillary uses.
Leisure hotel	1.1 per room	Plus required spaces for ancillary uses.
Restaurant/lounge	10.0 per 1,000 SF GLA	
Meeting area/banquet room	30.0 per 1,000 SF GLA	Parking standard refers to "gross assembly or viewing area" per RMMC.
Convention center	20.0 per 1,000 SF GLA	



**Table 10
Parking Standards**

Topic	Standard	Notes	
Residential			
Multifamily			
0–1 bedroom	1.50 per unit	All spaces must be enclosed in a garage.	
2+ bedroom	1.75 per unit	All spaces must be enclosed in a garage.	
Single-family attached	2.00 per unit	All spaces must be enclosed in a garage.	
Guest spaces	0.15–0.25 per unit	0.15 multifamily / 0.25 single family. Spaces may be uncovered.	
Structured Parking			
Landscaping	50% coverage of visible concrete surfaces	Parking structure facades shall achieve 50 percent coverage of visible concrete surfaces with landscaping. Coverage can be achieved through measures such as planters along the visible edge of the structure planted with cascading vines, or through a vertical trellis surface with vines planted at the parking level, or by other means.	
Bicycle Parking			
Bicycle parking	1 rack per project that requires 40 or more non-residential parking spaces	For any development that requires 40–80 non-residential parking spaces, 1 rack that can support 2 bicycles shall be provided. For any development that requires more than 80 non-residential parking spaces, 1 rack that can support 4 bicycles or 2 racks that can support 2 bicycles shall be provided. This requirement does not apply to live/work units. See Section 17.26.100 of the RMMC for location and design requirements.	
Alternative Vehicle Parking and Carpooling			
Golf cart or neighborhood electric vehicle parking	1 dedicated space for 0–25 residential units; 2 dedicated spaces for 26–50 residential units; 3 dedicated spaces for 51–100 residential units; 4 dedicated spaces for 101 or more residential units	Parking stalls shall be provided in a central location accessible to the residential units. Parking stalls shall be covered to shield the vehicles from sun and weather. The parking area shall also function as a recharging station by providing electric vehicle ports. As an alternative, projects may provide these spaces within an enclosed garage as part of an individual unit, so long as the space is in addition to the area needed to accommodate the standard parking spaces.	
Electric vehicle charging station	1 charging station for every application for 100,000 or more square feet of non-residential development	The charging station shall be installed and function prior to the issuance of final building permits for half of the proposed square footage. The charging station location shall be as close as feasible to majority of the building entrances.	
Carpool parking	Three percent of parking spaces shall be set aside exclusively for carpools	This requirement applies only to general or medical office buildings with a GLA greater than 20,000 square feet. Fractional requirements shall be rounded down to the nearest whole number.	
Parking Dimensions			
Parking for non-residential use		Parking for residential use	
90 degree parking stall	9 feet x 18 feet	90 or 45 degree parking stall	9 feet x 18 feet
45 degree parking stall	9 feet x 19 feet	Parallel degree parking stall	8 feet by 22 feet
Parallel degree parking stall	9 feet x 26 feet		
Parking for alternative vehicle use			
90 or 45 degree parking stall	8 feet x 16 feet		
Parallel parking stall	8 feet x 20 feet		



5.5 NOISE AND VIBRATION

5.5.1 NOISE

- 1) Where residential uses are potentially exposed to interior or exterior noise levels greater than those permitted by Chapter 8.45 (2009) of the RMMC, certification from a licensed acoustical engineer shall be obtained to document attenuation to those maximum levels. The exterior standards shall be measured either at the property line or the nearest noise-sensitive use such as a patio, yard, or landscaped open space, whichever is closer.
- 2) Commercial uses shall be designed and operated, and hours of operation limited, where appropriate, so that neighboring residents are not exposed to offensive noise, especially from traffic, trash collection, routine deliveries, or late-night activity. No use shall produce continual loading or unloading of heavy trucks at the site between the hours of 8 PM and 7 AM.
- 3) As part of a development application, the applicant shall prepare and submit to the City a site-specific noise study and shall design and implement noise-attenuation measures such as walls, berms, landscape buffers, soundproofing, or other features that would reduce exposure to excess roadway noise or excess noise levels generated by trains on the Union Pacific Railroad (UPRR) mainline tracks.
- 4) Prior to issuance of building permits, the applicant shall submit a detailed acoustical study demonstrating that all project structures will meet applicable City interior noise levels and exterior living-area noise levels, in accordance with applicable noise standards and zoning regulations.
 - a) The study shall be prepared by a City-approved acoustical expert, to the satisfaction of the Director of Community Development.
 - b) The study shall document projected ultimate noise exposure for interior office, retail, and residential space and shall demonstrate that project design plans have incorporated adequate sound attenuation measures to achieve the applicable noise standards.
- 5) Noise mitigation and proper design may include, but shall not be limited to, building orientation, double- or extra-strength windows, wall and ceiling insulation, and orientation and insulation of vents. Where it is necessary that windows be closed in order to achieve the required level, means shall be provided for ventilation/cooling to provide a habitable environment.



5.5.2 VIBRATION

- 1) No use, activity, or process shall produce continual vibrations or noxious odors that are perceptible without instruments by the average person at the property lines of the site or within the interior of residential units on the site.
- 2) Vibration-sensitive uses within 215 feet of the UPRR mainline tracks shall retain an acoustical engineer to evaluate the potential for trains to create perceptible levels of vibration indoors. If vibration-related impacts are found, building materials such as concrete, iron, steel, or masonry materials, shall be used to ensure that levels of vibration amplification are within acceptable limits to building occupants.



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6. Design Standards and Guidelines

Quality development is achieved through attention to detail that is implemented from the initial conception of a project to the final construction of buildings, pathways, entry features, signage, and other design elements. This section outlines the design standards and guidelines for the Section 19 Specific Plan that will encourage the highest level of design quality and creativity in site planning and architectural design, while allowing for variation and flexibility. All development within the Section 19 Specific Plan must address the fundamental elements of the design features covered in this chapter. The following guidelines establish a design framework to help the City of Rancho Mirage staff, citizens, design professionals, and developers understand and implement this project.

Due to the mix of project types that the Section 19 Specific Plan allows, the buildings will include a variety of design styles, and therefore a range of colors, materials, building detailing, and building orientations. However, these guidelines provide the necessary direction to ensure a coherent and complementary project. These guidelines supplement, but do not override, the Americans with Disabilities Act, Title 24 of the California Code of Regulations, and additional requirements set forth in local and State of California building codes.

Similar to development standards, design standards constitute regulations, requirements, and by-laws by which development must abide, and are indicated by the use of the word “shall.” Design guidelines generally use the word “should” and identify actions or outcomes that are encouraged but not mandatory.

The design guidelines are divided into seven sections: site design, building design, signage, lighting, walls and fences, hardscape, and environmental design. As a visual summary of these guidelines, Figures 33–35 are provided to more clearly communicate the intent and desired outcome for the Specific Plan. The illustration shown in Figure 33 is purely conceptual and future developments are not required to match in terms of intensity or site layout. It is intended to illustrate the overall circulation and streetscape concepts, where buildings are brought to the street to create a pedestrian-friendly environment. The building elevations shown in Figures 34 and 35 are also conceptual and are intended to provide examples of how many of the design standards and guidelines in this chapter should be implemented.



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Figure 33 Conceptual Illustrative Site Plan



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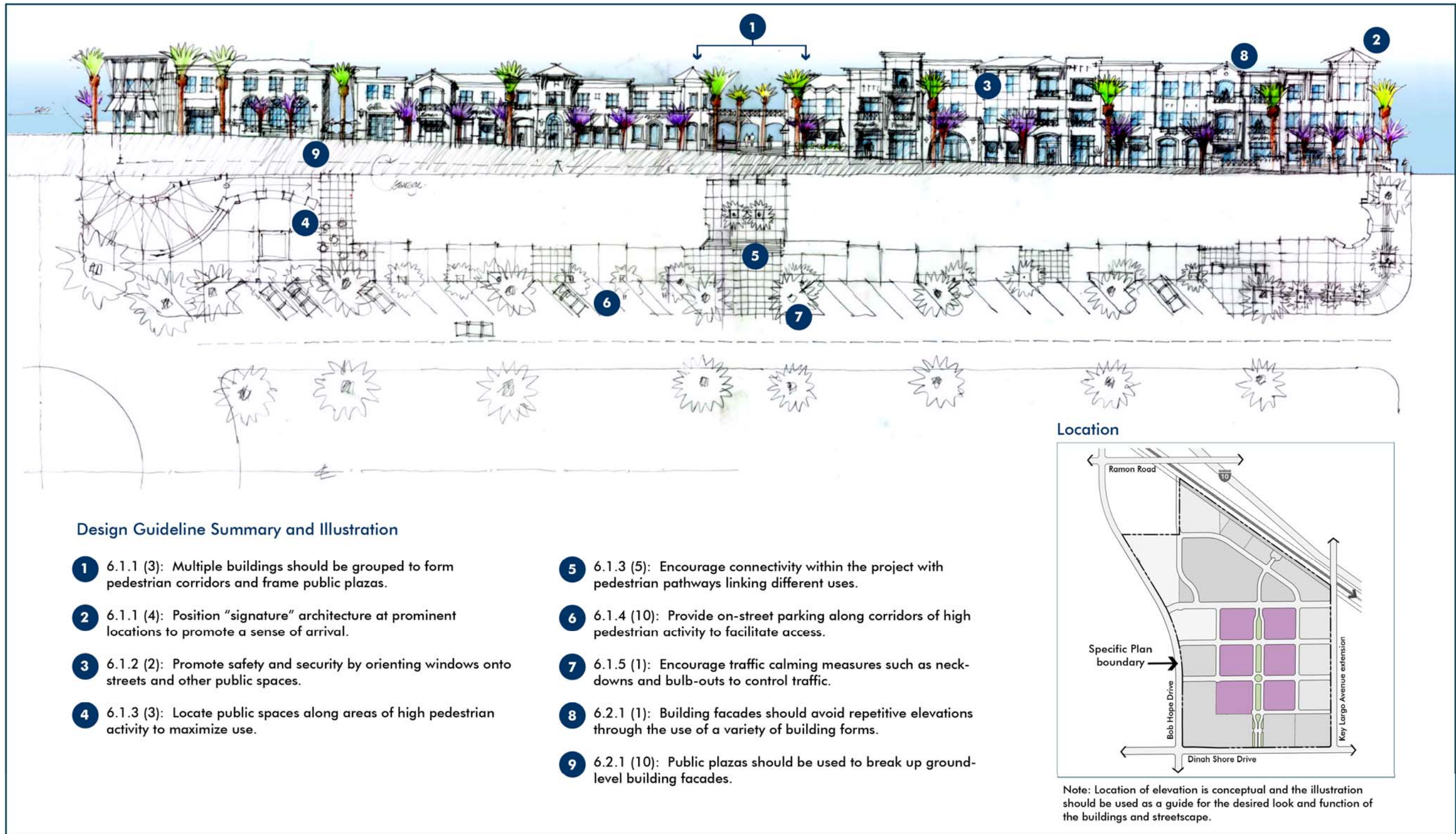


Figure 34 Conceptual Illustrative 1: Design Standards and Guidelines



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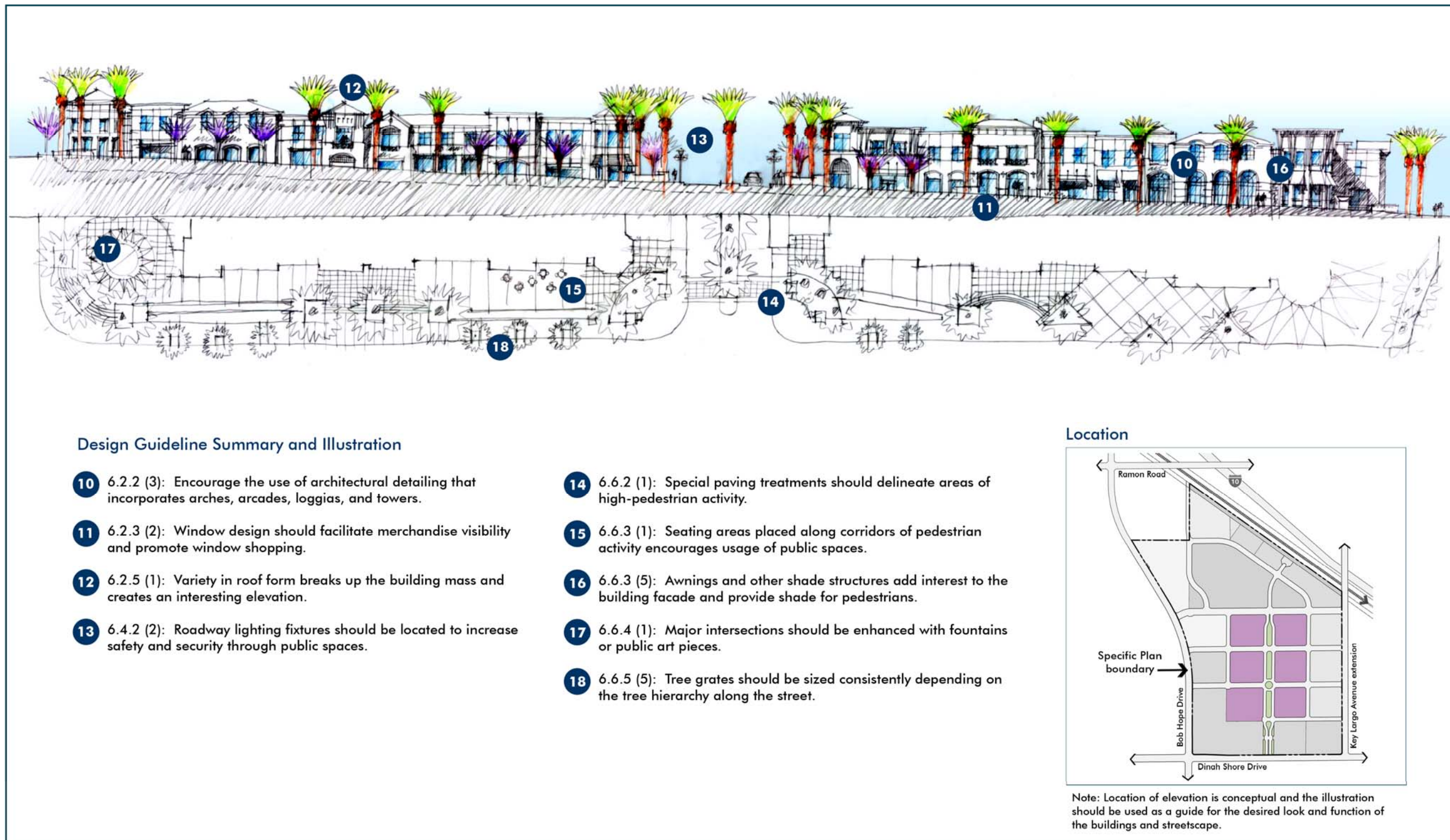


Figure 35 Conceptual Illustrative 2: Design Standards and Guidelines



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6.1 SITE DESIGN

The integration of buildings, entries, parking lot layout, open spaces, and pedestrian and vehicular circulation is critical to achieving an overall sense of place. With the mix of proposed land uses within the Section 19 Specific Plan area, it is important to set guidelines to coordinate site planning between distinct building types and to ensure the connectivity of public spaces. Site design guidelines also provide direction for building placement and orientation, creating a defined streetscape that gives the project a distinct character from the surrounding development.

For the Section 19 project, the streetscape along the external roads should have a more traditional Rancho Mirage feel with regard to scale and development character, with lower building heights and frontage parking. However, for roads internal to the project, buildings should be positioned closer to the street to create a more intimate character. These site design guidelines encourage development to incorporate courtyard parking, which places buildings along the street edge with parking internal to each block. This creates a more engaging streetscape that promotes pedestrian activity along the street rather than allowing parking areas directly adjacent to the street, which would isolate the pedestrian.

The guidelines below are organized by area use. For structures within the Section 19 project that combine commercial and residential uses within the same building, the site design guidelines for both commercial and residential areas shall apply.

6.1.1 COMMERCIAL AREAS

- 1) Building placement and orientation shall be organized to create visual interest along public rights-of-way, particularly at intersection nodes and project entryways.
- 2) Buildings shall be oriented so that public access or windows face areas of pedestrian activity, such as public plazas and pedestrian pathways.
- 3) Multiple buildings in a single area should be grouped and organized to demonstrate a positive functional relationship to one another. The grouping of multiple buildings should be clustered to create functional plazas and pedestrian corridors. Where clustering is impractical, a visual link should be established between buildings through the integration of an arcade system, trellis, colonnade, or other such open structure.



Courtyard parking locates parking areas behind building façades.



Building orientation shall reinforce areas of pedestrian activity.



Unique architectural elements should be placed on corners of significant intersections.



Residential projects should create small-scale public uses for residents.



Residential structures should be oriented toward streets to increase security.

- 4) Provide enhanced or “signature” architecture at prominent locations. Buildings with unique architectural elements, such as clock towers and other landmark structures, shall be positioned on corners of significant intersections or entryways to enhance the sense of arrival and project monumentation.
- 5) Stacking lanes for drive-through food service windows shall accommodate for a stacking of at least eight cars, and all other service windows shall accommodate stacking for at least four cars.
- 6) Drive-through businesses shall be visually screened and shall be situated so as not to block any other drive aisle or parking space.
- 7) Drive-throughs shall be separated from residential properties by an intervening building or a maximum 6-foot-high wall and a 10-foot-wide buffering landscape strip.

6.1.2 RESIDENTIAL AREAS

- 1) Where possible, housing should be oriented to streets and pedestrian walkways.
- 2) The windows of interior living spaces should overlook streets and public spaces to enhance community security and maximize view potential.
- 3) Residential buildings shall emphasize pedestrian access and connections to public sidewalks, paths, recreational facilities, and enhanced edges.
- 4) Structures should be configured and oriented to afford a sense of individuality and privacy and to create small-scale public spaces.
- 5) Residential and non-residential uses shall not have common entrance hallways or common balconies. This ensures the security of residents through the provision of separate and secure entrances and exits.



- 6) Recreational facilities shall be conveniently and centrally located for the majority of units.

6.1.3 PUBLIC OPEN SPACES AND PEDESTRIAN AREAS

- 1) Areas other than those spaces occupied by buildings, service drives, or other surface circulation should incorporate amenities such as enhanced landscape or hardscape features. These include outdoor seating areas, trellises, ornamental trees, benches, planters, open space, water features, and pedestrian-friendly elements. Additional guidelines for landscape and hardscape treatments are in Sections 4.5 and 6.6, respectively.
- 2) Land uses should coordinate their open space requirements to provide larger public spaces that are centrally located, functional, and serve multiple uses.
- 3) Public spaces should be strategically located along areas of pedestrian activity, such as shopping areas and major pedestrian thoroughways.
- 4) Public spaces shall be oriented to maximize their visual and physical link from adjacent streets and pathways.
- 5) Pedestrian pathways should provide connectivity within the project by connecting each project to neighboring properties and emphasizing links between different uses.
- 6) Pedestrian access shall be provided that links public transportation stops to adjacent building entrances and pedestrian connections to surrounding uses.
- 7) Safety and visibility shall be considered in the design of both public spaces and pathways for the security of residents and their guests.
- 8) Pedestrian connections shall be provided between buildings and parking areas.



Locating public spaces along corridors of pedestrian activity maximizes their use.



Pedestrian pathways should connect different uses within the project.



Access to parking areas should be clearly marked.



6-12



Example of a mixed-use structure with parking incorporated on the upper levels.



Straight-in, diagonal, or parallel parking should be provided along corridors of high pedestrian activity.



Vehicular and pedestrian circulation within parking areas should be separated.

6.1.4 PARKING AREAS

- 1) Entrance and exit points for parking areas and structures should be well marked with streetscape and landscape features, including enhanced paving, landscaping, and architectural features.
- 2) Entry drives into parking areas should be located as far as practical from street intersections.
- 3) Parking areas should be clustered where feasible, and large, expansive parking lots should be minimized to the greatest extent possible. This maximizes security and efficient access.
- 4) Parking areas shall be located behind or to the side of commercial buildings where possible. No more than 10 percent of the required parking shall be located adjacent to service loading areas within the project.
- 5) Parking should be screened from roadways with landscaped medians, berms, trellises, grade changes, or placement behind buildings.
- 6) One tree for every four spaces shall be planted within the parking areas. They may be clustered or planted in a standardized fashion, so long as they provide shade for vehicles and pedestrians along walkways and parking lot entrance points.
- 7) Use of split-level parking structures is encouraged for all uses. Either underground or tuck-under parking is encouraged for mixed-use or residential uses.
- 8) The use of “wrap” structures—where the shops, offices, and/or residential units are wrapped around a centralized parking structure—is highly encouraged.
- 9) Parking areas of 100 or more cars should exhibit clear circulation hierarchy, with only ADA parking permitted on the primary access drives.



- 10) On-street parking should be provided along the esplanade and adjacent to other high-activity areas.
- 11) Parking areas should clearly separate vehicular and pedestrian circulation systems. Pedestrian connections through parking areas should provide landscaping and amenities to create visual interest, pedestrian access, and rest breaks over long distances of pavement.
- 12) Public parking for commercial or office uses should be clearly separated from private residential parking areas by signage, pavement markings, and/or physical separation.



Special paving treatments notify vehicles of areas of high pedestrian activity.

- 13) Residential parking areas should be located as close as possible to residential uses. A space should be no more than 250 feet from the unit it serves.

6.1.5 TRAFFIC-CALMING AREAS

- 1) Use of neck-downs, bulb-outs (which can also act as planters), and other traffic calming measures are encouraged along the length of local streets and along the central esplanade.
- 2) Minimize the radius of corners where pedestrian walkways cross at intersections.
- 3) Utilize a variety of special paving treatments to identify and visually enhance intersections and pedestrian crossings.
- 4) Discourage cut-through vehicular movement to ensure that traffic noise and vibration levels are minimized throughout residential areas.



Variation in street width with the use of neck-downs and bulb-outs slows the pace of traffic.

6.1.6 SERVICE, TRASH, AND UTILITY AREAS

- 1) Service and utility areas should be incorporated within the building envelope. If this is not possible, these areas shall be oriented away from public view and provide ornamental screening. Roof-mounted mechanical equipment is highly recommended.
- 2) Exterior roof-access ladders should be avoided. Access should be provided from within buildings.



Utility areas should be discreetly located and screened from view.

- 3) Exterior on-site facilities, such as sewer, gas, water, electric, telephone, and communications equipment, should be installed underground where feasible. Transformers and other utility equipment that must be aboveground should be screened and incorporated into the adjacent structure or landscape wherever possible. An exception includes equipment that is outside of public view, e.g., within a parking structure.
- 4) Trash and recycling enclosures shall be contained within the building envelope. If this is infeasible, locate them in discreet places and design enclosures to architecturally integrate with the overall design theme of the development. Trash and recycling enclosures should be designed and constructed of concrete masonry units with finishes of similar materials as the project building.
- 5) Trash and recycling enclosures should be planted with vines if adjacent to or within a landscaped area to help screen the enclosure. When trash or recycling enclosures are placed in plain view away from other structures (such as in a surface parking lot), they shall incorporate decorative roof beams or trellises.



Service areas and trash enclosures should be incorporated into the building structure and screened from view.



Building articulation and modulation provides an attractive streetscape.

- 6) Trash and recycling enclosures and loading areas must be separated from adjacent parking stalls by minimum of 4-foot-wide planters, which shall contain low-growing plant material.
- 7) Trash and recycling enclosures are to be large enough to contain the refuse generated by that site. A separate pedestrian access shall be provided to each enclosure so that large gates do not have to be opened.

6.2 BUILDING DESIGN

The purpose of the Building Design Guidelines is to ensure a high level of architectural quality and an attention to detail. The Section 19 Specific Plan does not recommend a particular architectural style, but rather recommends consistency in styles throughout the project, and promotes a unique style of building design achieved through the creative use of massing, roof forms, and façades. The following guidelines direct the building form, architectural style, design details, and materials.



6.2.1 MASSING, SCALE AND FORMS

- 1) Buildings should be divided into distinct massing elements. Building massing addresses wall plan location, wall heights, and roof levels. Façades should be articulated with significant use of architectural elements and details, and repetitive elevations should be avoided by using a variety of building forms.
- 2) A variety of building heights and rooflines is encouraged to create visual interest. In residential areas, a combination of one-, one-and-a-half, and two-story unit faces should be utilized to create visual interest.
- 3) As individual building ground floor elevations step down or up to meet finish grade elevations along any building face, this vertical change in building elevation shall be accompanied by a minimum 2-foot horizontal break in building floor plate at the same point as the vertical change in building section (i.e., the building façade should be set back or pushed forward an additional two feet).
- 4) Building façades should be detailed in a way that makes them appear smaller. This can be achieved by articulating the separate floors with horizontal bands or by increasing the detail on the building at the street or ground level. All buildings should have a visual base that allows the building to appear more human in scale.
- 5) Allow for visual relief to break up long, blank, unarticulated building façades. There shall be no long, flat expanses of walls that exceed 50 feet (100 feet for buildings larger than 50,000 square feet in size) without at least two of the following: color change, material change, texture change, plane projections or recesses, trellises or vines, balconies, or windows.
- 6) Maximum total bland wall (without windows or entrances) shall not exceed 30 percent of the first-story wall.



Every side of the building façade should incorporate architectural elements to eliminate the appearance of blank walls.



Recessed entryways allow for transition space between the sidewalk and the building interior.



Example of building articulation through window placement.



Each residential unit should be recognizable from the building façade.

6-16



Architectural details such as awnings add interest to the building façade.



Architectural styles should be complementary to the desert landscape and promote visual diversity and interest.

- 7) Building entryways should be clearly marked and emphasized to invite passing pedestrians and break up building massing. Projecting or recessing building entrances is encouraged. Recessed entries allow the pedestrian space to transition from the sidewalk to the interior of the building.
- 8) Surface detailing shall not substitute for variation in building massing.
- 9) Vertical and horizontal articulation, such as strategic placement of window and door openings, or through the use of balconies, awnings, and canopies, should be incorporated into structures.
- 10) Maximizing daylight, natural ventilation, and outward views is recommended, especially through the use of courtyards, plazas, and atriums.
- 11) Public plazas, outdoor dining, and other pedestrian-oriented activities are encouraged to divide ground-level building façades.
- 12) For residential development, each housing unit shall be individually recognizable, either by varying front setbacks within the same structure or by staggering unit plans. The distinction between units also derives from projecting features such as balconies, porches, bays, and dormers.
- 13) Residential units shall be configured and oriented to provide privacy and individuality.
- 14) Long balconies and corridors that provide internal access to residential units are not permitted.

6.2.2 ARCHITECTURAL STYLE

- 1) The overall character of the Section 19 Specific Plan project should be established through complementary architectural styles that are compatible with the surrounding architecture of the existing communities.
- 2) Within the project, visual diversity should be promoted through the use of different, though complimentary, architectural styles. For example, different styles may be used to distinguish between residential and commercial buildings.



- 3) Chosen architectural styles should incorporate high quality architectural detail that includes the use of arches, arcades, loggias, towers, variations in building form, and color blocking to define buildings. Franchise architecture is strongly discouraged.
- 4) All exterior elevations shall receive architectural treatments, with an emphasis on the front façades.

6.2.3 DESIGN DETAILS

- 1) Design details should be included for each building. These include some combination of:
 - a) light fixtures
 - b) balconies on the second or higher floors
 - c) decorative bulkheads
 - d) shutters
 - e) awnings for windows or balconies
 - f) decorative cornices
 - g) pedestrian arcades
- 2) Building elements should be designed in a way that enhances the visibility of merchandise and encourages window shopping.
- 3) Depth and shadow interest should be incorporated with the addition of offsets, popouts, and overhangs.
- 4) Tower features help create the image of a landmark place, and add architectural interest to otherwise standard structures. The scale of tower details should be designed to create a statement, but should not appear imposing to adjacent plazas and walkways.
- 5) Energy efficiency is encouraged through the use of window overhangs, arcades, solar water heating, advanced heating and cooling systems, or other conservation measures.

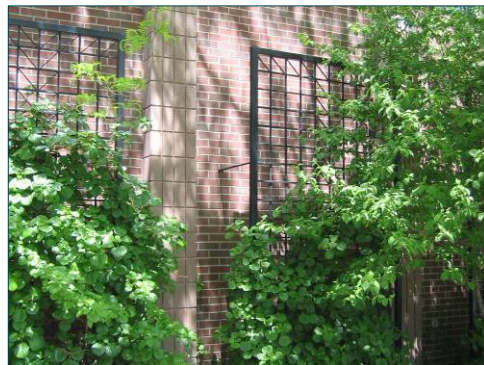


Awnings, balconies, and arcades should highlight entries and promote window shopping.

6-17



Tower features add a landmark character to the project.



Vines and trellises should be used to reduce the impact of large building expanses.

- 6) Vines and trellises are encouraged for large expanses of building faces to cool and provide shade for building surface, and soften the wall's appearance.

6.2.4 MATERIALS AND COLORS

- 1) High quality materials that weather well over time, with the appropriate use of colors and textures, should be used for building materials and design elements. Materials should convey a sense of durability and permanence.
- 2) Only materials that perform well in the desert environment are permitted. Prohibited materials include: plain concrete block, plain concrete, corrugated metal, plywood, sheet pressboard and vinyl siding. The use of highly reflective materials is discouraged.
- 3) To avoid monotony, a variety of color schemes is encouraged. However, building color and materials should be complementary throughout the project and consistent with the surrounding architectural styles.



Example of different colors, textures, and materials.



The variety in rooflines should be consistent with the building's architectural style.

- 4) Colors should include a base color and accent colors. Generally, a minimum of three colors should be used for each building. Color schemes should be selected with a harmonious range of accent materials and roof profile colors.

6.2.5 ROOF FORM

- 1) Breaks should be provided in roofline ridges to create a variety in roof form and elevation appearance.
- 2) A variety of roof types are permitted and encouraged within the project, including hip, gable, and shed roofs.
- 3) Roof types should be consistent with the architectural concept and the style, materials, and scale of the building.
- 4) Roof heights, pitches, and planes should vary to create interplay between the roof and walls of the structure. Varying roof pitches on the same building shall be avoided unless they are integral to the architectural style or extending over porches and balconies.



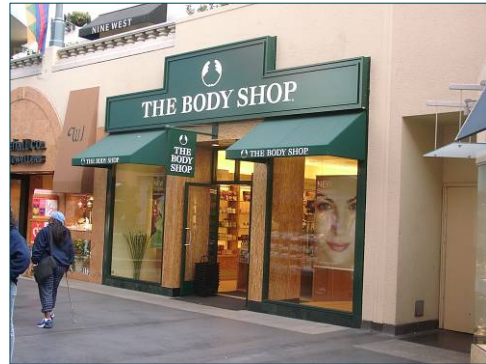
6.3 SIGNAGE DESIGN

Signage identifies places, provides direction, and advertises businesses. Along with communicating information, signage should add to the character of the community and reinforce a sense of place. These guidelines address the general design characteristics that pertain to the major sign systems for the Section 19 Specific Plan. The major systems of signage include freeway-oriented signs, primary entrance signs, secondary entrance signs, and individual project signs.

In addition to these guidelines, a sign program shall be required on a project-by-project basis. The sign program will identify the hierarchy of signs with a common theme, and specify the signage location and style. These sign programs shall be consistent and complementary within the Specific Plan.

6.3.1 GENERAL

- 1) Signs shall consist of high quality materials and color palettes that reflect the architectural themes of the Section 19 project. The designs should also complement the architecture of the surrounding environment.
- 2) The design of all signage within the project should be consistent and complimentary.
- 3) Sign programs shall respect the following signage hierarchy:
 - a) Freeway signage
 - b) Primary project entrance signage
 - c) Secondary project entrance signage
 - d) Individual project signs
- 4) Internally illuminated signs are discouraged. Externally illuminated signs or backlighting of individual sign letters should be the standard.
- 5) The use of pole signs, roof signs, temporary lettering on windows, and blinking/flashing signs are prohibited. The use of temporary signs is discouraged.
- 6) The use of natural stone as a base material is encouraged.



Example of pedestrian- and vehicular-oriented signage for a storefront.



Signs should use a variety of natural materials.

- 7) The location and placement of signs should not obstruct pedestrian or vehicular movement.

6.3.2 INDIVIDUAL SIGNAGE

- 1) Freeway signage should consist of a series of three repeated four-sided obelisks situated at the northern edge of the property adjacent and oriented to the freeway. Each form should contain a definable base, center and top, as well as the City of Rancho Mirage logo. The center section of each form may contain the signage for significant tenants.
- 2) Primary project entrance signage should be located at the corner of Dinah Shore Drive and Bob Hope Drive, and at the intersection of A Street and Bob Hope Drive. Tenant signage shall not be incorporated into primary project entrance signage.
- 3) Building signage shall be one sided, secured to building façades, and have a maximum area of 0.5 square foot per linear foot of tenant street frontage, up to a total of 50 square feet. In instances where a building has the principal entrance on a side façade, the side façade may be counted as street frontage in calculating maximum sign area.

6-20



Building signage should be proportional in size and complement the building façade.



Example of an externally illuminated sign.

- 4) Maximum signage heights shall be as follows:
 - a) Freeway signage: 100 feet
 - b) Primary project entrance signage: 15 feet
 - c) Secondary project entrance signage: 10 feet

6.4 LIGHTING DESIGN

Lighting design throughout the Section 19 project will highlight design and landscaping features, reinforce the community theme, and help ensure pedestrian and vehicular safety. Well designed lighting fixtures also establish quality design. This program addresses lighting for roadways, parking areas, and pedestrian areas, as well as architectural and landscape lighting.

6.4.1 GENERAL

- 1) Lighting shall be designed to protect the beauty of the desert sky and shall respect the requirements and guidelines of the Palomar restricted nighttime light zone, as per Riverside County's Ordinance No. 655. Up-lighting is discouraged. Maximum lamp wattage requirements should be established for



different lighting types to minimize obtrusive and unnecessary lighting and conserve energy resources to the greatest extent possible.

- 2) Warm white light is encouraged. Blinking, flashing, and oscillating lights are prohibited. Colored lights are not encouraged unless they contribute to the theming of commercial areas or establishments. Overly bright or glaring lights are prohibited.
- 3) Light fixtures should be consistent throughout the Section 19 project, and should be complementary to the architectural styles of the area.
- 4) Standards and fixtures shall comply with local and state safety and illumination requirements.
- 5) Exterior lighting should be designed and located so as not to project off-site or onto adjacent uses. This is especially critical with neighboring residential uses.
- 6) Outdoor lighting associated with the commercial uses shall not adversely impact the on-site or surrounding residential uses, but shall provide sufficient illumination for access and security purposes.
- 7) Automatic timers should be programmed to maximize personal safety at night while conserving energy. They should be reset seasonally to match the flux of dusk and dawn.

6.4.2 ROADWAY LIGHTING

Lighting fixtures on roadways contribute greatly to the visual quality of a streetscape and improve vehicular and pedestrian safety.

- 1) Stylized roadway lighting fixtures shall contribute to the community theme and establish a standard of quality for the entire development.
- 2) Lighting should be positioned to enhance safety at key points along the roadway, including intersections and crosswalks.



Outdoor lighting should not produce glare for surrounding uses.



Stylized lighting fixtures should coordinate with the project's sign program.



Stylized roadway lighting should complement the surrounding architectural theme.



Parking area lighting should promote security.

6-22



Pedestrian spaces should be well lit for evening and night use.



Example of architectural lighting flush with the building features.

6.4.3 PARKING AREA LIGHTING

Lighting for parking areas is crucial to the personal safety of the user, and should be designed at a human scale.

- 1) Parking area lighting should be designed using many small-scaled lights rather than fewer excessively tall lights. Parking area lighting shall be permitted to be up to 18 feet in height, or up to 25 feet in height with a CUP and approval of the Director of Community Development.
- 2) Lighting fixtures should be a continuation of the theme of surrounding architectural styles, and be in keeping with the quality of surrounding buildings.
- 3) Full cut-off luminaires should be obligatory for parking area lighting.

6.4.4 PEDESTRIAN AREA LIGHTING

It is very important that pedestrian areas, including public spaces and pathways, are well lit for evening and night use to ensure safety of residents and visitors.

- 1) Lighting in these areas should be scaled for pedestrians and consistent with the surrounding architectural theme.
- 2) If the lighting product is a lamppost style, the fixture should be not more than 12 feet high.
- 3) Fully shielded low wattage luminaires should be employed for pedestrian lighting fixtures.
- 4) Where appropriate, pocket lighting may be incorporated in walls, stairs, or bollards. Low wattage and fully shielded luminaires should be used for pedestrian bollard lighting.

6.4.5 ARCHITECTURAL LIGHTING

To accent walls, entries, and decorative architectural features, architectural lighting should be included in the design of buildings. Lighting should be used to subtly highlight architectural features, but not as an attempt to advertise the building to passing vehicles.



- 1) Architectural lighting should either be incorporated into design features of the building or concealed and flush with building walls.
- 2) Architectural lighting should not be visible in daylight.
- 3) Architecturally mounted luminaires shall be low wattage and provide lamp shielding.

6.4.6 LANDSCAPE LIGHTING

Landscape lighting should be used to highlight important landscape features.

- 1) String lights—nonblinking with white bulbs—may be used to accent trees or trellises within public spaces to create a festive atmosphere at night.
- 2) Landscape lighting fixtures should be concealed or flush with grade, unless their design plays a role in defining the character of the surrounding space.



String lights may be used to accent trees and create a festive night atmosphere.

6.5 WALL AND FENCE DESIGN

Section 19 is intended to function as a large, mixed-use project that facilitates a high level of pedestrian connectivity. Walls, fences, and gates are generally discouraged unless needed to screen uses such as trash receptacles or service areas.

- 1) If walls, gates, or fences are necessary, they should not exceed 6 feet in height. Solid walls should only be used when absolutely essential.
- 2) Walls, fences, and gates should appear consistent in style and material, complementing the surrounding architectural styles.
- 3) Walls, fences, and gates should be constructed of high quality, durable materials. No chain-link is permitted within the Section 19 Specific Plan.
- 4) Landscaping elements should be densely planted and layered to provide screening. Vines and trellises are encouraged to help soften hard edges and screen walls from view.



Wall breaks and dense planting breaks up the façade of solid walls.



Creativity in wall and fence design adds a sculptural quality to the project.

- 5) Where feasible, dense planting and hedges should be used in place of a wall or fence.
- 6) Break up long stretches of walls or fences with landscape screening, wall breaks, vertical piers or columns, or façade detailing.

6.6 HARDSCAPE DESIGN

Hardscape elements are objects that are decorative and functional elements of the streetscape, aside from landscaping features. Hardscape elements may include paving treatments, benches, chairs, tables, shade structures, bollards, drinking fountains, tree grates, trash receptacles, planters, bicycle loops, and newspaper stands. Some combination of these should be used as decorative elements for streetscapes and public spaces.

6-24

6.6.1 GENERAL

- 1) Consistent design themes should be used for all the street furnishing elements throughout the project, and should complement surrounding architectural styles.



Hardscape elements include fountains, kiosks, bollards, chairs, benches, and other street furnishings.

- 2) Furnishings should be constructed of high-quality, durable materials that can withstand the elements without showing wear.
- 3) Furnishings should be placed where pedestrian traffic, viewsheds, or building ingress and egress will not be obstructed.
- 4) Decorative features that prevent skateboarding along street furnishings shall be incorporated within the design of the street furnishings.

6.6.2 PAVING TREATMENTS

Distinctive paving treatments give visual clues to users and emphasize different areas within the streetscape and public spaces. Patterned, enriched, or textured paving treatments should be used to denote important crosswalks, highlight pedestrian pathways and public spaces, and delineate parking areas adjacent to areas of high pedestrian activity. The use of specialized paving materials is encouraged along the major thoroughways and through public spaces.



Unique paving treatments highlight public spaces and pedestrian areas along the streetscape.

- 1) Even and durable surfaces should be used in high pedestrian circulation areas.



- 2) The identification of building access points through the use of patterns and colors is encouraged.
- 3) Acceptable paving materials for the streetscape and public spaces include interlocking concrete pavers, native stone, precast pavers, and brick. The use of stained or stamped concrete should be discouraged, unless highly decorative and creative in nature.
- 4) Painted paving surfaces should not be used except to indicate traffic lanes or parking spaces.

6.6.3 SEATING AREAS

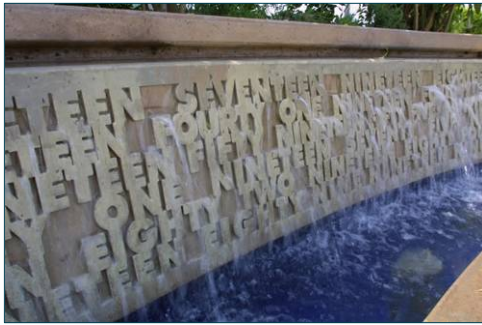
- 1) Benches should be placed at selected locations along major thoroughways and focused around public spaces to promote pedestrian activity.
- 2) Seating areas adjacent to landscaping and public spaces invite people to enjoy the amenities provided. Moveable furniture should be provided to permit for a greater variety of seating arrangements and encourage use by individuals or groups.
- 3) Planters that also function as seating areas should be provided.
- 4) Trellises, umbrellas, gazebos, and other forms of shade structures are encouraged along the major thoroughways and public spaces within the Section 19 project.
- 5) Market umbrellas and awnings are encouraged, as they provide shade, soften hard building edges, and add color to the sidewalk area in front of shops and restaurants.



Seating areas should be located around public spaces and at focal points along the streetscape.



Shade structures provide comfort and incentives for pedestrians to use public spaces.



Water feature design may incorporate public art elements.



Planters may also function as bollards.



Locate bicycle racks near areas of pedestrian activity.

6.6.4 WATER FEATURES AND PUBLIC ART

Water features and public art are streetscape elements that may be used to highlight public spaces and focal points along the streetscape. These components add an interesting characteristic to public spaces, and may be used as decorative features or interactive elements.

- 1) Water features and public art pieces should be located along viewsheds as accent features for public spaces and areas of high pedestrian activity.
- 2) These streetscape elements should not be placed where pedestrian traffic or building ingress and egress would be obstructed.
- 3) Water features and public art shall be well maintained to preserve their quality and appearance.

6.6.5 MISCELLANEOUS HARDSCAPE ELEMENTS

- 1) Bollards should be located at intersections of local internal access streets and external periphery streets and thoroughfares, and in combination with handicapped/wheelchair access ramps, in order to protect pedestrians from vehicular traffic that might inadvertently try to access sidewalk areas via ramp sections along the street edge.
- 2) Bollards should be placed a maximum of four feet on center in order to prevent vehicular access, and chain loops between bollards should be avoided in order to minimize pedestrian hazards.
- 3) Bollards should be fabricated and finished to match surrounding street furnishing elements.
- 4) Bicycle racks should be provided at convenient locations throughout the Section 19 Specific Plan area.
- 5) Tree grates should be uniform in size according to the tree hierarchy established in the landscape plan.



For example, major trees along the primary thoroughways should share similarly sized tree grates, while trees clustered around a separate public space may share a different set of consistently sized tree grates.

- 6) Trash receptacles should be located on at least one corner of all internal street intersections.
- 7) Additional trash receptacles should be adjacent to outdoor dining and parking areas.

6.7 ENVIRONMENTAL DESIGN

The environmental setting of Section 19 requires that certain measures be taken to protect the long-term social and economic investments that will be made through this Specific Plan. In particular, the hot, dry climate in Rancho Mirage calls for drought-tolerant landscaping and increased energy requirements to cool buildings. Additionally, blowsand has the potential to scar buildings and vehicles, while also creating a generally unpleasant atmosphere for walking or sitting outside. Ironically, these two elements—the sun and wind—generate renewable energy for many buildings and developments in the Coachella Valley through solar technology and local windfarms.

The following standards and guidelines will help to ensure that development created through the Specific Plan is designed to take advantage of the opportunities and protect against the dangers of the desert environment.

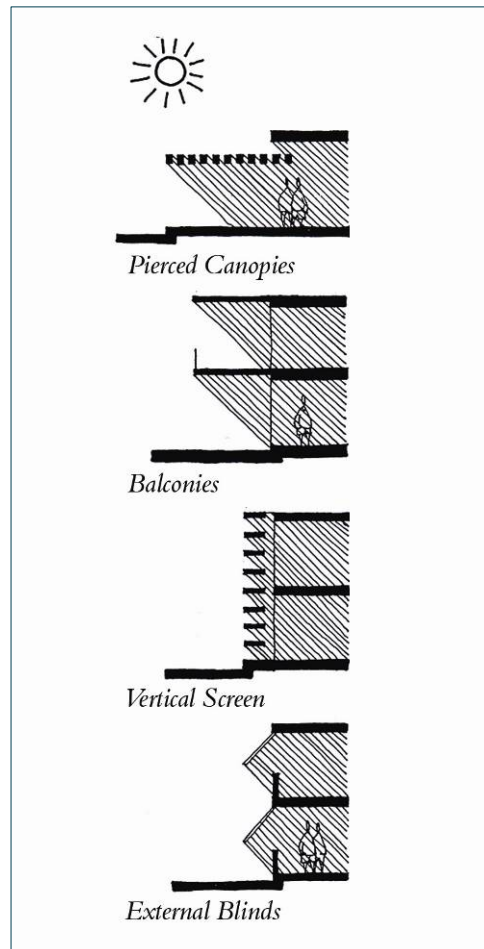
6.7.1 SITE DESIGN AND INFRASTRUCTURE

- 1) Shading devices and techniques, such as roof overhangs, arcades, and trees, shall be incorporated into buildings and outdoor spaces to minimize unnecessary solar heat gain. Particular emphasis shall be placed on shading devices when east-west orientation is appropriate, such as development along Center Street that is encouraged to face the esplanade.



Trash receptacles should be located adjacent to pedestrian areas to maximize use.

6-27



Examples of different shading devices and techniques.

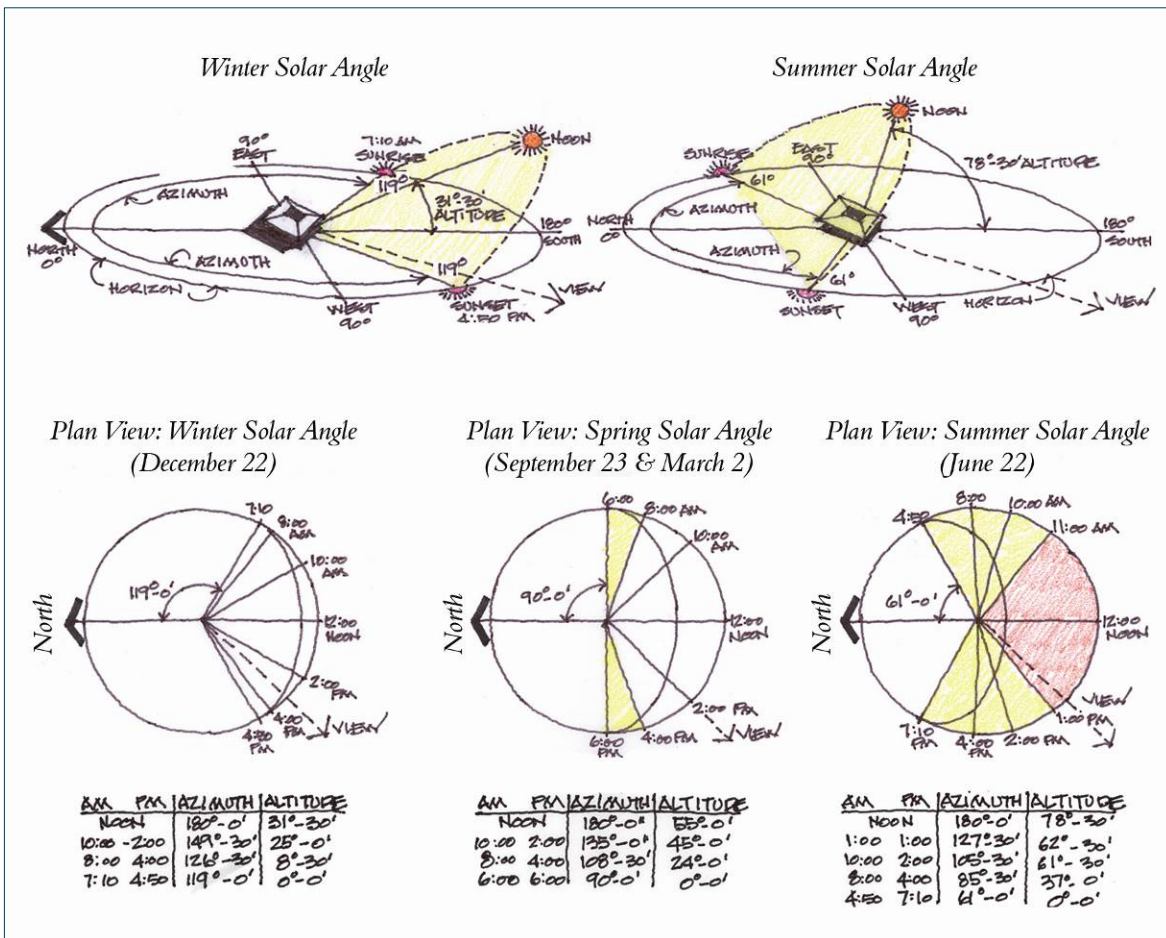


6-28



Solar orientation of the building, overhangs, and other devices placed on the exterior of buildings reduce direct sunlight into interiors, lowering heat gain and the amount of energy needed for cooling.

- Whenever appropriate, orient buildings so that the long axis of the building is oriented east–west to maximize the opportunity for north- and south-facing windows, which receive indirect, diffused light with low heat gain for the building, reducing cooling costs during summer months. Outdoor spaces such as plazas should be similarly oriented. For further information regarding the sun angles for different seasons in the Coachella Valley, see the graphic below.



The solar angles illustrated above should influence building orientation within the Section 19 Specific Plan.



- 3) The use of recycled-content aggregate (reused and crushed concrete and asphalt) is highly encouraged in areas such as, but not limited to, drainage backfill and under driveways, sidewalks, and building slabs.
- 4) The use of grass swales, particularly with native or drought-tolerant grasses, is encouraged to collect and filter water runoff.
- 5) Developments should optimize stormwater retention in surface or subsurface storage areas for nonpotable uses such as irrigation and sewage conveyance.
- 6) Developments shall develop a recycling program for residential and commercial uses to recycle paper, glass, plastic, and other by-products of business or residential activities.
- 7) Projects are highly encouraged to exceed Coachella Valley Water District water efficiency goals by 10 percent.

6-29

6.7.2 BUILDING DESIGN AND MATERIALS

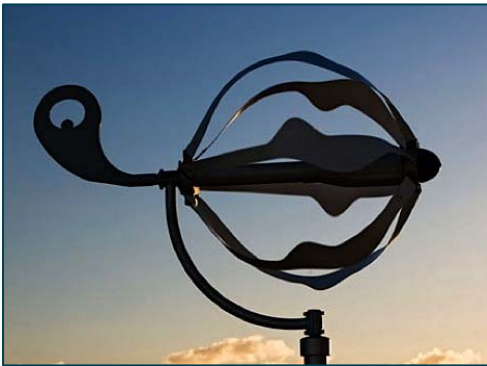
- 1) The pursuit of already established sustainable best management practices, such as Leadership in Energy and Environmental Design (LEED) certification, ComfortWise, and EnergyStar Home, is strongly encouraged. The comprehensiveness of these certification programs guarantees, for their respective types of development, the achievement of a high minimum standard. For maximum flexibility, however, developers and builders may implement sustainable building and development practices most appropriate to the specific context within Rancho Mirage.
- 2) Builders are also encouraged to participate in programs offered or sponsored by local utilities such as California EnergyStar New Homes Program, Residential Property Development Program, California Home Energy Efficiency Rating System (CHEERS) Program, and Savings by Design Program.
- 3) Buildings should be designed to facilitate and accommodate photovoltaic cells for solar power. Solar-heated water is one efficient way to reduce energy needed for household activities.



Solar power technology offers a clean source of energy for the residents and businesses of Section 19. Buildings and parking areas should be designed to maximize the potential use of solar energy technology.



6-30



Microturbines are becoming smaller, more attractive, and increasingly efficient, with some needing winds no greater than 2 miles per hour to generate enough energy to support street lighting.

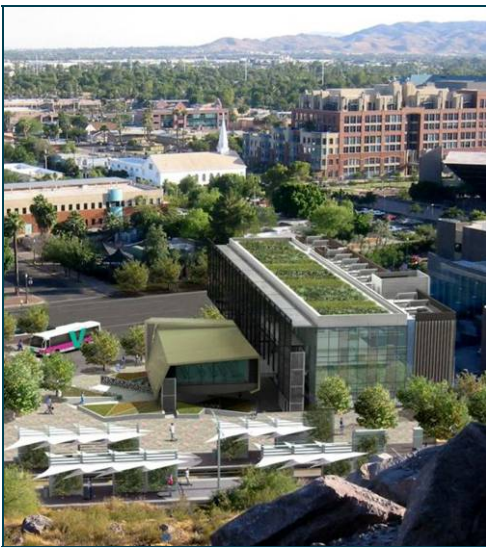
- 4) Whenever possible, buildings should be designed and oriented to facilitate the generation of wind energy. Small microturbines can be integrated into building facades, rooflines, streetlights, artwork, and other publicly visible areas to provide attractive and sustainable methods of generating energy for lighting and other elements of public or private infrastructure.
- 5) Architectural features that increase daylighting, such as light shelves that bounce light further into interior spaces, should be installed to reduce the need for additional electrical light.
- 6) Developments shall minimize light pollution by avoiding outdoor lighting where unnecessary, emphasizing shielded fixtures and avoiding overhead lighting of areas such as walkways. Low-scale, accent, and back lighting shall be used to highlight key entry points, signage, enhanced intersections, and feature landscaping. The use of LED (light-emitting diode) lighting or OLED (organic light-emitting diode) lighting is encouraged, so long as blue or cool-white LEDs are shielded properly to prevent light pollution.
- 7) Builders are encouraged to use flooring and insulation products that are low-emitting in terms of volatile organic compounds (VOCs) and formaldehyde. Low- and zero-VOC paints, finishes, adhesives, caulks, and other substances are also recommended to improve indoor air quality and reduce the harmful health effects of off-gassing.
- 8) The use of light-colored roofing materials to reflect heat and reduce cooling requirements of buildings, particularly Energy Star-labeled roofing materials, is encouraged.
- 9) Energy Star-labeled appliances (e.g., water heaters—particularly tankless) should be installed to the greatest feasible extent. Solar, electric (efficiency rating of at least 0.92), or lower-nitrogen-oxide (as defined by the Air Quality Management District) gas-fired water heaters are strongly encouraged.
- 10) Buildings shall not be constructed primarily of materials that perform poorly in environments subject to blowsand, such as glass and wood.



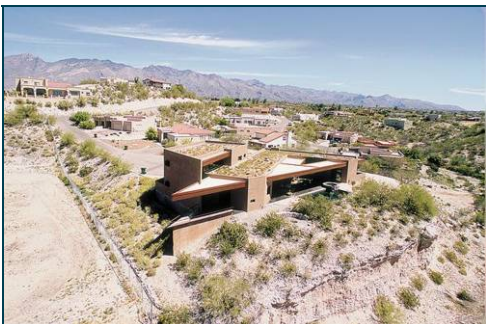
- 11) A green roof is a roof of a building that is partially or completely covered with vegetation and soil, planted over a waterproofing membrane. Buildings are strongly encouraged to incorporate green roofs as a method of providing open space, thermal insulation/heat shields, and stormwater retention; reducing the building's energy use; and offering a visual benefit for people at the street level, on top of the building itself, and on top of adjacent buildings.

Green roofs shall use native, drought tolerant landscaping or native, edible landscaping. If the roof is to be used as an active open space, shade structures shall be provided that cover at least one-quarter of the area designated for open space.

Green roofs are preferred over Cool Roof technology. Given the site's topography and variety of building heights, which will create greater visual exposure of the rooftops of nearby buildings, green roofs are considered more attractive and functional than Cool Roof techniques. Cool Roof technology involves the application of high-albedo roof coatings or paint ("albedo" is a term that refers to a surface's ability to reflect and reject heat; high-albedo coatings are typically white or light grey in color and smooth in texture).



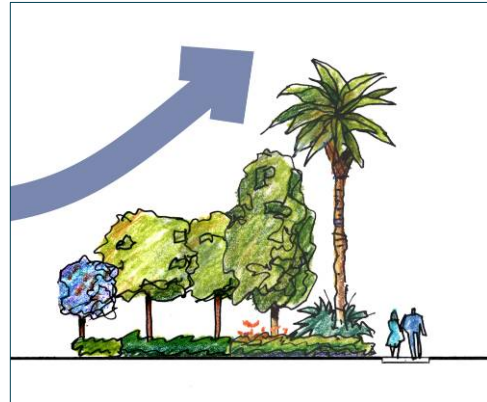
Examples of green roofs in desert environments can be found in public buildings such as the Transit Center in Tempe, Arizona (above and left), and in private residences, such as this two-story home in Tucson, Arizona (below).



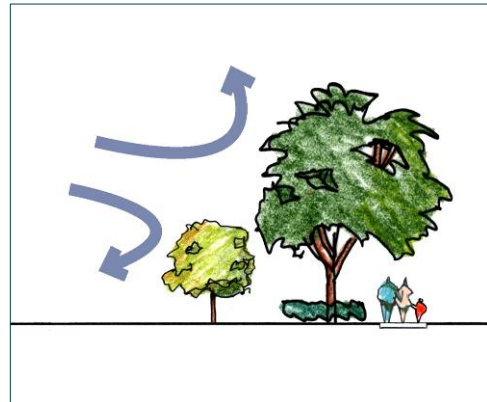


6.7.3 LANDSCAPING

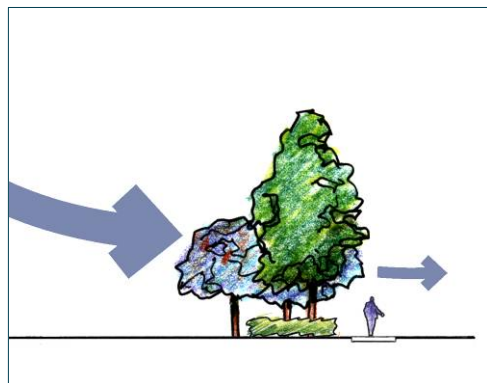
- 1) Landscaping should consist primarily of plant materials and species that are native, drought tolerant and/or have low water demand. Plants with similar water requirements should be grouped together, a technique known as hydrozoning.
- 2) Automated, high-efficiency irrigation systems (such as bubbler irrigation and low-angle, low-flow sprayheads) shall be installed to reduce water demand and use. Moisture sensors and other similar irrigation technology should be utilized to ensure that landscaping is watered only as needed.
- 3) Grey water and/or collected rainwater should be used wherever feasible for landscape irrigation.
- 4) Drought-tolerant or native canopy or deciduous tree species should be located around and near buildings, walls, windows, and paved areas to reduce solar heat absorbed by buildings and paved areas.
- 5) A combination of large and small trees and shrubs should be installed to mitigate the effects of blowsand conditions and allow for sand filtration, particularly along the perimeter of the Specific Plan boundary. Non-vegetative groundcover should also be wind resistant and allow for sand filtration, such as 3/8-inch cobblestone. Care should be taken to design the site so that the sand collection areas (on the windward side of the vegetative screen) are in spaces that are appropriate and easily maintained.
- 6) Prior to and during site construction, measures shall be taken to stabilize the sand and soil to prevent erosion and minimize blowsand.
- 7) Prior to construction, windbreaks and wind-resistant groundcover shall be installed to reduce the effects of blowsand along the western and eastern boundaries of the specific plan (along Bob Hope Drive and Key Largo Avenue). Examples of windbreaks include temporary nurseries or orchards (particularly of landscaping that will be used in future development), sand fences, and art installations that can strategically slow the wind and filter or capture blowsand. Examples of



Wind control: deflection.



Wind control: obstruction.



Wind control: filtration.



- wind resistant groundcover and other windbreak landscaping are included in the plant palette listed in Table 3.
- 8) To the extent possible, windbreaks and wind-resistant groundcover shall be installed on vacant properties outside of the specific plan to the west of Bob Hope Drive and to the east of Key Largo Avenue.
 - 9) Edible landscaping is encouraged throughout the project, provided such vegetation is native and properly maintained. Edible landscaping should be harvested as appropriate to prevent an unsightly appearance on the plant or surrounding ground. Only plants and trees that maintain year-round vegetative mass should be installed (though the harvesting of food may only occur on a seasonal basis). Examples of edible landscaping are included in the plant palette listed in Table 3.
 - 10) Buildings are strongly encouraged to incorporate green screen elements along otherwise blank walls and similar opportunity locations. These include:
 - 1) wall-mounted trellis panels;
 - 2) freestanding trellis fence;
 - 3) column trellis elements;
 - 4) aboveground planters; and
 - 5) horizontal shade structures.

In addition to aesthetic and environmental control values, these elements are also useful for security and graffiti control purposes. Such elements can be particularly useful on walls in areas that transition from a residential to a non-residential use.



Green screen elements such as this wall-mounted trellis can act as heat shields and enhance the façade of a building.



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7. General Plan Consistency

7-1

7.1 CONSISTENCY ANALYSIS

The Rancho Mirage General Plan (2005) vision, land use policies, and associated technical studies influenced the direction for the comprehensive programming and planning efforts of the Section 19 Specific Plan. This chapter documents the consistency of the Section 19 Specific Plan with the goals and policies of the Rancho Mirage General Plan.

**Table 11
Consistency Analysis**

Relevant Policy	Analysis of Project Consistency
LAND USE ELEMENT	
Citywide Land Uses, Goal 1: A resort residential community of desirable neighborhoods and a variety of community facilities and high quality development.	
Citywide Land Uses, Goal 2: A balanced mix of functionally integrated land uses, meeting general social and economic needs of the community through simplified, compatible and consistent land use and zoning designations.	
<p>Policy 1: Specific plans shall be required to ensure new development achieves high quality building, design, and development standards and provide amenities above those expected in conventional development.</p> <p>Policy 2: Specific plans shall be utilized to assure the phased, logical and cost-effective extension of infrastructure and buildout of new development.</p> <p>Policy 4: The City shall ensure adequate visibility and accessibility for commercial development while preserving the scenic viewsheds from adjoining properties and public rights-of-way.</p> <p>Policy 5: The City shall ensure privacy and safety for residential neighborhoods by providing adequate buffering and screening, particularly those adjoining or integrated with commercial developments.</p> <p>Policy 6: The Community Development and Economic Development Departments shall actively pursue opportunities to attract high quality retail commercial establishments and resort hotels in the City.</p> <p>Policy 7: The City shall maintain a cooperative planning process with appropriate jurisdictions, including the County of Riverside and the Agua Caliente Band of Cahuilla Indians, assuring an effective advisory role regarding any and all development and land use planning issues proposed within or in close proximity to the City and its Sphere of Influence.</p>	<p>The City has organized the Section 19 Specific Plan to create a unique mixed-use development that provides for commercial, resort, open space, and high quality residential land uses. The document sets forth guidelines and standards for site planning, architecture and design, landscape, and streetscape elements to produce a special place for the City of Rancho Mirage.</p> <p>The Specific Plan provides the phasing and infrastructure plans necessary to provide for the cohesive and organized development of the Section 19 area.</p> <p>The Section 19 Land Use Plan locates commercial land uses along major traffic corridors for maximum exposure. Building height restrictions provided in the Specific Plan are consistent with the General Plan and provide for the necessary viewsheds from public rights-of-way. Residential land uses are located away from the major arterial roads and the Union Pacific Railroad for adequate buffering from the associated noise and views. Mixed-use planning areas between the residential and commercial uses provide a transition and buffer area between these land uses.</p> <p>This distinctive mixed-use project with a combination of commercial, residential, resort, and live/work uses allows the City to realize a project that goes beyond the typical commercial or residential development. The standards and policies in this Specific Plan present opportunities for high quality commercial, resort, and residential development for the City.</p> <p>The Section 19 Specific Plan evolved from the integrated planning efforts between the City and affected property owners, including the Agua Caliente Band of Cahuilla Indians. The Land Use Plan successfully integrates the Agua Caliente Band of Cahuilla Indians' proposed development with the remaining Section 19 project.</p>
Affordable Housing, Goal 1: The preservation and enhancement of the predominantly low density, high quality residential character of the City.	
<p>Policy 1: Areas of existing residential development and surrounding vacant lands shall be planned in a manner that preserves neighborhood character and assures a consistent and compatible residential land use pattern.</p>	<p>The Section 19 Land Use Plan locates residential land uses along the eastern edge of the project, providing a consistent and compatible transition to the proposed residential community across Key Largo Avenue.</p>
<p>Policy 2: Density transfers (the transfer of allowable dwelling units from one area of land to another) may occur in planned residential</p>	<p>The Specific Plan includes a Residential Development Transfer Program that permits residential units to be transferred between</p>



Table 11
Consistency Analysis

Relevant Policy	Analysis of Project Consistency
<p>developments in conjunction with the provision of common area amenities and open space. Golf courses, greenbelts, pool areas and other open space uses incorporated into these developments shall be designated as Open Space areas to assure their preservation as such.</p> <p>Policy 3: The City shall consider the issues of slope disturbance, development area and lot coverage, view preservation, revegetation, compatibility, public safety, and access when assessing potential residential developments.</p> <p>Policy 4: Lower income housing shall be dispersed where feasible, appropriate, and compatible with surrounding land uses.</p>	<p>planning areas, subject to certain limitations and procedures.</p> <p>In addition, open space standards and requirements provide for the necessary common area amenities and open spaces needed for the Section 19 project.</p> <p>The existing slope for the Section 19 project will be preserved with the provided Grading Plan. The Development Regulations provide for the necessary density and intensity standards to control development area and lot coverage. Public safety and access conditions are provided in the Section 19 Circulation Plan.</p> <p>Lower income housing may be provided in any of the Residential and Mixed-Use Core planning areas, at the discretion of the City/property owner. Under the California Density Bonus Law, density bonus provisions may be afforded to qualifying affordable housing projects within Section 19. The Specific Plan also allows for buildings to increase in height with the addition of affordable housing units under the state density bonus.</p>
<p>Commercial Growth Potential, Goal 1: High quality commercial land uses conveniently and appropriately distributed throughout the City, meeting the community's current and future needs and taking full advantage of emerging development and economic opportunities.</p>	
<p>Policy 1: The City shall designate sufficient lands to provide revenue to the City and a full range of commercial services to the community and surrounding areas for present and future years.</p> <p>Policy 2: The City shall pursue high quality retail uses along Highway 111, within Section 19, and in other areas of the City.</p>	<p>The Specific Plan provides for the creation of up to 3,096,500 square feet of resort and commercial uses in a unique, pedestrian-friendly mixed-use plan. The Specific Plan facilitates an integrated plan between multiple property owners within the Section 19 area, encouraging redevelopment of the site as a unified whole to increase property values and maximize the commercial potential of the project site.</p>
<p>Institutional Uses, Goal 1: Institutional uses that are efficiently located to serve the community and are compatible with surrounding land uses.</p>	
<p>Policy 1: Institutional uses and facilities shall be developed in a manner that assures adequate levels of service, while remaining compatible with existing and future land uses.</p>	<p>The Specific Plan permits institutional uses and facilities (e.g., community centers, libraries, specialized schools), in various planning areas, which would not only serve the future Section 19 community, but also the surrounding communities. Additionally, individual project applicants within the Section 19 project area would be required to pay all applicable Development Impact Fees set forth by the City of Rancho Mirage, including school and library fees, to mitigate impacts on public services.</p>

**Table 11
Consistency Analysis**

Relevant Policy	Analysis of Project Consistency
Open Spaces, Goal 1: The conservation of open space areas that protect environmental resources, guard against environmental hazards, provide recreational opportunities, and enhance the aesthetic character of the City.	
Open Spaces, Goal 2: A land use pattern that preserves the City's resort residential atmosphere, including scenic resources such as hillside and mountain vistas, waterways, and native desert communities.	
Policy 3: The City shall maintain a Development Code that encourages the provision and preservation of open space areas through flexible development standards.	The open space standards require specific open space provisions for the different land uses within the Specific Plan. The open space standards allow the different land uses to combine open space requirements to create larger and more functional open space areas. These standards require the open space necessary to provide for recreational and people-gathering areas within the project. For usable common open space, a minimum level surface dimension of 20 feet in any direction is required.

CIRCULATION ELEMENT

Goal 1: A safe, efficient, attractive, and economical circulation network meeting current and future demands in a manner consistent with the resort residential character of the community

<p>Policy 1: The City's street system shall be designed and constructed to maximize mobility, minimize congestion, and assure that all intersections and street segments shall operate at LOS "D" or better during the peak hours of traffic, as generated by the buildout of the Land Use Plan.</p> <p>Policy 2: A detailed traffic analysis shall be required for development proposals or other activities that might potentially require roadway improvements above and beyond those evaluated in the Circulation Element and General Plan EIR.</p> <p>Policy 3: The City shall require improvements at critical intersections beyond those needed to meet standard levels-of-service at the discretion of the City Engineer.</p> <p>Policy 4: The number of access points and intersections along arterials shall be limited in order to preserve mid block and intersection capacities and to maintain public safety.</p> <p>Policy 5: Access points shall be coordinated between future development in Section 31 and any future development of the properties on the west side of Bob Hope Drive.</p> <p>Policy 6: The City shall actively participate in a wide range of regional transportation planning and programs to improve the capacity, efficiency, and safety of the shared circulation system.</p> <p>Policy 7: The City shall develop a system of continuous and convenient bicycle routes and multi-use trails to places of employment, shopping centers, schools, and other high activity areas; as well as a golf cart transportation program.</p>	<p>The roadways within the proposed project's circulation plan would be consistent with the roadway standards outlined in the City's General Plan. They would be designed to maximize the regional and local vehicular circulation system for the project area. Chapter 4 of the Specific Plan dictates the street dimensions and setback requirements that would create environments appropriate for residential, mixed-use, live/work, and commercial areas within the project site.</p> <p>Additionally, the design standards and guidelines outlined in the Specific Plan address streetscape elements that would enhance the character of each street. Further, a project-specific traffic study was completed for the proposed project and concluded that all intersections and street segments would operate at LOS D or better during the peak hours of traffic.</p> <p>The Circulation Plan for Section 19 streamlines and coordinates access points entering the project to maximize efficient circulation surrounding and within the project area. The Specific Plan also encourages the use of traffic calming measures within the project site to ensure the safety of pedestrians, such as the roundabout feature along Center Street. Truck routes are accommodated within the street sections along Dinah Shore Drive and portions of Bob Hope Drive.</p> <p>Alternative modes of transportation systems are incorporated in conjunction with the Section 19 Circulation Plan, including pedestrian, cyclist, golf cart, neighborhood electric vehicle, and public transit circulation. These systems connect to the surrounding routes to provide an alternative mode of transportation for residents and visitors.</p>
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**Table 11
Consistency Analysis**

Relevant Policy	Analysis of Project Consistency
<p>Policy 8: The local street system within developing neighborhoods shall be established through a cooperative public/private planning process.</p> <p>Policy 9: Circulation and access for undeveloped parcels shall be coordinated with surrounding properties.</p> <p>Policy 10: Streets within private planned residential areas shall be installed and maintained as private streets, and shall be developed in accordance with development standards set forth in the Zoning Ordinance and other applicable standards and guidelines.</p> <p>Policy 11: City streets should not be converted to private streets when it would diminish circulation alternatives.</p> <p>Policy 12: City truck routes shall be designated and limited to Ramon Road, Dinah Shore Drive, Gerald Ford Drive, Monterey Avenue, Highway 111, and portions of Bob Hope Drive, Frank Sinatra Drive, and Country Club Drive.</p>	<p>With the current SunLine transit route and the potential future multi-modal transit station adjacent to the Union Pacific Railroad, the site could benefit from the ability to use mass transit. The Specific Plan creates a concentration of residential and commercial uses adjacent to these transit lines to utilize this service.</p> <p>Many of the properties within the Specific Plan will be managed by property or homeowner associations. Prior to the approval of any new development, the applicant shall provide a clear description of the role of the association in providing and maintaining private roadways, amenities, landscaping, and other improvements.</p>
HOUSING ELEMENT	
<p>Goal 1: A variety of housing types that meet the needs of residents within the City.</p>	
<p>Goal 2: Housing to meet the needs of the City's lower income households.</p>	
<p>Goal 3: The preservation and maintenance of the City's affordable housing supply in a safe and sanitary condition.</p>	
<p>Policy 1: The General Plan shall provide for a mixture of residential densities dispersed throughout the City.</p> <p>Policy 2: The City's residential development standards shall allow for a diversity of housing types while adhering to the General Plan's community design policies.</p> <p>Policy 12: The City may, whenever it deems feasible and necessary, reduce, subsidize or defer development fees to facilitate the development of affordable housing.</p> <p>Policy 13: The City shall apply its density bonus provisions to all qualifying affordable housing projects.</p> <p>Policy 14: The Housing Authority shall prepare an analysis of the potential for additional very low, low and moderate income units within currently designated High Density lands in Section 19.</p>	<p>The Specific Plan provides a range of residential densities within the Mixed-Use Core and Residential planning areas. A maximum of 1,899 units are proposed, which includes high quality multifamily residential units in addition to mixed-use and live/work housing types. Residential uses incorporate both rental and for-sale dwelling units. The Development Standards provide for permitted uses, densities, setbacks, and building heights that are consistent with the vision of creating an exemplary community environment.</p> <p>Lower income housing may be provided within any of the Residential and Mixed-Use Core planning areas, at the discretion of the City/property owner. Under the California Density Bonus Law, density bonus provisions may be afforded to qualifying affordable housing projects within Section 19. The Specific Plan allows for buildings to increase in height with the addition of affordable housing units under the state density bonus.</p>

**Table 11
Consistency Analysis**

Relevant Policy	Analysis of Project Consistency
CONSERVATION AND OPEN SPACE ELEMENT	
Parks And Recreation Resources, Goal 1: A balanced system of parks, trails, and recreation facilities that meets active and passive recreation needs of all residents and visitors.	
Parks And Recreation Resources, Goal 2: Trails for recreational use in the Santa Rosa Mountains that allow City residents and visitors to experience the desert environment and that preserve the mountains and associated sensitive plants and animals in their natural state.	
<p>Policy 1: The City's park system shall consist of mini-parks, local parks, and community parks.</p> <p>Policy 2: To the extent feasible, the City shall provide at least 3 acres of local and community parkland per 1,000 in population, which include park facilities for all age segments of the population.</p> <p>Policy 3: To the extent feasible, the design of City parks and trails shall accommodate the special needs of the disabled and senior population in Rancho Mirage.</p> <p>Policy 4: The design of local parks shall consider neighborhood suggestions for facility needs.</p> <p>Policy 5: Class III bikeways shall only be permitted in the City where Class I or II bikeways are not feasible and where an essential regional bicycle route connection is missing.</p> <p>Policy 6: The City shall participate in regional trail planning programs.</p> <p>Policy 9: Through coordination with the local utilities, service providers, and the Coachella Valley Water District, the City shall maximize the use of flood control and utility easement areas for inclusion in a multi-use trail system providing alternative transportation links to parks and open space areas.</p>	<p>The open space standards require the necessary provisions to provide for recreational and people-gathering areas within the project. Open space requirements and streetscape design elements work to create a pedestrian-friendly environment for residents and visitors. The amenities provided for in the plan emphasize function and use with a minimum level surface dimension of 20 feet in any direction required for usable common open space.</p> <p>The Specific Plan requires 3 acres of parkland per 1,000 in population. Design Standards and Guidelines provide the location and orientation guidelines necessary for the design of local parks within the project. If future developments do not provide parkland space on the site, in-lieu fees must be paid.</p> <p>Alternative modes of transportation systems delineated in the Alternative Transportation Circulation Plan provide additional transit routes to accommodate different population segments within Rancho Mirage. Class III bikeways are permitted in order to link development to a potential future multi-modal transit station at the northern edge of the project.</p> <p>The Specific Plan recommends that the drainage channel incorporate a Class I bikeway to link the project to adjacent developments and planned trails.</p>
Biological Resources, Goal 1: The protection and preservation of biological resources within Rancho Mirage, especially sensitive and special status wildlife species and their natural habitats.	
<p>Policy 3: The City shall encourage the use of naturally occurring desert plant materials and discourage the use of non-native plant materials that are harmful to native plant and animal species in landscaping for development projects to the greatest extent possible.</p>	<p>The Landscape Plan utilizes native plant materials within the Plant Palette to encourage the use of these desert-friendly plants throughout the project's landscape theme.</p>
Energy and Mineral Resources, Goal 1: The conservation, efficient use, and thoughtful management of energy sources and mineral deposits.	
Energy and Mineral Resources, Goal 2: The long-term viability of limited and non-renewable resources.	
<p>Policy 1: The City shall promote energy efficiency and conservation in all areas of community development, including transportation, development planning, and public and private sector construction and operation, as well as in the full range of residential and non-residential projects.</p>	<p>The Land Use and Circulation Plans for Section 19 focus on the transportation and development planning to encourage alternative modes of transportation and promote efficient circulation and land use systems in connection with energy conservation. The potential multimodal transit station could allow residents and employees to</p>



**Table 11
Consistency Analysis**

Relevant Policy	Analysis of Project Consistency
<p>Policy 2: The General Plan and other community plans shall assure an efficient circulation system and land use pattern in the City.</p> <p>Policy 3: Major developments that provide significant employment centers shall be required to provide convenient and safe access to the public transit system.</p> <p>Policy 5: The City shall support public and private efforts to develop and operate alternative systems of solar and electrical production that take advantage of local renewable resources.</p>	<p>commute to and from the Section 19 area without significant reliance on personal automobiles. Access to the multimodal station through a variety of non-vehicular trails encourages convenient and safe access to the public transit system.</p> <p>The Environmental Design Guidelines also include provisions for energy efficiency and conservation through site design, building design, and landscaping measures.</p>
<p>Water Quality and Resources, Goal 1: A dependable, long-term supply of clean and healthful domestic water to meet the needs of all segments of the community.</p>	
<p>Water Quality and Resources, Goal 2: An informed public that respects the City's finite water resource and maximizes protection and conservation efforts for the benefit of the entire community.</p>	
<p>Policy 1: To the greatest extent practical, the City shall encourage the use of drought tolerant landscaping as a means of reducing water demand.</p> <p>Policy 2: The City shall evaluate all proposed land use and development plans for their potential to create groundwater contamination hazards from point and non-point sources and confer with other appropriate agencies to assure adequate review.</p> <p>Policy 4: The City shall require the use of alternative water supplies, such as recycled or canal water, for urban irrigation, where available.</p> <p>Policy 5: New developments shall establish and confirm the ability to meet current and future water resource demands.</p>	<p>The Landscape Plan requires use of native and desert-friendly plant species to reduce water demand for the project. The Design Standards and Guidelines also contain provisions for the use of reclaimed water for landscaping and irrigation.</p>
<p>Archeological And Historic Resources, Goal 1: The preservation, maintenance, continuity, and enhancement of cultural heritage and resources in the City of Rancho Mirage, including historic and prehistoric cultural artifacts and traditions.</p>	
<p>Policy 1: The City shall exercise its responsibility to preserve archaeological, historical, and cultural sites.</p> <p>Policy 2: Development or land use proposals that have the potential to disturb or destroy sensitive cultural resources shall be evaluated by a qualified professional and appropriate mitigation measures shall be incorporated into project approvals, if necessary.</p> <p>Policy 3: The City shall ensure the protection of sensitive archaeological and historic resources from vandalism and illegal collection.</p> <p>Policy 4: The City shall support the listing of eligible properties, structures, or sites as potential historic landmarks and their inclusion in the National Register of Historic Places.</p>	<p>The project's accompanying EIR evaluated the potential for impacts on cultural resources as a result of the project development. Adequate mitigation measures for the protection of cultural resources were identified and are outlined in the EIR. For example, Mitigation Measure 5-4 requires the retention of a qualified archaeologist to monitor all grading and ground-altering activities within the project site prior to the issuance of grading permits.</p>

**Table 11
Consistency Analysis**

Relevant Policy	Analysis of Project Consistency
AIR QUALITY ELEMENT	
Goal 1: Preservation and enhancement of regional air quality for the protection of the health and welfare of the community as a whole.	
<p>Policy 2: The City shall promote the development of pedestrian-oriented retail centers, as well as community-wide multi-use trails and bike paths, dedicated bike lanes, and other desirable alternatives to motor vehicle traffic.</p> <p>Policy 3: The City shall promote the appropriate and cost effective development and coordination of mass transit/shuttle service linking residential, shopping, resort, and commercial centers of the City, and participate with CVAG, SCAG, and public/private service providers to improve and optimize regional transportation services.</p> <p>Policy 4: The City shall encourage the use of clean alternative energy sources for transportation, heating, and cooling whenever practical.</p> <p>Policy 5: The City shall review all development proposals for potential adverse effects on air quality and require mitigation of any significant impacts.</p> <p>Policy 6: The City shall encourage the widening of roadways and strive towards achieving a level-of-service C to improve traffic flow, minimize idling time, and reduce air emissions.</p>	<p>The Section 19 Specific Plan provides the land planning and circulatory systems necessary to create a pedestrian-friendly, unique mixed-use development. The transportation and development planning encourages alternative modes of transportation and promotes efficient circulation and land use systems. The potential future multimodal transit station will allow residents and employees to commute to and from the Section 19 area without significant reliance on personal automobiles. Access to the multimodal station through a variety of trails encourages convenient and safe access to the public transit system.</p> <p>The Environmental Design Guidelines also include provisions for energy efficiency and conservation through site design, building design, and landscaping measures.</p>
NOISE ELEMENT	
Goal 1: A noise environment providing peace and quiet that complements and is consistent with the City's resort residential character.	
<p>Policy 1: The potential of land use patterns, associated traffic and its distribution, and individual development shall be assessed for their potential to generate adverse and incompatible noise impacts. Significant impacts identified shall be appropriately mitigated.</p> <p>Policy 2: Noise sensitive land uses, including residences, resorts, community open space, schools, libraries, churches, hospitals, and convalescent homes shall be protected from high noise levels emitted by both existing and future noise sources.</p> <p>Policy 4: Land uses that are compatible with higher noise levels shall be encouraged to locate adjacent to the City's major arterial roads and highways or the UPRR/I-10 corridor to maximize noise-related land use compatibility.</p> <p>Policy 3: Project designs shall be required to include measures that assure that interior noise levels for residential development do not exceed 45 dBA.</p> <p>Policy 5: Develop and maintain a circulation plan that is consistent with the resort residential character of the City, avoids impacts to existing and planned sensitive receptors/uses, and provides fixed routes for existing and future truck traffic.</p>	<p>The Section 19 Land Use Plan locates compatible land uses adjacent to one another and protects noise-sensitive land uses such as residential planning areas by locating them away from noise sources such as the Union Pacific Railroad and truck traffic along the adjacent major arterials. The Section 19 project includes feasible development standards and mitigation measures to ensure compatibility between land uses and address potentially significant impacts regarding noise.</p> <p>The Specific Plan provides that no use, activity, or process shall produce continual vibrations or noxious odors that are perceptible without instruments by the average person at the property lines of the site or within the interior of residential units on the site.</p>



**Table 11
Consistency Analysis**

Relevant Policy	Analysis of Project Consistency
SAFETY ELEMENT	
Geotechnical Hazards, Goal 1: A community that plans for and is protected from the effects of seismic and geological hazards.	
<p>Policy 2: The City shall establish ordinances and guidelines to reduce the hazards from wind-blown sand and dust.</p> <p>Policy 8: New septic tank leach fields, seepage pits, drainage facilities and heavily irrigated areas shall be located away from foundations and other structural supports to minimize the creation of a localized collapse of soils and associated hazards.</p> <p>Policy 10: The City shall encourage the incorporation of wind barriers, architectural design or features, and drought resistant ground coverage in new development site designs to mitigate the impacts from erosion and wind-blown sand.</p>	<p>The Environmental Design Guidelines in Chapter 6 contain provisions for wind barriers and wind-blown sand to protect development naturally and reduce the associated hazards.</p> <p>Installation of the drainage facilities proposed within the Section 19 project area would be required to follow the applicable construction and development standards (e.g., required setbacks) outlined in the California Building Code, Rancho Mirage Municipal Code, and Section 19 Specific Plan in order to offset the potential collapse of localized soils and associated hazards.</p>
Flooding And Hydrology Hazards, Goal 1: Protection of lives, property, and essential facilities from flooding and other hydrologic hazards in the City of Rancho Mirage.	
<p>Policy 2: The City shall provide drainage controls and improvements that enhance local conditions and are consistent with and complement the Master Drainage Plans.</p> <p>Policy 3: The City shall provide direction and guidelines for the development of on-site stormwater retention facilities consistent with local and regional drainage plans and community design standards.</p> <p>Policy 5: Design major drainage facilities, including debris basins and flood control washes and channels, to balance their enhancement as wildlife habitat and community open space amenities with the functional requirements of these facilities.</p>	<p>The Section 19 Drainage Plan incorporates the systems necessary to contain and percolate drainage on site. Retention basins designed and installed in accordance with City standards will function in conjunction with the Mid Valley Drainage Channel to support the City's Master Drainage Plans.</p> <p>The Landscape Plan contains landscape treatment guidelines for the retention basins and well sites within Section 19 so that they will appear as heavily landscaped open space areas to passing residents and visitors.</p>
PUBLIC SERVICES & FACILITIES ELEMENT	
Water, Sewer and Utilities, Goal 1: Water, sewer and utility facilities which safely and adequately meet the needs of the City at build out.	
Water, Sewer and Utilities, Goal 2: Conservation of the quality and quantity of the water basin.	
Water, Sewer and Utilities, Goal 3: A City-wide sewer system.	
Water, Sewer and Utilities, Goal 4: Lower electrical rates.	
Water, Sewer and Utilities, Goal 5: All utility lines placed underground.	
<p>Policy 9: Utility lines on major streets shall have primary consideration for under-grounding.</p> <p>Policy 10: Major utility facilities shall be sited to assure minimal impacts to the environment and the community, and minimize potential environmental hazards.</p> <p>Policy 3: The City shall support the formation of neighborhood-wide Assessment Districts for the purpose of sewer installation.</p> <p>Policy 4: All subdivisions shall be connected to sewer lines.</p>	<p>The Design Standards and Guidelines require exterior on-site facilities, including sewer, gas, water, electric, telephone, and communications equipment be installed underground where feasible. Transformers and other utility equipment that must be aboveground should be screened and incorporated into the landscape wherever possible.</p> <p>Utility lines will connect to existing infrastructure facilities. The sewage, water, drainage, and solid waste plans are designed to serve the proposed development within the Section 19 project.</p>

**Table 11
Consistency Analysis**

Relevant Policy	Analysis of Project Consistency
Fire and Police Protection, Goal 1: A high level of police and fire protection and paramedic service.	
<p>Policy 1: All new and improved developments shall be reviewed for their impact on safety and the provision of police and fire protection services.</p> <p>Policy 2: Enforce fire standards and regulations in the course of reviewing building plans and conducting building inspections.</p>	<p>Individual projects within the Specific Plan area would be reviewed by the City of Rancho Mirage and would be required to comply with requirements in effect at the time building permits are issued, including payment of police and fire fees upon improvements to property. Additionally, during the building permitting process, the Riverside County Fire and Sheriff's Departments would review development plans to ensure that adequate facilities and infrastructure are provided to serve the needs of the fire and police departments.</p>
Schools and Libraries, Goal 1: Education and library facilities that provide the City with adequate services and convenient access.	
<p>Policy 1: The City shall assist and coordinate with the local school districts and state agencies in the planning, site acquisition, development and provision of educational facilities for the residents of the City.</p>	<p>To address the increase in student enrollment as a result of the project development, individual project applicants would be required to pay school impact fees to reduce any impacts to the school system in accordance with Senate Bill 50. These fees are collected by school districts at the time of issuance of building permits for commercial, industrial, and residential projects.</p> <p>Additionally, funds from the City's Development Impact Fees would help the library system of Rancho Mirage purchase additional resources to provide an appropriate level of library services for the residents, businesses and visitors drawn to the City of Rancho Mirage by the proposed project. Furthermore, the Specific Plan permits the development of schools and libraries by right or through the approval of a conditional use permit in certain planning areas of the project site.</p>
Public Buildings and Facilities, Goal 1: A coherent, compatible, and aesthetically satisfying integration of public buildings and facilities into the overall planning of the City.	
<p>Policy 2: The City shall coordinate with public utilities and special districts to assure the least intrusive and most compatible integration of related buildings and facilities into the land use pattern of the City.</p> <p>Policy 3: The siting of equipment storage and maintenance yards and facilities will be conducted in a manner which is sensitive to and has a minimum impact on surrounding existing and future land uses.</p>	<p>Public facilities are permitted within specific planning areas in the Section 19 project. The Design Standards and Guidelines provide direction for the integration of these uses with the adjacent development.</p> <p>Service areas, utility areas, and trash enclosures are required and encouraged to be incorporated within the adjacent building envelope. If this is infeasible, these areas should be oriented away from public view, within design enclosures that provide ornamental screening and landscaping.</p>
Arts and Culture, Goal 1: The development and prospering of the arts and culture of the City of Rancho Mirage, and enhanced access to these amenities for all residents through City and private efforts and participation.	
Arts and Culture, Goal 2: An enhanced sense of cultural awareness and identification in the City of Rancho Mirage, through cultural events, and the development of all aspects of the arts.	
<p>Policy 1: The artistic and cultural heritage of Rancho Mirage shall be enhanced through efforts to expand the available venues for fine arts and crafts, as well as music and other performance arts.</p>	<p>The Specific Plan requires the provision of public art for new development under Development Regulations. The location and orientation of these public art pieces within plazas and open spaces will enhance the artistic and cultural environment for the project.</p>



Table 11
Consistency Analysis

Relevant Policy	Analysis of Project Consistency
<p>Policy 3: Proactively demonstrate support for the City's desert resort heritage through the development of art in public places.</p>	<p>The esplanade and Center Street areas also provide opportunities for arts and crafts or music festivals. This area may be temporarily closed to traffic to accommodate such festivals and farmers' markets for the surrounding community.</p> <p>Smaller retail space will also be available in the Mixed-Use Core land uses surrounding Center Street, providing opportunities for fine arts and crafts stores. Event venues permitted in the Resort Flex land use provide additional settings for music and performance art events.</p>
COMMUNITY DESIGN ELEMENT	
<p>Community Identity, Goal 1: Preservation and promotion of the special identity of Rancho Mirage as a "Garden in the Desert," combining quality development with scenic, natural, and open space amenities.</p>	
<p>Policy 1: The City's symbolic identity shall be enhanced through distinct signage, gateways, architecture, and landscaping.</p> <p>Policy 2: Unique views of mountains and other natural open spaces from the City's streets shall be preserved and enhanced.</p> <p>Policy 3: The City shall ensure the development of high quality, visually distinctive commercial uses.</p> <p>Policy 4: The planning and design of residential neighborhoods shall provide distinctive and characteristic design elements along public rights-of-way and within the project, creating a recognizable sense of place.</p> <p>Policy 5: Multifamily residential projects shall provide well-designed and neighborhood enhancing living space, usable and safe private and common open space areas, adequate parking and appropriate automobile storage, screened trash enclosures, a comprehensive landscape program, and perimeter walls and fencing.</p>	<p>The provisions of this Specific Plan will foster the creation of a recognizable, landmark development for the City of Rancho Mirage that appeals to the City's population, stands out from other developments, and provides a unique environment for people to walk and gather. The mix of land uses provided under the Section 19 Specific Plan, in conjunction with the open space requirements, provide for the development of a high quality, pedestrian-friendly community.</p> <p>The Development Standards contain the necessary requirements for building intensity, setbacks, and height needed for the development of high quality commercial and multifamily residential uses. Viewsheds will also be preserved through the building setback and height standards. Sufficient recreational and public gathering spaces are required according to the open space standards.</p> <p>The Design Standards and Guidelines outline standards for streetscape elements such as signage, lighting, street furnishings, walls, fences, gates, paving materials, water features, and public art. The standards involve a high level of planning and design for the Section 19 project, and require that the design elements be consistent and complementary throughout the project. The Landscape Plan establishes landscape treatments in addition to the streetscape elements to provide unique design elements for public rights-of-way and open spaces.</p> <p>Finally, one of the project's primary features is a 4-acre esplanade, which is designed to reflect three desert themes: The Tranquil Desert, The Artistic Desert, and The Edible Desert. Design features and land uses are incorporated to enhance the image of the project and the City of Rancho Mirage as a Garden in the Desert.</p>



Table 11
Consistency Analysis

Relevant Policy	Analysis of Project Consistency
Gateways, Goal 1: Clearly marked major entry points into Rancho Mirage that incorporate distinctive landscaping, signage and wall treatments achieving a sense of arrival and symbolizing the City's identity.	
Policy 1: The City shall distinguish important primary gateways into Rancho Mirage.	The Landscape Plan establishes guidelines for the design and location of specialized landscape treatments (Enhanced Intersections and Project Nodes) surrounding the project and along internal streets to signal a sense of arrival to residents and visitors. The Specific Plan also contains guidelines for signage design in coordination with the Landscape Plan.
Scenic Roadways, Goal 1: Scenic roadways that impart a sense of place and are attractively landscaped, provide visual continuity along adjacent uses, preserve views, and create focused intersection landscaping.	
<p>Policy 1: The City shall develop and maintain high-quality roadways that frame views, buffer surrounding residential development, and enhance commercial uses.</p> <p>Policy 2: The City shall ensure the development of well-designed, richly landscaped intersections that are attractive to drivers and pedestrians alike.</p> <p>Policy 3: View corridors shall be preserved through streetscape improvements and specialized design standards.</p>	<p>Landscape treatments for the different roadways within the Specific Plan contain provisions for framing views and allowing exposure to adjacent commercial uses. Important intersections are identified with special landscape treatments that are consistent with the General Plan's "oasis" concept.</p> <p>The streetscape components addressed within the Design Standards and Guidelines section set forth requirements for signage, lighting, special paving, walls and fences, and street furnishings within public rights of way. The treatment and use of these elements also emphasize intersections within the community.</p>
Landscaping, Goal 1: A landscape program that promotes aesthetics, conservation and place-making.	
Policy 1: Landscape plans submitted to the City shall be consistent with this Element.	The landscape plan provides guidelines for the treatment of areas within the Section 19 Specific Plan, including parkways, medians, development edges, project entries, and open space areas. The landscape plan establishes an identity and theme for the project and will act as an overall unifying element, transcending parcel boundaries and defining open space areas.
Perimeter Walls/Fencing, Goal 1: Walls and fences that act as attractive elements of the streetscape, while providing privacy and views, creative design, and visual continuity.	
Policy 1: Wall and fence designs shall be considered important components of the design review process and overall streetscape improvement plans.	Wall and fence provisions are addressed in the Design Standards and Guidelines section. These elements are required to be consistent in quality and design with the surrounding Section 19 project areas.
Signage and Lighting, Goal 1: Signage of the highest level of design and construction quality.	
Signage and Lighting, Goal 2: Protection of the star-studded desert night sky from excessive glare.	
<p>Policy 1: The City shall encourage high-quality, low-scale signage that effectively communicates in an attractive manner.</p> <p>Policy 2: Lighting features that preserve the beauty of the desert night while still performing directional, safety, and informational functions shall be designed and incorporated into development projects.</p>	The Sign and Lighting Standards set forth general requirements for signage and lighting within the Section 19 project. In addition to the signage requirements, a Sign Program that identifies a hierarchy of signs with a common theme, as well as the signage location and style shall be required on a project by project basis. These Sign Programs shall be consistent and complementary within the Specific Plan area.



**Table 11
Consistency Analysis**

Relevant Policy	Analysis of Project Consistency
Architecture, Goal 1: Architecture that is sensitive to its context, blending quality materials, distinctive detailing, and a strong sense of living with nature.	
<p>Policy 1: The City shall encourage cohesive yet flexible architectural design to all structures within the City.</p> <p>Policy 2: The City shall encourage new development to incorporate “green building” practices to maximize resource conservation and be compatible with the surrounding desert environment.</p>	<p>Architectural styles selected for the Section 19 project are required to be compatible with the surrounding architecture of the existing community. Visual diversity is promoted through the use of different architectural styles within the project, but these styles must be complementary within the project and with the existing architectural styles. Guidelines for building mass, scale, and form encourage the modulation and articulation of building form to avoid repetitive or blank elevations. Additional architectural guidelines focus on design details, shadow elements, roofline variations, and accent features such as towers to ensure architectural interest.</p> <p>The Environmental Design Guidelines also encourage “green building” practices with provisions for energy efficiency and conservation through site design, building design, and landscaping measures.</p>
Commercial Development, Goal 1: Retail centers in Rancho Mirage that are visually attractive, people-friendly, and economically successful.	
<p>Policy 1: Projects shall incorporate architectural interest and variety within the context of a unified setting, including commonalities of color, landscaping, signage and lighting. Strong architectural detailing including façade articulation and varied building materials, colors, and massing shall be encouraged.</p> <p>Policy 2: Projects shall provide comfortable, attractive, and distinctive pedestrian amenities including sitting areas, shade structures, fountains, and arcades.</p> <p>Policy 3: The City shall encourage the incorporation of arcades and covered paseos in the design of retail commercial structures.</p> <p>Policy 4: Projects shall incorporate a sense of playfulness into the design of public places with novelty fountains and public art.</p> <p>Policy 5: Projects shall design highly visible entrances through accent landscaping, monument signs, back lighting, specialized paving, and other design amenities.</p> <p>Policy 6: Projects shall incorporate rich and varied paving materials on entry driveways, pedestrian connections from parking areas, pedestrian paths along storefronts, and within plazas and courtyards.</p> <p>Policy 7: Monument, retail and directional signs shall use accent lighting.</p> <p>Policy 8: Projects shall incorporate monument signs near corners or</p>	<p>The Section 19 project proposes up to 3,096,500 square feet of retail, resort, and restaurant uses to offer local residents enhanced shopping, eating, and entertainment opportunities while enhancing the City’s revenue generation. The Specific Plan sets forth guidelines and standards for site planning, architecture and design, landscape, and streetscape elements to produce unique retail and mixed-use areas for the City of Rancho Mirage.</p> <p>The Design Standards and Guidelines contain provisions for building orientation, pedestrian spaces, parking areas, service areas, architectural styles and building design, and streetscape elements. Architectural Guidelines encourage the use of arcades and covered paseos to incorporate shade elements within the façade of the building. The Hardscape Guidelines direct the use of seating areas, shade features, special paving, and other streetscape features important to shaping the character of plazas and public spaces within the Section 19 project.</p> <p>The Specific Plan provides for the design of major intersections and entries to create a sense of entrance and arrival through the use of landscape design, themed signage, lighting, and hardscape elements.</p> <p>The Signage Standards include general guidelines for signage, and require sign programs on a project-by-project basis. The sign programs will identify a hierarchy of signs with a common theme, such as monument, retail, and directional signs, in addition to the sign locations.</p>

Table 11
Consistency Analysis

Relevant Policy	Analysis of Project Consistency
<p>entrances to retail centers.</p> <p>Policy 10: Service bay doors, necessary for tire stores, service stations, lube shops, and car washes, should be oriented away from public streets and screened from neighboring properties.</p>	<p>Service areas and utility areas are required and encouraged to be incorporated within the adjacent building envelope. If this is infeasible, these areas should be oriented away from public view, within design enclosures that provide ornamental screening and landscaping.</p>
<p>Parking Design, Goal 1: Distinctly designed parking areas in the City's commercial centers that incorporate rich paving materials, generous landscaping, clear and safe pedestrian and vehicular access, and protection from the desert climate.</p>	
<p>Parking Design, Goal 2: Parking areas that are screened from public streets to the greatest extent possible.</p>	
<p>Policy 1: Parking shall be screened from roadways by encouraging landscaped medians, berms, trellises, grade changes, or placement behind buildings.</p> <p>Policy 2: The City shall encourage layered landscaping with overstory canopy trees for shade and understory shrubs and groundcover in parking areas.</p> <p>Policy 3: Projects shall configure parking areas to allow for the free flow of vehicular traffic and convenient vehicular access to customers.</p> <p>Policy 4: The City shall encourage the landscaping of parking areas to reduce the deflection of heat into nearby buildings and to reduce the paved surface area. Shade trees are especially encouraged.</p> <p>Policy 5: Lighting shall be directed downward to protect from nighttime glare and illuminate pedestrian pathways with bollard lighting.</p> <p>Policy 6: Projects shall provide directional and identification signs in contiguous parking areas one acre or greater in size to facilitate pedestrian movement to and from parked vehicles.</p>	<p>Parking areas are required to be clustered where feasible, to avoid large, expansive parking lots and maximize security, surveillance, and efficient access. Parking areas are encouraged to locate behind buildings so that the building façade can face directly onto the street, creating a more engaging pedestrian corridor. Landscaping and trellises may also be used to screen parking areas, as stated in the Design Standards and Guidelines.</p> <p>The Landscape Plan provides landscape treatment specific to parking areas within Section 19, which incorporates canopy trees for shade in addition to shrubs and groundcover. Streetscape elements such as lighting and signage work in conjunction with the landscape themes to emphasize access and safety. Lighting standards contain provisions for pedestrian scale lighting to promote safety. Entrance and exit points are encouraged to be well marked with streetscape and landscape features for efficient access.</p>
<p>People-Gathering Places, Goal 1: A City that is noted for lively and attractive public plazas with a combination of quality seating, shade, fountains and other pedestrian amenities.</p>	
<p>Policy 1: Commercial developments shall be designed to incorporate attractive, people-friendly spaces.</p>	<p>The open space standards and applicable Design Guidelines and Standards establish functional gathering spaces for the community. The building orientation and siting guidelines encourage public spaces along Center Street, which fosters pedestrian activity along the esplanade. The Hardscape Guidelines include provisions for the design and location of seating areas, shade structures, and other elements of the streetscape for public spaces within the Section 19 project.</p>



Table 11 Consistency Analysis	
Relevant Policy	Analysis of Project Consistency
ECONOMIC AND FISCAL ELEMENT	
Goal 1: A growing and balanced economic base which serves the needs of Rancho Mirage residents, businesses, and visitors while maintaining the City's high standards of development and environmental protection.	
Goal 2: A prudent and progressive financial management program to maintain and enhance the City's strong fiscal position.	
<p>Policy 1: The City's land use designations shall provide opportunities for a broad range of residential, commercial, office, institutional, and light industrial development in appropriate locations.</p> <p>Policy 2: The City may require market studies to justify requests for rezoning to a higher intensity of land use.</p>	<p>The Section 19 Land Use Plan provides for a mix of commercial, office, institutional, and residential land uses. These uses have been planned so that commercial areas are situated adjacent to major traffic corridors for maximum exposure, office and commercial areas are adjacent to noise sources such as the UPRR easement, and residential uses are buffered by mixed-use designations and adjacent to other proposed residential projects.</p> <p>Office and retail studies were performed to identify key market demand for uses within the Section 19 project. The higher intensity of land uses proposed by the Specific Plan creates a dynamic mix of land uses to create a unique, pedestrian-friendly community for the City of Rancho Mirage.</p> <p>Several mechanisms (reimbursement agreement and public financing mechanisms) are described and outlined to fund public improvements necessary to develop the Section 19 Specific Plan.</p>



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8. Implementation

8.1 GENERAL ADMINISTRATION

The Section 19 Specific Plan and its provisions shall be implemented as the zoning for the site. In the event of a conflict between the provisions of the Section 19 Specific Plan and the provisions identified in the 2006 Rancho Mirage Municipal Code (RMMC), the Specific Plan shall prevail. If the Specific Plan is silent regarding any development standard or process, the provisions identified in the Municipal Code shall prevail.

8.1.1 INTERPRETATION

In case of uncertainty or ambiguity to the meaning or intent of any provision of this Specific Plan, the Director of Community Development has the authority to interpret the intent of the provision. The Director may, at his/her discretion, refer interpretations to the Planning Commission for consideration and action. Such a referral shall be accompanied by a written analysis of issues related to the interpretation. All interpretations made by the Director may be appealed to the Planning Commission in accordance with the appeal procedures set forth in Title 17 of the RMMC.

8.1.2 SEVERABILITY

If any section, subsection, sentence, clause, or phrase of this Specific Plan, or future amendments or additions hereto, is for any reason held to be invalid or unconstitutional by the decision of any court, such decision shall not affect the validity of the remaining portions of the plan.

8.1.3 APPROVAL AUTHORITY

The responsibilities of the Director shall include administering, interpreting, and enforcing all requirements and standards of the Section 19 Specific Plan, including the acceptance and processing of all land use permit applications. The Director or designated representative may approve, conditionally approve, or deny minor development plan permits that meet the requirements of this



Specific Plan and requests for minor variances of Specific Plan standards. The Director holds final approval authority for and enforcement of building permits, certificates of occupancy, sign permits, and temporary use permits.

The Planning Commission may recommend approval, conditional approval, or denial of conditional use permits, applications for variances, specific plan amendments, and appeals to Director actions to the City Council.

The City Council may approve, conditionally approve, or deny conditional use permits, applications for variances, specific plan amendments, and appeals to Planning Commission actions.

8.1.4 ENVIRONMENTAL REVIEW

The Environmental Impact Report (EIR) serves primarily as a source of environmental information for the City of Rancho Mirage as lead agency for the project. The EIR describes the potential impacts that could result from the adoption of the Section 19 Specific Plan. Subsequent development projects within the Section 19 Specific Plan are anticipated and, while the EIR has been prepared as a program EIR (as defined by Section 15168 of the California Environmental Quality Act Guidelines), subsequent projects that are within the scope of this EIR may be subject to a more limited environmental review process if deemed necessary by the Director.

8.2 REVIEW AND APPROVAL PROCESS

8.2.1 PRE-APPLICATION CONFERENCE

A pre-application conference with the Director or his/her designee should be held before a proposed project can be submitted and accepted for processing, as identified in Section 17.36.030 in the RMMC. Pre-application conferences are recommended before the submittal of any land use permit or approval applications.

8.2.2 DESIGN REVIEW

All applications for a development plan permit, conditional use permit, minor variance, or variance shall be subject to the Design Review process outlined in Chapter 17.40 of the RMMC. Because the successful implementation of the Section 19 Specific Plan is dependent on the careful planning and synergistic relationship of the uses permitted in the project area, it will be extremely important to ensure that, as projects are developed at different times and phases, all new development is thoughtfully integrated into existing or proposed development within each planning area and between adjacent planning areas.

In particular, the internal circulation and open space systems are integral to the efficient use and interconnectivity of properties within the Specific Plan area. It is likely that the location and design of internal roadways and open space features in each planning area will be influenced by the first property owner to submit a proposed development for a planning area. The Director may therefore require an applicant to submit concept plans that illustrate open space and circulation systems for



pedestrians, bicyclists, golf carts, and other vehicles within the planning area, even on properties that are not owned or developed by the applicant.

The City recognizes that any such plans will be conceptual in nature and will not bind a particular property or property owner to a specific site design. The intent is to gain a comprehensive understanding of potential development and to ensure cohesive development that maximizes connectivity throughout the Specific Plan. The City strongly encourages property owners to work together to design and develop entire planning areas at one time so that a comprehensive plan can be created.

8.2.3 DEVELOPMENT PLAN PERMIT

Applications that comply with the provisions of the Specific Plan and do not require the approval of a Conditional Use Permit may be approved or conditionally approved by the Director through a Development Plan Permit. Uses requiring the approval of a Development Plan Permit are identified on Table 4 in this Specific Plan as permitted uses (P). The Development Plan Permit process shall be the same as that outlined in Chapter 17.42 of the RMMC except that no hearing shall be required for approval of a Preliminary or Final Development Plan permit. Development Plan Permits shall also be subject to the supplemental findings required as part of this Specific Plan (Section 8.2.11).

Once the Director has taken administrative action on a Preliminary Development Plan Permit, the applicant, the owner or owners of the property subject to the Development Plan Permit, and the Planning Commission shall be promptly notified of the Director's decision. The Director shall also present approvals or denials of Preliminary Development Plan Permit applications at the Planning Commission meeting following the date of actions.

The decision of the Director shall be final and effective 14 days after a written determination has been made unless, within said time, a written appeal to the Planning Commission is filed by the applicant, property owners subject to the Development Plan, or by any member of the City Council or Planning Commission. Appeals shall be undertaken in compliance with the procedures outlined in Chapter 17.76 of the RMMC.

The approval of a project subject to a Development Plan Permit is conditional upon the privileges being utilized within one year after the effective date, and if they are not utilized or construction work is not begun within this time and carried on diligently in accordance with conditions imposed by the Director, this authorization shall become void, and any privileges, permit, or conditions granted shall have elapsed. Upon written request, the Director may grant one extension of an additional 12 months in compliance with Section 17.68.070 of the RMMC. Approved projects that will be carried out in two or more phases are not required to begin all phases of work within the 12-month approval window.

8.2.4 CONDITIONAL USE PERMITS

Uses requiring the approval of a Conditional Use Permit are identified on Table 4 in this Specific Plan. Uses in this Specific Plan requiring a Conditional Use Permit shall be subject to the requirements and procedures identified in Section 17.48 of the RMMC. Uses shall be subject to the



findings required for Conditional Use Permits in the RMMC, as well as the supplemental findings required as part of this Specific Plan (Section 8.2.11).

The approval of a project subject to a Conditional Use Permit is conditional upon the privileges being utilized within one year after the effective date, and if they are not utilized or construction work is not begun within this time and carried on diligently in accordance with conditions imposed by the City Council, this authorization shall become void, and any privileges, permit, or conditions granted shall have elapsed. Upon written request, the Director may grant one extension of an additional 12 months in compliance with Section 17.68.070 of the RMMC. Approved projects that will be carried out in two or more phases are not required to begin all phases of work within the 12-month approval window.

8.2.5 TEMPORARY USE PERMITS

Temporary Use Permits identified in Table 4 of this Specific Plan shall be subject to the provisions of Chapter 17.46 of the RMMC.

8.2.6 MINOR VARIANCE

Minor variances from the requirements and standards of this Specific Plan shall be subject to the provisions of Chapter 17.50 of the RMMC. In addition to the minor modifications permitted by this Chapter of the RMMC, the Director shall have the authority to approve the expansion or reduction of the acreage covered by a given planning area within the Specific Plan up to a maximum of 10 percent.

8.2.7 VARIANCE

Where practical difficulties or unnecessary hardships would occur as a result of the strict interpretation and application of the provisions of this Specific Plan, a variance may be considered subject to the requirements and findings of Chapter 17.52 of the RMMC.

8.2.8 AMENDMENTS TO THE SPECIFIC PLAN

Approval of this Specific Plan indicates acceptance by the City Council of a general framework for community development. Part of that framework establishes specific development regulations that constitute the zoning regulations for the Section 19 Specific Plan. It is anticipated that certain modifications to the Specific Plan text, exhibits, and/or project may be necessary during the development of the project. Any modifications of the Specific Plan shall occur in accordance with the process prescribed by Section 17.54.110 of the RMMC. In all cases, Specific Plan Amendments must be found to be in conformance with the objectives and intent of the Section 19 Specific Plan.

Amendments may be requested at any time pursuant to Section 65453(a) of the California Government Code. Depending on the nature of the proposed Specific Plan Amendment, a supplemental environmental analysis may be required, pursuant to the California Environmental Quality Act, Section 15162.



8.2.9 LIVE/WORK UNITS

Live/work units shall be processed through a Development Plan Permit or Conditional Use Permit, as identified on Table 4 of this Specific Plan. Live/work units shall not be subject to the requirements or procedures of Chapter 17.44 of the RMMC.

8.2.10 TIMESHARE/CONDO-HOTEL UNITS

Timeshare or condo-hotel units are only permitted within the northern half of Planning Area 1.02. Consistent with Chapter 17.30 (Ordinance 698) of the RMMC, the maximum number of timeshare or condo-hotel units shall be equal to one-half of the number of hotel rooms proposed or constructed within Planning Area 1.01. The City Council shall have the authority to approve additional timeshare or condo-hotel units above this maximum equal to 10 percent of one-half of the number of hotel rooms.

8.2.11 SUBMITTAL REQUIREMENTS

Projects subject to a Development Plan Permit or a Conditional Use Permit shall be required to submit as part of any entitlement application:

A. A site plan containing:

1. Dimensions, shape, and orientation of the parcel(s)
2. Placement of buildings and structures on the parcel(s)
3. Height, setbacks, bulk, and building materials
4. Distance between buildings or structures
5. Location, number, and layout of parking and loading spaces, including plans for shared parking
6. Internal vehicular patterns, bikeways, golf cart circulation, pedestrian circulation, and pedestrian safety features
7. Location, amount, and nature of landscaping, walls, and fences
8. Location, amount, and design plans for private open space, common open space, and public plaza areas, specifying location and extent of landscaping and irrigation systems
9. Placement, height, and direction of illumination of lighting features
10. Location, number, size, and height of signs (a subsequent Master Sign Program shall be provided outlining the construction details of signs proposed for the application)
11. Location and method of screening refuse and storage areas, roof equipment, pipes, vents, utility equipment, and all equipment not contained in the main buildings of the development
12. Provisions for property- or homeowner association(s) where that method of management is proposed
13. Documentation identifying the existing and proposed square footage, residential units, and traffic generation in the specified planning area and verification that the proposed project is within the thresholds as set forth by this Specific Plan
14. Other information that the Director may require to make the necessary findings that the provisions of this Specific Plan are met

- B. A comprehensive recycling plan consisting of a construction debris recycling program and a general recycling program for residential, office, and commercial uses prior to building permit approval.
- C. A blowsand mitigation plan prepared by a licensed civil engineer submitted to the City Engineer for plan check and approval. The blowsand mitigation plan shall identify the specific measures and describe the specific procedures that will be implemented to adequately mitigate blowsand impacts on all of the project's on-site and off-site improvements. All improvements for blowsand protection shall be depicted on the project's grading and drainage plan.

8.2.12 REQUIRED FINDINGS

8-6

Applications for new projects within the Section 19 Specific Plan area may be approved or conditionally approved if it is determined that the project can, based upon the application, plans, and materials submitted, meet these Specific Plan objectives (where applicable). Overall, the project shall:

- A. Establish a unified vision for the project site to guide a cohesive, complementary mix of uses structured around a comprehensive set of circulation and infrastructure systems.
- B. Create a new mixed-use regional destination that stimulates a major new source of tax base for the City of Rancho Mirage.
- C. Plan for an appropriate mix of commercial, office, entertainment, hotel, and residential uses, within the context of a master-planned Town Center, to meet the trade area's growing demand and build in the flexibility to respond to changes in the market.
- D. Apply innovative planning and design solutions to create a sense of place at multiple scales.
- E. Provide new housing concepts for the community, encouraging high quality, high density residential units that appeal to residents seeking shorter commutes to jobs, restaurants, and entertainment opportunities.
- F. Take advantage of the area's location and exposure by establishing a welcoming gateway to the City, characterized by distinct and attractive signage, architecture, and landscaping, both on-site and in the public right-of-way.
- G. Implement a circulation concept that optimizes circulation for both vehicular and pedestrian traffic, internalizes pedestrian activity to buffer it from the vehicular traffic along perimeter roadways, and establishes connectivity between uses infused with pedestrian-friendly and walkable spaces.
- H. Not exceed the traffic thresholds established by the certified EIR.
- I. Not exceed maximum development thresholds established for the project as a whole.



8.3 RESIDENTIAL DEVELOPMENT TRANSFER PROGRAM

While the Specific Plan’s potential impacts must be identified and addressed, the precise breakdown and distribution of residential units must be able to respond to the changing market to achieve success for the City’s current and future residents, employees, and visitors. To provide greater flexibility, the Section 19 Specific Plan includes a Residential Development Transfer Program that permits residential units to be transferred between planning areas, subject to these limitations and procedures:

- A. The maximum number of units identified for each planning area in Table 5 may be exceeded through the transfer of units from one planning area to another through this program.
- B. This program shall only apply to Planning Areas (PAs) 2.01 through 2.06 and 4.01 through 4.03. This program shall not apply to PAs 1.01 or 1.07, which shall maintain a maximum residential development potential of 50 units and 750 units, respectively.
- C. The total number of residential units developed in PAs 2.01 through 2.06 and 4.01 through 4.03 shall not exceed 1,099.
- D. When units are transferred to a planning area, the density of the receiving planning area may increase above the 28-unit-per-acre maximum stated in Table 6, but shall not exceed 35 units per acre, except as provided for by Chapter 17.22 of the RMMC.
- E. When units are transferred to a new planning area, the transferred units shall be governed by the land use category, development regulations, design standards, and design guidelines of the new planning area.
- F. Residential units may be transferred so long as the increase in units does not cause impacts that are greater than those identified in the certified EIR for the Specific Plan. In no case shall the total number of units constructed within the Specific Plan exceed 1,899 units, except as provided for by Chapter 17.22 of the RMMC.

8.3.1 SUBMITTAL AND APPROVAL PROCEDURES

The transfer of units under the Residential Development Transfer Program shall occur through these procedures:

- A. The transfer of residential units shall occur by the applicant filing a request for such action with the Community Development Department. This request shall specifically identify the exchange in the number of residential units proposed at that time, accompanied by sufficient data to review the request.
- B. If any proposed transfer of residential units involves two or more property owners, the signed, written consent of all involved property owners shall be required as part of the application.



- C. The approval of the conversion of uses under the Residential Transfer Program may be approved or conditionally approved by the Director of Community Development.
- D. Upon review, the Director must determine that the proposed transfer of units would not result in any environmental impacts that would be greater than those identified in the certified EIR. Should the Director determine that the environmental impacts of the proposed transfer of units does not exceed the environmental impacts addressed in the certified EIR, the requested exchange in land uses shall be granted. However, should the Director conclude that the proposed transfer of units would result in environmental impacts which are greater than those identified in the certified EIR, then the request shall be denied subject to further analyses and findings, pursuant to CEQA.

8.4 IMPLEMENTATION, MAINTENANCE, AND MONITORING

- A. Residential Development Transfer Program
 - 1. A tracking system shall be established to monitor the number of residential units permitted in each planning area, the number of constructed units, remaining development potential, any approved transfers of residential units, and the resulting density of each planning area after any approved transfers.
- B. Traffic Thresholds
 - 1. The development of the Section 19 Specific Plan must not exceed the maximum traffic thresholds allowed by the certified EIR. The maximum number of average daily trips (ADT) is provided in Table 6 for each planning area; however, these are provided as an initial guide. Proposed development must also demonstrate compliance with other traffic thresholds, such as the AM/PM peak-hour rates stated in the certified EIR.
 - 2. For each planning area addressed by an applicant, the applicant shall provide an analysis that evaluates the traffic impacts of existing development, potential traffic impact of the proposed development, and the remaining traffic threshold (ADT, AM/PM peak-hour trips, etc.) for any undeveloped land in the planning area. The analysis shall be prepared by a City-approved traffic engineer and submitted to the City Traffic Engineer for approval.
- C. Parking Management
 - 1. Parking management in the Specific Plan will entail a combination of providing physical spaces for parking and managing those spaces so that they are properly allocated to and used by residents, visitors, and businesses. Parking management can help prevent situations such as permanent residents using guest parking spaces instead of the enclosed parking spaces allocated to them.
 - 2. For developments using shared parking, a tracking system shall be established to determine whether a new or revised shared parking study is necessary due to changes in tenants or uses.
 - 3. Covenants, conditions, and restrictions (CC&Rs) for the Section 19 Specific Plan shall address the shared nature of parking for the overall project. It is the intent that residential uses proposed within the Specific Plan area be allowed to use parking spaces within



commercial areas in off-peak hours. Mechanisms shall be instituted in the owner associations' CC&Rs that clearly define this relationship and how the parking will be managed throughout the project.

D. Owner Associations

1. It is envisioned that the majority, if not all, of the properties within the Specific Plan will be managed by one or more property, business, or homeowner association(s). If roadways internal to the project remain under private ownership, then those roadways will be privately maintained by one or more of the owner associations.
2. Prior to the approval of any new development, the applicant shall provide a clear description of the role of the association in providing and maintaining private roadways, amenities, landscaping, and other improvements.

E. Right-of-Way Transfer

1. The right-of-way previously used for the abandoned Rio del Sol Road shall be consolidated with adjacent property(ies), as appropriate to facilitate development of the Specific Plan.

F. Removal of Tamarisk from Railroad Right-of-Way

1. The existing Tamarisk vegetation located next to the Union Pacific Railroad will be removed as part of an assessment district or by the developer prior to or during the development of the Specific Plan. The Tamarisk is considered to be an extremely invasive nonnative species of vegetation that negatively impacts native vegetation and biodiversity.

8.5 PHASING PLAN

The Section 19 project is envisioned to build out in four phases. The numbering of each planning area relates to the estimated phase of development: the first digit of each planning area number corresponds to the phase number that the planning area will develop, and the succeeding numbers relate to the sequence of development for those planning areas within each phase. For example, PA 1.01 is estimated to develop prior to PA 1.04 in the first phase and before PA 3.05, which would occur in the third phase.

Approximately 800 units, 2,117,900 square feet of non-residential uses, and 530 hotel rooms are expected to be constructed in PAs 1.01 through 1.07 for phase one of the Section 19 project by the year 2015. For phase two, approximately 511 units and 533,800 square feet of non-residential uses are expected to be constructed in PAs 2.01 through 2.09 by 2020. An estimated 444,800 square feet of non-residential uses and 50 hotel rooms are expected to be constructed in PAs 3.01 through 3.07 by 2025, and another 588 units will be constructed in PAs 4.01 through 4.03 for phase four by 2030.

**Table 12
Phasing Plan**

Phase	Buildout Year	Units	Square feet	Hotel Rooms	Planning Areas
1	2015	800	2,117,900	530	1.01–1.07
2	2020	511	533,800	0	2.01–2.09
3	2025	0	444,800	50	3.01–3.04
4	2030	588	0	0	4.01–4.03

8.6 FINANCING

8-10

Several mechanisms will need to be considered to fund the public improvements necessary to develop the Section 19 Specific Plan. These improvements include but are not limited to traffic signals, access driveways and drive aisles, and median improvements. Some of the public improvements are incremental in nature and can be borne by an individual developer. Other public improvements benefit the project as a whole and may need to be constructed and paid for prior to buildout of the Specific Plan. In these instances, the City must create a mechanism by which each property owner and developer pays their fair-share cost of the public improvements.

8.6.1 REIMBURSEMENT AGREEMENT

One method to distribute and capture costs for larger public improvements is to create a reimbursement agreement, which is a contract between the City, property owners, and/or developers under which parties can be reimbursed for funding public improvements that benefit properties beyond their own and/or are larger than would be required by their development.

An example of such an improvement is the creation of a new signalized intersection. Although one developer may pay the initial costs of installing the new intersection to create access to their own property, the new intersection is necessary for the full buildout of the Specific Plan and will enhance access for adjacent property owners within the Specific Plan. Accordingly, the developer would seek to be reimbursed for the costs of installing the new intersection, less their own fair-share portion of the costs.

To create a reimbursement agreement, the City will need to first perform an analysis to determine the total costs of public improvements required for the Section 19 Specific Plan. Then, a fair share analysis will need to be conducted to determine the distribution of costs for each planning area and type of development. Both of these analyses will form the basis for a reimbursement agreement, to be created and managed by the City.

The City will also establish a separate reimbursement agreement to recover the costs of preparing the Section 19 Specific Plan and associated Environmental Impact Report (EIR). Upon the final adoption of the Specific Plan and certification of the EIR, the total costs of preparation will be determined and form the basis for a reimbursement agreement between the City and property owners, to be created and managed by the City. The agreement will recognize payments already made by current property owners for the explicit purpose of funding the Specific Plan and EIR.



8.6.2 PUBLIC FINANCING MECHANISMS

A variety of public debt financing mechanisms are available to fund public improvements in California. The advantage of most of these programs is that they provide tax-exempt financing at lower interest rates than are available through construction loans and other forms of conventional financing. To avoid the imposition of additional burdens on the City's General Fund and existing taxpayers, and to recognize the fact that the project is outside of the City's redevelopment boundaries, the available financing mechanisms will be limited to those that would be solely the responsibility of the property owners located in the Specific Plan itself:

- ❖ Community Facilities Districts
- ❖ Special Assessment Districts
- ❖ Infrastructure Financing Districts

It is envisioned that, prior to any major construction activities, all of the property owners within the Section 19 Specific Plan will form and participate in a project-wide assessment or other special district to fund the installation of the project's public improvements. If this does not occur, the financial responsibility for public improvements necessary for proposed development activities will become the responsibility of each individual property owner and/or developer.

8.6.3 KEY LARGO FEE BENEFIT PROGRAM

The City of Rancho Mirage established a Development Impact Fee (DIF) to pay for public improvements and determine a fair-share responsibility for new development. Improvements around the perimeter of the specific plan area that have already been incorporated into the DIF include:

- ❖ Construction of the Key Largo overcrossing over Interstate 10
- ❖ Street widening of Bob Hope Drive
- ❖ Intersection improvement of Bob Hope Drive and Dinah Shore Drive
- ❖ Signal installation at Bob Hope Drive, one-half mile north of Dinah Shore Drive
- ❖ Median islands and bikepaths along Bob Hope Drive and Dinah Shore Drive

Future public improvements in the vicinity of Section 19 that can be considered citywide benefits may be added to the DIF in the future to help implement the Specific Plan.



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SECTION 19 SPECIFIC PLAN
CITY OF RANCHO MIRAGE