

City of Big Bear Lake



VILLAGE SPECIFIC PLAN

PREPARED FOR THE CITY OF BIG BEAR LAKE

John Spivey, Mayor
Norman Reinik, Jr., Mayor Pro Tem
John C. Emingers
Don Morris
A. W. "Bill" Speyers

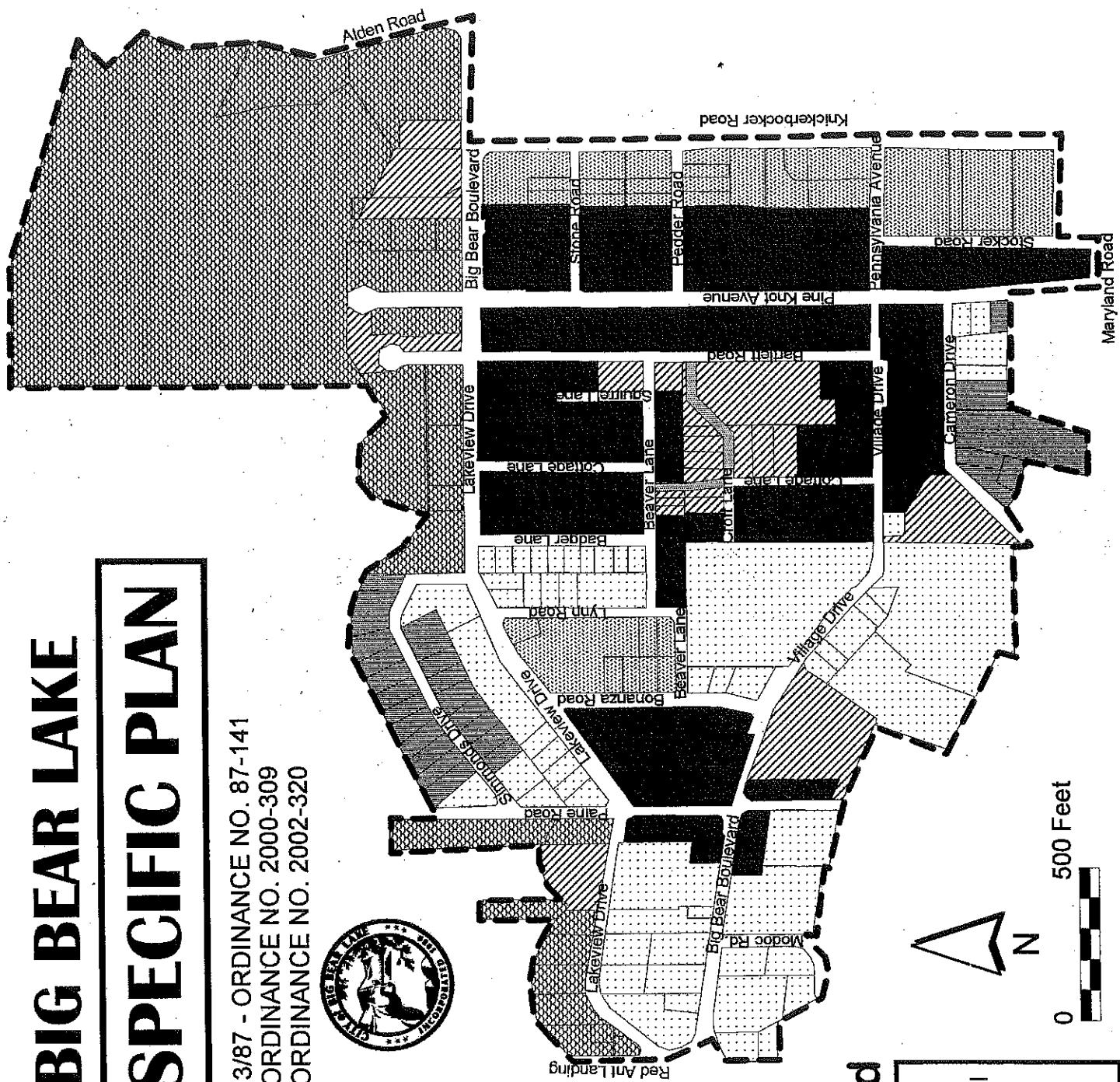
Robert Van Nort, City Manager

THE PLANNING CENTER

CITY OF BIG BEAR LAKE

VILLAGE SPECIFIC PLAN

ADOPTED 05/13/87 - ORDINANCE NO. 87-141
09/11/00 - ORDINANCE NO. 2000-309
04/22/02 - ORDINANCE NO. 2002-320



PLEASE NOTE:

This is not an engineered map.
For street and parcel dimensions
see tax assessor's parcel maps.
Always confirm latest zoning
designation through the City
Planning Division.

Land Use Legend

Commercial Lodging	Lakefront Commercial
Neighborhood Commercial	Public-Quasi Public
Residential Multi-Family	Village Retail
Village Drive	Specific Plan Boundary

BIG BEAR VILLAGE SPECIFIC PLAN

TABLE OF CONTENTS

I. Introduction

- A. Ordinance Adopting Village Specific Plan
- B. Authority and Scope
- C. Setting
- D. Location, including figures
- E. Background/Overview
- F. Intent
- G. Objectives

II. Land Use Plan

- A. Goals and Objectives
- B. Consistency with the General Plan
- C. Village District Map

III. Circulation Plan

- A. Existing Conditions/Recommendations
- B. Parking Facilities
- C. Intersection Concepts
- D. Bicycle/Pedestrian Trail Design Standards

IV. Open Space

- A. Lake/Marina Plan Concept
- B. Recreation/Node Treatment Standard

V. Special Design Concepts

- A. Entry Sign
- B. Public Signage
- C. Streetscape Design

TABLE OF CONTENTS
continued

VI. Development Standards

- A. Introduction
- B. Land Use Summary Table
- C. General Provisions/Procedures
- D. Tourist Serving Commercial
- E. Community Commercial
- F. Tourist Residential
- G. Village Residential
- H. Public Quasi-Public
- I. Signage Overlay Zone
- J. Parking Floating Zone
- K. Lot Consolidation Incentive
- L. Parking Standard/Requirements

VII. Plan Implementation

- A. Introduction
- B. Village Design Review Board
 - 1. Authority
 - 2. Procedures
- C. Financing Options Available

VIII. Appendices

- A. Market Potentials
- B. Circulation Element
- C. Village Design Notebook

I. INTRODUCTION

A. ADOPTING ORDINANCE

RESOLUTION NO. _____

**A RESOLUTION OF THE CITY
COUNCIL OF THE CITY OF BIG BEAR LAKE
APPROVING THE VILLAGE SPECIFIC PLAN**

WHEREAS, the California Government Code, Title 7, Division 1, Chapter 3, Article 8, Sections 65450 through 65457 authorizes cities to adopt Specific Plans to implement their General Plan; and

WHEREAS, the City of Big Bear Lake has directed the consultant preparation of a Village Specific Plan in conformance with said authorization; and

WHEREAS, the Big Bear Lake General Plan recommends that a Specific Plan be prepared for the Village area; and

WHEREAS, the Planning Commission of the City of Big Bear Lake has conducted all required hearings on said Specific Plan; and

WHEREAS, no real property shall be developed, rehabilitated, or otherwise changed after the date of the adoption of the Specific Plan except in conformance with the provisions of the Specific Plan; and

WHEREAS, the City Council is fully apprised of the plan area and the Village Specific Plan and accompanying Technical Resource document, technical documentation reports, maps, drawings, and design guidelines, presentations made at all of the Planning Commission and City Council public hearings; and

WHEREAS, the Final Village Specific Plan incorporates all needed information heretofore referenced for adoption as a complete document; and therefore;

BE IT RESOLVED that the City Council of the City of Big Bear Lake hereby adopts the Final Village Specific Plan as contained in the Report and Map adopted herewith by reference; and

BE IT FURTHER RESOLVED that Chapter V shall be adopted as development regulations for the effectuation of

this Specific Plan, within Chapter __, Article __ of the City Code; and

BE IT FURTHER RESOLVED that the following definition be added to the list of definitions in Chapter __, Article __ of the City Code:

"Specific Plan" is a planning document consisting of appropriate narrative, graphics and data established under authority of the California Government Code, Article 8, Sections 65450 through 65457, as amended, for the purpose of comprehensively implementing the General Plan in particular areas of the City of Big Bear Lake in which conventional policy or regulatory mechanisms cannot adequately address the scope and depth of subjects required by law in a Specific Plan. The Specific Plan may be adopted as policy by City Council resolution, regulation by City Council Ordinance, or some combination of both. A Specific Plan must include two or more development related land uses (other than public or open space).

APPROVED AND ADOPTED this ____ day of _____, 1987.

Mayor of the City of Big Bear Lake

ATTEST:

City Clerk of the City of Big Bear Lake

I, _____, City Clerk of the City of Big Bear Lake, California, do hereby certify that the foregoing resolution was duly and regularly adopted by the City Council of the City of Big Bear Lake at a regular meeting thereof, held on the ____ day of _____, 1985, by the following vote, to-wit:

AYES:

NOES:

ABSENT:

City Clerk of the City of Big Bear Lake

APPROVED AS TO FORM:

City Attorney

B. AUTHORITY AND SCOPE

The Big Bear Village Specific Plan has been prepared pursuant to the provisions of Section 65450 through 65453 of the California Government Code. The Plan has been adopted by local government to provide a guide for future growth and development of the Big Bear Village Area. It is intended that this Specific Plan, through its maps, illustrations and text, will incorporate all the regulations and development standards affecting the use and reuse of land within the Specific Plan area, and reflect the interest and concerns of local residents and merchants of the community through these standards and regulations. The Big Bear Village Specific Plan provides the regulations, conditions and programs necessary to implement each applicable element of the General Plan.

C. SETTING

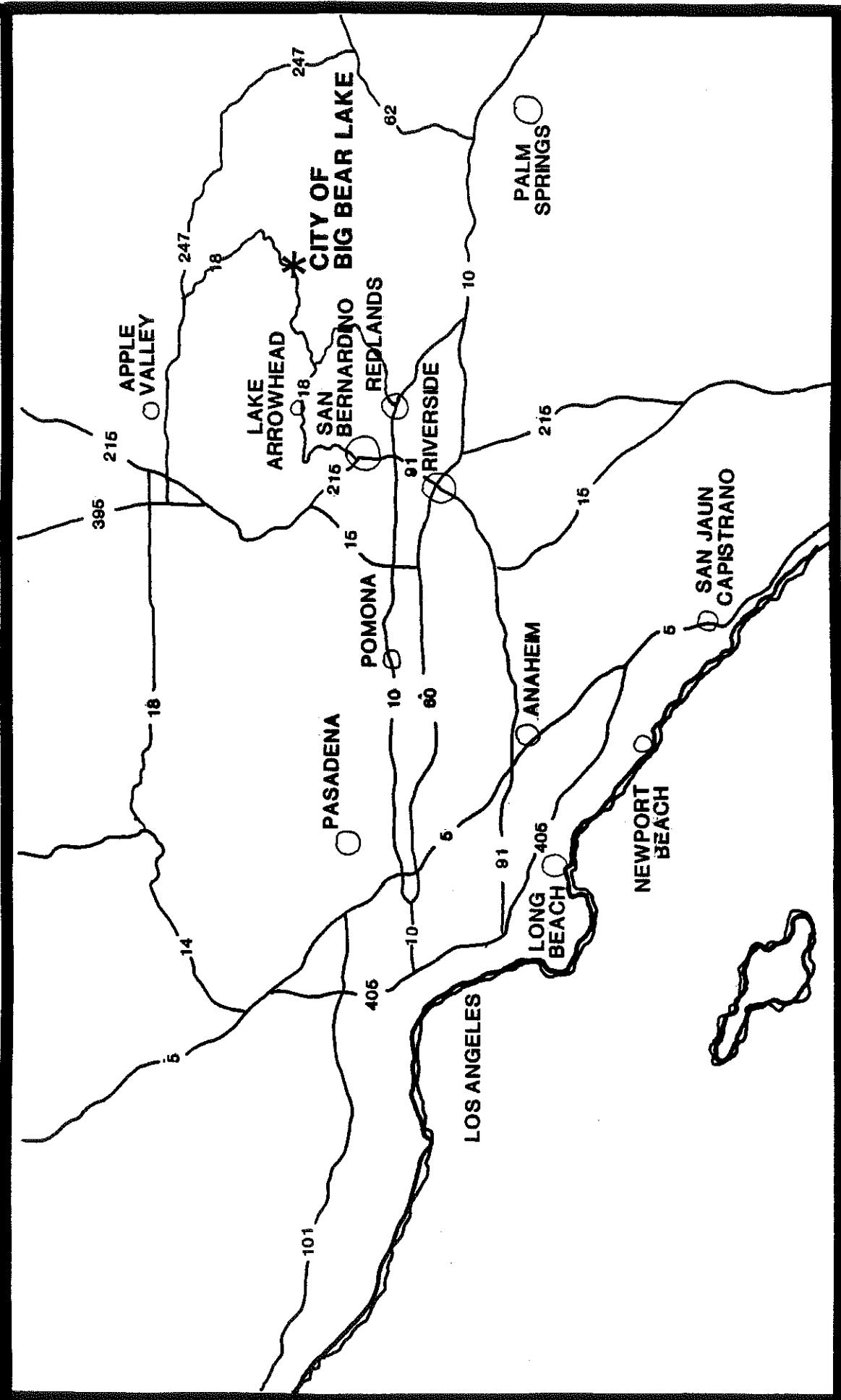
Big Bear Village is the central business core of the Big Bear Valley. It is the major focal center of the valley because of its proximity to Big Bear Lake, business activity, schools, amusement centers, restaurants, and accommodations for vacationers. The study area is bounded by the lake on the north, Knickerbocker Road to the east, Paine Road to the west and Cameron Drive and the rear property lines of Big Bear Lake Boulevard to the south. This 'Village' area comprises approximately 108 acres and is characterized by various commercial retail establishments, tourist lodging; single family residential uses, and vacant land. Vacant land comprises approximately 27 percent of the total Village Area. Given this amount of vacant land coupled with underdeveloped parcels in this area, there is a significant impact for the future to be dealt with in this Specific Plan.

In general, commercial uses line both sides of Big Bear Boulevard and Pine Knot Avenue. Some peripheral commercial uses are located along Lakeview Drive. The central area of the Village is generally underutilized and/or vacant. One of the most obvious situations in the village area is its close proximity to Big Bear Lake and the lack of public pedestrian access and the general lack of business orientation to it.

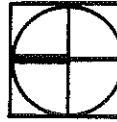
D. LOCATION

Big Bear Valley is located in the San Bernardino Mountains, which are in San Bernardino County, some 100 miles east, or 2 hours drive from Los Angeles or Newport Beach. The

REGIONAL LOCATION



Big Bear Village Specific Plan

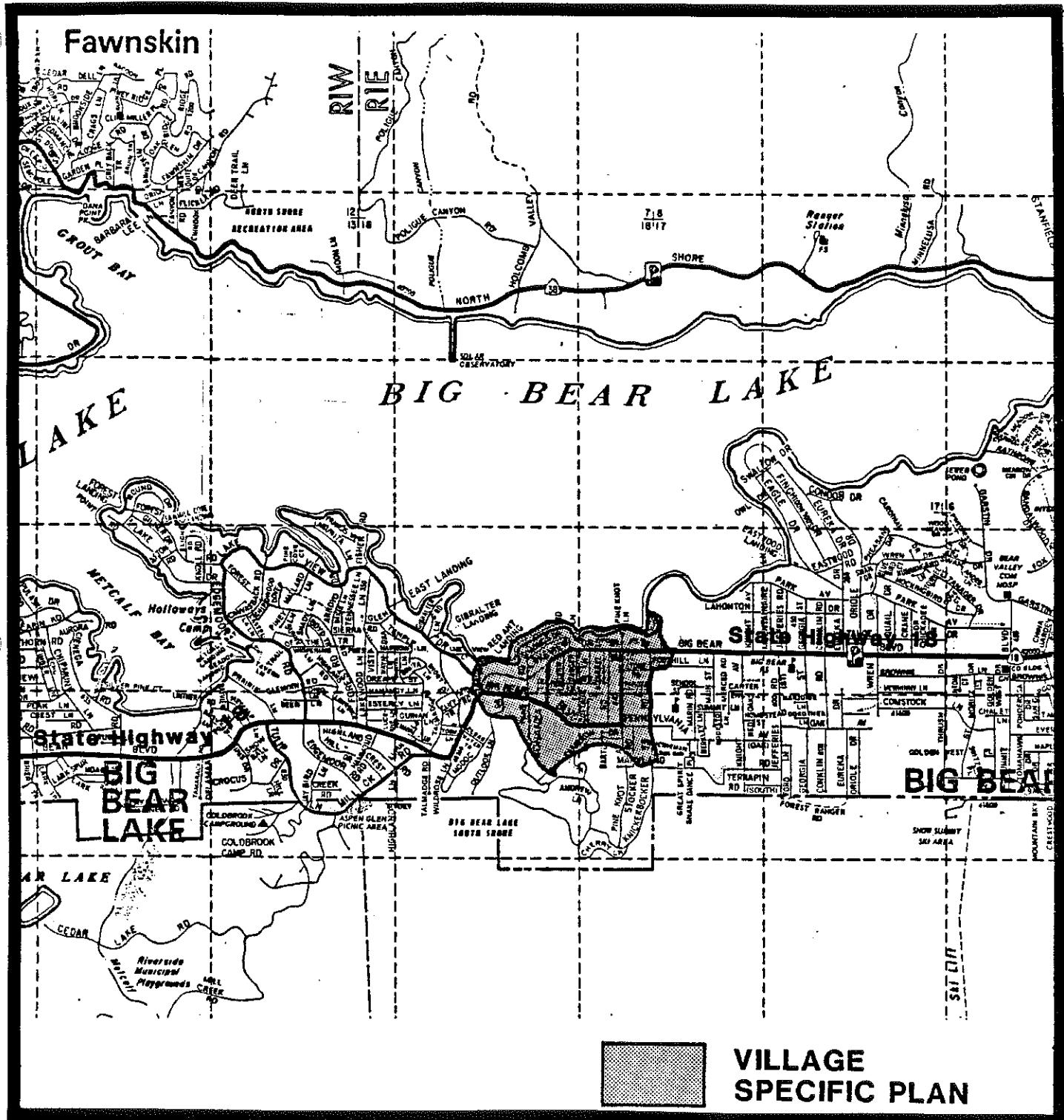


卷之三

NO SCALE

EXHIBIT 1

VICINITY MAP



Big Bear Village Specific Plan



THE
PLANNING
CENTER

240 NEWPORT CENTER DRIVE SUITE 215
NEWPORT BEACH CA 92660 (714) 640-4911

NO SCALE

EXHIBIT 2

Village is nestled at nearly 7,000 feet above sea level. The Village is also located on a small portion of the southern shore of Big Bear Lake which is approximately 7 miles long and 1 mile wide. The Big Bear Valley and the Village are served by State Highway 18 which runs directly through the Village.

E. BACKGROUND/PLAN OVERVIEW

The City of Big Bear Lake is a relatively young City. Governed by charter, the City incorporated in November of 1980. Upon incorporation, the City adopted the San Bernardino County Code, Development Code and General Plan. Shortly thereafter, the City began proceedings necessary to update its General Plan. That updating is now complete and this Specific Plan is consistent with the applicable elements of the General Plan.

The Village area should be planned as a cohesive resort/residential/commercial area built upon already successful commercial and lake open space opportunities. The plan stresses land uses, pedestrian and vehicular circulation and establishes discernible subareas within the Village.

These subareas include:

Area 1: Marine/Lake Development

Ideal area for lake related commercial and open space uses such as boat rentals, docking facilities, boat launching, food establishments, public picnic areas, continuous boardwalk area and tourist residential uses.

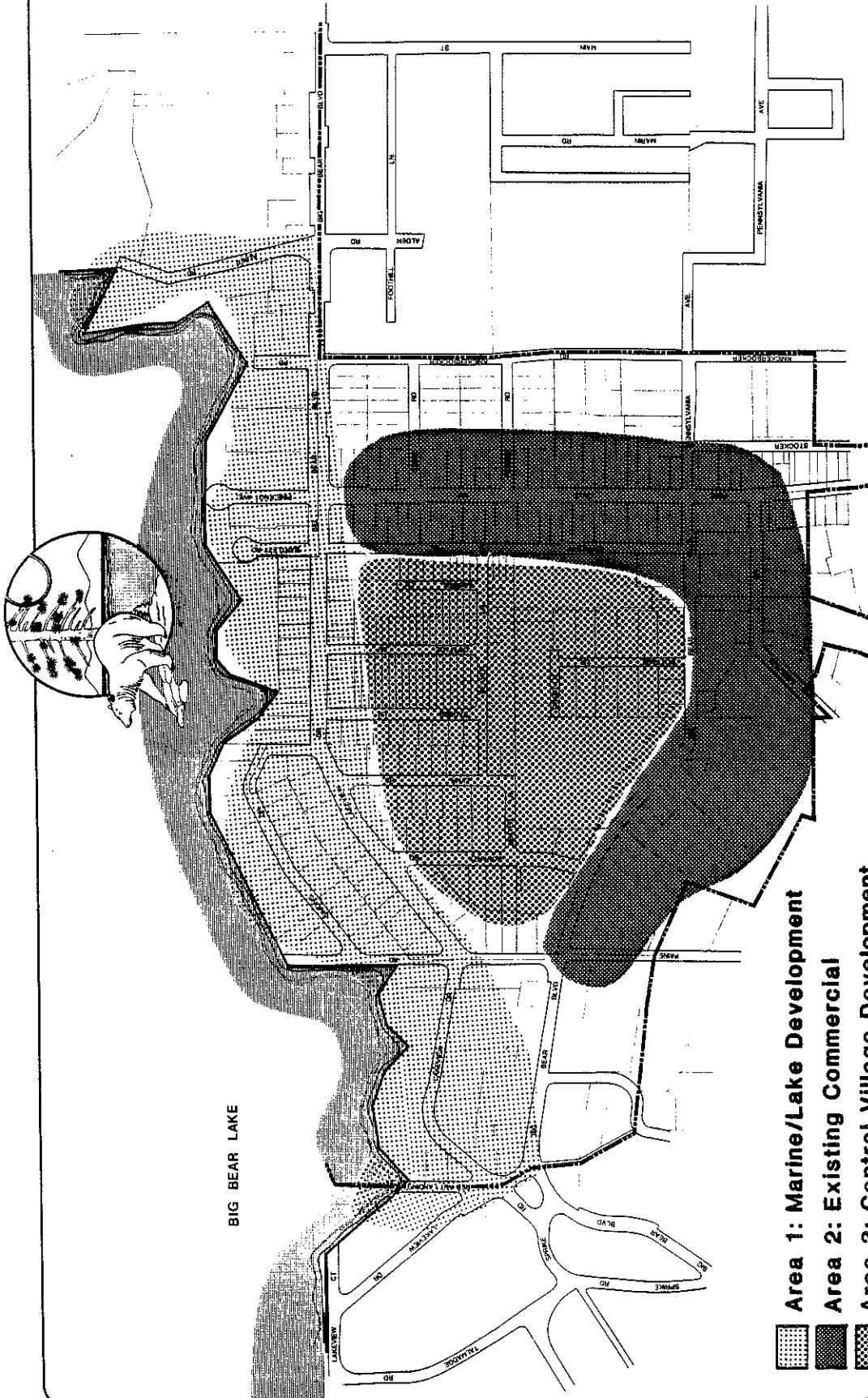
Area 2: Existing Commercial Area ("Old Town")

Plans for this area will focus upon strengthening the design and aesthetics of commercial properties along Big Bear Boulevard and Pine Knot Avenue. Streetscape improvements, parking programs, sign controls, and special focal points will dominate the planning for this area. Specific entry sign identification will inform visitors on their approach to the Village.

Area 3: Central Village Development

As a destination resort, the development focus for the central Village area will be tourist serving commercial and residential uses. Specific sites with views of the lake should be utilized for restaurants and lodging facilities. This area will also contain specific public parking areas meant to serve the other two planning areas - namely the

Village Subareas (Conceptual only)



VILLAGE SPECIFIC PLAN

PREPARED FOR THE CITY OF BIG BEAR LAKE

Area 1: Marine/Lake Development

Area 2: Existing Commercial

Area 3: Central Village Development

EXHIBIT 3

Marine and Commercial areas. Because many people come to Big Bear to experience the out-of-doors, a series of pedestrian/bicycle trail links are envisioned for the entire planning area (see Figure 1).

F. INTENT

This Specific Plan has been prepared for the City of Big Bear Lake, California in response to a General Plan recommendation and provides the regulations, conditions and programs necessary for implementation of applicable elements of the General Plan.

The Specific Plan defines the future development plans for the "Village" area of Big Bear. It addresses all relevant planning issues. These represent the primary opportunities and constraints for the Village, the more significant of which include:

- o expansion of urban development while preserving valuable scenic and open space amenities;
- o enhancement of the appropriate character of development given environmental conditions, the marketplace and surrounding land uses;
- o preservation of significant natural features;
- o enhancing the nature of a resort destination and its fiscal and public services implications; and
- o opening up the lakefront to the public.

A key function of the Specific Plan is to reduce the need for subsequent detailed planning and environmental review procedures for development of the Village. The Specific Plan provides the necessary regulations for the project area so that future development proposals consistent with the Specific Plan may proceed with Tentative Tract Maps, Site Plans, Design Review Permits and/or other discretionary permits without a requirement for new environmental documentation.

Government Code Section 65507 gives a legislative body authorization to adopt an ordinance or resolution requiring that a specific plan be prepared when it is in the public interest to do so. As outlined in Government Code Section 65451, specific plans shall include all detailed regulations, conditions, programs and proposed legislation necessary for systematic implementation of each element of the general plan with respect to the following:

1. The location of business, housing industry, open space, parking, recreation facilities with regulations establishing height, bulk and setback limits for such buildings and facilities.
2. The location and extent of existing or proposed streets and roads, the proposed widths with reference to prospective standards for their location and use of all other transportation facilities, whether State or City maintained.
3. Standards for population density and building density, including lot size, permissible types of construction, and provisions for lot coverage, parking, landscaping, and the location of special design elements.
4. Standards for the conservation, development, and utilization of natural resources (lake, ski areas, flood control channel).
5. The implementation of all applicable provisions of the General Plan.
6. Other measures as may be necessary or convenient to ensure the execution of the General Plan.

G. OBJECTIVES

Objectives of the specific plan include but are not limited to:

- o Assessment of potential overlay districts which would guide development's architectural treatments, materials, signing and landscaping.
- o Development of streetscape plans which will improve the overall visual character and impact of the Village.
- o Development of public open space areas specifically along the lakefront and within the central Village area itself.
- o Assessment of different methods of providing the Village area with a "sense of entry" and feeling of arrival.
- o Assessment of a variety of methods to improve overall vehicular and pedestrian circulation.
- o Development of a methodology for plan implementation.

II. LAND USE PLAN

A. GOALS AND OBJECTIVES

GOALS

- o Improve the gross retail sales environment for all commercial uses within the Village.
- o Attract more people to the Village and encourage more lodging and eating in this area.
- o Improve the visual character of Village.
- o Improve open space opportunities in the Village.
- o Avoid incompatible land uses in the Village Area.
- o Develop an economically viable business climate in cooperation with the Improvement Agency.

OBJECTIVES

- o Introduce more resort/residential use areas within the Village to establish a more constant tourist population.
- o Delineate areas immediately adjacent to the lake for public pedestrian malls and parks.
- o Re-establish commercial areas along major travel routes and high volume pedestrian ways.
- o Provide central public parking lots in close proximity to commercial uses.
- o Avoid the introduction of governmental and industrial uses from the center of the Village area.
- o Establish an open space common area within the center of the village to provide a "central focus" and "gathering" spot for the shopping tourists within the Village.
- o Establish resident oriented commercial areas on the periphery of the Village relate to help support tourist uses.
- o Expand the existing Marina use so that public open space opportunities along the lakefront can be maximized.

Land Use Concept

The Big Bear Village land use plan provides for a mixture of

uses, when combined, is a balance between open space, tourist residential, commercial and support facilities. The viability of these land uses and the resort character of the Village will be assured through the continuance of recreation assets in the Big Bear Valley such as the Lake and adjacent skiing areas.

The dominant elements of the plan are the proposed Village hotel site, the existing marina on Big Bear Lake, the existing commercial core, and the new central village uses. These four items begin a series of nodes which when connected by roadway and trail linkages, begin to give definition to the overall layout of the land use concept map. Some basic and sound design concepts directed the land use concept map. They include but are not limited to:

- o Concentrate uses and activities in the historical and geographic center of the Village.
- o Diversify and mix uses. Historically, small resort villages have provided a variety and mix of activities for many people.
- o Cluster complementary uses. Many activities function better both economically and socially when they are placed close to one another.
- o Maintain the relationship to Big Bear Boulevard and Pine Knot Avenue. Activities and uses should have direct visual and physical access to pedestrian and auto circulation. Large parking lots, building setbacks or heavy landscaping will discourage social interaction of people.
- o Create a secondary commercial area at the entry to the Village to provide a focus for development concern in that area.
- o Take advantage of the close proximity of Big Bear Lake. Cluster complimentary uses along or facing toward this special amenity. Provide much needed open space corridors along the Lake's edge.

B. CONSISTENCY WITH THE GENERAL PLAN

1. Land Use Plan

The Specific Plan Land use plan is consistent with the goals and objectives of the General Plan in that it calls for:

- o open space and pedestrian access networked
- o balancing of land uses to assure year round revenues

- o specific architectural styles for Village area
- o a balanced economic structuring of land uses
- o improvements and enhancement of existing shopping areas
- o development regulations allow maximum use flexibility

2. Circulation Element

The Specific Plan circulation element is consistent with the goals and objectives of the General Plan in that it calls for:

- o eliminating bottlenecks by re-routing SR 18
- o developing a comprehensive public parking program
- o developing a pedestrian access network
- o a phased program of street improvements, widenings, re-routing, etc.
- o Appropriate signing for currently underutilized parking lots

3. Public Facilities/Infrastructure

The Specific Plan is consistent with the goals and objectives of the General Plan in that it calls for:

- o improving sidewalks and roadways
- o utilizing the flood control channel as a community asset
- o encouraging lot consolidation therefore reducing infrastructure hook-ups
- o utilizing the street system more evenly

4. Economic Development Element

The Specific Plan is consistent with the goals and objectives of the General Plan in that it calls for:

- o improving the Villages year round fiscal climate
- o improving lake access which will help balance out the summer and winter skiing seasons
- o more intensive development standards within the village area
- o a nicer, pedestrian shopping experience

5. Recreation and Parks Element

The Specific Plan is consistent with the goals and objectives of the General Plan in that it calls for:

- o improving access to Snow Forest through the use of a separate chair lift
- o providing expanded public open space opportunities along the edge of Big Bear Lake
- o development of open space nodes along the pedestrian

- o trail network allowing for passive recreational opportunities
- o allowing uses conducive to active recreational opportunities within the Village

6. Housing Element

The Specific Plan is consistent with the goals and objectives of the General Plan in that it calls for:

- o providing second home opportunities throughout the Village area
- o encouraging new land uses and intensities for areas with substandard housing units

7. Environmental Resources Element/Open Space Element

The Specific Plan is consistent with the goals and objectives of the General Plan in that it calls for:

- o maintaining an architectural style for development in the Village which is compatible with the sub-alpine forest environment
- o requiring that new developments appropriately landscape their sites with locally proven plant species
- o requiring the preservation of significant trees on development sites
- o development of a streetscape theme sensitive to the mountain environment
- o requiring that a pedestrian accessway along the edge of the lake be preserved to be located on public land connecting the 2 marinas.
- o development of a pedestrian trail network around the Village

8. Scenic Highways Element

The Specific Plan is consistent with the goals and objectives of the General Plan in that it calls for:

- o certain required minimums of open space per site
- o preservation of large trees
- o adequate building setbacks
- o strict sign regulations, restricting unusually high or obnoxious signs
- o design guidelines for street lighting, sidewalks, benches and the like
- o entry statements at key Village entry points

9. Seismic Safety, Noise and Public Safety Elements

The Specific Plan is consistent with the goals and

objectives of the General Plan in that it calls for:

- o providing regular and well designed vehicular and pedestrian access
- o restricting uses in noise impacted areas to non-residential uses
- o restricting high noise producing uses
- o providing specially textured crosswalks for the protection of pedestrians
- o providing a series of public pedestrian trails throughout the Village Area
- o restricting any more driveway entrances on Big Bear Boulevard and Pine Knot Avenue

III. CIRCULATION PLAN

A. GOALS AND OBJECTIVES

Goals

- o Facilitate efficient movement of people and goods.
- o Promote a safe transportation system.
- o Maintain and maximize economic potentials for the entire Village Area through transportation improvements, both vehicular and pedestrian.
- o Promote a balanced transportation system.
- o Minimize negative impacts of new and existing transportation facilities.
- o Promote a comprehensive pedestrian circulation system for the Village Area.
- o Minimize the need for vehicular movement within the Village Area.

Objectives

- o Eliminate major bottlenecks on the street system.
- o Improve and develop the street system to provide route options for local trips.
- o Promote efficient utilization of parking facilities.
- o Minimize the volume of through traffic on residential streets.
- o Provide alternatives to automobile access wherever possible by increasing transit accessibility and improving pedestrian and bicycle circulation opportunities.
- o Design new developments to provide an appropriate hierarchy of streets.
- o Stage street improvements to minimize impact on adjacent development.
- o Maximize efficiency of existing transportation facilities.
- o Develop public parking areas in close proximity to the "Old Town" portion of the Village area.

- o Develop a comprehensive trail loop through the Village which links major activity nodes.
- o Enhance sidewalk opportunities throughout the Village area.
- o Open the lakefront edge to pedestrian traffic.
- o Enhance sidewalk treatments and encourage private developments to provide additional pedestrian amenities.

B. EXISTING CONDITIONS SUMMARY

Traffic Volumes

The existing weekday traffic volumes for SR 18 in the vicinity of Big Bear Village are tabulated in Table 1.

TABLE 1
EXISTING TRAFFIC VOLUMES³

<u>Street</u>	<u>Location</u>	<u>Average Daily Traffic</u>		
		<u>Peak Month</u> ²	<u>Annual</u> ¹	<u>Peak Hour</u>
Big Bear Blvd.	West of Pine Knot	11,700	9,400	940
Pine Knot	North of Big Bear	16,100	13,000	1,300
Big Bear	East of Pine Knot	19,800	15,200	1,500
Lakeview	West of Pine Knot	7,300	N/A	N/A

Notes:

1. Annual Average Daily Traffic is defined as the total annual traffic volume divided by 365 days per year.
2. Peak Month Average Daily Traffic is defined as the total peak month traffic volume divided by the number of days in that month.
3. Source: 1983 Traffic Volumes on California State Highways, Caltrans.

Volume-Capacity Analysis

The standard method for determining roadway adequacies or deficiencies is by a volume-capacity analysis. This procedure determines the ability of the roadway to serve traffic volumes based on the physical characters of the roadway at various levels of services.

There are six levels of service, Levels A through F, which relate to driving conditions from best to worst, respectively. Level of service "E" is defined as capacity. It is the maximum number of vehicles which have a reasonable expectation of passing over a given time period under prevailing traffic conditions. However, unpredictable factors such as accidents and vehicle breakdowns can totally disrupt traffic flow at which time the practical service volume of a roadway is significantly reduced.

The Level of Service "C" is defined as a condition of stable traffic flow but speeds and maneuverability are somewhat restricted. This level is generally used as the basis for design and is considered to be an acceptable level of traffic flow. A relatively satisfactory operating speed is still obtained and most drivers do not find this operation objectionable. It is felt that a level of service A or B really isn't necessary or desirable from a merchandising and ultimately a "Village Atmosphere" point of view.

The operating level of service on the major Village streets are summarized in Table 2.

TABLE 2
VOLUME - CAPACITY ANALYSIS FOR MAJOR STREETS

<u>Street Location</u>	<u>Peak ADT</u>	<u>Level of Service</u>
Big Bear Boulevard from Paine to Pine Knot	11,700	D
Pine Knot Avenue north of Pennsylvania	16,100	E
Lakeview Dr. west of Pine Knot	7,300	B
Big Bear Boulevard east of Lakeview	19,800	E

The volume-capacity analysis indicates that Pine Knot Avenue between Pennsylvania and Lakeview and Big Bear Boulevard west of Pine Knot Avenue is operating below the desirable level of service at the current weekday traffic volume during the peak month. In addition, a number of deficiencies and problems that adversely impact traffic flow but are not considered in the volume-capacity analysis are summarized as follows:

1. Diversion of through traffic from CBD
2. Varying cross-section (width) of streets
3. Conflict between tourist and through traffic
4. No direct access from major street to parking
5. Uncontrolled driveway access
6. On-street parking maneuvering adversely impacting traffic flow
7. Delays to traffic by parking and left turns
8. Designation of R/W

C. CIRCULATION IMPROVEMENT RECOMMENDATIONS SUMMARY

1. Big Bear Boulevard and Pine Knot Avenue cannot adequately accommodate future traffic volumes. Traffic volumes indicate that four lanes are desirable now to maintain a high level of service. Widening Big Bear Boulevard and Pine Knot Avenue to four lanes is not considered a viable option at this time due to the desire to maintain a "Village Center" atmosphere. Therefore, Lakeview should be designated as the through highway (SR 18) and appropriate signing be installed to attract tourists and resident shoppers into the Village while other motorists not wishing to stop in the Village are not encouraged to use Big Bear Boulevard.

Lakeview Drive in being designated a State Road would fall under Caltrans roadway design standards. It would be safe to assume that Caltrans would ultimately want to increase the roadways capacity commensurate with their current road widening project on Big Bear Boulevard, east of Pine Knot. Representatives with Caltrans cautioned, however, that actual roadway design would be dependent on traffic counts and intersection turning movements recorded for this reach of highway. Minimum design standards for four lane facility would be a 64 foot roadway with an ultimate 80 foot right of way.

There are two measurable benefits to this recommendation:

- 1) Enable the provision of an alternate route of through traffic from Big Bear Boulevard and Pine Knot Avenue (which have capacity problems) to Lakeview which has adequate right of way to provide sufficient capacity.
- 2) Redesignation of Big Bear Boulevard and Pine Knot Avenue as a City street would allow total local control of roadway development including sidewalks, lane widths, surface treatments, landscaping, special lighting, entry signs, curbs, seating areas, etc.

The phasing of the relocation of the State Route 18 from Big Bear and Pine Knot to Lakeview is critical. It is therefore recommended that the City not initiate the actual relocation until all public improvements (especially signage) have been planned and construction is ready to commence on Big Bear Boulevard and Pine Knot Avenue, and parking, streetscape design (trees/streetlights) are under construction.

2. Within the next 7-8 years, Lakeview should be widened from two to four lanes to accommodate existing and future traffic volumes. This work should not commence until the appropriate signing and entry treatments for the Village area have been completed and Lakeview begins to reach capacity.
3. All angled street parking along Big Bear Boulevard should be modified from angle to parallel parking. The residual (private property) area should be paved and a pedestrian sidewalk/plaza installed. This action will reduce present on-street parking by one half, but will considerably improve roadway capacity, delaying but not abandoning all roadway widening scenarios.
4. Additional capacity over the next fifteen year period between the northern and southern portion of Big Bear Boulevard will be necessary. There are several alternatives which include the following:
 - a. Widening of Pine Knot Avenue;
 - b. Upgrading Knickerbocker to relieve traffic from Pine Knot;
 - c. Upgrading of Bartlett Road; and
 - d. Designation of a one-way couplet, i.e., Bartlett Road and Pine Knot.

From a purely traffic operations point of view, the preferred alternative is the widening of Pine Knot from two to four lanes. However, this alternative would render the major goals of the Village defunct in that any "small village" atmosphere would be difficult to maintain.

It has been concluded that the long term (12-15 year) solution to decreasing congestion in the Village will be the upgrading of Knickerbocker. However, Knickerbocker should not be upgraded to the status of a major street but to that of a collector. This measure should only be initiated after the Village Center has developed a significantly higher building mass and Pine Knot Avenue begins to show signs of severe inefficiency.

5. All right-of-way and street improvements shall be implemented through the existing City ordinance which would require a developer to dedicate the right-of-way or improve prior to obtaining a building permit.
6. It is recommended that a shuttle service between the proposed parking facilities in the Village and the ski areas be considered. These parking facilities should be clearly identified and signed so that prime commercial short term spaces aren't filled.
7. It is recommended that the City consider renaming Lakeview Avenue to Big Bear Boulevard or that some street name be given to the Big Bear Boulevard reach between Paine and Pennsylvania Avenue.
8. It is recommended that a shuttle service within the Village limits alone be considered in the near future.
9. It is recommended that as an interim measure, angled parking on smaller side streets such as Bartlett and Pedder Road, be initiated to create more public parking.

D. PARKING PROGRAM CONCEPT

It is anticipated that the Village Center area of Big Bear Lake will need an organized system of public off-street parking facilities in the near future. This off-street parking system is necessary to attract tourists and residents as well as assist development/redevelopment concerns in an area of small, odd sized lots.

The City of Big Bear Lake can play a major role in alleviating current Village parking problems. Most notably it can take an active part in the physical development of new Village parking facilities. Essential to this program would be an effective means of land acquisition and financing.

Land Acquisition

The City has numerous methods available to it for acquiring land for parking purposes located close to commercial activity. Some of these are briefly discussed here.

1. Negotiated Purchase - Most frequently, land is acquired through negotiations with private property owners. Neither party to the acquisition will be disadvantaged because the price is agreed upon voluntarily.
2. Lease - Out right purchase of property for a public purpose ordinarily removes it from the tax rolls. Under

some circumstances, it may be more expedient to lease properties needed for off-street parking facilities for a stipulated period of time and under designated terms and conditions. Such an arrangement will require a much smaller immediate outlay of funds than outright purchase and may be an appropriate device for Big Bear Lake. A disadvantage, however, is that when the lease expires or is terminated, the land may be changed to a non-parking use. Nevertheless, a lease may be drawn to contain an agreement for municipal purchase of the property at a stated price at the termination of the lease.

3. Condemnation - When negotiations with a property owner have not produced a reasonable compromise, a municipality may exercise its power of eminent domain as authorized by law. For technical reasons, such as the need to clear title, court proceedings are sometimes necessary. Forcible acquisition should usually be considered as a last resort because it tends to be costly and time-consuming and sometimes contributes to poor public relations.
4. Multiple Use of Existing Public Lands - To provide off-street parking, Big Bear may utilize land that was originally acquired for other public purposes, such as for parks, public squares, or streets. Such conversions should be made only with full awareness of their recreational, visual and circulatory consequences.

Other options available but not discussed here due to their limited application potential in Big Bear include:

- o Land dedications
- o Land donations
- o Reclamation of marginal lands
- o Air rights
- o Excess condemnation

Financing

The following options are open to the City of Big Bear Lake for financing land acquisition, construction and operation of parking facilities:

1. General Revenue Financing - Big Bear might consider financing parking projects from their annual operating or capital improvement budgets. This method is feasible primarily where the required parking projects are limited in scope such as the old town portion of the Village. General revenue financing is also appropriate in cases where parking lots can be developed on public land or where property can be purchased or leased at a

low cost.

2. Municipal Parking Fund - Many communities develop off-street parking facilities from funds accumulated from parking revenues. The net revenues, or a fixed percentage of gross revenues, derived from parking meters are deposited in a reserve fund for acquisition and development of additional sites. If local zoning regulations permit, contributions to this fund might also come from developers who wish to pay into the fund in lieu of providing required off-street parking. The limitation of the parking fund approach is that it can be a lengthy process, and funds may not accumulate as fast as parking demands grow.
3. Revenue Bonds - Financing through revenue bonds is becoming increasingly popular. Only the parking system revenues are pledged to the retirement of the indebtedness, and, as a consequence, the general debt of the municipality is not affected. The parking system indebtedness is paid off solely from system revenues. These revenues could be gained through parking meters, or an active system of parking fees. It may even be possible to utilize assessments to retire the general debt.

Because this limitation leads to a higher risk, the rate of interest for revenue bonds is higher than for general obligation bonds. An additional safeguard sometimes required by purchasers of revenue bonds is that the debt coverage ratio for future years -- that is, projected revenues compared to debt service requirements -- must be demonstrated to be 2.0 or greater. Generally speaking, as the debt coverage ratio increases above 2.0, the issue becomes easier to sell and/or the rate of interest becomes lower.

4. General Obligation Bonds - An advantage of general obligation bonds for financing parking facility construction is that they normally carry lower interest rates than revenue bonds. This is because the full faith and credit of the municipality is pledged to the redemption of general obligation bonds. Furthermore, if revenue from the new parking facility are sufficient, it will not be necessary for additional taxes to be levied to meet annual debt service charges. The principal disadvantage in using general obligation bonds is that the potential credit available for non-parking uses in the community (e.g., parks and public buildings) is reduced by the amount of the bond issue. Statutory debt limits would also be affected.

5. Special Assessment - Under the special assessment procedure, properties within the Village area near the proposed parking facilities are assessed in accordance with their benefit, and the moneys so derived are used to retire special assessment bonds sold to finance construction. Assessments may be spread through the use of simplified or highly sophisticated procedures. These procedures might consider distance from the new facility, the parking demand generated by individual properties, and/or assessed valuation, and they may grant credit for the number of parking spaces that are already provided by a commercial property. However, a logical case can be presented for spreading assessments solely according to the assessed value of land, on the theory that the entire Village benefits from adequate parking.

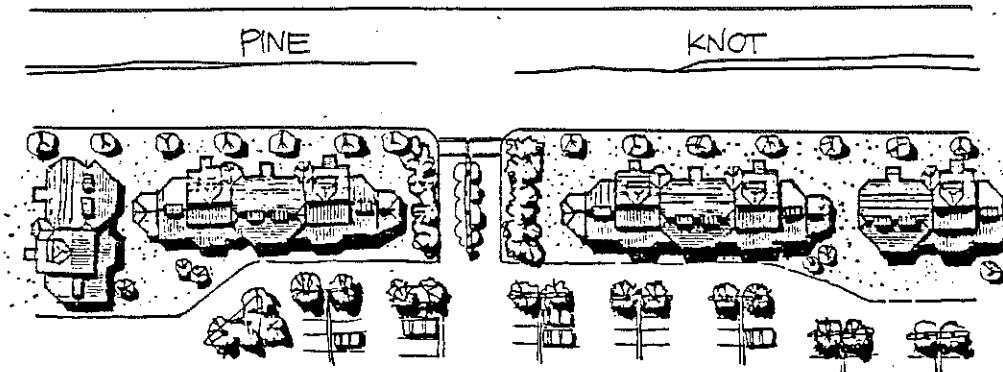
This Specific Plan contains many implementation programs with specific goals in mind which can and will result in immediate or near term implementation of the recommendations. It would be noted however, that certain additional actions will be necessary, or may be determined to be desirable to continue moving toward a Village Center development program in an expeditious, efficient manner.

Many of the programs for acquisition and financing of public parking areas for the Village cannot be settled at this time. Additional actions and decisions are necessary at the City level before the above mentioned programs may proceed. These additional actions may include 1) site selection, 2) potential special zoning amendments, 3) preparation of final design, cost estimates and working drawings for parking lot construction, 4) preparation and approval of a financial package to pay for construction and maintenance, and 5) initiation of workshops through which property owners and merchants are apprised of assessments, in-lieu fees, or special conditions.

On the other hand there are some specific actions that the City can take in the near future to help alleviate current parking problems.

1. Restrict parking reductions to 20%.
2. Provide appropriate signing to existing lots.
3. Begin site selection studies.
4. Begin determining appropriate charges for fees-in-lieu if that program is selected.
5. Restrict parking location for Pine Knot Avenue future developments to the rear of the site.

Private Parking Concept (Pine Knot Avenue & Big Bear Boulevard)



What this graphic is depicting is how all future on-site parking for, Pine Knot Avenue and Big Bear Boulevard will be restricted to the backs of the property with a coordinated series of interconnecting drives between parking lots. All development should be required to develop in this manner in the future.

Potential Public Parking Locations

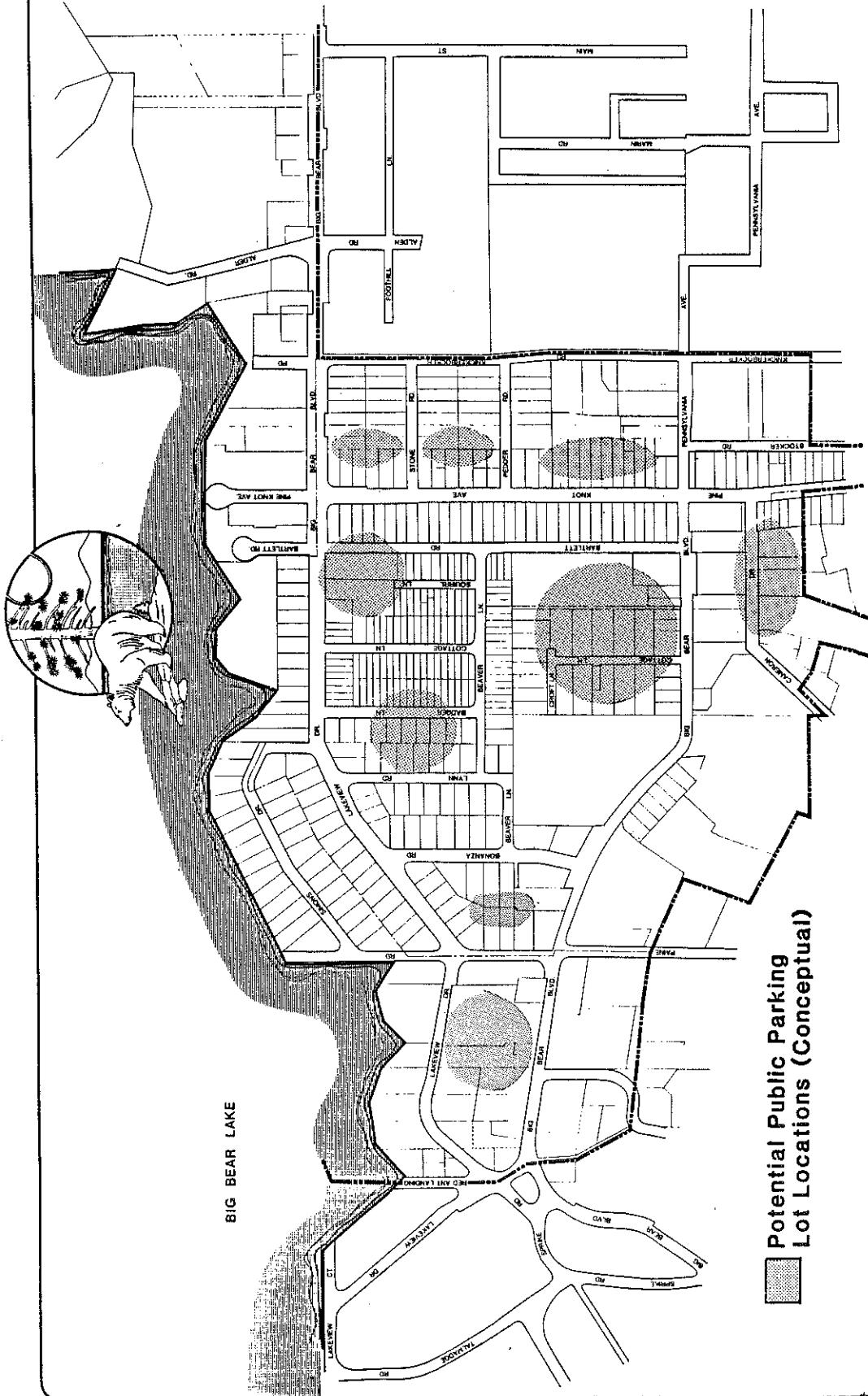
The following graphic depicts in a general manner where in the Village public parking facilities might be located.

E. VILLAGE PARKING ASSESSMENT DISTRICT

The purposes for which a parking district may be created under the Parking District Act of 1979 authorizes the City of Big Bear Lake to impose assessments or charges or both on businesses within a parking district and business improvement area, which is in addition to any assessment fees, and to use such proceeds for the benefit of businesses within such district by doing any of the following:

1. The acquisition, construction or maintenance of parking and restroom facilities for the benefit of the area.
2. Decoration of any public place within the area.

Potential Public Parking



 Potential Public Parking
Lot Locations (Conceptual)

VILLAGE SPECIFIC PLAN

PREPARED FOR THE CITY OF BIG BEAR LAKE

EXHIBIT 4

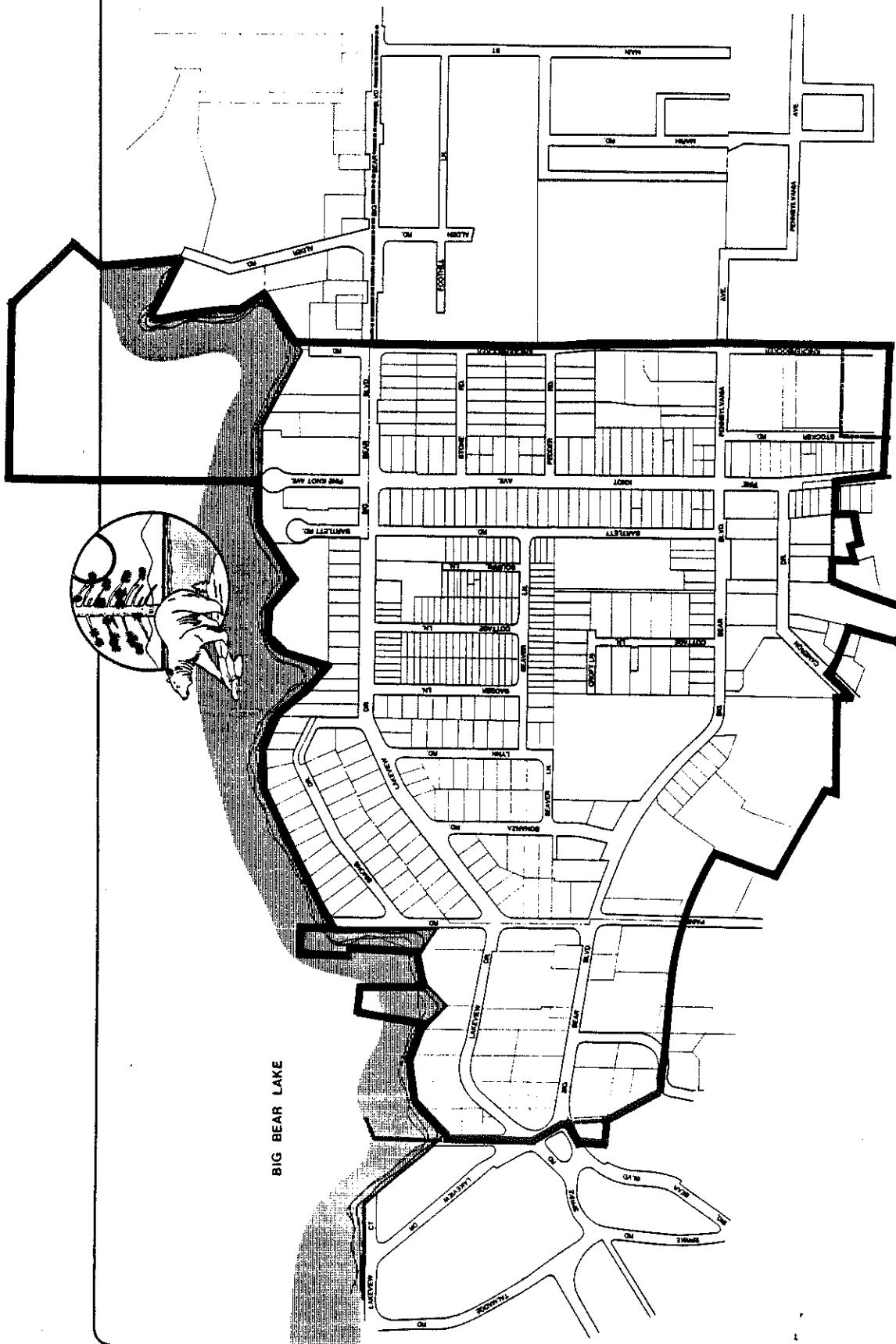


— PROPOSED PARKING DISTRICT BOUNDARY



VILLAGE SPECIFIC PLAN

PREPARED FOR THE CITY OF BIG BEAR LAKE



3. Promotion of public events which are to take place in public places in the area.
4. Furnishing of music in any public place in the area.
5. The general promotion of business activities within the area.

The first step in the creation of such a district is the adoption of a resolution of intent to establish the area. The resolution is required to adopt the following information:

1. A description of the boundaries of the proposed area.
2. The time and place of the hearing to be held at the City Council for consideration of the establishment of the area.
3. The purposes for which the proposed revenue created within the district shall be allocated.
4. A description of the system of assessments or charges which will be used and the businesses upon which a levy will be made. If a classification system is to be used in making the assessment or charges, there shall be included a description by class or business of the classification system to be used. A reference line to California Government Code Section 36521.

DESCRIPTION OF THE AREA

The parking assessment area is generally a large portion of the Village Specific Plan boundary area. For clarity a map of the proposed parking district Number 1 is included.

The following is a legal description of the proposed parking district Number 1.

Proposed
City of Big Bear Lake Parking District

That portion of Section 19, Township 2 North, Range 1 East, San Bernardino Meridian, in the City of Big Bear Lake, County of San Bernardino, State of California, described as follows:

Beginning at the center line intersection of Knickerbocker Road and Big Bear Boulevard;

Thence Westerly along said center line of Big Bear Boulevard to an intersection with the Southerly prolongation of the

West line of Bartlett Road;

Thence Northerly and Northwesterly along said prolongation and the West line of Bartlett Road to the most westerly point on the West line of Bartlett Road;

Thence parallel with the center line of said Bartlett Road Northerly to the Southerly high water line of Big Bear Lake, said line being the City Limits of the City of Big Bear Lake;

Thence Westerly along said City Limit line to the North-easterly corner of Lot 19, Tract No. 2820, as per map recorded in Book 40, Page 5 of Maps, in the office of the Recorder of said County;

Thence Southerly along the East line of said Lot 19 at its southerly prolongation to the centerline of Simonds Drive;

Thence Northwesterly along said centerline to an intersection with the Northwesterly prolongation of the Northeasterly line of Lot 21 of said Tract No. 2820;

Thence Southwesterly along the Southeasterly lines of Lots 21 through 32 of said Tract No. 2820 and the Southwesterly prolongation of the Southeasterly line of said Lot 32 to the centerline of Paine Road;

Thence Southerly along said centerline of Paine Road to an intersection with the centerline of Big Bear Boulevard;

Thence Easterly along said centerline to the centerline of aforesaid Paine Road;

Thence Southerly 351.42 feet along said centerline of Paine Road;

Thence South $65^{\circ}56'15''$ East, 300.57 feet;

Thence South $20^{\circ}06'$ West, 258.01 feet;

Thence South $61^{\circ}14'50''$ East, 379.15 feet;

Thence South $19^{\circ}59'25''$ West, 38.36 feet;

Thence South $89^{\circ}26'20''$ East, 358.27 feet;

Thence South $0^{\circ}18'30''$ West, to an intersection with the centerline of Cameron Drive;

Thence Northeasterly along said centerline to an intersection with the Northwesterly prolongation of the Southwesterly line of that certain parcel of land conveyed to Ralph R. Raper and Edell Raper, husband and wife, and recorded September 10, 1971 in Book 7750, Page 351, Official Records of said County;

Thence Southeasterly along said prolongation to the Southwesterly corner of said Raper property;

Thence South $48^{\circ}27'$ East, 57.42 feet;

Thence South $74^{\circ}03'30''$ East, 122.34 feet;

Thence South $9^{\circ}18'$ West, 21.71 feet;

Thence South $14^{\circ}31'30''$ West, 185.93 feet;

Thence in a straight line Easterly to a point which is South $19^{\circ}00'17''$ West, 279.1 feet of a line that is parallel with and 165 feet Southerly of the South line of Cameron Drive;

Thence North $19^{\circ}00'17''$ East, 279.1 feet;

Thence Easterly along a line that is parallel with and 165 feet Southerly of the South line of Cameron Drive to an intersection with the Westerly line of Tract No. 2022, as per map recorded in Book 30, page 20 of Maps, in the office of the Recorder of said County;

Thence Northerly along said Westerly line to an intersection with the South line of the North half of Lot 28 of said Tract No. 2022;

Thence Easterly along said South line and its Easterly prolongation to an intersection with the centerline of Pine Knot Avenue;

Thence Southerly along said centerline to an intersection with the Westerly prolongation of the South line of Lot 11 of said Tract No. 2022;

Thence Easterly along said prolongation of the South line of Lot 11 of said Tract No. 2022;

Thence Easterly along said prolongation and the South line of said Lot 11 and its Easterly prolongation to an intersection with the centerline of Stocker Road;

Thence Northerly along said centerline to an intersection with a line that is Southerly 429.87 feet from the Southeast corner of the intersection of Pennsylvania Avenue and said Stocker Road;

Thence South $88^{\circ}39'30''$ East, to an intersection with the centerline of Knickerbocker Road;

Thence Northerly along said centerline to the Point of Beginning.

Contains 85 acres.

Purposes

The purposes for which the proposed revenue created within the district shall be allocated are:

1. To purchase or lease land in order to construct parking lot(s) open to the public.
2. To construct and landscape parking lots, garages or restroom facilities.
3. To maintain parking lots, garages or restroom facilities and their appurtenant landscaping.
4. To provide snow removal or plowing service for the parking lot(s).

The Assessment

The most difficult part of the formation of such a district will be a decision on which of the assessing mechanisms are to be utilized to pay for the construction and maintenance of a parking lot.

Such methods may include:

1. Property owners may buy into the parking district, should additional parking be required. This would allow someone to add on to an existing use and still meet parking standards, or construct a use at a higher intensity with off-street parking provided in the district parking lot, or,
2. All property owners contribute to the district whether or not they utilize it. Should property owners use it,

a higher rate may be charged.

In order to be equitable it is recommended that a method of assessment utilizing both (1 and 2) suggestions be employed thus:

o Property owners may buy into the parking district should they require additional parking in order to expand their businesses and other property owners desiring to construct higher intensity new businesses without on-site parking must buy into the district in order to provide required parking.

AND

o A base assessment shall be levied to all properties in the district whether or not they utilize it."

AND

o The assessment would be levied per the following program. It is impossible to determine exactly what the parking assessment should be. An engineer especially familiar with these types of programs should be employed to determine costs.

1. Annual parking district assessment for every business within the district. The annual assessment would be based on need.
2. Any business that wishes to expand or build new and cannot provision the parking on-site shall pay into the parking district a one-time amount. This is in addition to the annual operation/maintenance costs of Item 1 above.
3. Businesses wishing to provide more than twelve spaces in the district parking areas may work out a payment schedule with the City for a period to be determined by the City.

Exemptions

It is recommended that all single family and two family residences not be included in the parking district assessment.

F. THE PEDESTRIAN NETWORK

One of the major guiding principles behind a large part of the ideas encompassed in this plan is to attract the visitor and resident to the Village Center, and get them to park

their car and do most of their shopping on foot. A recent survey done in conjunction with the National Park Service indicated that the "number one" activity that visitors said they wanted to participate in when coming to the Big Bear Valley was to "go walking."

The central Village presently relies to some degree on pedestrian foot traffic. The Village is tied together, however, by a physical network based not on the needs of pedestrians, but rather those of automobiles and other motor vehicles. As a result, the pedestrian in the Village often faces the danger of being run down by a car or truck. Being an area so dependent on pedestrians, the problems of narrow sidewalks, non-existent sidewalks, and many cross streets, these problems are some of the Village's major functional and competitive liabilities, particularly when confronted with competition from nearby highway commercial shopping centers.

Many downtown planning efforts, while giving major consideration to car and truck movement, pay relatively little attention to the CBD's needs for pedestrian circulation, safety, and amenities. The following sections will provide some guidance for the Village Center area in helping to alleviate this deficiency. The pedestrian improvements are categorized and described in the following text according to the objectives which they seek: (1) increased pedestrian safety, (2) improved circulation, and (3) improved aesthetics and amenities.

Pedestrian Safety

Physical measures to improve safety can include standardization of signs and signals, distinctive crosswalk delineation (particularly aimed at driver recognition of crosswalk zones), removal of obstacles to motorists lines of sight, upgrading of street lighting for pedestrian sidewalks, and any other physical improvements which contribute to pedestrian safety.

Crosswalks - Major emphasis is to be provided at the intersection of Pine Knot Avenue, Big Bear Boulevard, and Pennsylvania Avenue by providing textured designed paved pedestrian crosswalks. (See design details.)

- Major emphasis is to be provided at the intersection of Paine Road and Big Bear Boulevard by providing textured designed paved pedestrian crosswalks and lighted bollards.
- Major emphasis is to be provided at Lakeview

Drive and Paine Road by providing textured designed paved pedestrian crosswalks and lighted bollards.

- Major emphasis is to be provided at the intersection of Pine Knot Avenue, Lakeview Drive, and Big Bear Boulevard by providing textured designed paved pedestrian crosswalks and lighted bollards.
- Minor emphasis is to be provided at the intersection of Big Bear Boulevard and Cottage Lane by providing textured designed paved pedestrian crosswalks.
- Minor emphasis is to be provided on Pine Knot Avenue in the location of Pedder Road by providing textured designed paved pedestrian crosswalks.
- Minor emphasis is to be provided at both ends of Bartlett Road by providing textured designed paved pedestrian crosswalks.
- Minor emphasis is to be provided at Bonanza and Lakeview Drive by providing textured designed paved pedestrian crosswalks.

Sidewalks

Another way of reducing conflicts between pedestrians and vehicles is to create continuous sidewalk systems throughout the Village so that people do not wander into the street. Effective walkway widths should be an absolute minimum of 5 feet with widths between 10 feet and 15 feet preferable.

- Major emphasis on sidewalks of at least 10 feet wide should be located along both sides of Pine Knot Avenue and Big Bear Boulevard. Sidewalks should be of a continuing, common surface, not interrupted by steps or abrupt changes in level.
- Whenever walks and roadways cross, the curb should be cut and the sidewalk ramped to road level.
- Sidewalks should have non-slip surfaces.

Pedestrian Circulation Improvements

Pedestrian circulation can be improved in the Village area

by first widening sidewalks as mentioned previously. In order to accomplish this, building setbacks along Pine Knot Avenue and Big Bear Boulevard should be increased and the space provided limited to pedestrian walkways.

Pedestrian Trail Network

A comprehensive trail network throughout the Village area is necessary to maximize the pedestrian orientation of the Village. The network should begin in the "established" Village areas. The lakefront area should also contain a comprehensive walkway system. See Graphic depiction of recommended route. Pedestrian trails shall be on publicly owned land existing or acquired. Eminent domain shall not be used. Acquisition shall be based on a mutual agreement with property owner.

Pedestrian Park Node

The degree to which pedestrian routes in the Village can be enjoyed is determined in large part by its convenience, its length, and the scale and character of the buildings and special spaces which lay along the way.

In the Village area, the spacing of recreation nodes or a rest area can be very important. Good locations for these havens do exist at the present time. One area that is deficient of such a place is the long expanse or distance along Pine Knot Avenue from Big Bear Boulevard down to the lake. There is good reason, and a great need to create a local gathering spot somewhere in the middle of Pine Knot Avenue on practically any vacant parcel of land, provided it can be designed effectively and economically. The City could offset the cost of the acquisition of the land and subsequent development costs by leasing portions of the site for commercial development which could serve park visitors.

The following site plan depicts a conceptual design for such a pedestrian park node.

Improvements in Aesthetics

Pleasant pedestrian routes can attract even those people who must go out of their way to use them. Careful planning should therefore go into the design of the sidewalk and streetscapes along Big Bear Boulevard and Pine Knot Avenue. A number of measures discussed here can be effective in beautifying pedestrian routes.

Surface Material - Due to the cold winters in Big Bear Lake, the materials used for sidewalks should be easy to shovel snow

off of; should be resistant to spalling (chipping or flaking) due to freezing and thawing and should be a non-slip surface. A heavy "broom finish" concrete with a gray cobble accent band is recommended.

Lighting Equipment -

The choice of lighting equipment should be made on the basis of its contribution to the overall image of the Village Center. As one of the most visible elements in the village-scape, the lighting system must blend and harmonize with the character of the architectural elements. The components which go into the makeup of lighting equipment -- such as luminaries and posts -- should work together. It is recommended that a wooden post with a single or double luminaire with smoked plexiglass be utilized in the area. A natural stone base should also be employed. Mounting height should not exceed 25 feet above the sidewalk.

Street Trees

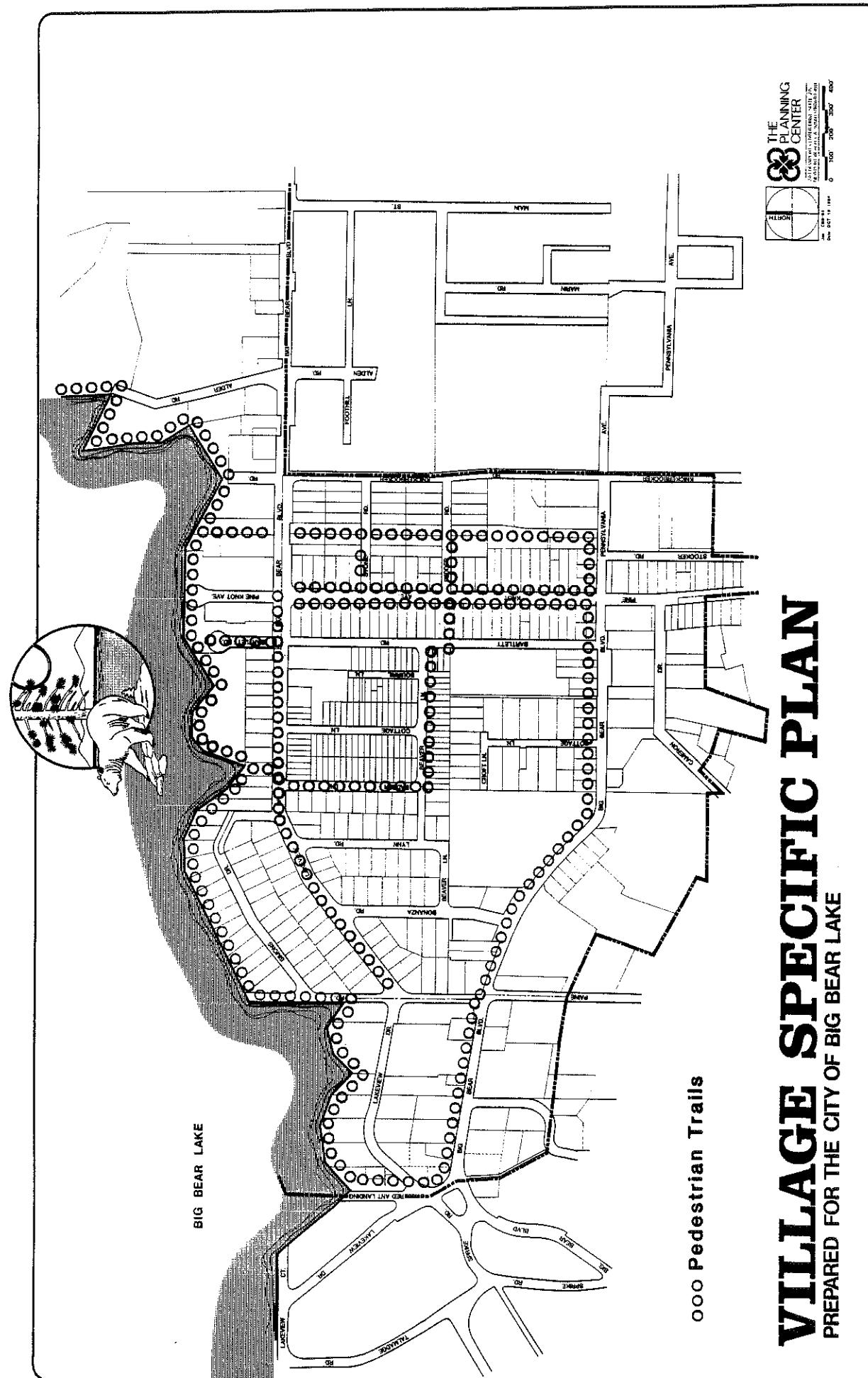
- Different trees have been selected for street planting use. These include but are not limited to red maple, silver maple and sunburst locust. Trees should be planted in random arrangements along sidewalks rather than in regimental rows in order to maintain "Village" character. Tree selection to be by the Village Design Review Board.

Other Streetscape Items

- Entry signs, benches and special crosswalks paving have certain design and functional criteria to be established before their final selection. The design criteria for these items can be found in the Village Design Guidelines booklet.

G. SHUTTLE BUS PROGRAM

The need for mass transportation is often more difficult to appraise in a Village setting than it is in a large urban area. It is, therefore, the task of those promoting transit in the Village to determine if the need for new transit



VILLAGE SPECIFIC PLAN

PREPARED FOR THE CITY OF BIG BEAR LAKE

000 Pedestrian Trails

EXHIBIT 6

service actually exists and can be documented.

There are a number of ways of providing public transportation service to an area. Each of these alternatives needs a different level of patronage and requires a different population density to be financially feasible.

The best way in which the City of Big Bear Lake can determine whether a specific transit system would be feasible for the Village is to first undertake a pilot project based on ridership estimates in and around the Village as well as ridership estimates from lodging and parking to outlying ski areas. This evaluation process should include demand for services, costs required to run it, and the revenues that would be generated. This evaluation process should also measure the extent to which each alternative provides transportation benefits to the resident population and tourists, any subsidies that would be required to run it successfully and its potential for reducing traffic congestion and enhancing safety.

Once the "best" alternative is selected for the Village's particular objectives, a detailed transportation program should be prepared.

It is recommended, based on similar transit systems in other Villages that:

1. A separate transit system, largely seasonal in nature be set up to shuttle skiers from the Village to outlying ski areas;
2. A second full time transit system be established within and around the Village;
3. All buses or shuttles be run by an independent contractor, rather than the City becoming involved as the prime contracting agency;
4. That a route system should be established which stops at all major Village area inns on a regular basis. Some flexibility should be built into the fixed route however.

The following graphic depicts a potential in-Village shuttle route.

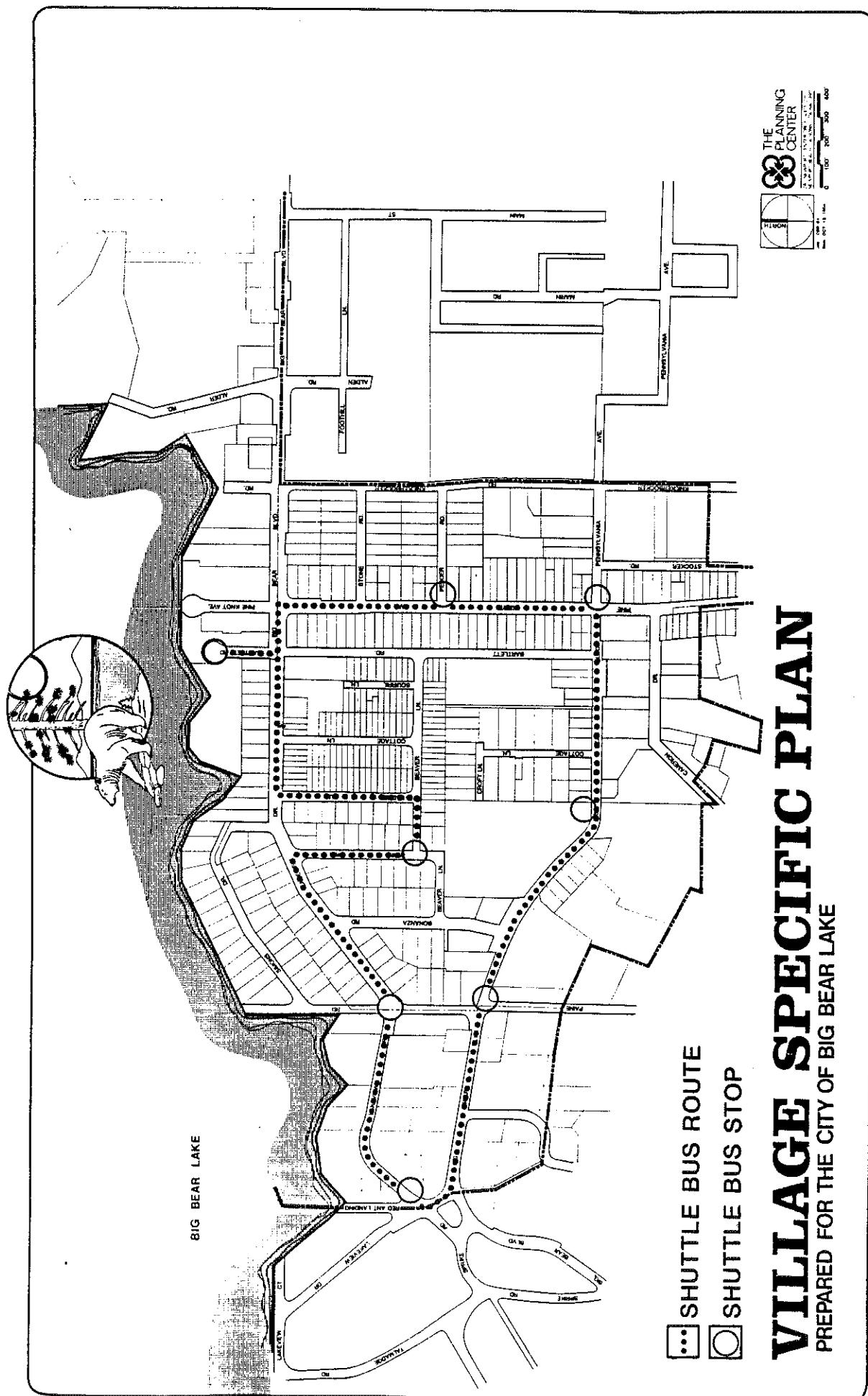


EXHIBIT 7

IV. SPECIAL DESIGN CONCEPT

A. Introduction

There are certain elements in every successful downtown or village area which enliven and make that place something special to be in. These elements might be referred to as the "community structure." These are special design considerations within the Village area which will tie the Village together as a cohesive unit. Of course, there are no simple answers to what will make the Village work but there are at least guiding principals that were considered each time a special design element was considered for the Village. The City's commitment to upgrading the environmental quality of the Village is reflected in these goals.

- o Beautify the street, improve environmental quality.
- o Develop open spaces within the Village.
- o Enhance pedestrian amenity throughout the Villages.
- o Develop identifiable "entry" points to the Village.
- o Directional signage off of Lakeview Drive for parking and commercial uses.

The principal objective of undertaking major improvements to the Village Project Area is to establish an image of regional magnitude that promotes the area as a high quality, distinctive resort destination. Because of competing resorts in nearby Lake Arrowhead and elsewhere, the conceptual design component must possess a scale and character equal to the task at hand.

In seeking the guiding factors to facilitate design, two major design concepts became apparent:

- IDENTIFICATION
- ACTIVITY

Having reached this design concurrence, the next effort was to proceed with the special design elements which reinforce - Identification and Activity. The overall design effort has concluded with three major products under each category.

IDENTIFICATION

Entry Signs
Public Signage
Intersection Node

ACTIVITY

Lakefront Concept Plan
Recreation Node
Ski Area Lift

B. VILLAGE ENTRY SIGNS

Two different scale entry signs are proposed for specific locations in the Village. A large, road-spanning overhead sign and a smaller monument sign are suggested.

The architectural style of "Big Bear" will be reflected in the choice of colors and materials for the signs. Colors will be enhanced by the light of the sun and the snows of winter.

The primary palette of materials for the "hard" items will be natural stone and unfinished, bulky large dimension wood structural elements. Indirect lighting should be considered for night illumination.

The entry sign for the Village area plays two very important roles, 1) the architectural materials of the sign establish the architectural image for the Village and 2) it identifies and announces the entries to the Village, as a place to stop, shop, eat, etc. See graphic.

C. PUBLIC SIGNAGE

There are few elements present in the streetscape which can contribute more to visual clutter than public signage. In order to contribute to the 'aesthetic' function of the Village, these elements will be proposed in a system approach of international graphic symbols. In order to blend signage in with the architectural flavor of the area, the signs will incorporate special color accents (see graphic), and provide direction for motorists seeking parking and varying uses.

D. INTERSECTION NODE/STREETScape

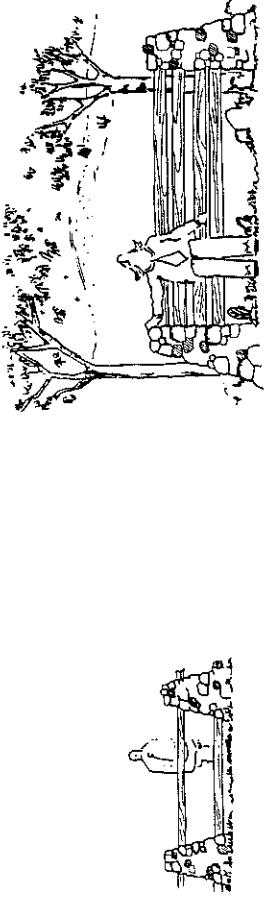
The concept behind providing a series of specially designed intersection nodes is to create and reinforce a pedestrian trail system linked together by a series of conveniently spaced oases. These nodes shall incorporate the elements of the entire streetscape as well as special seating areas, snow storage, crosswalks, native rock and lighted bollards. In order to maximize the size of these nodes the adjacent on street parking area will be abandoned and utilized for additional pedestrian area and plantings as well as utilizing building setback areas into the overall node design. See graphic.

E. LAKEFRONT CONCEPT PLAN

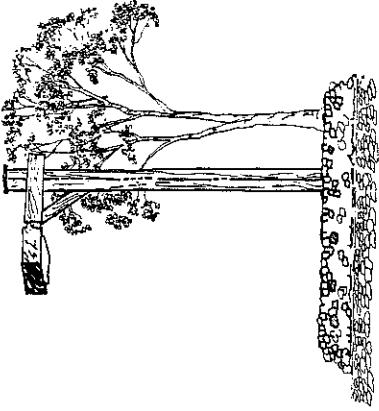
Currently, Big Bear Lake, one of Big Bear's biggest assets, is relatively unexploited in terms of its potential to

VILLAGE IDENTIFICATION MATRIX

FENCE

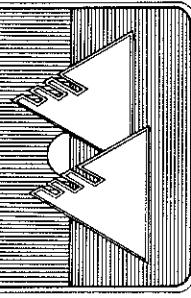


BENCH

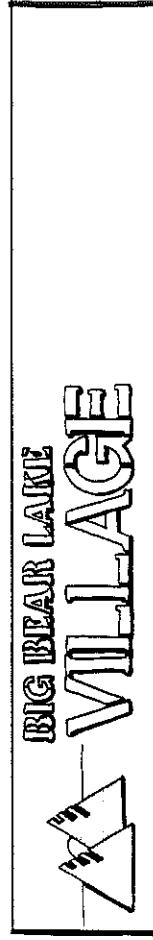


LIGHTING

VILLAGE CENTER IDENTIFICATION PROTOTYPE

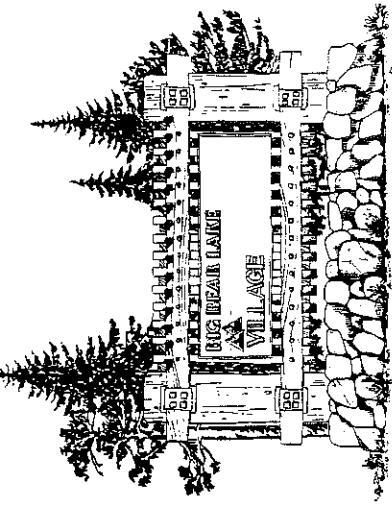


LOGO

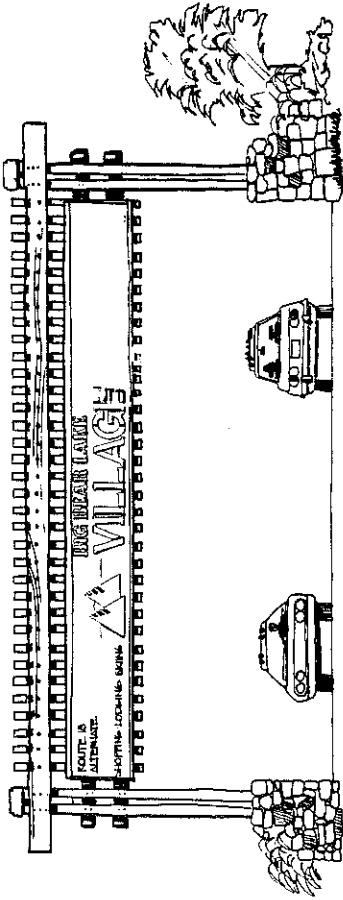


VILLAGE CENTER IDENTIFICATION PROTOTYPE

SECONDARY VILLAGE ENTRY SIGN

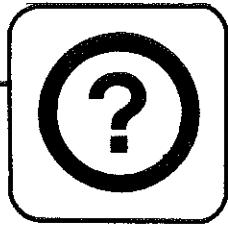
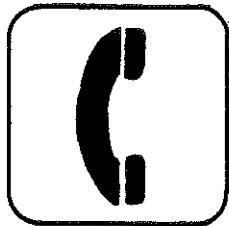
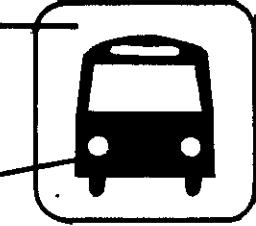
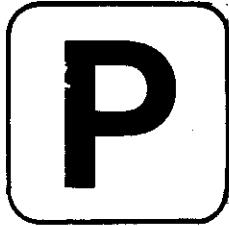
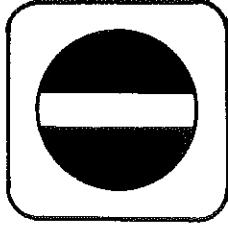
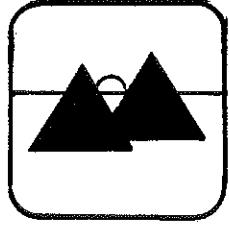


PRIMARY VILLAGE ENTRY SIGN

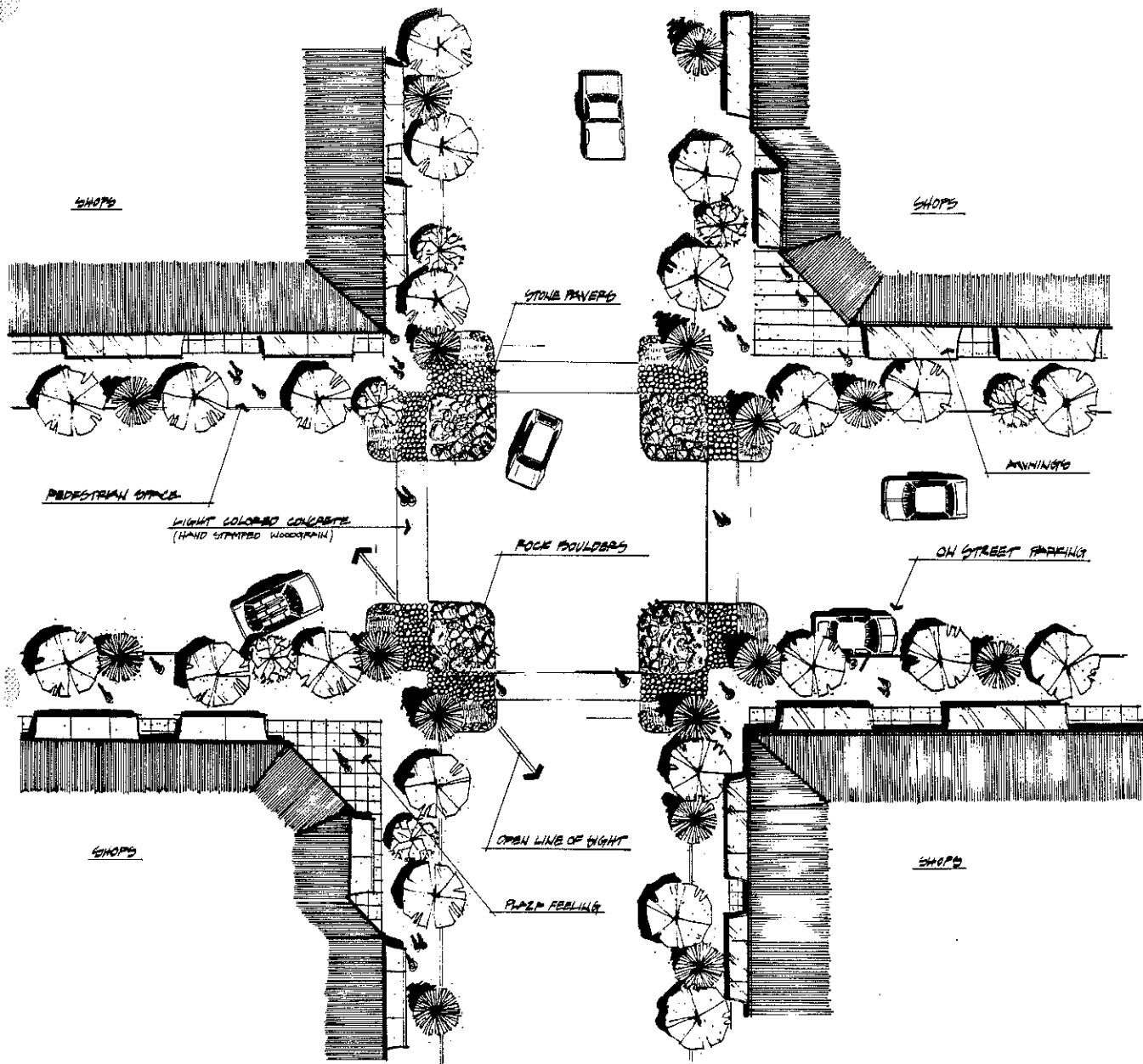


NOTE: THE CITY COUNCIL OR CHAMBER OF COMMERCE SHOULD SPONSOR A VILLAGE LOGO DESIGN COMPETITION.

VILLAGE PUBLIC SIGNAGE

EDGE: EVERGREEN			
BACKGROUND: WHITE			
SYMBOL: RED	SHUTTLE BUS STOP	PARKING	NO PARKING
SIGN POLE: 4X4 LUMBER W/REDWOOD STAIN			
	NO ENTRY	VILLAGE LOGO	VILLAGE ROUTE

INTERSECTION NODE



City of Big Bear Lake

 THE PLANNING CENTER
EXHIBIT 10

attract economically and aesthetically desirable projects. In order to protect or maintain public access to the lakefront it is proposed that an aggressive public/private partnership between the City and Lakefront property owners begin as soon as possible. This partnership is considered necessary before a comprehensive lakefront development can occur. Both the public and private sectors have much to gain should this partnership be formulated. First and most important of all, the public access to the Lake is maintained while the economic desirability of retail commercial uses along this public space are greatly enhanced by the sheer numbers of people visiting there.

It is believed that active and passive recreational opportunities should be available to the pedestrians that visit this area. It is envisioned that sitting, viewing, shopping, strolling and fishing make up the bulk of activities in this area. Access to the lakefront will be gained through a variety of locations with the major access at Bartlett Road.

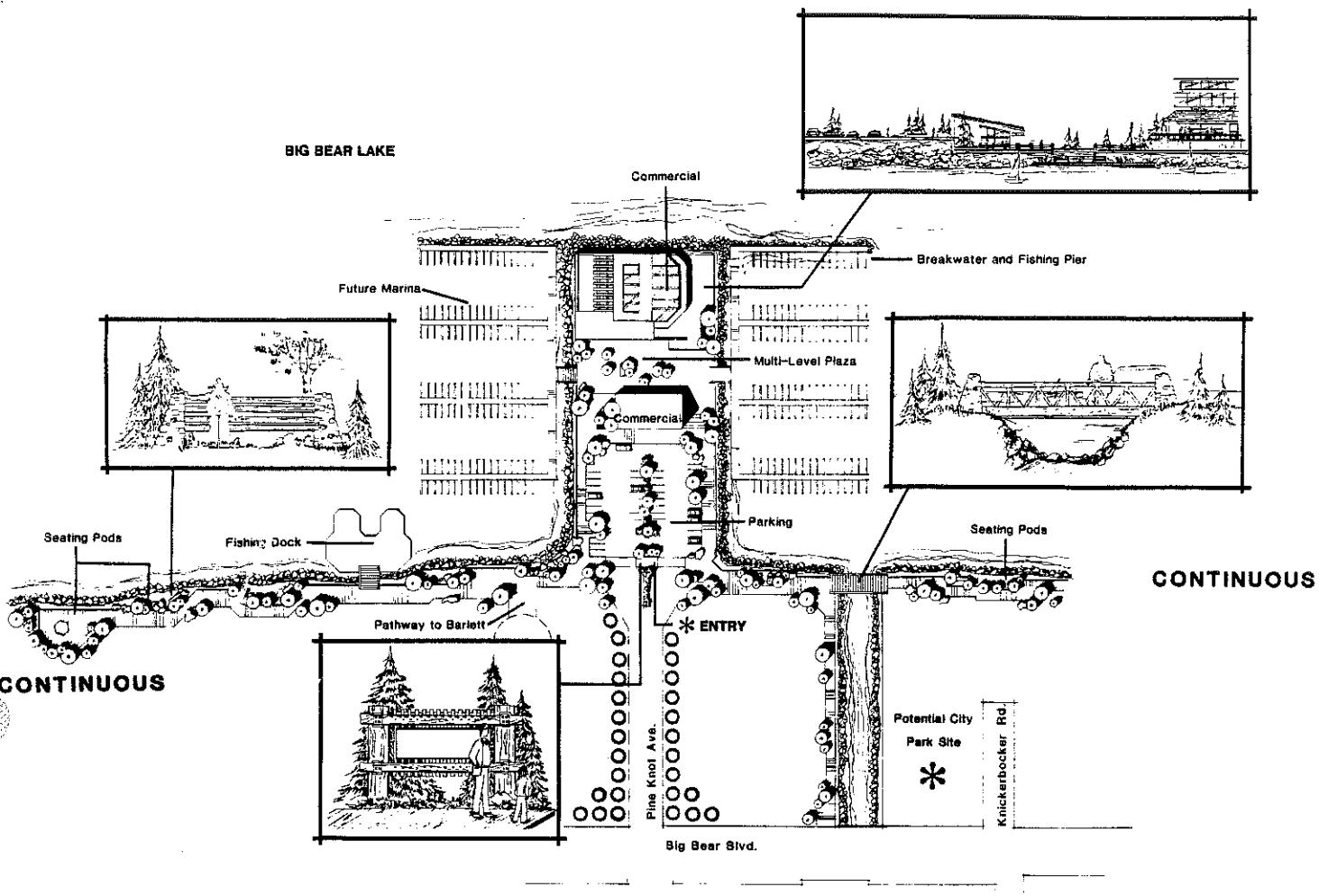
Commercial expansion of the Marina is seen as an important ingredient to success of the Lakefront Plan. Small specialty shops and a restaurant on the tip of the marina island should be considered. Refer to the Lakefront Concept Plan graphic for a development concept including the above mentioned facilities.

F. CREATION NODE

One of the major elements recommended for inclusion into the Village area is an open space or 'Village Green' within the busiest pedestrian area of the "Old Town" portion of the Village. This location should be carefully selected between pedestrian destination points within the Village. Placement of such an area on a fringe location or at the end of a pedestrian route might render the site unused and forgotten.

Presently, the best locations for such a site would be on either side of Pine Knot Avenue approximately halfway between Big Bear Boulevard and the Lake.

Much like the Lakefront Plan, active and passive recreational uses should be included in such a plan. It is recommended that a festive atmosphere be created which includes many trees, flags, benches, a small pond with large boulders around it, a small "village green" area, restrooms and a tower or some other vertical structure to attract local attention. It is also recommended that a certain amount of the site remain open to encourage driving through or walking through to Bartlett Road to inns, parking and additional commercial uses. Adjoining properties could orient their



ooo Pedestrian Sidewalks

LAKEFRONT CONCEPT PLAN

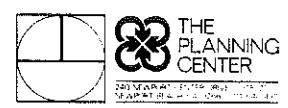
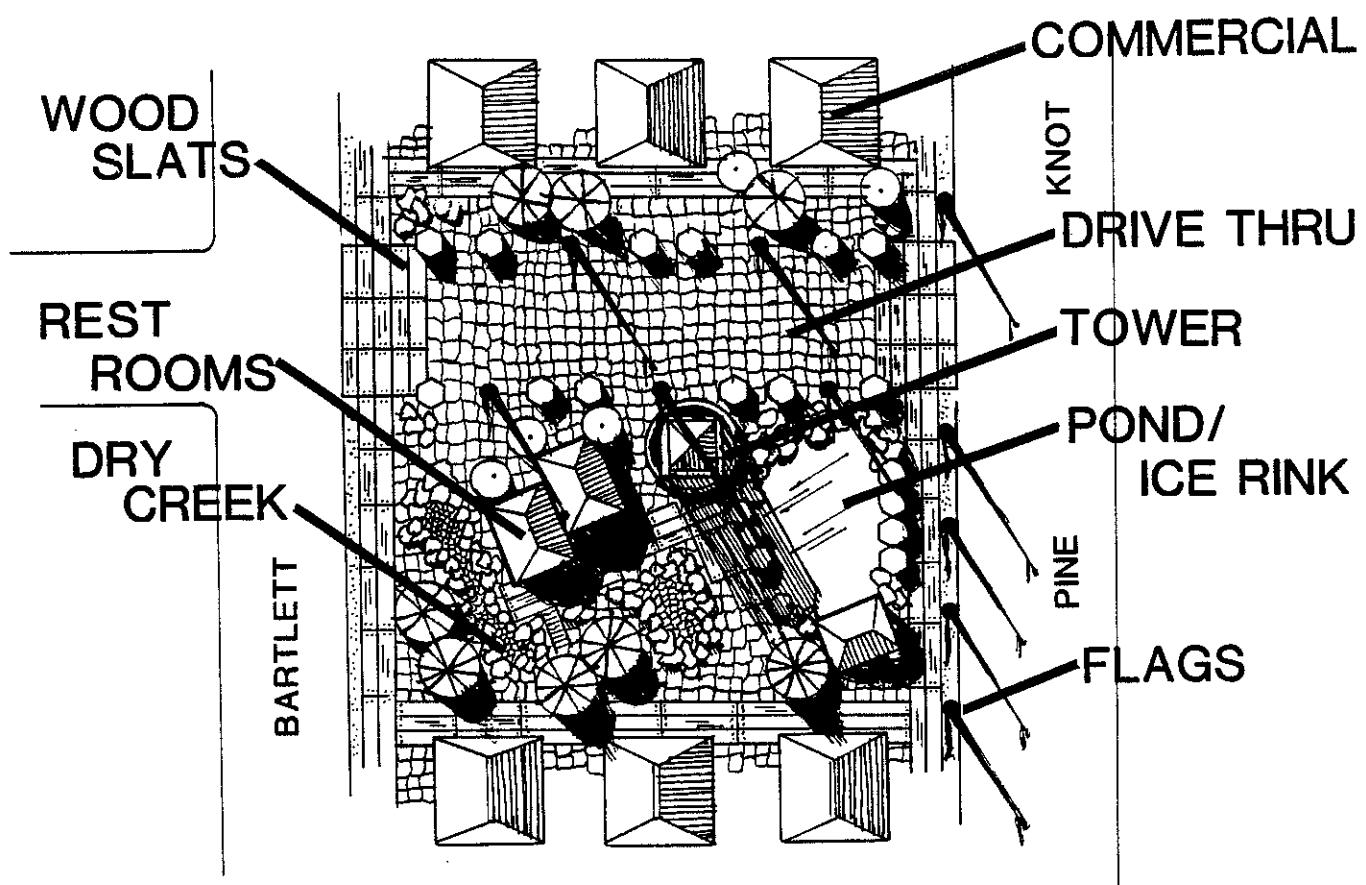


EXHIBIT 11

RECREATION NODE



City of Big Bear Lake

entrances to this public space to gain additional "visibility." See Recreation Node graphic.

G. SKI AREA LIFT

In order to set the Village Center area of Big Bear Lake apart from other local mountain communities, a really unique visual element proclaiming the Village's ski area resort status is necessary. A workable idea that has been considered is the connection of various large hotels in the Village area with Snow Forest Ski Area by means of a chair lift or gondola system. Initial research has shown that both the hotels and ski area are interested in such a venture and would participate in such a program. Review of aerial photographs suggest that an acceptable route could be gained from the Village to the Snow Forest Area. Naturally, the scope of this study will not permit detailed route selection criteria or chair lift operation constraints, however, the City and private development concerns should continue to work toward such a worthwhile goal.

V. DEVELOPMENT STANDARDS

A. Forward

The Village Development Regulations implement the Village Specific Plan. The regulations are adopted by ordinance pursuant to Article 8, Authority for Scope of Specific Plans of the Planning and Government Code in compliance with Sections 65450 and 65503 of the Government Code.

B. General Purpose

The development regulations found in this specific plan intend to provide the standards, criteria and procedures necessary to achieve the following objectives:

1. Implement the provisions and policies of the General Plan.
2. Provide maximum opportunities for innovative community design and site planning, consistent with orderly development and protection of sensitive and natural resources, with a logical and timely sequence of community and government review and input.
3. Improve the visual image, economic well-being and overall integrity of the Village area.

C. Application

1. These development regulations shall be applied only in the Village Specific Plan area.
2. If any portions of these regulations are, for any reason, declared by a court of competent jurisdiction to be invalid or ineffective in whole or in part, such decisions shall not affect the validity of the remaining portions thereof. The City Council hereby declares that it would have enacted these regulations and each portion thereof, irrespective of the fact that any one or more portions be declared invalid or ineffective.
3. Terms used in these regulations shall have the same definitions as given in the City of Big Bear Lake Zoning Code.

D. General Provisions

1. All construction and development within the Village Specific Plan shall comply with applicable provisions of the Uniform Building Code and the various related

mechanical, electrical, plumbing codes, grading and excavation code and the Subdivision Codes, as currently adopted by the City Council. In case of a conflict between the specific provisions of any such code and this Specific Plan, the Community Development Director shall resolve the conflict by written determination in a manner consistent with the goals and policies of this Specific Plan.

2. The setback requirements are as specified by each district of this Specific Plan. All setbacks shall be determined as the perpendicular distance from the existing street right-of-way line, or property line, to the foundation point of the closest structure.
3. If an issue, conditions or situation arises or occurs that is not sufficiently covered or provided for so as to be clearly understandable, those regulations of the City Development Code that are applicable for the most similar issue, condition or situation shall be used by the Community Development Director as guidelines to resolve the unclear issue, condition or situation. This provision shall not be used to permit uses or procedures not specifically authorized by this Specific Plan or Zoning Code.
4. This Specific Plan may be amended by the same procedure as it was originally adopted. Each amendment shall include all sections or portions of the Specific Plan that are affected by the change. An amendment may be initiated by the City Council or the Planning Commission. Any such amendment requested by a property owner shall be subject to the fee schedule adopted by the City Council.
5. Any person, firm or corporation, whether a principal, agent, employee or otherwise, violating any provisions of these regulations shall be guilty of a misdemeanor, and upon conviction thereof shall be punishable by a fine of not more than five hundred dollars (\$500.00) or by imprisonment in the County Jail of San Bernardino County for a term not to exceed six (6) months or by both fine and imprisonment. Such person, firm or corporation shall be deemed guilty of a separate offense for each and every day during any portion of which any violation of this Specific Plan is committed, continued, or permitted by such person, firm or corporation and shall be punishable as herein provided.
6. Nothing herein contained shall be deemed to prevent the connection of buildings by covered malls or covered, enclosed or unenclosed walkways.

7. Whenever a use has not specifically been listed as being a permitted use in a particular Use District classification within the Specific Plan it shall be the duty of the Community Development Director to determine if said use is (1) consistent with the intent of the use district and (2) the said use is compatible with other listed permitted uses. Any person aggrieved by the determination may appeal that decision to the Planning Commission.

8. **Parking**

a. Purpose and Intent

The purpose of this section is to assure the provision and maintenance of safe, adequate, well-designed off-street parking facilities in conjunction with any use or development. The intent is to reduce street congestion and traffic hazards and to promote an attractive environment through design and landscaping standards for parking areas. The standards set forth in this section shall be considered minimums, and more extensive parking provisions may be required by the Design Review Board as a condition of project approval requiring a Design Review Permit. It is also intended that excessive spaces shall not be devoted to parking which reduces the efficiency of the site.

b. Applicability

Unless otherwise provided in the specific, applicable use district, the provisions of this section shall apply to all appropriate uses and development within the Village Specific Plan area.

c. Maintenance of Parking Spaces

No parking area or parking space provided for the purpose of complying with the provisions of this section shall thereafter be eliminated, reduced or converted in any manner unless equivalent facilities approved by the City are provided elsewhere in conformity with this section. The permit for the use for which the parking was provided shall immediately become void upon the failure to comply with the requirements of this section.

d. Required Number of Spaces: General

- 1) The minimum number of parking spaces specified in this section shall be provided and continuously maintained in conjunction with the related use or development.

- 2) For additions to existing developments, the increased parking requirement shall be based on the aggregate total of the floor area of all existing and proposed buildings or structures on the property.
- 3) For the purposes of this section, gross floor area shall be the measure of total square footage for a project; however, open unenclosed corridors, stairways and outdoor eating facilities shall be excluded.
- 4) Where the standards require any fractional space, the next larger whole number shall be the number of spaces required.
- 5) In the case of mixed use facilities the total requirement shall be the sum of the number of spaces required for the individual uses computed separately except where the uses have different hours of operation in which case the parking may be reduced up to 20%. If uses are unknown or may change, the number of spaces will be the average of those uses permitted in the development.
- 6) All parking areas shall provide handicapped parking spaces and access as required under State law. The size of such a space shall be 12x19, and shall be provided per the following standards:

1 - 40	parking spaces	-	1	handicapped parking space	
41 - 80	"	-	2	"	" spaces
81 - 120	"	-	3	"	"
121 - 160	"	-	4	"	"
161 - 300	"	-	5	"	"
301 - 400	"	-	6	"	"
401 - 500	"	-	7	"	"

Over 500 - one handicapped parking space for each additional 200 parking spaces.

These spaces should be placed as close as possible to a major entrance of a building or function, preferably no more than 100' away. These spaces should be properly signed and identified.

- 7) At no time shall overall parking requirements be reduced by more than 20% unless it is specifically stated (shared parking) or it can be shown that some other extenuating circumstance may allow a reduction of more than 20%.

e. Revision of Parking Spaces Required Upon the Change of Use

Upon the change of any use, the number of parking spaces to be provided shall be determined according to the requirements of this section for the new use. Any previous parking determinations by the City shall be null and void.

f. Required Number of Spaces: Residential

Parking spaces are to be permanently maintained on the same building site on which the dwelling(s) is located:

For all residential units: 2.0 spaces per dwelling unit

For guest parking: .25 spaces per dwelling unit

g. Required Number of Spaces: Non-Residential

<u>Use</u>	<u>1 space required per</u>
Retail Commercial	300 sf of gross floor area
Restaurants/ Banquet Rooms	150 sf of gross floor area (no additional requirements necessary for outdoor eating facility unless outdoor eating area exceed 20% of restaurant's total seating)
Theatres	7 seats
Financial institution	300 sf of gross floor area
Health club	400 sf of gross floor area
Home improvement, building materials, furniture and appliance, paint, hardware	600 sf of gross floor area
Hotels and Motels	room
Service Commercial	400 sf of gross floor area
Neighborhood Shopping Center	150 sf of gross floor area
General business office	350 sf of gross floor area

ORDINANCE NO. 87-142

ATTACHMENT A

Amending Page V-5 of the Village Specific Plan Development Standards relative to parking standards for hotels, banquet/meeting rooms, and restaurants as follows:

A. Hotels

1 space - per room (bedroom).

1 space - per employee (based on maximum shift).

B. Meeting/Banquet Rooms within hotels

0 spaces - up to 30% of the meeting room's total square footage.

1 space - per each 30 square feet of remaining meeting room floor space (above the 30% figure).

C. Meeting/Banquet Rooms not located within a hotel

1 space - per each 30 square feet of meeting room floor area.

D. Restaurant within hotels

0 spaces - up to 30% of the dining room's total square footage.

1 space - per 45 square feet of dining/bar occupancy area.

1 space - per employee (based on maximum shift)

E. Restaurants

1 space - per 45 square feet of dining/bar occupancy area. No additional parking needed for outdoor eating facility for up to 20% of the total indoor seating.

1 space - per employee (maximum shift).

Overall parking requirements can be reduced by no more than 20% for shared/mixed uses.

Medical office

200 sf of gross floor area

If parking spaces in any specific category of use are not contained herein, the Community Development Director shall determine the parking requirements for said use.

h. Exceptions or Modifications to Off-street Parking Requirements

Minor exceptions or modifications of these standards may be applied for where the nature of the use involved or other relevant circumstances make the requirements of this provision insufficient or excessive and to encourage the most productive use of the land provided the Planning Commission (upon recommendation of the Design Review Board) determine said exceptions will result in a development plan that is superior to that which could be obtained by the rigid applications of these parking requirements.

- 1) Any property owner, his authorized agent, or the City of Big Bear Lake may apply for exceptions to, or modifications of, these parking standards.
- 2) Parking Management Plan. Facilities may decrease their required number of parking spaces from 1 percent to 20 percent subject to the approval of the Planning Commission (upon recommendation of the Design Review Board) based upon a detailed parking management plan supplied by the applicant which may include but is not limited to provisions for mass transit, carpooling, staggered work hours, or shared parking between uses with different peak business hours.
- 3) In all districts of the Specific Plan area, individual large trees (over 12 inches) in diameter shall be retained in parking stalls. The area around the tree (dimension of a parking stall) shall be left unpaved and protected from vehicular traffic. A credit for a parking space will be granted toward meeting the parking requirement. The reduction in parking spaces for the provision of retaining large trees shall not exceed 20% unless the Planning Commission (upon recommendation of the Design Review Board) determines that the parking can be adequately handled and that the trees are worth saving.

i. Minimum Dimensions, Location and Design

Minimum dimensions for parking layouts

a Angle	b Stall	c Aisle	d Overall	e Layout
30°	16'	12'	44'	16'-0"
45°	19'	14'	52'	11'-3 3/4"
60°	20'	20'	60'	9'-4 1/4"
90°	18'	24'	60'	8'-6"

Parallel Parking:

Minimum aisles are 10'

8.5'x24' minimum size parking space

90° Parking:

End stalls parallel to walls or fences shall be a minimum of 10' width

Tour Bus Parking:

All parking stalls provided for tour bus operations shall be 15'x50'. These parking spaces shall be provided for larger hotel uses within the Village as determined by the Community Development Director.

2) Location:

--Off-street parking spaces shall not be located in the required front or side yard setback area unless specifically permitted in the applicable use district regulations. Provisions shall be made for direct access from the street to each parking space. Such access shall be adequate for standard size automobiles.

--For all types of businesses, the required parking spaces shall be provided on the same site on which the business is located or within 800 feet of the property in either a common off-site lot or leased space.

3) Construction and Design:

--The structural section of uncovered parking areas and driveways shall be paved to the satisfaction of the City Engineer.

--Parking spaces shall be marked and access lanes clearly defined. Bumpers and wheel

stops shall be installed as necessary.

- Parking areas shall be so designed that no vehicle shall be required to encroach into a street or sidewalk when backing out of a parking space with exception for implementing a public parking program.
- Areas for snow storage shall be identified on each parking plan. A minimum of 5% of the parking lot is recommended.
- The design of parking areas shall not require the moving of any car to gain access to a required parking space (tandem parking). Employee tandem parking may be considered by the Village Design Review Board.
- All parking areas serving uses operating at night shall be adequately lighted. Such lighting shall be so arranged as to direct the light away from adjoining residences. Light standards shall not exceed 20' in height or building height whichever is less.

j. Landscape/Screening of Parking Areas

Parking area includes the parking spaces and the maneuvering space necessary for use of such spaces.

- 1) Where trees already exist on the property, the design of the parking area should make the best use of this growth and shade. Whenever a tree over 8" caliper is removed it must be replaced with a specimen tree of mature size (greater than 4" caliper).
- 2) Screening shall be provided along the boundary of the parking area when such parking is adjacent to a public street or lesser land use. A fence and/or sufficient shrubbery shall be provided within a five foot (5') strip. A two foot (2') vehicle overhang into landscaped strip is permitted. Fences or walls abutting streets and lesser land uses shall be ornamental in texture, pattern or shadow relief. Plantings, fences or walls abutting streets shall not exceed four feet (4') in height. Screening (landscaping or fencing) or physically separating adjacent parking shall be prohibited.
- 3) When the total uncovered parking area on the

property (including adjoining parcels over which the property has parking privileges) exceeds 12 parking spaces, the following shall be required, in addition to other provisions of this section, as part of a landscape plan:

- Trees, shrubbery, and ground cover shall be as provided at suitable intervals in order to break up the monotony of the parking area. Planting islands for such trees and shrubs shall be protected from automobile traffic by a 6" concrete curb or other materials such as redwood, treated wood, stone, etc. Trees shall be provided in parking lots on a 1 tree per 6 parking spaces basis.
- Business operator or tenant shall snow plow and keep clear their parking area within a reasonable time after a storm.
- All ends of parking lanes having more than six parking stalls shall have landscaped islands containing a minimum of two trees.

k. Off-Street Loading Facilities

- 1) For every building hereafter erected, which is to be occupied by retail store, wholesale store, market, hotel, hospital, mortuary, laundry, dry cleaning, or other uses similarly requiring the receipt or distribution of materials and merchandise by vehicles, off-street loading spaces shall, if necessary, be provided as determined by the Community Development Director and the Design Review Board.
- 2) Each loading space shall not be less than ten (10) feet in width, thirty (30) feet in length, and with an overhead clearance of at least fourteen (14) feet.
- 3) Such space may not be located in any part of any required front yard.
- 4) Such space shall be so designed that it will not interfere with vehicular circulation or general parking nor with pedestrian circulation.
- 5) Such space shall be sited to avoid views from public streets.

1. Driveways

Special Requirements

Upon recommendation of the City staff or upon their own initiative when considering any project, in seeking to improve site efficiency, the Design Review Board may place special requirements on an individual building site that will have the effect of reducing or increasing the number or width of driveways or prescribing their location on the building site when the Board determines that such special requirements either reduce or do not create traffic hazards or street parking problems. Minimum driveway dimensions shall be 10 feet wide for a one way driveway and 20 feet for a two way drive. See 8i for required aisle/driveway widths.

9. Automotive vehicles or trailers of any kind or type without current license plates shall not be parked or stored on any property within the Specific Plan area unless it is in a completely enclosed building.
10. Exceptions to height limitations contained in the District Regulations apply to spires, clerestory windows, steep roofs, cupolas, antennas, chimneys or other architectural appurtenances required to meet the intent of the architectural design guidelines and not intended for human occupancy.

11. Non-conforming Uses of Land

Where at the time of passage of this Specific Plan lawful use of land exists which would not be permitted by the regulations imposed by this Specific Plan, and where such use may be continued so long as it remains otherwise lawful, provided:

- a) No such non-conforming use shall be enlarged or increased, nor extended to occupy a greater area of land than was occupied at the effective date of adoption or amendment of this Specific Plan;
- b) No such non-conforming use shall be moved in whole or in part to any portion of the lot or parcel other than that occupied by such use at the effective date of adoption or amendment of this Specific Plan;
- c) If any such non-conforming use of land ceases for any reason for a period of more than 180 days, any subsequent use of such land shall conform to the regulations specified by this Specific Plan for the district in which such land is located;

- d) No additional separate structure not conforming to the requirements of this Specific Plan shall be erected in connection with such non-conforming use of land.

12. Non-conforming Structures

Where a lawful structure exists at the effective date of adoption or amendment of this Specific Plan that could not be built under the terms of these regulations by reason of restrictions on area, lot coverage, height, yards, its location on the lot, or other requirements concerning the structure, such structure may be continued so long as it remains otherwise lawful, subject to the following provisions:

- a) No such non-conforming structure may be enlarged or altered in a way which increases its non-conformity, but any structure or portion thereof may be altered to decrease its non-conformity;
- b) Should such non-conforming structure or non-conforming portion of structure be destroyed by any means to an extent of more than 50 percent of its replacement cost at time of destruction, it shall not be reconstructed except in conformity with the provisions of this Specific Plan;
- c) Should such structure be moved for any reason for any distance whatever, it shall thereafter conform to the regulations for the district in which it is located after it is moved.

E. DISTRICT REGULATIONS

1. Permitted Uses

A. The "Uses Permitted by District" chart (1) identifies the permitted uses for each district within the Village Specific Plan. Symbols for the required processing for each use permitted are identified below.

- 1 - Permitted with "site plan/design review" approved by the Design Review Board.
- 2 - Requires additional review, i.e., "site plan/design review" and one of the following applications depending on the proposed use:

A. Conditional Use Permit

Site Approval - example: hotel or gas station;
or

Large-Scale Housing - example: condominium project with five or more units.

- B. Time share permit
- C. Subdivision (Tentative Tract or Minor Subdivision)

The adopted districts for the Village Specific Plan include:

CL - Commercial Lodging
LC - Lakefront Commercial
NC - Neighborhood Commercial
VR - Village Retail
PQP - Public/Quasi Public
R - Residential, Multi-Family

B. Public Notices/Procedures

Public notices/procedures for applications identified above under Item 2 are the same as required by the Development Code.

The "site plan/design review" application will follow the same procedures as a plot plan review with Design Review Board Approval.

C. Legal Non-conforming Uses

Any legal non-conforming use that is destroyed or wishes to be expanded may do so subject to the development standards of the applicable district and a site plan design review permit approval of the Village Design Review Permit.

D. Permitted Use Interpretation

The permitted uses identified in the "Uses Permitted by District" Chart (1D) summarize acceptable uses. Other uses similar to the identified uses are permitted as determined by the Community Development Director or Planning Commission.

E. Purpose and Intent of Districts (CL)

- i. Commercial Lodging District (CL)

This use district is intended to allow for hotels, motels, lodging and conference accommodations, recreational facilities, visitor service and other similar and complementary land uses which would enhance the scenic beauty and Village character of the area and preserve its natural amenities.

The uses permitted are intended to offer a wide range of recreational activities and support services relating to lodging.

ii. Lakefront Commercial District (LC)

The Lakefront Commercial District is between the Village core and the scenic lake. This coupled with large lots within the district having a potential for larger than average development, creates a concern to the public to ensure visual lake access.

It is the intent of these regulations to promote development that enhances lake visibility and enjoyment. This is to be accomplished through the use of a lakefront pedestrian promenade, continuous pedestrian circulation from public roads to lakefront, incorporate public viewpoints in development, and public activity uses within development such as restaurants, shops and services. Development shall be designed to establish an "open" character with higher portions of buildings well spaced and oriented maintaining/creating public lake views and ensuring the visual impact is not only minimized but the visual quality is enhanced.

iii. Neighborhood Commercial District (NC)

The Neighborhood Commercial District is to allow commercial uses which provide basic goods and services for the community complementing the other districts within the Village. Although this district is intended to be tailored to the needs of the citizens, it is also intended to serve the commercial uses of the Village Retail and Commercial Lodging District.

iv. Village Retail District (VR)

The Village Retail District has historically been the business core of Big Bear Valley, comprising the structure spine of the Village. It is the intent of these regulations to provide continuation and strengthening of existing development within

the district, encouraging pedestrian activity and linkage marina, lakefront and lodging areas. Development shall be compact and provide continuity as a prime tourist retail/service area.

In recognition of the changing function of the Village and its growing dependence on and orientation to the tourist and to improve the experience for local residents, it is also intended to increase its function as a central gathering place. This is accomplished through maximizing pedestrian convenience, safety and comfort while minimizing adverse effects of automotive traffic.

v. Public/Quasi-Public (PQP)

The Public/Quasi-Public District is intended to allow for public, quasi-public and institution land uses, ensuring that development within the District will be compatible with surrounding uses with respect to adequate vehicular access and circulation, off-street parking, architectural and site design, landscaping and other features.

vi. Residential (R)

It is the intent of these regulations to provide for the protection, continuance and enhancement of existing multi-family, single-family and small scale overnight facilities for tourists seeking a decidedly different lodging experience. Recognizing that the Village will become more tourist-oriented over time, this district provides for a transition from residential to low intensity lodging such as bed and breakfast establishments while protecting the residential character of the area.

f. Uses Permitted by District Chart

	CL	LC	NC	VR	PQP	R
<u>Residential</u>						
Single Family						1
Multi Family (maximum 12 DUs per acre)						
Condominiums	2AC	2AC	2AC	2*AC		2AC
Apartments			2A	2*A		2A
4/Less Units	1		1	1*		1
* Prohibited on Pine Knot Ave., Big Bear Blvd. on first floor						
Manager's Unit	1	1	1	1	1	1
Time Share	2B	2B	2B	2*B		2B
* Prohibited on Pine Knot Ave., Big Bear Blvd., and main arterial						
Boarding/Rooming Houses			2A			
Group Homes/Social Care Facilities			2A			2A
Retirement Complexes	2*A		2A	2*A		2A
* Prohibited on Pine Knot Ave., Big Bear Blvd., Lakeview, and Paine Road						
<u>Lodging</u>						
Hotel/Motel (example)	1	2A		2*A		
Condo Hotel	2AC	2AC	2AC	2*AC		
Bed and Breakfast	1		2A	2*A		2A
* Prohibited on Pine Knot Ave., Big Bear Blvd.						

	CL	LC	NC	VR	PQP	R
<u>Recreation</u>						
Outdoor Recreation	2A	2A		2A	2A	
Indoor Amusement/Recreational	1	1	1	1		
Marinas		2A				
Theater	1	1	1	1		
Health Club	1	1	1	1		
<u>Administrative/Professional</u>						
Financial Institutions	1	1	1	1	1	
General Office/Professional Buildings	1*	1*	1	1*	1	
Medical Offices	1*	1*	1	1*	1	
* Located in conjunction with a hotel or retail project when on Pine Knot Ave., and Big Bear Blvd. Permitted without limitations in other circumstances.						
<u>Personal Services</u>						
Including but not limited to: Barber, Beauty Shops, Shoe Repair, Dry Cleaners, Travel Agencies, Real Estate, Photo Studios, Printing Services, and the like, excluding vehicle repair.	1*	1*	1	1*		
* Located in conjunction with a development, hotel or retail complex when on Pine Knot Ave., Big Bear Blvd., Lakeview Drive, Paine Road. Permitted without limitation in other circumstances.						

	CL	LC	NC	VR	PQP	R
<u>Personal Services</u> (continued)						
Bicycle Rentals	1	1	1	1		
Cabinet Shop			1			
Storage Facilities, Enclosed			2A			
<u>Commercial</u>						
Retailing of goods, gifts, books, stationery, florists, specialty shops, clothing, and the like	1	1	1	1		
Restaurant/Cocktail Lounge/ Bars	1	1	1	1		
Grocery			1			
Drug Store			1	1		
Hardware, Appliance Store including incidental repairs	1*		1		1*	
Delis, Liquor "mini mart", sandwich shops, ice cream parlors	1	1	1	1		
Furniture Stores	2A		1	2A		
Galleries (art & craft)	1	1	1	1		
Gas Service Station including auto repair	2A		2A	2A		
* Prohibited on Pine Knot Ave., Big Bear Blvd., Lakeview Road, and Paine Road						

	CL	LC	NC	VR	PQP	R
<u>Manufacturing</u>			1	1*		
Incidental manufacturing essential to a retail or service/business where all products are sold at retail on premises. Maximum of 5 persons engaged in manufacturing.						
* Prohibited on Pine Knot Ave., Big Bear Blvd.						
<u>Public & Private Noncommercial</u>						
Churches	2A	2A	2A	2A	2A	2A
Parks, Pedestrian Activity Areas	2A	2A	2A	2A	2A	2A
Day Care Centers			2A	2*A	2A	
* Prohibited on Pine Knot Ave., Big Bear Blvd.						
Post Office			1		1	
Fire and Police Stations			1		1	
Museums	1	1	1	1	1	
Clubs/Lodges			1		1	
Public Utilities and Storage			2A	2*A		
Governmental Facilities			2A	2*A	2A	
* Prohibited on Pine Knot Ave., Big Bear Blvd. Offices permitted, same restrictions as Admin/Prof. uses.						

	CL	LC	NC	VR	PQP	R
<u>Temporary Uses and Structures</u>						
Parades, Festivals, Gatherings, Rallies	1	1	1	1	1	
Street Performers	1	1	1	1	1	
Cabanas	1	1	1	1	1	
Mobile Food Vendors				1		
Arts & Crafts Exhibits	1	1		1		
Flowers, Art, Craft Sales	1	1		1		
Bicycle/Ice Skate Rentals	1	1	1	1	1	
Boat/Car Shows	1	1	1	1	1	
Christmas Tree and Other Seasonal Sales	1	1	1	1		
Parking	1	1	1	1	1	
Any use incidental to any of the permitted uses	1	1	1	1	1	1
<u>Accessory Uses</u>						
Uses typically accessory to the operation of the permitted use.	1	1	1	1	1	1
Community/Commercial Information Centers/Booths	1	1	1	1	1	
Shuttle Bus Stop Shelters	1	1	1	1	1	1

	CL	LC	NC	VR	PQP	R
<u>Setbacks</u>						
Min. Street Setback from property line after necessary road dedication.	20' lodg- ing & res. 15' all other uses.	15'	15'	5' min 10' max* 20' all other areas	20'	15'
* Pine Knot Ave. and Big Bear Boulevard						
Covered Entry Walkway to Curb	X	X	X	X		
Covered Pedestrian Colonnade				X		
Min. side yard setback	0'*	10'**	0'	0'	20'	5'
Min. rear yard setback	0''*	15'***	0'	0'	20"	10'
10' Setback when Abutting Res.						
** Increase 1' side yard for 1' increase in building height above 25'. Lake views considered important.						
*** Applies to setbacks. May be reduced with greater side yard setbacks.						
<u>Lot/Building Area</u> (Lots created after plan adoption)						
Min. Lot Area, sq. ft.	10,000	10,000	10,000	0	10,000	7,200
Min. Lot Width/Depth	0' / 0'	0' / 0'	0' / 0'	0' / 0'	0' / 0'	60' / 100'
Max. Bldg. Lot Coverage	50%	50%	60%	70%	40%	60%

	CL	LC	NC	VR	PQP	R
<u>Height, Building</u> (measured from finished grade)						
* Based on meeting the intent of the district						
<u>Pedestrian Open Space</u>						
% of Lot	20%	20%	20%	10% w/ on-site parking 15% w/ off- site parking		
<u>Density</u>						
Residential					3,600 s.f. 1t 1,400 s.f. 1t	1 DU/ 1 unit/

7. VILLAGE SIGNAGE REGULATIONS

A. Purpose and Intent

It is recognized that signs perform important functions in the community which are essential for the public safety and general welfare, such as communicating messages, providing information about goods and services available, and providing orientation. It is further recognized that because of their potential detrimental impact on the visual and perceptual environment, signs must be regulated in order to:

1. Prevent hazards to vehicular and pedestrian traffic.
2. Prevent conditions which have a blighting influence and contribute to declining property values.
3. Provide for easy recognition and legibility of all permitted signs and other uses in the immediate vicinity.
4. Preserve the amenities and visual quality of the Village and curb the deterioration of the mountain environment.

It is the intent of this article to protect property values, create a more attractive business climate, enhance and protect the physical appearance of the community, provide a more enjoyable and pleasing community and to encourage the most appropriate use of land.

B. Applicability

All signs within the Village Specific Plan Area shall conform with the regulations contained herein. No signs shall be hereinafter constructed, maintained or permitted except in accordance with the following regulations.

C. Definitions

Sign:

Any permanent structure, device, letter, word, model, insignia, trade flag, streamer, display, emblem, or representation used as, or which is in the nature of, an advertisement, announcement, or direction, or is designed to attract the eye. This definition shall include signs located within a window when illuminated. Marquees, canopies, clocks, thermometers and calendars shall be subject to the provisions when used in conjunction with signs as defined above.

A sign shall be painted, posted or otherwise securely affixed to a substantial intermediate removable surface and, except for freestanding signs, such surface shall be securely affixed to the face of the building front, which can be street or parking lot frontage, but shall be in a single, unbroken plane. The foregoing shall not prevent installation of a sign by individual letters or devices cut into or securely affixed to the exterior wall of a building, provided that such letters or devices have a minimum depth or projection of one-fourth of an inch. The material of the sign and intermediate surface and the manner of affixation of the sign to the intermediate surface and of the intermediate surface to the wall of the building shall be subject to the approval of the Building Inspector for the purpose of protecting the safety of the public.

Sign, Accessory:

Any sign that, with respect to the premises on which it is erected, advertises or indicates one or more of the following: the person occupying the premises, the business, transacted on the premises, and directional or parking instructions, or the sale or letting of the premises or any part thereof.

Sign Area, Area of a Sign, Signage:

The entire area within a single continuous perimeter, and a single plane, composed of a square, circle or rectangle which encloses the extreme limits of the advertising message or announcement or wording together with any frame, background, trim or other integral part of the display excluding the necessary supports or uprights on which such sign is placed. Sign area of a standing or pole sign is the entire area of one side of such sign such that two faces which are back to back are counted only once for the purposes of standing or pole sign area.

Canopy:

A roof-like covering, as a canvas awning on a frame that is affixed to a building projecting over a sidewalk portion of a way, and carried by a frame.

Sign, Facing or Face:

The surface of a sign board, background area, and structural trim upon, against or through which a message is displayed or illustrated on the sign.

Sign, Freestanding:

A sign not a part of or attached to any building but generally located elsewhere on a lot.

Sign, Monument:

A freestanding sign located on or close to the ground, the top of which shall not be higher than four (4) feet above the ground.

Sign, Standing:

A free-standing sign not exceeding fifteen (15) feet in height with eight (8) feet of clearance under the sign area and erected upon supporting devices or stands.

Sign, Permanent:

Any sign as defined above, intended to be erected and maintained for more than sixty (60) days.

Sign, Projecting:

Any sign which is attached to a building or other structure and any part of which projects more than twelve (12) inches perpendicular from the wall surface of that portion of the building or structure in front of which the sign is positioned.

Sign, Roof:

Any sign erected, constructed and maintained upon or over the eave line of any building.

Sign, Portable:

A freestanding sign not permanently affixed, anchored, or secured to the ground or a structure on the lot it occupies, including trailered signs but excluding signs affixed to or painted on a vehicle.

Sign, Temporary:

Any sign, including its supporting structure intended to be maintained for a continuous period not to exceed sixty (60) days.

Sign, Wall:

A sign not exceeding four (4) feet in height securely affixed to a wall projecting no more than twelve (12)

inches from and parallel to the face of such wall, not projecting beyond the building face fronting on a street or parking lot nor above the eave line of the building to which it is attached.

A wall sign shall be no higher than the lowest of the following: (a) twenty-five (25) feet above grade; (b) the bottom of the sills of the first level of windows above the first story; or (c) the cornice line of the building at the building line.

If attached to a parapet, sign shall not exceed the height of the parapet.

Sign, Primary Wall:

A sign on the building face fronting on a street or parking lot frontage.

Sign, Secondary Wall:

A sign located on any building face fronting on a street or parking lot frontage other than that of the primary wall sign. The cumulative area of all secondary wall signs shall not exceed fifty (50) percent of the maximum possible area of the primary wall sign.

Signs, Window:

Signs painted or posted on an interior translucent surface including windows and doors, such coverage not to exceed twenty-five (25) percent of the area visible from the exterior of the building.

D. General Regulations

The provisions of this section shall be general and controlling for all signs within the Village Specific Plan Area. Specific regulations by use district are set forth individually herein. Any standards not specifically covered by these regulations shall be covered by the City of Big Bear Lake Sign Code.

1. Prohibited Signs

Flashing (string lights will not be prohibited however)
Inflatable
Pendant Flags
Streamers
Freestanding Signs, except along Lakeview Drive
Roof Signs

- Window signs which cover more than 25 percent of the area of the window
 - Portable signs
 - Projecting signs in the CL, LC or VR districts
 - Signs, except awning signs, painted directly on the exterior surface of any wall
 - Off-premises sign, billboard
 - Free-standing signs over five (5) feet in height
2. Any traffic directional or identification sign owned and installed by a governmental agency shall be permitted in accordance with the design guidelines of this Specific Plan.
 3. A sign (including interior window displays or banners, either temporary or permanent) or its illuminator shall not by reason of its location, shape, size, or color interfere with traffic or be confused with or obstruct the view or effectiveness of any official traffic sign, traffic signal or traffic marking.
 4. No red or green lights shall be used on any sign, if, in the opinion of the Building Inspector with the advice and consent of the Police Chief, such light would create a driving hazard.
 5. All illumination shall be either interior, non-exposed or exterior, shielded and directed solely at the sign and shall be steady and stationary, of reasonable intensity.
 6. Signs in any district, if illuminated, shall be illuminated by white or non-flashing lights only.
 7. Exempt Signs
 - Governmental
 - Holiday decorations
 - Signs not visible from a street, lake or public walkway
 - Realty
 - Open house
 - Hours of operation
 - Parking
 - Open/Closed/Vacancy
 - Construction
 - Institutional/Directional Sign
 8. All temporary signs shall be regulated by the existing City of Big Bear Lake Sign Ordinance.

9. All residential signs shall be regulated by the existing City of Big Bear Lake Sign Ordinance.

E. Sign Controls For Each Specific Plan Use District

<u>DISTRICT</u>	<u>ALLOWABLE SIGN TYPE(S)</u>	<u>SIGN AREA</u>	<u>NUMBER SIGNS</u>	<u>HEIGHT</u>
CL, LC (Commercial Lodging) (Lakefront Commercial)	wall	1 sq. ft./ lin. ft. of tenant bldg. front	1/tenant	eave line
	monument	32 sq. ft.	1/parcel	5 feet
	canopy	6 sq. ft.	1 tenant/ canopy	8 feet
	Projecting	24 sq. ft.	1/parcel	8 feet lower edge roof line upper edge
Freestanding (along Lake- view Drive)	24 sq. ft.		1/parcel	8 feet lower edge of sign
VR (Village Retail)	Wall	1 sq. ft./ lin. ft. of tenant bldg. front	1/tenant	eave line
	Canopy	6 sq. ft.	1/tenant/ canopy	8 feet
NC (Neighborhood Commercial)	Wall	1 sq. ft./ lin. ft. of tenant bldg. front Maximum	1/parcel	no higher than first floor

<u>DISTRICT</u>	<u>ALLOWABLE</u>			
	<u>SIGN TYPE(S)</u>	<u>SIGN AREA</u>	<u>NUMBER SIGNS</u>	<u>HEIGHT</u>
All Districts	Monument	32 sq. ft.	1/parcel	5 feet
	Canopy	12 sq. ft.	1/tenant	8 feet
	Projecting	24 sq. ft.	1/parcel	8 feet lower edge roof line upper edge
	Free-standing	24 sq. ft.	1/tenant	8 feet
R (Residential)	Pedestrian oriented walkway sign	4 sq. ft.		
	All signs in residential districts are regulated by the City of Big Bear Lake Sign Ordinance.			
PQP (Public/Quasi-Public)	All signs in this district are regulated by the City of Big Bear Lake Sign Ordinance under Residential type signs.			

8. BIG BEAR VILLAGE LOT AND PARKING CONSOLIDATION PROGRAM

a. Purpose and Intent

The consolidation or integration of numerous small lots or the consolidation joint use of separately owned parking areas is encouraged particularly for new development or large scale development rehabilitation so as to benefit from greater land use efficiencies and reduced incremental development costs associated with large development projects. Lot consolidation refers to the legal combination of lots. Parking consolidation refers to the legal co-use of two or more parking areas that are adjacent, under separate ownership and are designed in such a way as to allow a vehicle to travel to each parking area without utilizing the public street. The nature of the lot or parking consolidation formula for the Village encourages the packaging of numerous similarly sized small lots or co-designed parking areas rather than separate large lots consolidating smaller parcels.

b. Applicability

This incentive program will be available to any lot in the Village Specific Plan area. In order to be eligible for a lot or parking consolidation incentive, the total square footage of the consolidated lots must exceed a minimum of 1.3 times the largest single lot of record in the consolidation scheme.

c. Incentive Program

There are a number of actions the City can pursue to encourage the consolidation of parking lots. Some potential actions are very direct and aggressive involving public funds and full use of the Redevelopment Agency's powers. Other techniques involve minimal public action and relieve site development standards. The exact level of the Incentive Program benefit will be delineated on Table 1.

d. Examples of How Incentive Program Works

1. Four lots are to be consolidated. Respectively they measure:

10,000 sq. ft.
4,000 sq. ft.
25,000 sq. ft.
4,500 sq. ft.

To figure out their incentive factor add all four lots and divide by 25,000 (the largest lot). The answer is 1.38; therefore the lot incentives under column (A) are available.

2. Three lots are to be consolidated. Respectively they measure:

10,000 sq. ft.
12,000 sq. ft.
9,000 sq. ft.

Adding the three lot square footages and dividing by 12,000, the lot incentive figure is 1.83, found under column (B).

TABLE 1
LOT INCREASE FACTORS**

<u>Incentive Description</u>	(A) * <u>1.3-1.6</u>	(B) <u>1.7-2.0</u>	(C) <u>2.1-over</u>
1. Parking Standard Reduction	5%	10%	20%
2. Increased Floor Area Ratio	5%	10%	20%
3. Processing Fees Reduction	20%	25%	To be considered on a case-by-case basis. Minimum of 25% reduction.
4. Increased height limits	5'	1 story	2 stories
5. Agency loan for infrastructure improvement (requires developer payback).		To be considered on a case-by-case basis.	

* 1.3 represents the minimum lot square footage increase factor.

** The Planning Commission, upon recommendation from the Design Review Board, shall ultimately decide which incentives will be granted under the appropriate factor on a case by case basis. These incentives will only be granted when the Planning Commission feels that the incentives granted to the property owner(s) are responsive and not in conflict with the intent of the Village Specific Plan Design Guidelines.

F. SUPPLEMENTARY ADMINISTRATIVE REGULATIONS

1. General

All regular provisions of the Big Bear Municipal Code shall apply to all properties within the Village Specific Plan Area, including but not limited to conditional use permit, variance, time limit, public notice and hearing provisions. The provisions herein add to or supplement the above referenced provisions.

2. Design Review Permit

a. Method and Purpose

The Village Specific Plan shall be implemented through the use of a Design Review Permit. A Design Review Permit shall be required for all rehabilitation, redevelopment, expansion of existing use or structures and new development within the Specific Plan area. A Design Review Permit will also be required for rehabilitation of a structure where there is no square footage increase or use intensification but visible exterior changes. This requirement is instituted for the following reasons:

- 1) To ensure consistency with the intent of the Specific Plan Regulations and Design Guidelines.
- 2) To encourage innovative community design and development.
- 3) To assure substantial long range compliance with the General Plan.
- 4) To promote the highest contemporary standards of site design, building architecture and sign design.
- 5) To adapt to specific or special development conditions that occur from time to time while continuing to implement the Specific Plan.
- 6) To facilitate complete documentation of land use entitlements authorized and conditions pertinent thereto.

b. Applicability

This design review ordinance is applicable to all new structures and outdoor uses, exterior alterations, exterior additions, and exterior changes in the Village Specific Plan Area which are not exempt from review.

Prior to receiving a building permit on making alterations to structures/uses, a special Design Review Permit subject to the design review procedures and standards contained herein shall be obtained.

c. Exemptions

The following is a list of activities which are exempt from the design review process. This list is not all-inclusive, the Community Development Director may exempt other special activities not covered by this example listing.

- o All interior changes, alteration, construction
- o Repainting with same color (maintenance) including acceptable colors established by the Committee.
- o Reglazing, new mullions
- o Re-landscaping of existing structure
- o Re-roofing with similar style roofing materials
- o Minor exterior remodels resulting in less than 50% of the wall surface area may be approved by the community development department
- o Demolition
- o Exterior mechanical (heating, air conditioning, water heater) provided equipment is screened
- o All single family/two family dwellings

d. Procedures

Site Plans, which contain plans, drawings, illustrations, designs, reports and other detailed information as outlined in the Village Specific Plan Design Manual as required, shall be submitted to the City staff for review and comment. Applicants are encouraged to submit preliminary plans for review and comment by the Community Development Department prior to the final preparation of a Design Review Permit application. Comment from other City departments and service agencies shall be sought by the staff prior to preparing a recommendation on the finalized Design Review Permit application.

Applicants should insure that they have obtained a copy of the design guidelines which supplement this Specific Plan. This will assist the developer in achieving consistency with the Specific Plan and generally facilitate a quality project.

Upon determination that the development plans comply with the provisions of the Specific Plan and that the application for a Design Review Permit is complete, the staff may prepare a staff report with recommendations which shall be submitted along with the Design Review

Permit Application to the Design Board before the earliest possible regular meeting. The Design Review Board shall approve, deny or conditionally approve the Design Review Permit.

e. Environmental Determination

The Design Review Permit process is discretionary, not ministerial, and is therefore subject to the requirements of the California Environmental Quality Act (CEQA).

f. Time Limitation

Design Review Permits approval in accordance with this policy, shall be valid for a period of two years plus extensions pursuant to the Big Bear Lake Development Code. If construction of a project does not commence within that period and proceed with due diligence thereafter, the approval of the Design Review Permit shall terminate and an additional submittal, review and approval will be required unless written approval is obtained from the Community Development Director.

g. Revisions

Revisions that are minor in nature or reasonable extensions other than those applied as a condition of approval by the Design Review Board, shall be submitted for review and approval administratively by the Community Development Director. Significant changes, additions or omissions shall be submitted for review and approval by the Design Review Board.

h. Requirements for a Design Review Permit

A Design Review Permit shall consist of plans, drawings, illustrations, designs, reports and other detailed information as required to determine compliance with the provisions of the Specific Plan and responsiveness to design guidelines. The following list of plans and information is required:

- 1) All applicable tentative tract maps or tentative parcel maps.
- 2) A legal and physical description of the site, including boundaries, easements, existing topography, natural features, existing buildings, structures and utilities.

- 3) Location, grades, widths and types of improvements proposed for all streets.
- 4) A site plan showing location of all structures, landscape and hardscape areas, parking areas, walks, internal circulation, access, adjacent streets, sign type and placement and fence-wall type and placement.
- 5) Elevation drawings of the front facade of the structure complete with material and color identification.
- 6) Description of the extent to which Village design guidelines have been used in the plan.
- 7) A tabulation of square footage, area devoted to parking, location of snow storage areas, landscape coverage, building heights and number of residential units (if applicable).
- 8) Such applications and environmental assessment forms as are provided by the City staff.
- 9) Any other requirements as specifically outlined in the Village Design Manual.

i. Mandatory Findings for Approval of a Design Review Permit

The Design Review Board shall make the following written findings before approving or conditionally approving any Design Review Permit.

- 1) The proposed project is compatible with the intent and purpose of the Specific Plan.
- 2) The development plan will not have an adverse impact on the public health, safety, interest, convenience or the general welfare.
- 3) The Site Plan is compatible with the intent and purpose of the Village Design Guidelines.

City of Big Bear Lake



VILLAGE SPECIFIC PLAN

DESIGN GUIDELINES

PREPARED FOR THE CITY OF BIG BEAR LAKE

MARCH, 1987

John Spivey, Mayor
Norman Reinik, Jr., Mayor Pro Tem
John C. Emlinger
Don Morris
A. W. "Bill" Speyers

Robert Van Nort, City Manager

T H E P L A N N I N G C E N T E R

DESIGN REVIEW

PURPOSE

The purpose of this section is to provide individual detailed review of all development which has a substantial impact upon the character of the Village Specific Plan Area. It is held that the appearance of open spaces, buildings, structures and signage in the Village area have a material and substantial relationship to property values, commercial success, and the aesthetic well-being of the City of Big Bear Lake.

Many other resort communities have deteriorated in the past by reason of poor planning, neglect of proper design standards and the erection of buildings and structure unsuitable and incompatible with the character of the resort, resulting in declining tourism, reduction of property values, and the impairment of the public health, safety and welfare.

It is therefore the declared intent of the City of Big Bear Lake that the Village Design Review will serve the following purposes:

1. To improve the general standards of orderly development of the Village through the design review of individual buildings, structures and their environs.
2. To augment the regulatory standards included in the Village Specific Plan in order to promote creative and innovative building and designs which create a special sense of place for the Village.
3. To encourage the orderly and harmonious coupling of site and building designs, including the design and location of signs, parking, vehicular and pedestrian circulation and landscaping.

APPLICATION

This design review ordinance is applicable to all new structures and outdoor uses, exterior alterations, exterior additions, and exterior changes in the Village Specific Plan Area which are not exempt from review. Prior to receiving a building permit or making alterations to structure/uses, a special Design Review Permit, subject to the design review procedures and standards contained herein, must be secured.

covered by this example listing.

- o General maintenance involving same materials and color
- o All interior changes, alterations, construction
- o Repainting with same color (maintenance) including acceptable colors established by the Committee.
- o Minor exterior remodels resulting in less than 50% of the wall surface area may be approved by the Community Development Department
- o Reglazing, new mullions
- o Re-landscaping of existing structure
- o Re-roofing with similar style roofing materials
- o Demolition unless determined to be of historical significance
- o Exterior mechanical (heating, air conditioning, water heater) provided equipment is screened
- o All single family/two family dwellings

EXEMPTIONS

The following is a list of activities which are exempt from the design review process. This list is not all-inclusive, the Community Development Director may exempt other special activities not

CREATION AND ORGANIZATION DESIGN REVIEW BOARD

Composition of the Board

Village Design Review Board

We would recommend that this body be called a Village Design Review Board. Often these Boards are called Architectural Review Boards. The term "Architectural" connotes a preoccupation with matters of building style. This design manual, however, addresses much broader implication of design for the Village Area including site orientation, landscaping and parking design. For ease of understanding this Board's function, 'Village Design Review Board' should be the terminology used.

Design review necessarily requires the exercise of judgment on a case by case basis. For the system to work properly, these judgments should be entrusted to specially qualified people. Courts have often stressed the importance in aesthetic regulation of having decision-makers sensitive to the task.

This should not be taken to mean that unless all Board members are architects, the design review system is in legal jeopardy. It does support the notion that the authority granted a design review board must be exercised, in the main, by people knowledgeable in design matters.

This section does not mean to preclude the appointment of some members with no special training. Board members serve as public advocates for improving the quality of the environment, and good advocates are found throughout the Village Area.

It is recommended that the Village Design Review Board have five full time members appointed by the City Council. Three or more of the five regular members shall be property owners or merchants within the Village Area. Two of the members shall have a design background, or be a building professional or have a design or building professional background.

Members shall be appointed as per the following schedule to assure Board continuity.

Members appointed shall serve staggered four-year terms.

Compensation

Compensation, if any, shall be determined by the City Council. Costs of postage, mailings and a secretary to take minutes of each meeting could be borne by the Community Development Department.

Minutes

Meeting minutes shall be kept for each proposal before the Village Design Review Board.

Quorum

A quorum shall be three members of the Board. A tie vote is considered a denial.

Conflicts of Interest

All members shall be entitled to vote, provided, however, that no members shall participate in presenting, reviewing or voting on any work in which he or any partner or professional associate is the author, or which he or they have any direct or indirect financial interest.

Powers of the Village Design Review Board

- o Establish acceptable color list
- o Establish street tree selection
- o Recommend any street name changes
- o Other duties as may be required by the City Council and Board.

Prior to the issuance of any Building Permit alteration of structure or use, the applicant shall conform to the standards contained within this Village Design Manual. The Village Design Review Board shall review the plans submitted by the applicant and pursuant to the provisions of this Design Manual shall either (1) approve the proposal, (2) approve the proposal with conditions, (3) deny the project, (4) request more information. Upon approval with or without conditions, the Board shall instruct the Community Development Director to issue a Design Review Permit.

Other responsibilities of the Board includes:

- o Building inventory of historical significance, if any

Procedures

When a Design Review Permit is necessary, the project proponent or his agent shall request a design review application as well as a copy of the Village Design Manual from the Community Development Department.

Prior to the filing of a submission to the Community Development Department it is strongly recommended that the applicant and/or the architect meet with the Community Development Department and/or Board to ascertain all submission requirements and to obtain information on the interpretation of design guidelines by the Design Review Board.

Once the proponent submits plans and materials for design review, the Community Development Department will have 5 working days to determine if the application is complete. If additional information is needed, City staff shall inform the applicant within this time limit.

Submittal Requirements

To aid the Board in making findings required and the staff in preparing the advisory report the applicant shall submit the following materials at the time of application.

- o Contour lines
 - o Public streets
 - o Existing and proposed driveways
 - o Vehicular access/circulation
 - o Pedestrian circulation
 - o Parking -- showing dimensions of space and aisles
 - o Easements
 - o All existing trees over 6" in diameter (32") to be removed (8") and retained utilizing these symbols
 - o Trash and storage areas
 - o Indication of the proposed use of construction
 - o Location of walls, fences (height and type)
 - o Location of snow storage area
 - o Locations of exterior lighting
- Landscape Plan** (may be included in site development plan)
- o Landscaping materials, size and configuration
 - o Proposed and existing buildings

- o North arrow
- o Scale
- o Bar scale
- o Address
- o Property lines

- o Sidewalk location
- o Automatic irrigation location (if needed)
- o Street furniture type and location
- o Open space square footage and percentage of total lot

Elevations

A building elevation is a scaled drawing of the exterior of the building showing what the building will look like when it is constructed. Each new wall shall be shown. If your building's height is near the maximum height established by the Specific Plan, you will have to provide drawings of all elevations so that the height of the building can be calculated. The elevations must be drawn to scale (typically $1/4" = 1'0"$).

Show the outline of all doors, windows and identify the type of exterior materials to be used. Label each elevation so it is clear which wall is being shown. Walls are typically labeled according to the direction they face (e.g., north elevation, south elevation). You do not have to draw in all the bricks or siding. You need only illustrate a portion of the wall.

In addition to the basic elevation drawing requirement the following shall also be shown or provided:

- o All signage, illumination type, locations
- o Exterior colors
- o Roof material
- o Building height relation to adjacent buildings
- o Sample building material and color palette

Time Limitations

The Village Design Review Board shall meet within twenty days of acceptance of a complete submittal to determine whether or not the development/redevelopment succeeds in adequately addressing the design directions presented for the Village Area.

If thirty days elapse after submittal and acceptance of the application package by the Community Development Department and if not extended with the consent of the applicant, no action has been taken with regard to the application, the proposal shall be "deemed" approved. The Design Review Board may not "stall" a project by asking for additional information past the sixty day time limit.

Minimum Acceptance Standards

Whenever the Board takes action on a project, the action shall be based on findings.

Findings

1. The proposed project does (does not) meet the purpose, and standards of the Village Design Guidelines.
2. The proposed project is (is not) consistent with the General Plan goals, objectives and policies.
3. The proposed project does not (does) create any environmental impacts that cannot be mitigated to a level of non-significance.

Rights of the Public to Attend

All meetings shall be open to the public.

Public Notice

All meetings of the Village Design Review Board shall be regularly scheduled on certain days of the month and at a particular location. A public notice will not be necessary.

Appeals

All appeals of the Village Design Review Board actions shall be taken up before the Planning Commission. Planning Commission actions may be appealed to the City Council.

INTRODUCTION

This booklet has been created to provide design guidelines for the implementation of the Village Specific Plan. It has been designed as a companion document to the Specific Plan and is intended to aid planners, architects, landscape architects, engineers, property owners and developers in adhering to guidelines which will promote an improved image and character for the Village area.

There are key design elements which contribute significantly to the overall visual impact of a community. These include architecture or building design and criteria which deal with signs, lighting, landscaping, parking and other details. The Specific Plan provides regulations for minimum design standards of these criteria. This report addresses each of these elements in general terms or long range goals and establishes the

guidelines which can be used and expanded upon in more detail through the site planning process. Sketches are included to reflect some of the design concepts or "visions". Other sketches are provided to illustrate the application of design ideas for specific or special conditions.

This document is not intended to limit the work of designers, but to provide a flexible framework to accomplish an overriding design concept and to encourage quality development which will establish "The Village" area of Big Bear Lake as a special place to work, shop and recreate.

VILLAGE DESIGN

Theme

At the present time two different design themes have become the predominant architectural styles in the Village Area. The two themes are either Western Ranch or Bavarian architecture. The Western style originates from Big Bear Valley's history of gold mining and the towns and settlements that grew as a result. The imported Bavarian design concept arises from the mountain atmosphere of Big Bear and the alpine winter sports activities that are so predominant yet special for Southern California.

In evaluating the two existing themes, it becomes apparent that both themes have been utilized in the Village area with the Bavarian architecture holding a slight numerical edge. It would be impractical and very costly to stipulate only a West

Design Goals

- o To create a high level of activity
- o To create a special "sense of place"
- o To enhance the economic potential of the area by attracting quality developments
- o To establish and maintain a distinct pedestrian feel to "The Village" area

ern or Bavarian theme at this time. It would take a great deal of time to have all buildings conform. Architectural themes contribute little when they are superimposed upon areas with the pluralistic design characteristics of the Village area.

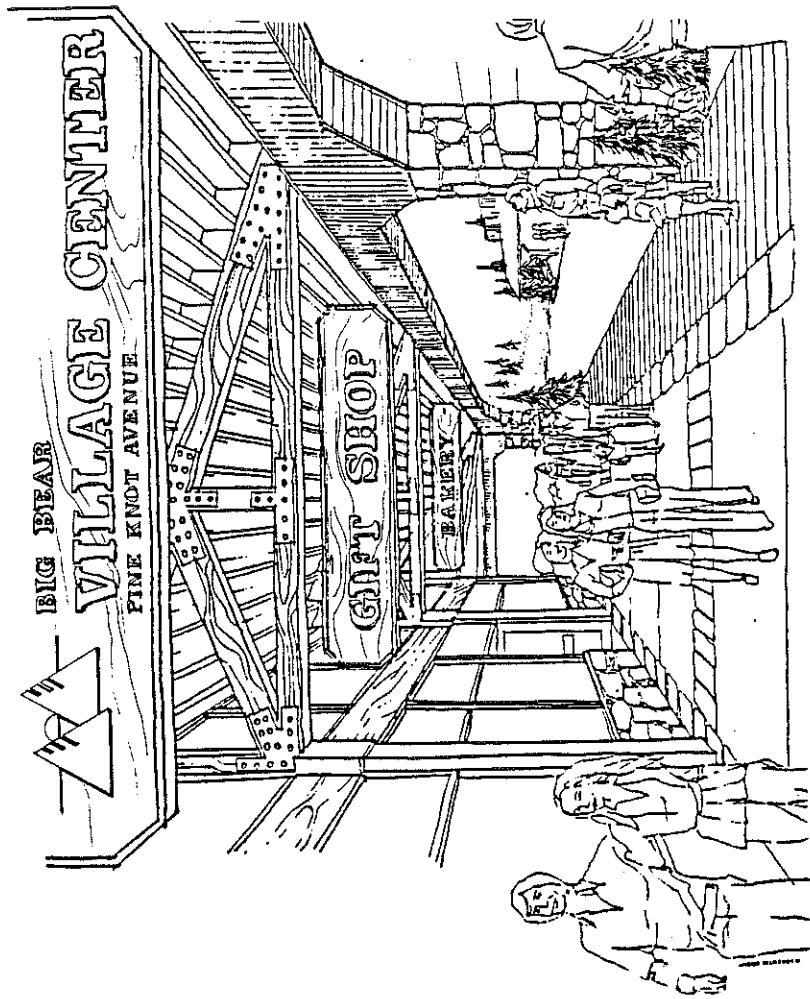
Before any architectural direction was developed, criteria were identified by which various alternative concepts would be evaluated. The criteria utilized were:

- 1) Establish an overall "Village" identity rather than various design pockets or subscribing to an overall Big Bear Valley 'theme'
 - 2) Market appeal
 - 3) Timeless architecture (that won't go out of style)
 - 4) Economic Feasibility
 - 5) Related directly to Big Bear history and location
 - 6) Weather and climate
 - 7) Be resort oriented
 - 8) Fit well with the existing architectural style directions
- Four borrowed design styles were evaluated for the Village area; they were 1) Western Ranch, 2) Bavarian, 3) Rustic Wood and, 4) Mexican Adobe.
- Unfortunately, none of the identified design styles could cumulatively score well on the eight criteria, all of which are necessary for the success of Big Bear Village. As a result, a unique challenge and opportunity to create a theme which could maximize all the established criteria and blend with the architecture already established was presented. To meet this challenge, the historical architectural heritage of the Big Bear Valley was researched. The goal was to identify characteristic architectural elements and subsequently blend and contemporize these to yield a new design. The name of the

style or theme that has been identified for the Village area will come to be known as 'Big Bear' architecture. This style of architecture is not an imported style but rather is sensitive to Big Bear's location in the Western United States and its alpine climatic conditions above 7,000 foot elevation and finally its resort market appeal.

In order to strengthen the Big Bear design direction in the existing pluralistic architectural context of the "Village", it will be very important to be able to tie all of the architecture together with a common thread in the near future. This thread will be developed through a consistent "streetscape" approach. Consistent paving, landscape, lighting, awning colors and signage will do much more to tie the divergent styles of existing architecture together than trying to create a "little Italy", "Denmark" or "New England". This "Disneyfication" of the Village has little

relationship to the historical function of the Village and the Big Bear Valley and its ski resort status. It would also be difficult to accomplish this village transformation with so many existing divergent styles. Therefore, this booklet will recommend various means to create a special sense of place for the Village given the existing potpourri of architectural styles.



COVERED SIDEWALK ALONG PINEKNOT AVENUE

DESIGN GUIDELINES

This booklet is intended to provide guidelines that will result in buildings of high quality and character. Projects conforming with these guidelines are highly recommended. Nevertheless, careful consideration will be given to creative, sensitive projects which do not conform in all respects to these guidelines.

The qualities and design elements for buildings that are most actively encouraged are:

- o Richness of surface and texture
- o Play of light (shapes and shadows)
- o Small arcades or plazas
- o Large display windows
- o 'Big Bear' or harmonious contemporary architecture
- o Pedestrian orientation
- o High void to solid building wall ratios
- o Pitched, multi-planed roofs

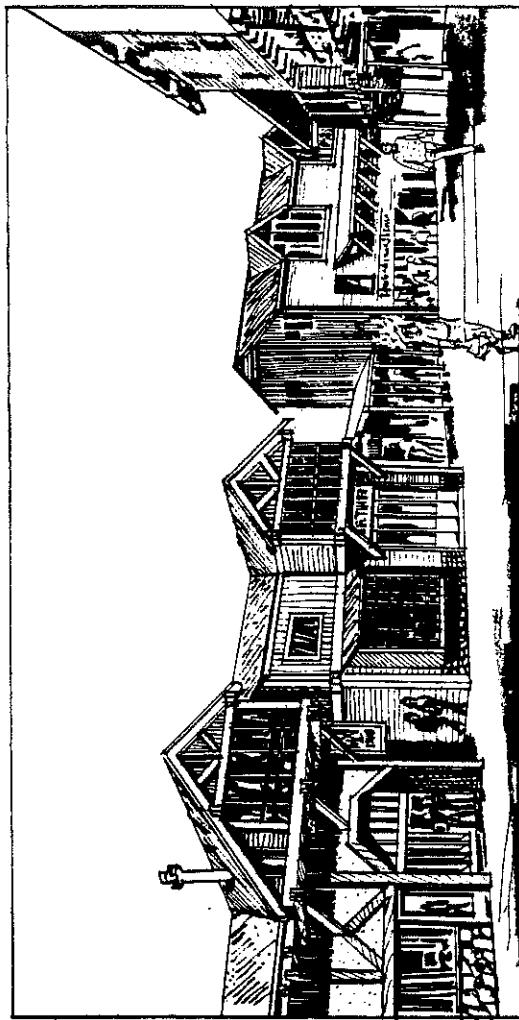
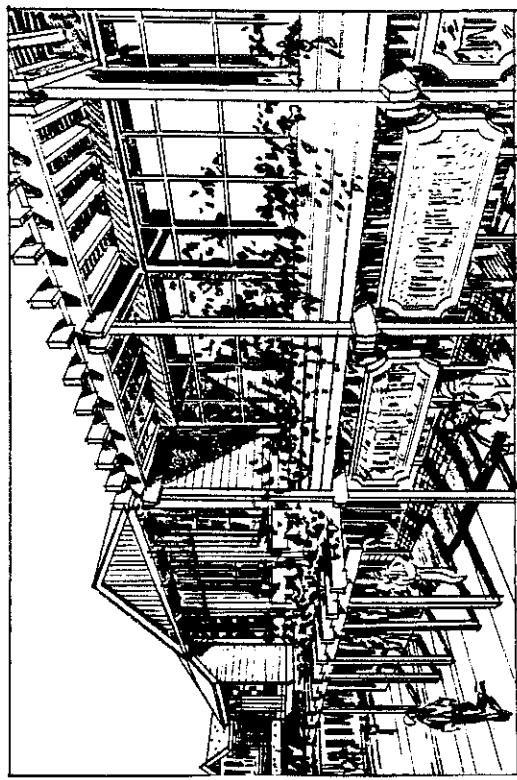
The elements to avoid or minimize:

- o Large, extremely deep front setbacks on the major shopping streets (Big Bear Boulevard, Pine Knot Avenue)
- o Extreme repetition
- o Highly reflective surfaces
- o Spanish or ranch-style architecture
- o Large blank walls
- o Large metal surfaces
- o Flat roofs, barrel or Quonset roofs
- o Adobe, slump stone masonry
- o Heavily textured plaster walls
- o Mission type barrel tiles (roof)
- o Exposed concrete block
- o Chain link fence
- o False fronts

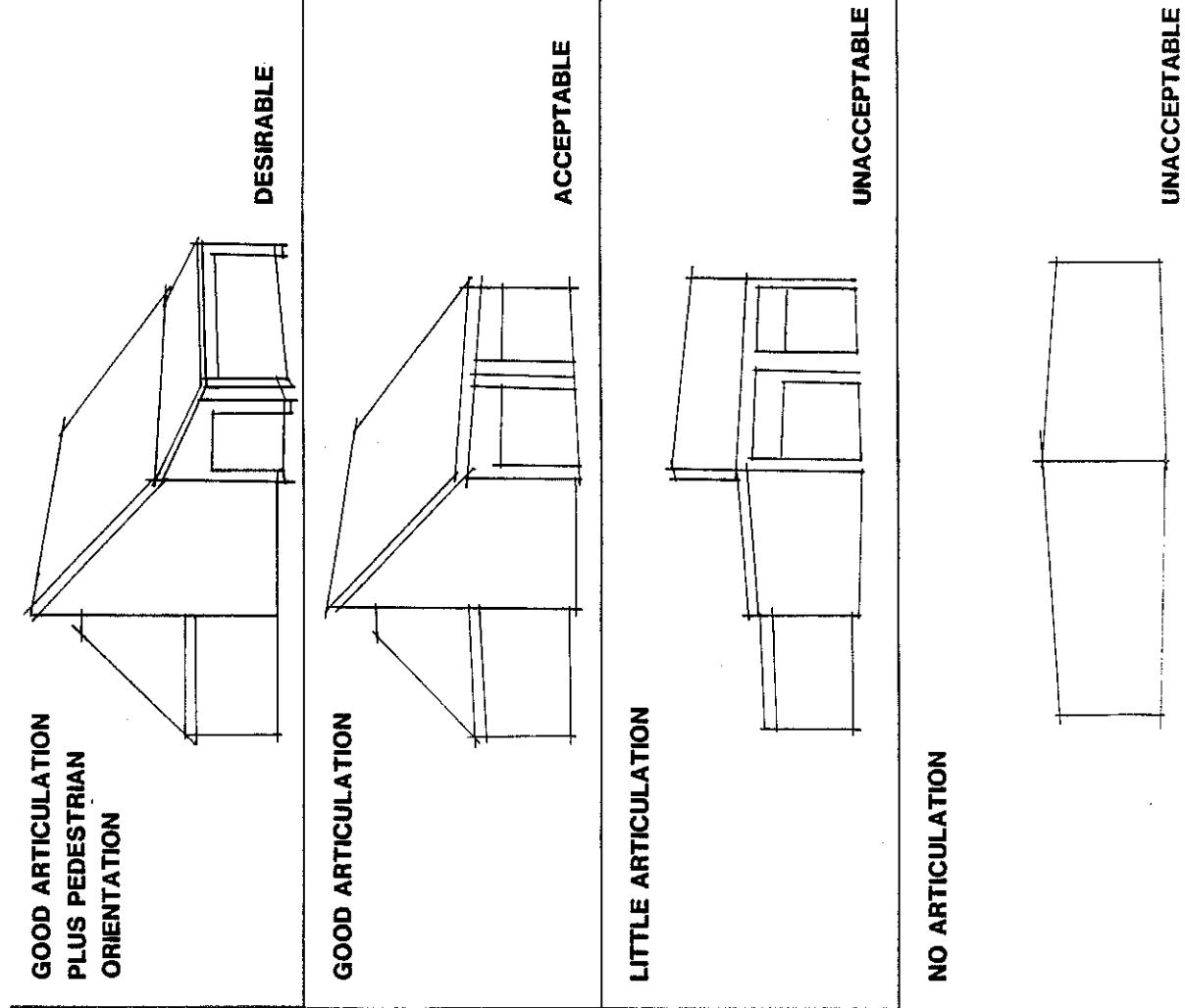
ARCHITECTURE

1. Exterior elevations shall conform to the 'Big Bear Theme'. Specific elements embodied in this theme include:

- o Considerable roof overhangs
- o Two-story height in most cases
- o Pitched, multi planed roofs
- o Gable ends of roof facing toward the street
- o Horizontal and diagonal wood (or other material simulating wood) siding
- o Split stone face veneer treatments
- o Heavy looking structural carrying timbers (natural color, or treated)
- o Exposed rafter tails
- o Large glass expanses

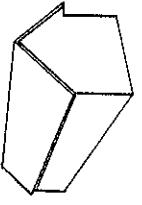
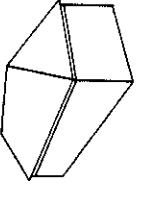
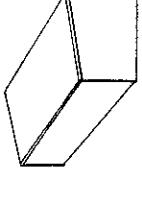
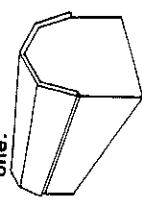


ARTICULATION: THE BIG BEAR ARCHITECTURAL STYLE ENCOURAGES VARIETAL BUILDING WALL DEPTH, SLOPE, AND ANGLE. THE FOLLOWING ARE SIMPLE EXAMPLES OF ARTICULATION.



2. Building Materials and Colors
 - o The use of textured materials is encouraged. Primarily exterior materials should include such features as horizontal and diagonal wood siding (natural color or treated), stone, brick and stucco. The use of stucco should be minimized as much as practical as its use in an alpine climate is inappropriate. Thick plaster walls in excess of a foot thick are acceptable however. Accent materials should include brick, split stone (indigenous to Big Bear), glass and wood.
 - o The overall image of the Village area can be greatly enhanced through the use of uniform, harmonious trim and accent colors. Colors synonymous with the alpine and mountain environment are encouraged. Colors such as deep forest greens, grays (full range), brown (full range), rust and white, are highly recommended.
 - o Paint schemes should be limited to one uniform color with contrasting accent materials. Bright colors for accent materials should include deep reds, forest greens, white and dark blues. Avoid pinks, light greens, purples and other pastel colors.
 - o It is recommended that all awnings (potential maintenance problem) in the Village utilize a similar color. Village Design Board shall establish similar colors if appropriate.
 - o Inappropriate exterior materials include aluminum, plastic siding (simulated materials), and reflective glass. Large expanses of uninterrupted wall area should be avoided. Additionally, stucco, treated as "hacienda" or "mediterranean" texture will be prohibited.

ROOFS: FOLLOWING ARE SOME TYPES OF ROOFS. SOME ARE APPROPRIATE FOR THE VILLAGE, OTHERS ARE NOT.

<p>RECOMMENDED</p> <p>Gable roof has a pitched roof with ridge and vertical ends.</p> 	<p>Hip roof has sloped ends instead of vertical ends.</p>  <p>RECOMMENDED</p> <p>Shed roof (lean-to) has one slope only and is built against a higher wall.</p> 	<p>ACCEPTABLE</p> <p>Jerkin-head (clipped gable or hipped gable) is similar to gable but with the end clipped back.</p> 	<p>ACCEPTABLE</p> <p>ACCEPTABLE</p> <p>Gambrel roof is a variation of a gable roof, each side of which has a shallower slope above a steeper one.</p>  <p>UNACCEPTABLE</p> <p>Flat roof has little or no slope.</p> <ul style="list-style-type: none"> Geodesic Dome A-Frame Quonset or Barrel Roof 
---	--	--	--

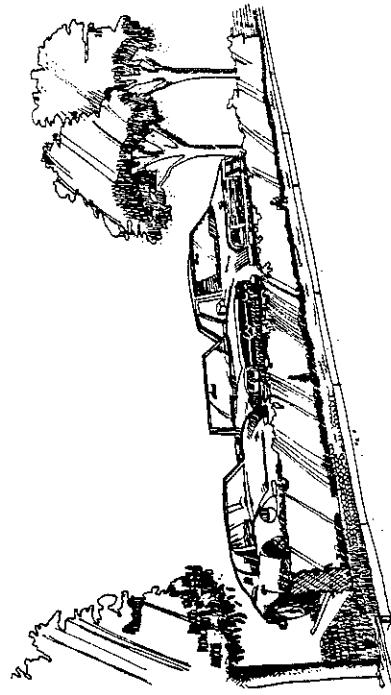
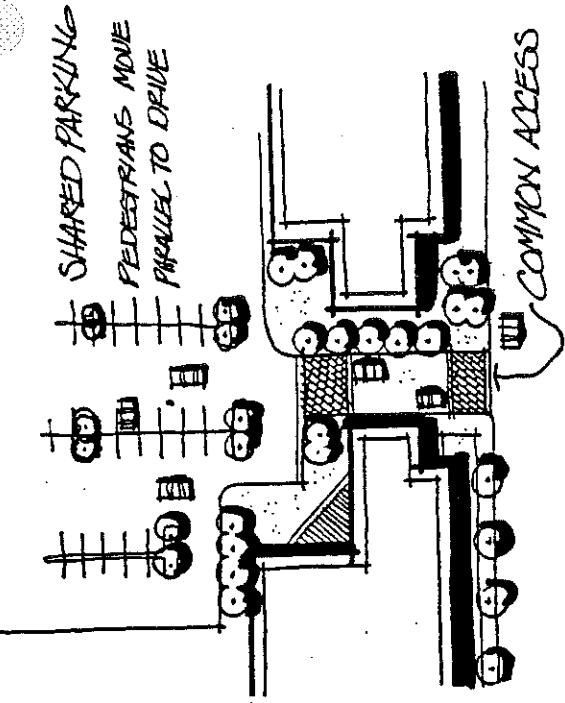
- o All buildings shall be designed to minimize the danger to pedestrians from snow slipping off roofs. It is recommended that gable ends of roofs face the street.

- o It is recommended that each building within the Village area utilize the small white Christmas string lights to outline building and window silhouettes. This should be instituted immediately to create a cohesiveness of design within the Village area.

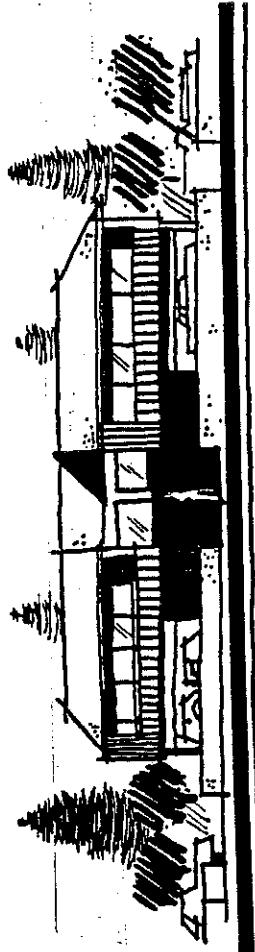
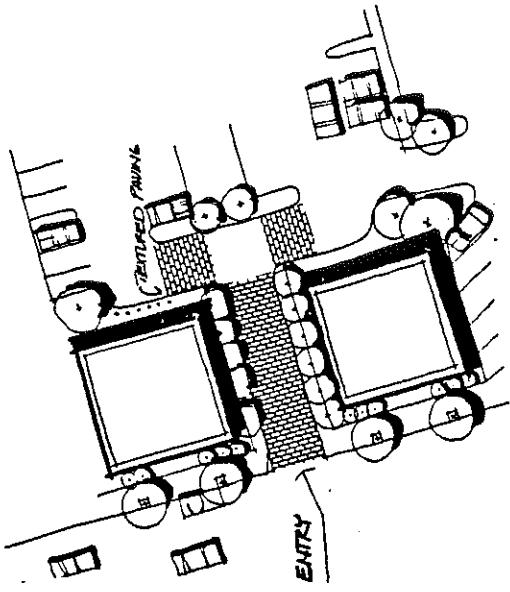
- o Metal roofing shall be color anodized and non-reflective.

PARKING

- o Develop multi-user parking areas shared between different buildings and uses.
- o Create a single major access point with secondary access points having one way access or exits only.
- o Break up massive parking areas with entrance features and parking lot trees. Hide cars from view through the use of berms, hedges or stone walls making sure that these elements will not impede snow removal.
- o Do not place off-street parking areas in the front yards of any use proposed along Big Bear Boulevard or Pine Knot Avenue. Whenever possible, parking should be placed at the rear or side of the site.



- o Major access point for parking lots should be located as far from street intersections as possible.
- o Create major entry statements to parking lots through the use of enriched paving that is smooth enough to be snow plowed.
- o Internalize parking areas to enhance open space areas and to provide efficient and safe pedestrian access to buildings.
- o Utilize the concept of "tuck-under" parking where multiple storied buildings are involved to reduce the amount of parking lot which must be snow plowed.

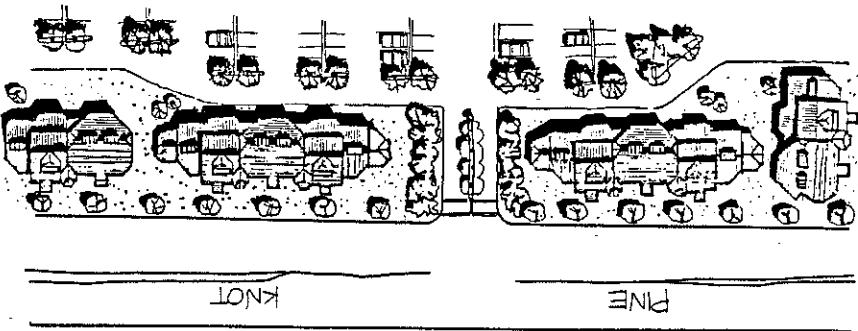


- o All areas not specifically used for vehicle parking or access shall be landscaped with trees, shrubbery and ground cover. Trees, at least equal in number to one (1) per each seven (7) parking stalls or fraction thereof, shall be provided in all parking areas. Minimum tree size shall be fifteen (15) gallons. Minimum shrub size shall be five (5) gallon. Smaller trees and shrubs will not tolerate snow storage pushed against them.
- o Orient parking to permit pedestrian flow to shops without having to cross through numerous vehicle travel lanes.
- o Parking facilities shall be designed so that a car within a facility will not have to enter a street to move from one location to any other location within the same parking facility.

- o Bumpers or tire stops shall be provided in all parking lot areas abutting a sidewalk, planting area, street or alley. Individual concrete tire stops are not recommended.
- o In areas where parking lot paving abuts a public sidewalk, building or alley; a five foot setback of that parking shall be provided. This setback area may be used to either widen the sidewalk, screen the parking spaces, or for snow storage.
- o All parking spaces shall be clearly outlined on the surface of the parking facility.
- o Parking facilities shall be designed in such a manner that any vehicle on the property will be able to maneuver as necessary so that it may exit from the property traveling in a forward direction.

o In order to promote and enhance a pedestrian or village scale, the elevation of lighting standards in parking areas shall not exceed twenty (20) feet in height, or building height, whichever is less.

o All parking lots shall provide an area or areas on the property for the provision of snow storage. This area shall amount to no less than five (5) percent of the total paved parking areas. This area shall be delineated on all site plans submitted to the City. If no provisions are made for snow storage, a snow removal plan shall be submitted by the project proponent to the City's Community Development Director.

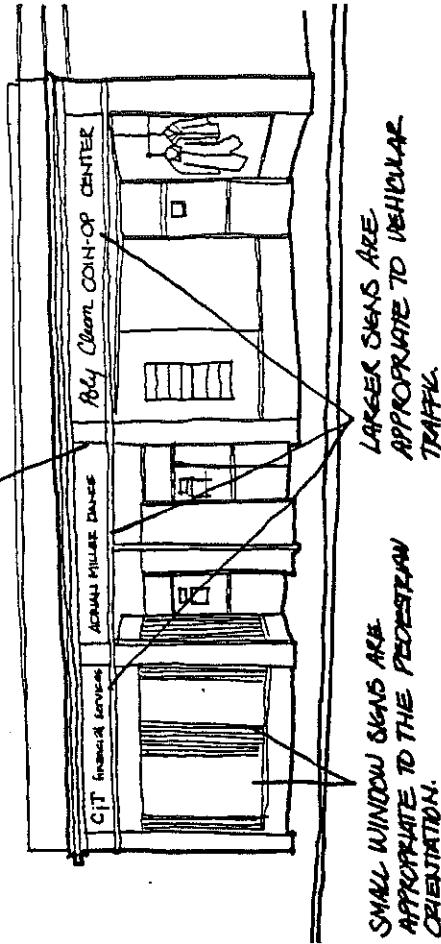


SIGNS

Signs are a vital part of projecting the "Village Center" theme. With a sign, you call attention to your business and create an individual image for your store. But it is often forgotten that signs contribute to the overall village image as well. Merchants try to out-shout one another with large, flashy signs. A successful sign can reinforce the image of the Village Center as well as serve the needs of the business. Consider the following guidelines:

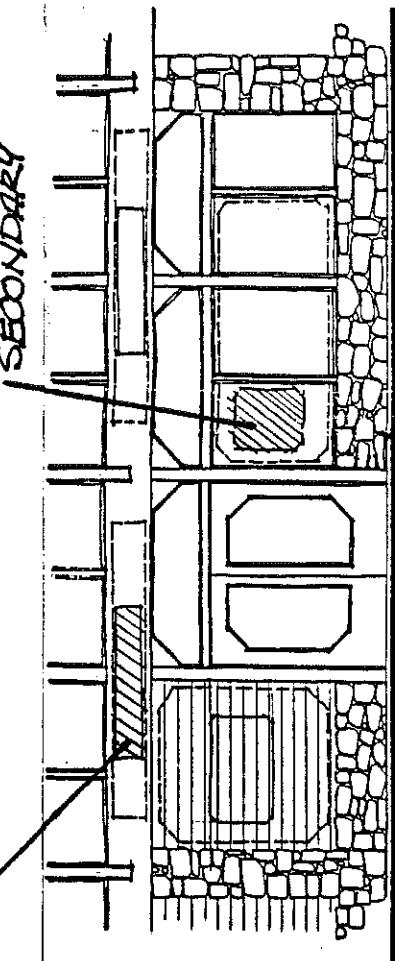
- o A sign should express an easy to read, direct message: Keep it simple.
- o A storefront should not have more than two signs-one primary and one secondary.
- o A flush-mounted sign board may extend the width of the storefront display windows and in the case of two story buildings below the second-story window

*SIMPLE SIGN SHAPES
AND CONSISTENT PLACEMENT
MAKE EASIER READING*



LARGER SIGNS ARE APPROPRIATE TO VEHICULAR TRAFFIC.

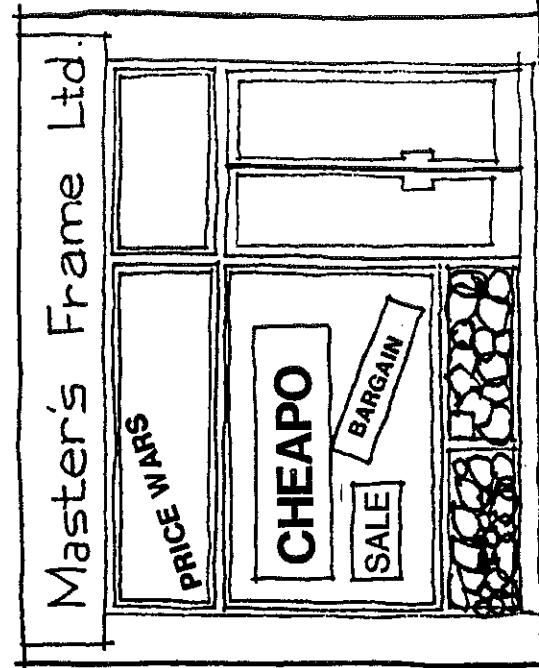
PRIMARY SIGN



SECONDARY

- o Sills. Generally, lettering should be 8 to 18 inches high and occupy only about 65 percent of the sign board.
- o Signs may identify only the firm name or major enterprise on the premises on which the sign is located. Materials and products advertising shall not be permitted except as primary identification of an establishment. Additional identifying logos of products may be incorporated within the signing but are considered part of the overall allowed sign area.

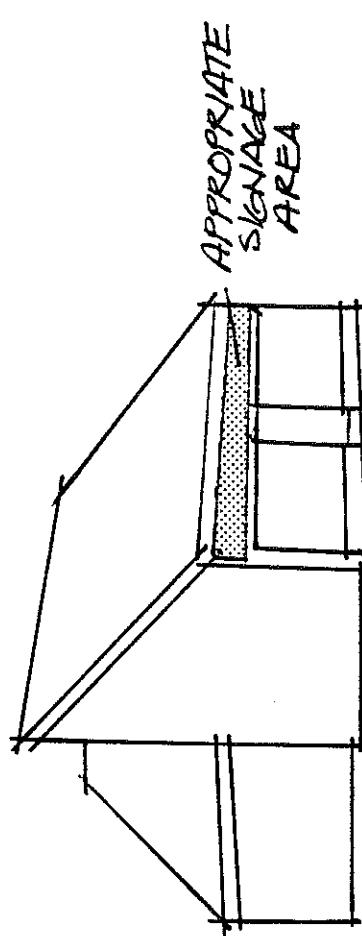
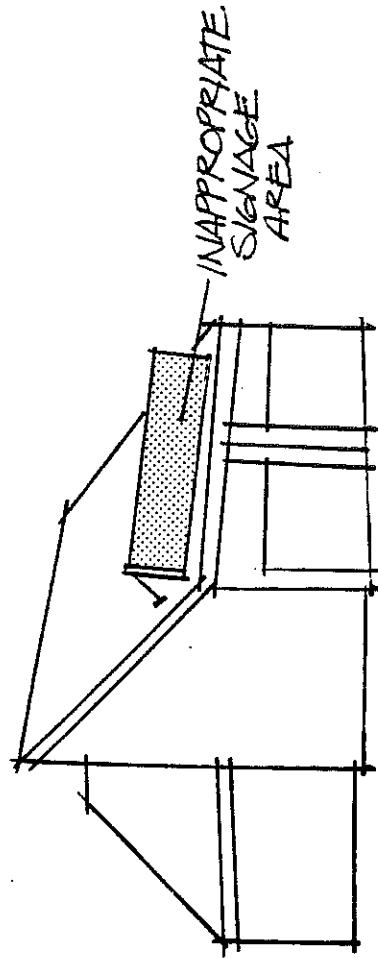
- o Pedestrian-oriented signs may be applied directly to the face of the building. The shape of the sign can be a positive feature.
- o Signs with strange shapes. Signs that are unnecessarily narrow or oddly shaped can restrict the legibility of the message.



- o Temporary signs placed on the exterior or in windows are discouraged. These signs tend to present a cluttered, unattractive appearance which will detract from the village area's overall aesthetic impression. Special event signs are permitted pursuant to City standards.

AVOID THIS !

- o All materials should be flat or matte finishes - none should be shiny or slick in appearance, except glass applications. Metals should have a dull finish.
- o No signs shall extend higher than the eaveline of the structure.
- o In order to encourage the use of signage on items such as awnings and display window, the sign will not count as the primary sign, however no more than 25% of the awning or display window may be utilized to display the business name only.
- o Window signs should not obscure the display area. The color of the letters should contrast with the display background.
- o Awnings can also serve as signs with contrasting letters painted or sewn onto the valance. Usually, 6 to 8-inch letters are sufficient.



o There are hundreds of letter styles available. A letter style should be chosen that is easy to read and that reflects the image of the business it represents.

light letters on a dark opaque background. Exposed neon letters can also be effective, adding color and vitality to the street. Each application for such a neon sign shall be accompanied by a drawing of such sign.

o Letters can be painted or mounted directly on a sign board. Three dimensional letters are available from sign makers in wood, marine plywood, metal and plastic. Remember, letters should not be too large.

o Choose a sign maker carefully. Quality of workmanship and construction is as vital as any of the considerations just discussed. Ask where you can see examples of previous work.

o Sign colors should complement the colors of the building. Light colored letters on a dark background are easier to read.

o Illuminated signs can be appropriate in the Village if they respect the proportions of the storefront and the guidelines outlined above. Painted signs can be directly illuminated with fluorescent or incandescent lights. Internally lit signs are most effective with

SITE ORGANIZATION

- o Buildings should be sited in a manner that will complement the adjacent buildings and landscape. Building sites should be developed in a coordinated manner to provide order and diversity and avoid a jumbled, confused street scene.
- o On Pine Knot Avenue and Big Bear Boulevard, new buildings should not be set back from the front property line more than 15 feet or less than 10 feet. This will help maintain an uninterrupted building facade along the street as well as a widened pedestrian promenade.
- o Buildings should orient their public entrances and siting toward the major streets as much as possible, except for buildings located on the Lake in which case public entries on both sides (lake, street) of the building are suggested.
- o Buildings with parking lots located at the rear of the site should develop attractive building entrances at the rear of the building or an open air pedestrian access to the front of the building, consisting of appropriate lighting levels, business identification, and inviting entrance. It should have a finished looking rear facade, landscaping and parking leading to the door. The architectural theme of the buildings' front facade should be carried to the rear.
- o Buildings with large unarticulated expanses of parking on one or both sides of them, creating a large gap between them and the next building are specifically prohibited along Big Bear Boulevard and Pine Knot Avenue.

LANDSCAPING

Landscaping in the "Village" area is to achieve two specific ends: one, to soften and "ruralize" the urban nature of the Village as the most intensively developed area in the Big Bear Valley and two, to provide pleasant and convenient outdoor space for pedestrian activity, be it active or passive.

- o Planting masses should assume a non-uniform arrangement. The diversity of massing types should be great enough to provide interest, but be kept to a level which evokes a relaxed, natural feeling.
- o Avoid shrubs and landscaping that create a security problem.
- o Establish a low, foreground apron of colorful groundcover. Use evergreen shrubs as screens adjacent to parking lots, loading areas and waste disposal units.
- o Use small, colorful ornamental trees, groundcovers and annuals at entrances and areas where a visual focal point is desired.
- o Locate trees and evergreen shrubs in planting pockets at the end of each parking row.
- o Accent colors should be used to prevent monotony. Color accents in plant material can come from seasonal color, such as that from deciduous or floral characteristics, or from foliage having two colors, such as variegated or juvenile growth different from mature growth.
- o Limit the types of plant materials (see the following Plant Matrix).

SUGGESTED PLANT MATRIX

NOTE: This matrix suggests only the basic plant materials. Other compatible plants could be used for a specific project.

STREETSCAPE ELEMENTS

Streetscape elements include landscape furnishings, such as seating, entry signs, lighting and paving. The following sketches are of elements that will appear in the Village. They are examples of style which represent the type of character to be created within the Village. The intended purpose of this section is to illustrate by the use of specific examples, the desired character for the "Village". The particular illustrations were chosen to illustrate the range of "in-character" design expressions that would be appropriate. The intention was to demonstrate, not require, that specific products will be able to achieve individual identity within that range.

SIGNS AND GRAPHICS

It is assumed that the majority of future visitors to the Village will gain their first exposure to the area through the printed media. Therefore, in addition to marketing the Village Center in an economic sense, all literature should begin the process of establishing an image of the "Village" at Big Bear Lake in the mind of the prospective tourist or investor by utilizing a prototype sign including a special logo.

The Logo

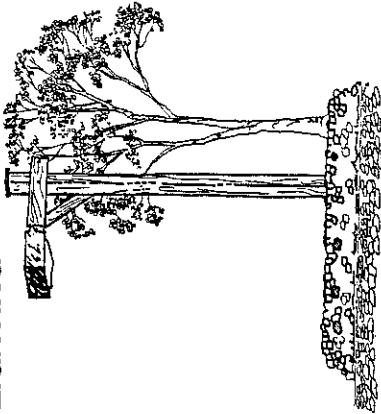
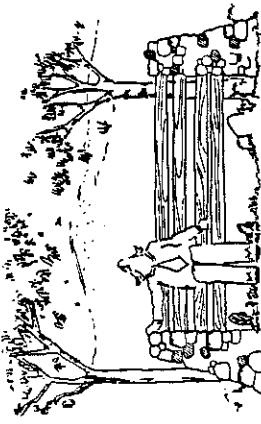
It is suggested that a Village Logo become an integral and essential component of the streetscape. It is recommended that the City Council sponsor a contest to develop such a logo. The City could then select from a number of good ideas for a logo rather than utilizing the example in this booklet. When the new logo is selected it should be displayed throughout the village area, in addition to the main entry signs.

VILLAGE IDENTIFICATION MATRIX

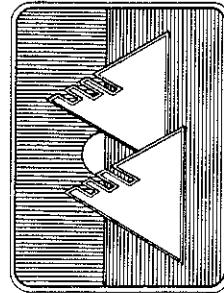
FENCE

BENCH

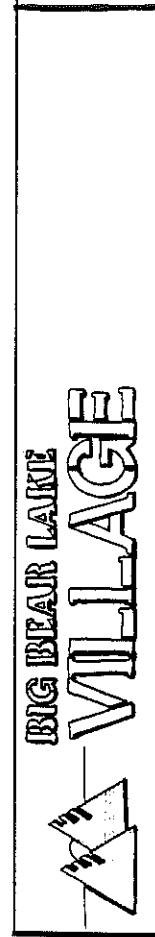
LIGHTING



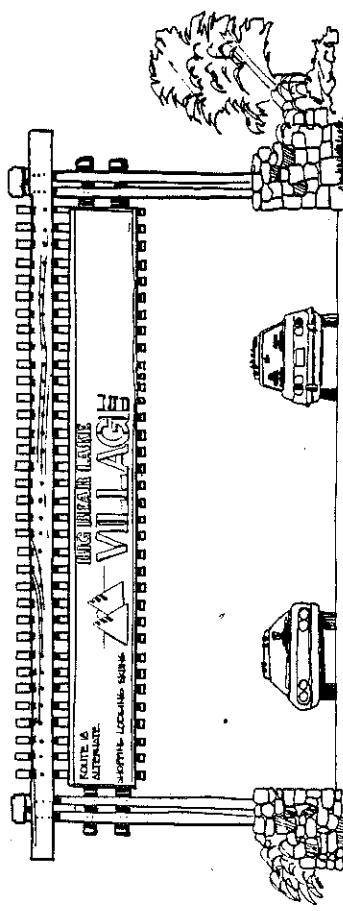
LOGO



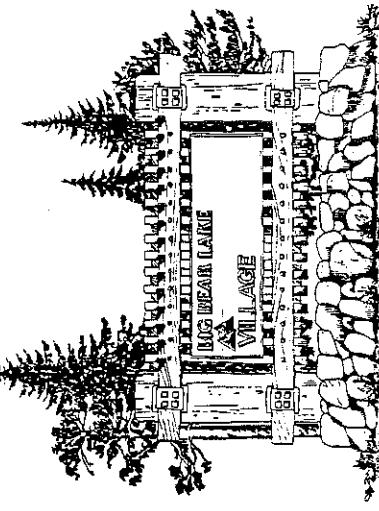
VILLAGE CENTER IDENTIFICATION PROTOTYPE



PRIMARY VILLAGE ENTRY SIGN



SECONDARY VILLAGE ENTRY SIGN



In advertising, flags, correspondence, and newsletters the logo should become a easily recognizable symbol of the Village area.

The logo design for the Village should be a graphic reminder of the the mountain location of the Village and be different enough from the City of Big Bear Lake's official seal.

Village Entry Signs

The entry sign for the Village area plays two very important roles, 1) the architectural elements of the sign set the appropriate architectural image for the Village and 2) attract tourists and visitors away from the relocated State Highway 18 and into the shopping and lodging environment of the Village.

Street Lighting

Lighting will serve an essential function in the Village Center by enabling the area to be used at night. In addition, it serves a number of other purposes, each of which should be considered when determining the adequacy of the existing or proposed lighting for the Village. Lighting can and does make a significant contribution in:

- o providing for the safe movement of traffic, both vehicular and pedestrian;

It is important to realize that an outdoor pole or luminaire performs its function as a viewed object and an element in the Villagescape 24 hours every day, but it performs its function as a source of light only during the night hours. It is crucial to understand that an important design implication flows from this distinction - that the lighting must be rural in character (avoiding flatland standards) in order to maintain the Village Center's overall character.

Benches

- o providing security and crime prevention; and
 - o enhancing the residents appreciation of the neighborhood at night.
- Pedestrian movement can be optimized with much lower light standards and a more viable level of illumination (so long as potential lurking places are well lit).

- o Select materials which do not retain heat or cold
- o Sitting heights of 18"-20" are preferable
- o Provisions for arm and back rests increase comfort
- o Seat surface should be pitched to shed water

- o Provide a 24" setback to keep legs from interrupting adjacent pedestrian traffic
- o Wood and native stone should be included in the design if possible

Crosswalk Paving

- o Select materials which are differently colored than typical asphalt
- o The surface should provide a non-slip finish/texture
- o Texture should not be so great that people turn their ankles and injure themselves when walking on it
- o A unique pattern in the crosswalk material should be developed. It is recommended that four by fours (4x4s) be taken, and their ends be sandblasted which leaves an interesting woodgrain appearance. It is then recommended that these 4x4 ends "stamp" the concrete into a stock/bond arrangement.

RESOLUTION NO. PC2006-31

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF BIG BEAR LAKE, COUNTY OF SAN BERNARDINO, STATE OF CALIFORNIA, CLARIFYING EXISTING LIMITING LANGUAGE GOVERNING THE ESTABLISHMENT OF ADMINISTRATIVE/PROFESSIONAL AND PERSONAL SERVICE USES WITHIN THE VILLAGE SPECIFIC PLAN AND FINDING THE ACTION EXEMPT FROM THE CALIFORNIA ENVIRONMENTAL QUALITY ACT.

A. RECITALS

- (i) The City of Big Bear Lake is a four-season resort community where tourists visit to escape everyday urban life in order to enjoy a unique mountain experience, including recreation activities and unique "small-town" mountain resort shopping opportunities.
- (ii) According to the Big Bear Lake General Plan, the local economy is primarily based on tourism, with the summer and winter months being the most heavily visited seasons. Therefore, a guiding principle of the City of Big Bear Lake is to encourage a year-round, well-balanced economic base while recognizing the importance of tourism in the local economy.
- (iii) Goals and Objectives of the VSP include, but are not limited to, improving the gross retail sales environment for all commercial uses within the Village; attracting more people to the Village and encouraging more lodging and eating in this area; and re-establishing commercial areas along major travel routes and high volume pedestrian ways.
- (iv) General Plan Policy L 2.1 which calls for the promotion of the Village district as the primary tourist-oriented shopping and entertainment district within the Valley, through continued implementation of the Village Specific Plan.
- (v) Due to Big Bear Lake's status as a resort community that offers unique small-town, mountain resort shopping opportunities, the City desires to maintain the small-town mountain character, spirit, and quality of life of the City Big Bear Lake.
- (vi) Pursuant to Section 15061(b)(3) of the California Environmental Quality Act (CEQA), this project is covered by the general rule that CEQA applies only to projects that have the potential for causing a significant effect on the environment. Where it can be seen with certainty that there is no possibility that the activity in question may have a significant effect on the environment, then activity is not subject to CEQA. The mere establishment of language that clarifies existing provisions within the Village Specific Plan will not have a

direct significant effect on the environment and is, therefore, exempt from the provisions of CEQA.

- (vii) On July 5, 2006, August 16, 2006, September 6, 2006, and September 20, 2006, the Planning Commission conducted duly noticed public meetings in Hofert Hall located at 39707 Big Bear Boulevard, City of Big Bear Lake, California and concluded said meeting on September 20, 2006.
- (v) All legal prerequisites to the adoption of this Resolution have occurred.

B. RESOLUTION

NOW THEREFORE, it is hereby found, determined, and resolved by the Planning Commission of the City of Big Bear Lake, as follows:

1. The Planning Commission hereby specifically finds that all of the facts set forth in the Recitals, part A of this Resolution, are true and correct.
2. The Planning Commission hereby finds this action exempt from the California Environmental Quality Act (CEQA) pursuant to Section 15061(b)(3) of the CEQA Guidelines.
3. Based upon substantial evidence presented to the Planning Commission the above-referenced public meetings including public testimony, and written and oral staff reports, the Planning Commission specifically finds as follows:
 - a. The Big Bear Village, specifically the Village L, is the primary tourist-oriented shopping and entertainment district within the Big Bear Valley. The Village has become a major destination point for vacationers and tourists because of its proximity to Big Bear Lake, and a concentration of tourist-related retail businesses, theaters, restaurants, events, attractions, and accommodations for vacationers. A main objective of the Village Specific Plan is to maintain and enhance the pedestrian-orientation of this commercial area through streetscape and traffic calming improvements including wide sidewalks, decorative pavement, plentiful pedestrian crosswalks, landscaped planters, street trees, seat walls and benches, decorative street lamps, and wood-carved street signs. Two public parking lots have been acquired and improved by the City and public restrooms installed to encourage pedestrian use of the Village. Therefore, the Commission finds that it is appropriate to clarify existing language within the Village Specific Plan as it relates to Administrative/Professional and Personal Service uses in the Village L to promote pedestrian oriented activities and uses.
 - b. The goals of the Village Specific Plan include, but are not limited to, improving the gross retail sales environment for the commercial uses within the Village; attracting more people to the Village and encouraging more lodging and eating in this area; improving the visual character of Village; and develop an economically viable business climate. Its stated intent as

contained in the General Plan is, amongst other things, to enhance the character of the marketplace, preserve valuable scenic and open space amenities, and to enhance the City's nature as a resort destination. Since adoption of the Village Specific Plan, significant progress has been made in implementing these objectives. A Bavarian/Alpine theme has been incorporated into the architecture of many of the buildings and existing buildings have been rehabilitated with façade improvements and upgraded sign programs. Land use regulations within the plan restrict more intensive commercial uses from locating in the Village area, to preserve its visitor-serving and pedestrian-oriented character. As such, it is imperative that its pedestrian oriented character be maintained so that a proliferation of Administrative/Professional and Personal Service uses do not detract from the economic viability of uses within the Village L. Therefore, the Commission finds that it is reasonably related to public health and welfare and beneficial at this time to clarify existing language within the Village Specific Plan as it relates to Administrative/Professional and Personal Service uses in the Village L to promote pedestrian oriented activities and uses.

- c. Clarifying existing language within the Village Specific Plan as it relates to the establishment of Administrative/Professional and Personal Service uses within the Village L promulgates General Plan Policy L 2.1 which calls for the promotion of the Village district as the primary tourist-oriented shopping and entertainment district within the Valley, through continued implementation of the Village Specific Plan.
4. Based on the findings set forth in the paragraphs 1, 2, and 3 above, the Planning Commission hereby clarifies existing limiting language contained within the Section V.E.1.F (Permitted Uses by Chart District) of the Village Specific Plan, attached hereto as Exhibit 1.

5. The Secretary of the Planning Commission shall certify to the adoption of this Resolution.

PASSED, APPROVED AND ADOPTED this 20th day of September, 2006

AYES: ZIMMERMAN, SMITH, HACKNEY
NOES: NONE
ABSENT: CARETTO
ABSTAIN: BRIGHAM

9-28-06
Date:


Rick Hackney, Vice Chair

ATTEST:



Lynne Rayner
Planning Commission Secretary

STATE OF CALIFORNIA)
COUNTY OF SAN BERNARDINO) ss
CITY OF BIG BEAR LAKE)

I, Lynne Rayner, Commission Secretary of the City of Big Bear Lake, California, do hereby certify that the whole number of the Planning Commission of the said City is five; that the foregoing Resolution, being **Resolution No. PC2006-31** as duly passed and adopted by the said Planning Commission and attested by the Commission Secretary of said City, all at a regular meeting of the said City held on the **20th day of September, 2006**, and that the same was so passed and adopted by the following vote:

AYES: ZIMMERMAN, SMITH, HACKNEY
NOES: NONE
ABSENT: CARETTO
ABSTAIN: BRIGHAM

Witness my hand and the official seal of said City this 20th day of Sept, 2006.

Lynne Rayner

Lynne Rayner
Planning Commission Secretary

Exhibit 1

The following shall apply to Section V.E.1.F (Permitted Use by District Chart), as it pertains to Administrative/Professional and Personal Services land uses:

“Retail complex” and “retail project” as used in Section V.E.1.F (Permitted Uses by District Chart) of the Village Specific Plan shall be defined to mean a structure consisting of a minimum of three (3) tenant spaces within the Village Retail (VR) and Commercial Lodging (CL) zone districts located along Village Drive between Paine Road and Pine Knot Avenue, and on Pine Knot Avenue between Big Bear Boulevard and Cameron Drive, and shall mean a structure with a minimum of two tenant spaces in all other instances.

Further, as used in Section V.E.1.F (Permitted Uses by District Chart), permitted uses listed under the Administrative/Professional and Personal Services land use categories denoted by an asterisk and located on Village Drive and Pine Knot Avenue, as described above, shall only be permitted on the second and third floor levels of a building, or within non-ground floor street-frontage portions of a building.

For the purposes of this provision, ground floor street frontage shall mean the first floor of a building that is immediately adjacent to and accessible from ground level, and immediately adjacent to a street, within a tenant space any portion of which is facing the street.