

Introduced and amended by the Land Use and Zoning Committee and amended on the floor by the Council:



Attach.tif

ORDINANCE 2009-157-E

AN ORDINANCE REZONING APPROXIMATELY 363.21 ACRES OF LAND LOCATED IN COUNCIL DISTRICT 13 ON THE EAST SIDE OF PHILIPS HIGHWAY ACROSS FROM ST. AUGUSTINE ROAD BETWEEN STATE ROAD 9A AND THE COUNTY LINE AND OWNED BY VARIOUS INDIVIDUALS AND CORPORATE ENTITIES AS LISTED IN EXHIBIT 4 ATTACHED HERETO, AS MORE PARTICULARLY DESCRIBED HEREIN, FROM RESIDENTIAL LOW DENSITY-60 (RLD-60), RESIDENTIAL RURAL-ACRE (RR-ACRE), COMMERCIAL OFFICE (CO), COMMERCIAL COMMUNITY GENERAL-2 (CCG-2), PLANNED UNIT DEVELOPMENT (PUD), INDUSTRIAL BUSINESS PARK (IBP), COMMERCIAL COMMUNITY GENERAL-1 (CCG-1), AND INDUSTRIAL LIGHT (IL) DISTRICTS TO PLANNED UNIT DEVELOPMENT (PUD) DISTRICT, AS DEFINED AND CLASSIFIED UNDER THE ZONING CODE, FOR MIXED USES ACCORDING TO THE COMMUNITY OF BAYARD SMART CODE, AS DESCRIBED IN THE APPLICATION FOR THE COMMUNITY OF BAYARD SMART CODE PUD, PURSUANT TO ADOPTED FUTURE LAND USE MAP SERIES (FLUMS) SEMI-ANNUAL LAND USE AMENDMENT APPLICATION NUMBER 2008D-021; PROVIDING AN EFFECTIVE DATE.

WHEREAS, the City of Jacksonville adopted a semi-annual land use amendment to the *2010 Comprehensive Plan* for the purpose of revising portions of the Future Land Use Map series (FLUMs) in order to ensure the accuracy and internal consistency of the plan, pursuant to

1 Ordinance 2009-156; and

2 **WHEREAS**, in order to ensure consistency of zoning district(s)
3 with the *2010 Comprehensive Plan* and the companion adopted semi-annual
4 land use amendment 2008D-021, an application to rezone and reclassify
5 from Residential Low Density-60 (RLD-60), Residential Rural-Acre (RR-
6 Acre), Commercial Office (CO), Commercial Community General-2 (CCG-2),
7 Planned Unit Development (PUD), Industrial Business Park (IBP),
8 Commercial Community General-1 (CCG-1), and Industrial Light (IL)
9 Districts to Planned Unit Development (PUD) District was filed by City
10 of Jacksonville regarding property owned by the various individuals and
11 corporate entities as listed in **Exhibit 4, attached hereto**, as more
12 particularly described in Section 1 and referenced therein as the
13 "Subject Property"; and

14 **WHEREAS**, the Planning and Development Department, in order to
15 ensure consistency of this rezoning with the *2010 Comprehensive Plan*
16 and the companion semi-annual land use amendment application number
17 2008D-021, has considered the rezoning and has rendered an advisory
18 opinion; and

19 **WHEREAS**, the Planning Commission has considered the application
20 and has rendered an advisory opinion; and

21 **WHEREAS**, the Land Use and Zoning (LUZ) Committee after due notice
22 held a public hearing and made its recommendation to the Council; and

23 **WHEREAS**, the City Council after due notice, held a public
24 hearing; and

25 **WHEREAS**, taking into consideration all oral and written comments
26 received during public hearings and the above recommendations, the
27 Council finds that such rezoning is consistent with the *2010*
28 *Comprehensive Plan* adopted under the comprehensive planning ordinance
29 for future development of the City of Jacksonville; and

30 **WHEREAS**, the Council finds that the proposed PUD does not affect

adversely the orderly development of the City as embodied in the *Zoning Code*; will not affect adversely the health and safety of residents in the area; will not be detrimental to the natural environment or to the use or development of the adjacent properties in the general neighborhood; and the proposed PUD will accomplish the objectives and meet the standards of Section 656.340 (Planned Unit Development) of the Zoning Code of the City of Jacksonville; now, therefore

BE IT ORDAINED by the Council of the City of Jacksonville:

Section 1. Subject Property Location and Description. The approximately 363.21 acres of land are located in Council District 13 on the east side of Philips Highway across from St. Augustine Road between State Road 9A and the county line, as more particularly described in **Exhibit 1** and graphically depicted in **Exhibit 2**, both **attached hereto** and incorporated herein by this reference.

Section 2. Owner and Applicant Description. The Subject Property is owned by various individuals and corporate entities as listed in **Exhibit 4**, **attached hereto**. The applicant listed in the application is City of Jacksonville with an address of 128 East Forsyth Street, Suite 500, Jacksonville, Florida 32202 and a telephone number of (904) 630-5985.

Section 3. Property Rezoned. The Subject Property, pursuant to adopted companion semi-annual land use amendment 2008D-021, is rezoned and reclassified from Residential Low Density-60 (RLD-60), Residential Rural-Acre (RR-Acre), Commercial Office (CO), Commercial Community General-2 (CCG-2), Planned Unit Development (PUD), Industrial Business Park (IBP), Commercial Community General-1 (CCG-1), and Industrial Light (IL) Districts to Planned Unit Development (PUD) District, subject to the written description dated January 12, 2009 and the site plan dated April 23, 2007 for the Community of Bayard Smart Code PUD, both **attached hereto** as **Exhibit 3**. The PUD District for the

Subject Property shall generally allow for mixed uses, as more specifically shown and described in the written description and site plan.

Section 4. Rezoning Approved Subject to Conditions. This rezoning is approved subject to the following conditions:

(a) Development shall proceed in accordance with the FDOT Memorandum dated February 18, 2009, or as otherwise approved by the Planning and Development Department and FDOT.

(b) **GRANDFATHER CLAUSE:** All existing lawful uses may continue, may be expanded (within the current property boundaries and consistent with all other requirements of the Ordinance Code), and may be transferred under this PUD. If there is a change in use, then the new use shall comply with the new requirements approved herein.

(c) **ARCHITECTURAL STANDARDS AND BUILDING CONFIGURATION STANDARDS OPTIONAL:** All architectural standards and building configuration standards in the attached Written Description and Site Plan are optional and are not mandatory architectural standards and building configuration standards.

(d) **NO MANDATORY PARK LOCATIONS:** All park locations within the Written Description and Site Plan are conceptual only and are not mandatory park locations.

(e) **NO MANDATORY POND LOCATIONS:** All pond locations within the Written Description and Site Plan are conceptual only and are not mandatory pond locations.

(f) **NO MANDATORY ROAD LOCATIONS:** All road locations within the Written Description and Site Plan are conceptual only and are not mandatory road locations, including but not limited to the road locations depicted on or near the following properties: R.E. Nos. 167936-0000 (Carter), 167936-0500 (Carter), 167963-0000 (Harold), 167963-0015 (Harold), 167963-0020 (Wilson), 167963-0030 (Wilson).

(g) **SERVICE ROAD DELETED:** The proposed service road identified in the Written Description and Site Plan is deleted.

(h) **NO MANDATORY SIGNS LOCATIONS:** All sign locations and standards in the attached Written Description and Site Plan are optional and are not mandatory sign locations and standards. All sign regulations effective as of the effective date of this ordinance may still apply.

(i) **NO MANDATORY SETBACKS:** All setback standards in the attached Written Description and Site Plan are optional and are not mandatory setback standards. All setback regulations effective as of the effective date of this ordinance may still apply.

Section 5. Consistency With Companion Land Use Amendment Adoption. The Council hereby finds the PUD to be consistent with the requirements of the State Comprehensive Plan, the Northeast Florida Regional Council Strategic Regional Policy Plan, and Rule Chapter 9J-5, Florida Administrative Code. Further, the Council finds this rezoning to be consistent with the Jacksonville 2010 Comprehensive Plan, as amended by Ordinance 2009-156-E, and that this PUD is consistent with the land use category criteria.

Section 6. Contingency. This rezoning shall not become effective unless and until the Department of Community Affairs issues a Notice of Intent finding the correlating amendment to the 2010 *Comprehensive Plan* in compliance with Chapter 163, Part II, Florida Statutes.

Section 7. Effective Date. The adoption of this ordinance shall be deemed to constitute a quasi-judicial action of the City Council and shall become effective upon signature by the Council President and Council Secretary.

Form Approved:

/s/ Shannon K. Eller
Office of General Counsel

Legislation Prepared By: Robert K. Riley

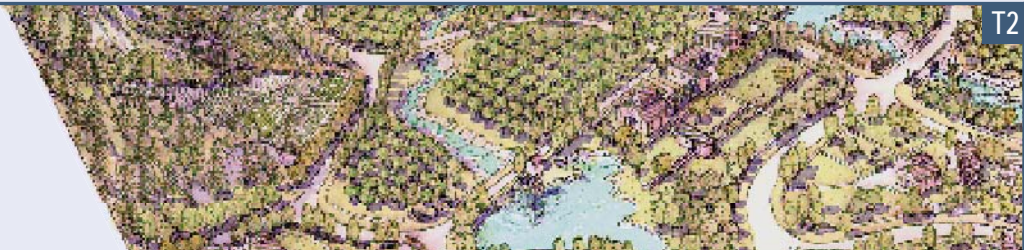
G:\SHARED\LEGIS.CC\2009\ord\SEMI ANNUALS\2009-157-E.doc

Community of Bayard

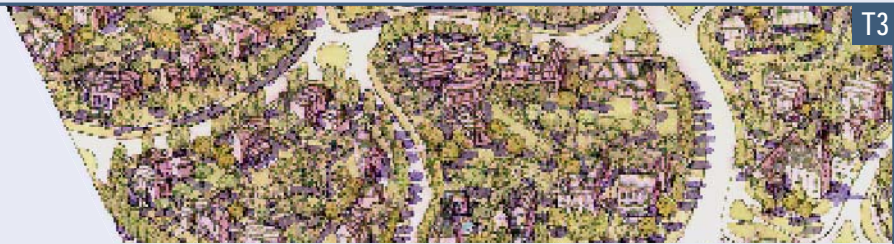
SMARTCODE



T1



T2



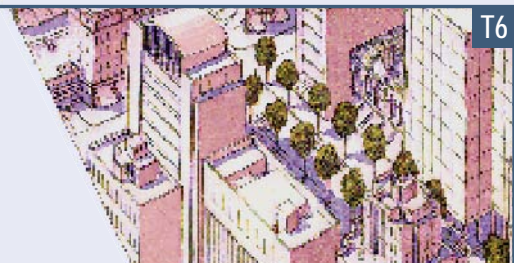
T3



T4



T5



T6

City of Jacksonville, Florida

DRAFT CODE - Subject to review and approval by the City Dept. of Planning & Development,
City Attorney, Planning Commission, Land Use & Zoning Committee and the City Council

June 1, 2007

ACKNOWLEDGMENTS

Credit: Duany Plater-Zyberk & Co.

The Bayard Community Plan is based on SmartCode Manual text and diagrams developed by Duany Plater-Zyberk & Co. The SmartCode Manual's planning and design tools provided the basis for establishing the future neighborhood context, character and quality of life for Bayard's future residents and businesses.

ARTICLE 1. GENERAL TO ALL PLANS

- 1.1 AUTHORITY
- 1.2 INTENT
- 1.3 APPLICABILITY
- 1.4 PROCESS
- 1.5 WARRANTS AND VARIANCES

ARTICLE 2. SECTOR SCALE PLANS

[RESERVED]

ARTICLE 3. NEW COMMUNITY SCALE PLANS

- 3.1 INSTRUCTIONS
- 3.2 TRANSECT ZONES
- 3.3 COMMUNITY TYPES
- 3.4 DENSITY CALCULATIONS
- 3.5 ENVIRONMENTAL REQUIREMENTS
- 3.6 STREETSCAPE REQUIREMENTS
- 3.7 CIVIC FUNCTIONS
- 3.8 SPECIAL REQUIREMENTS

ARTICLE 4. INFILL COMMUNITY SCALE PLANS

- 4.1 INSTRUCTIONS
- 4.2 TRANSECT ZONES
- 4.3 COMMUNITY TYPES
- 4.4 CIVIC FUNCTIONS
- 4.5 SPECIAL REQUIREMENTS
- 4.6 PRE-EXISTING CONDITIONS

ARTICLE 5. BUILDING SCALE PLANS

- 5.1 INSTRUCTIONS
- 5.2 SPECIFIC TO T1 & T2 ZONES
- 5.3 SPECIFIC TO T3 ZONES
- 5.4 SPECIFIC TO T4 ZONES
- 5.5 SPECIFIC TO T5 ZONES
- 5.6 SPECIFIC TO T6 ZONES [RESERVED]
- 5.7 CIVIC FUNCTIONS
- 5.8 SPECIAL REQUIREMENTS
- 5.9 PRE-EXISTING CONDITIONS

ARTICLE 6. STANDARDS & TABLES

- TABLE 1 TRANSECT ZONE DESCRIPTIONS
- TABLE 2 SECTOR/COMMUNITY ALLOCATION
- TABLE 3A VEHICULAR LANE DIMENSIONS
- TABLE 3B VEHICULAR LANE/PARKING ASSEMBLIES
- TABLE 3C THOROUGHFARE ASSEMBLIES
- TABLE 4A PUBLIC FRONTAGES - GENERAL
- TABLE 4B PUBLIC FRONTAGES - SPECIFIC
- TABLE 5 PUBLIC LIGHTING
- TABLE 6 PUBLIC PLANTING
- TABLE 7 PRIVATE FRONTAGES
- TABLE 8 BUILDING CONFIGURATION
- TABLE 9 BUILDING TYPE
- TABLE 10 BUILDING FUNCTION - SPECIFIC
- TABLE 11 BUILDING FUNCTION - GENERAL
- TABLE 12 PARKING CALCULATION
- TABLE 13 CIVIC SPACE
- TABLE 14 SMARTCODE SUMMARY
- TABLE 15 SUMMARY OF SPECIAL DISTRICTS
[RESERVED]
- TABLE 16 DEFINITIONS ILLUSTRATED

ARTICLE 7. DEFINITIONS OF TERMS**MAPS**

- MAP 1 TRANSECT ZONES
- MAP 2 SPECIAL REQUIREMENTS MAP
- MAP 3 THOROUGHFARE TYPES
- MAP 4 ILLUSTRATIVE PLAN

APPENDIX

- US-1 BOULEVARD SECTION AND ILLUSTRATIONS

1.1 AUTHORITY

1.1.1 The action of the City of Jacksonville, Florida in the adoption of this Special Area Plan Overlay District Code ("Code") for the Community of Bayard is authorized under:

- (a) the Charter of the City of Jacksonville, Florida, Chapter 92-341,
- (b) Chapter 656, Ordinance Code and Florida Statutes (F.S.) Chapter 163, Part II (Local Government Comprehensive Planning and Land Development Regulation Act (LGCPDRA)).

1.1.2 This Code, as land development regulation, providing both zoning and subdivision standards, is adopted as one of the instruments of implementation of the public purposes and objectives of the City of Jacksonville's adopted 2010 Comprehensive Plan. This Code and the Transect Map are declared to be in accord with the City of Jacksonville's 2010 Comprehensive Plan, is pursuant to Chapter 656, Ordinance Code and Florida Statutes (F.S.) Chapter 163, Part II (Local Government Comprehensive Planning and Land Development Regulation Act (LGCPDRA)), and is deemed necessary to promote the health, safety and welfare of the City of Jacksonville and its residents. This Code shall apply to, and be in full force and effect only within the boundaries of the Bayard Community Plan. The Community Plan created under this Code defines the New and Infill Communities as represented in the Transect Map.

1.1.3 This Code and Community Plan is to be adopted and amended by vote of the City Council of Jacksonville, Florida.

1.1.4 The adoption of this Code and Community Plan is to promote the health, safety, morals and general welfare of the City of Jacksonville, Florida, and its citizens, including without limitation, to regulate the use of land and buildings, energy and natural resources, reduction in vehicular traffic congestion, more efficient use of public funds, health benefits of pedestrian environment, historic preservation, education and recreation, reduction in sprawl development, improvement of the built environment and human habitat, and to implement the Comprehensive Plan adopted pursuant to Chapter 656, Ordinance Code and Florida Statutes (F.S.) Chapter 163, Part II (Local Government Comprehensive Planning and Land Development Regulation Act (LGCPDRA)).

1.2 INTENT

The purpose of this Code is to enable and encourage development within the City of Jacksonville and qualify the implementation of the following policies:

1.2.1 The Region

- a. That the region should retain its natural infrastructure and visual character derived from topography, woodlands, farmlands, riparian corridors and coastlines.
- b. That growth strategies should encourage Infill and redevelopment in parity with new communities.
- c. That development contiguous to urban areas should be structured in the Neighborhood pattern and be integrated with the existing urban pattern.
- d. That development non-contiguous to urban areas should be organized in the pattern of clusters, traditional Neighborhoods or Villages, and Regional Centers.

- e. That affordable housing should be distributed throughout the area to match job opportunities and to avoid concentrations of poverty.
- f. That transportation corridors should be planned and reserved in coordination with land use.
- g. That green corridors should be used to define and connect the urbanized areas.
- h. That the region should include a framework of transit, pedestrian, and bicycle systems that provide alternatives to the automobile.

1.2.2 **The Community**

- a. That Neighborhoods, Regional Centers and Town Centers be compact, pedestrian-oriented and mixed-use.
- b. That Neighborhoods, Regional Centers and Town Centers be the preferred pattern of development and that districts specializing in single-use should be the exception.
- c. That ordinary activities of daily living should occur within walking distance of most dwellings, allowing independence to those who do not drive.
- d. That interconnected networks of Thoroughfares should be designed to disperse and reduce the length of automobile trips.
- e. That within Neighborhoods, a range of housing Types and price levels should be provided to accommodate diverse ages and incomes.
- f. That appropriate building Densities and land uses should be provided within walking distance of transit stops.
- g. That Civic, institutional, and Commercial activity should be embedded in Downtowns, Regional Centers and Town Centers, not isolated in remote single-use complexes.
- h. That schools should be sized and located to enable children to walk or bicycle to them.
- i. That a range of open space including parks, squares, and playgrounds should be distributed within Neighborhoods and urban zones.

1.2.3 **The Block and the Building**

- a. That buildings and landscaping should contribute to the physical definition of Thoroughfares as Civic places.
- b. That development should adequately accommodate automobiles while respecting the pedestrian and the spatial form of public space.
- c. That the design of streets and buildings should reinforce safe environments, but not at the expense of accessibility.
- d. That architecture and landscape design should grow from local climate, topography, history, and building practice.
- e. That buildings and landscape design should provide their inhabitants with a clear sense of geography and climate through energy efficient methods.
- f. That Civic Buildings and public gathering places should be provided locations that reinforce community identity and support self-government.
- g. That Civic Buildings should be distinctive and appropriate to a role more important than the other buildings that constitute the fabric of the city.
- h. That the preservation and renewal of historic buildings should be facilitated to affirm the continuity and evolution of society.

- i. That the harmonious and orderly evolution of urban areas should be secured through graphic codes that serve as guides for change.

1.3 APPLICABILITY

- 1.3.1 Provisions of this Code are activated by "shall" when required; "should" when recommended; and "may" when optional.
- 1.3.2 The provisions of this Code, when inconsistent or in conflict, shall take precedence over those of other codes, ordinances, regulations and standards except the Local Health and Safety, Building and Fire Codes.
- 1.3.3 The City of Jacksonville's Zoning Code, Chapter 656, Land Development Procedures Manual, and Subdivision Ordinances (the "Existing Local Codes") shall continue to be applicable to issues not covered by this Code except where the Existing Local Codes would be in conflict with Section 1.2, Intent, in which case the conflict shall be resolved in favor of this Code. The Existing Local Codes shall be used for regulating Grandfathered Property.
- 1.3.4 Terms used throughout this Code shall be accorded their commonly accepted meanings or as defined in Articles 1-6 hereof or in the Definitions of Terms (Article 7). In the event of conflicts between these definitions and those of the Existing Local Codes, those of this Code shall take precedence.
- 1.3.5 The Definitions of Terms (Article 7) contains regulatory language that is integral to this Code. Capitalized terms in Articles 1-6 of this Code may refer to Article 7 Definitions.
- 1.3.6 In the event of any conflict or inconsistency between the provisions of Section 3.5, Environmental Requirements, and any applicable Local, State or Federal law, rule or regulation governing similar subject matter, such Local, State or Federal law, rule or regulation shall control.

1.4 PROCESS

- 1.4.1 The Transect Map represents the Community Plan prepared for the Community of Bayard which is comprised of New and Infill Communities (defined by extent and intensity in Articles 3, New Community Scale Plans, and 4, Infill Community Scale Plans) which are comprised of Transect Zones (defined by the elements appropriate to them in Article 5, Building Scale Plans, and in Article 6, Standards & Tables).
- 1.4.2 The standards for community types and the standards for each Transect Zone shall be determined through a process of public consultation with approval by the Jacksonville City Council. Once these determinations have been incorporated into this Code and the associated plans, projects that require no Variances or require Warrants, only shall be processed administratively without further recourse to public consultation.
- 1.4.3 The City of Jacksonville Department of Planning and Development (the "Planning Office") shall include a Consolidated Review Committee (CRC) comprised of a representative from each of the various regulatory agencies that have jurisdiction over the permitting of a project, and if possible, a representative of the Bayard Community. The CRC shall expedite the permitting process by providing a single interface between the developer and the agencies. The CRC shall develop the

necessary forms, applications, and procedures to properly administer the provisions of this Code, and may from time-to-time amend the said forms, applications and procedures as needed to effectively and efficiently carry out these provisions.

- 1.4.4 An owner or developer may appeal a decision of the CRC to the Jacksonville Planning Commission (the "Planning Commission") and may appeal a decision of the Planning Commission to the City Council.

- 1.4.5 Should a violation of an approved plan occur during construction, the Director of the Planning Office has the right to require the owner or developer to stop, remove, and/or mitigate the violation, or to require the owner or developer to secure a Variance to cover the violation.

- 1.4.6 Upon approval by the CRC, a New Community Plan or Infill Community Plan shall, upon request in writing submitted to the CRC by the owner or owner's agent, be placed on the next available agenda for final plat approval by the Jacksonville City Council.

1.5 WARRANTS AND VARIANCES

- 1.5.1 There shall be two levels of deviation from the requirements of this Code: Warrants and Variances. Whether a deviation requires a Warrant or Variance shall be determined by the CRC.

- 1.5.2 A Warrant is a ruling that would permit a practice that is not consistent with a specific provision of this Code, but is justified by its Intent (Section 1.2). The CRC shall have the authority to provide an Administrative Decision to approve or disapprove a request for a Warrant.

- 1.5.3 A Variance is any ruling on a deviation other than a Warrant. Variances shall be granted only in accordance with the procedure set out in Section 156.131(c) by the City of Jacksonville's City Council and Planning Commission.

- 1.5.4 The request for a Variance shall not subject the entire application to public hearing, but only that portion necessary to rule on the issue under consideration.

- 1.5.5 The following standards and requirements shall not be available for Warrants or Variances:

- a. The allocation ratios of each Transect Zone.
- b. The maximum dimensions of traffic lanes.
- c. The required provision of Alleys and Rear Lanes.
- d. The minimum Residential Densities.
- e. The permission to build ancillary apartments.
- f. The requirements of parking location.

1.6 INCENTIVES

- 1.6.1 [RESERVED]

RESERVED

3.1 INSTRUCTIONS

- 3.1.1 Article 3 shall be available as an optional overlay by right pursuant to the requirements set forth in Sections 3.2 and 3.3. This Article shall be applied in its entirety or not at all. Within the area of the Transect Map this Code replaces and takes precedence over the zoning, subdivision and development standards contained in the City of Jacksonville Zoning Code, Chapter 656, and its Land Development Procedures Manual. The Existing Local Codes shall remain available by right for application to Grandfathered Property.
- 3.1.2 [RESERVED]
- 3.1.3 New Community Plans may be prepared by a property owner, a developer, or by the City of Jacksonville, and they shall comply with this Article 3 and this Code. A "New Community Plan" means an Amendment to the Bayard Community Plan.
- 3.1.4 New Communities of the Types planned according to the provisions of this Code shall be approved administratively by the Consolidated Review Committee (CRC), in conjunction with obtaining any required approvals applicable to Local, State or Federal law, rule or regulation pertaining to permitting of a development plan.
- 3.1.5 [RESERVED]
- 3.1.6 [RESERVED]
- 3.1.7 New Community Plans shall comply with the Code and Transect Map, and shall respond to the existing conditions of the site, adjacent developments, connecting Thoroughfares, natural features and man-made traces, as determined by the CRC.
- 3.1.8 New Community Plans, according to its Type, and responding to existing conditions, shall be structured as one or several Pedestrian Sheds as specified in Section 3.3.
- 3.1.9 New Community Plans shall allocate the Transect Zones and Densities as specified in Sections 3.2 and Tables 2 and 14, while accommodating the environmental requirements as specified in Section 3.5.
- 3.1.10 Remnants of the site outside the Pedestrian Sheds may be Warranted as Natural Zones (T1), Rural Zones (T2), Sub-Urban Zones (T3) or as Civic Space (CS). (See Section 3.3)
- 3.1.11 New Community Plans shall lay out the Thoroughfare network according to the provisions of Section 3.6 and Tables 3A and B.
- 3.1.12 New Community Plans shall allocate the Civic Functions according to Section 3.7.
- 3.1.13 New Community Plans shall be detailed with the Special Requirements described in Section 3.8.
- 3.1.14 [RESERVED]
- 3.1.15 New Community Plans shall include a set of building standards prepared in accordance with the requirements of Article 5.

3.2 TRANSECT ZONES

- 3.2.1 Transect Zones shall be constituted of the Transect 1 through Transect 5 elements described in Table 1 and the standards summarized in Table 14, and the geographic

location of the Transect Zones shall be established by the Transect Map as adopted and amended by the City Council.

3.3 COMMUNITY TYPES

3.3.1 [RESERVED]

3.3.2 Traditional Neighborhood Development (TND)

- a. TNDs shall be permitted by right for New Community Plans of at least 40 contiguous acres within the G-3 Intended Growth Sector, and the G-4 Infill Growth Sector, as specified in Table 2, Sector/Community Allocation. The simultaneous planning of larger and adjacent parcels is encouraged.
- b. A TND may be comprised of a partial or entire Standard Pedestrian Shed (1/4 mile radius) or more than one Standard Pedestrian Shed, each with the individual Transect Zone requirements of a TND as specified in Tables 2 and 14A.

3.3.3 Regional Center Development (RCD)

- a. Regional Center Developments (RCD) shall be permitted by right within G-3 Intended Growth Sector and G-4 Infill Growth Sector for New Community Plans of at least 160 contiguous acres.
- b. The minimum developable area of a site to be planned as an RCD shall be 160 acres. The simultaneous planning of larger and adjacent parcels is encouraged.
- c. An RCD shall be limited to one Long Pedestrian Shed (1/2 mile radius) including T4 and T5 Zones as specified in Table 14A, and may be adjoined without buffers by one or several partial or entire Standard Pedestrian Sheds, each with the individual Transect Zone requirements of an RCD as specified in Tables 2 and 14A.

3.3.4 Transit-Oriented Development (TOD)

- a. An RCD that is on an existing or projected transit network shall be redesignated as a TOD and made subject to the additional Density shown in Table 14A and calculated in accordance with Section 3.4.

3.4 DENSITY CALCULATIONS

3.4.1 The Developable Areas of the site shall be considered the Net Site Area. The Net Site Area shall be allocated to the various Transect Zones according to the parameters specified in Table 14A.

3.4.2 The Overall Density shall be calculated in terms of housing units as specified for the area of each Transect Zone by Table 14B. For purposes of Density calculation, the Transect Zone Areas include the Thoroughfares but not land allocated to Civic Function.

3.4.3 [RESERVED]

3.4.4 [RESERVED]

3.4.5 The housing and other Functions for each Transect Zone shall be further adjusted at the building scale according to Article 5.

3.5 ENVIRONMENTAL REQUIREMENTS

3.5.1 General

- a. Transect Zones manifest a range of natural and urban conditions. In case of conflict, to the extent not inconsistent with applicable Local, State and Federal

law, the natural environment shall have priority in the more rural zones (T1-T3) and the built environment shall have priority in the more urban zones (T4-T5).

3.5.2 Specific to Natural and Rural Zones (T1-T2)

a. Within the T1 Zone the encroachment and modification of natural conditions, including,

1. Surface Waterbodies
2. Protected Wetlands
3. Protected Habitat
4. Riparian Corridors
5. Purchased Open Space
6. Conservation Easements
7. Transportation Corridors

shall be limited according to applicable Local, State and Federal law. Within the T2 Zone the encroachment and modification of natural conditions, including,

1. Flood Plain
2. Steep Slopes
3. Open Space to be Acquired
4. Corridors to be Acquired
5. Buffers to be Acquired
6. Legacy Woodland
7. Legacy Viewsheds

shall be limited according to applicable Local, State and Federal law.

b. The Public Frontage (Tables 4A and 14D) shall include trees of various species, naturalistically clustered, as well as understory trees and shrubs. The introduced landscape shall consist primarily of native species requiring minimal irrigation, fertilization and maintenance (Tables 4B and 6).

c. Impermeable surface shall be minimized and confined to the ratio of lot coverage by building specified in Table 14F.

d. To the extent not inconsistent with applicable Local, State and Federal law, storm water management on Thoroughfares shall be primarily through retention and percolation, channeled by curbside swales.

3.5.3 Specific to Sub-Urban Zones (T3)

a. Within T3 Zones, the continuity of the urbanized areas shall be subject to the precedence of the natural environmental conditions listed in Sections 3.5.2. The alteration of such conditions shall be limited according to Local, State and Federal law.

b. The Public Frontage (Tables 4A and 14D) shall include trees of various species, naturalistically clustered, as well as low maintenance understory trees and shrubs. The introduced landscape should encourage the use of native species requiring minimal irrigation, fertilization and maintenance (Tables 4B and 6).

c. Impermeable surface shall be minimized and confined to the ratio of lot coverage by building specified in Table 14F.

d. To the extent not inconsistent with applicable Local, State and Federal law, storm water management on Thoroughfares shall be primarily through retention and percolation, channeled by curbside swales.

3.5.4 Specific to General Urban Zones (T4)

- a. To the extent not inconsistent with applicable Local, State and Federal law, within T4 Zones, the continuity of the urbanized areas should take precedence over the natural environmental conditions listed in Sections 3.5.2 for T1 and T2 Zones. The alteration of such conditions, where necessary and to the extent not inconsistent with applicable Local, State and Federal law, may be mitigated off-site, and the determination for modification and mitigation shall be made by Warrant.
- b. The Public Frontage (Tables 4A and 14D) shall include trees planted in a regularly-spaced Allee pattern of single or alternated species with shade canopies of a height that, at maturity, clears three stories but remains predominantly clear of building Frontages. The introduced landscape shall consist primarily of durable species tolerant of soil compaction (Tables 4B and 6).
- c. Impermeable surface shall be confined to the ratio of lot coverage by building specified in Table 14F.
- d. To the extent not inconsistent with applicable Local, State and Federal law, storm water management on Thoroughfares and lots shall be primarily through underground storm drainage channeled by raised curbs. Retention or detention may be required on individual lots (excluding individual single-family residential lots) provided a Community master storm water management system has not been developed to accommodate storm water runoff.

3.5.5 Specific to Urban Center Zones (T5)

- a. To the extent not inconsistent with applicable Local, State and Federal law, within T5 Zones, the continuity of the urbanized areas shall take precedence over the natural environmental conditions listed in Sections 3.5.2 for T1 and T2 Zones. The alteration of such conditions, where necessary and to the extent not inconsistent with applicable Local, State and Federal law, may be mitigated off-site, and the determination for modification and mitigation shall be made by Warrant.
- b. The Riparian Corridors of all classes or Waterways may be embanked and crossed by Thoroughfares as required by the Thoroughfare network.
- c. Class I and II Wetlands may be modified if mitigated off-site at a two to one ratio. Thoroughfare crossings shall be allowed By Right.
- d. The Public Frontage (Tables 4A and 14D) shall include trees planted in Allees of a single species with shade canopies of a height that, at maturity, clears three stories but remains predominantly clear of building Frontages. The introduced landscape shall consist primarily of durable species tolerant of soil compaction (Tables 4B and 6).
- e. Impermeable surface shall be confined to the ratio of lot coverage by building specified in Table 14F.
- f. To the extent not inconsistent with applicable Local, State and Federal law, storm water management shall be primarily through underground storm drainage channeled by raised curbs. It is the intent that a master storm water management system will be developed to accommodate all storm water runoff within the Bayard Community. Use of these storm water facilities by individual projects will be provided on a prorata share basis. Retention or detention may be required on individual lots (excluding individual single-family residential lots) if master storm water facilities are not available.

3.5.6 **Specific to Urban Core Zones (T6)**
[RESERVED]

3.6 **STREETSCAPE REQUIREMENTS**

3.6.1 **General**

- a. The Thoroughfares are intended for use by vehicular and pedestrian traffic and to provide access to lots and open spaces.
- b. The Thoroughfares consist of vehicular lanes and Public Frontages (Table 16A). The lanes provide the traffic and parking capacity. They consist of vehicular lanes in a variety of widths for parked and for moving vehicles. The Frontages contribute to the character of the Transect Zone. They include the Types of sidewalk, curbing, planter, and street tree.
- c. Thoroughfares should be designed in context with the urban form and desired design speed of the Transect Zones through which they pass. Thoroughfares that pass from one Transect Zone to another shall adjust their Public Frontages accordingly or, alternatively, the Transect Zone may follow the alignment of the Thoroughfare to the depth of one lot, retaining a single Public Frontage throughout its trajectory.
- d. Within the more rural Zones (T1 through T3) pedestrian comfort shall be a secondary consideration of the Thoroughfare. Design conflict between vehicular and pedestrian movement shall be generally decided in favor of vehicular mobility.
- e. Within the more urban Transect Zones (T4 through T5) pedestrian comfort shall be a primary consideration of the Thoroughfare. Design conflict between vehicular and pedestrian movement shall be decided in favor of the pedestrian.

3.6.2 **Thoroughfares**

- a. The standards for vehicular lanes shall be as shown in Table 3.
- b. The Thoroughfare network shall be designed to define blocks not exceeding the size prescribed in Table 14C. The size shall be measured as the sum of lot Frontage Lines.
- c. All Thoroughfares shall terminate at other Thoroughfares, forming a network. Internal Thoroughfares shall connect wherever possible to those on adjacent sites. Cul-de-sacs shall be permitted only when Warranted by natural site conditions.
- d. Lots shall enfront a vehicular Thoroughfare, except that 20% of the lots within each Transect Zone may enfront a pedestrian passage.
- e. Thoroughfares along a designated Secondary Grid (Section 3.8.1a) may be exempted from the specified Frontage requirements.
- f. A bicycle network consisting of Trails, Routes and Lanes should be provided throughout as defined in Article 7 Definitions of Terms and allocated in Table 14D. The community bicycle network should be connected to existing or proposed networks wherever possible.

3.6.3 **Public Frontages**

- a. Public Frontages shall be designed as shown in Table 4 and allocated within Transect Zones as specified in Table 14D.
- b. Within the Public Frontages, the prescribed Type of street trees and street lights shall be as shown in Tables 4, 5 and 6. The spacing may be adjusted by Warrant to accommodate specific site conditions.

3.6.4 Specific to Special Districts

- a. The standards for Thoroughfares and Public Frontages within Special Districts shall be determined by Warrant.

3.7 CIVIC FUNCTIONS**3.7.1 General**

- a. Places for public use shall be required for each community and designated on the Community Plans as Civic Space (CS) and Civic Building (CB).
- b. Civic Spaces are public sites permanently dedicated to open space.
- c. Civic Buildings are sites dedicated for buildings generally operated by not-for-profit organizations dedicated to culture, education, government, transit and municipal parking, or for a use approved by the Jacksonville City Council.

3.7.2 Civic Space (CS) Specific to T3-T5 Zones

- a. Each Pedestrian Shed shall assign at least 5% of its urbanized area to Civic Space.
- b. Civic Spaces shall be designed as generally described in Table 13 and approved by the CRC and allocated to zones as described in Table 14E.
- c. Each Pedestrian Shed shall contain at least one Main Civic Space. The Main Civic Space shall be within 800 feet of the geographic center of each Pedestrian Shed, unless topographic conditions, pre-existing Thoroughfare alignments or other circumstances warrant it.
- d. Within 800 feet of every lot in Residential use, a Civic Space designed and equipped as a playground shall be provided.
- e. Each Civic Space shall have a minimum of 50% of its perimeter enfronting a Thoroughfare.
- f. [RESERVED]
- g. Parks may be permitted in Transect Zones T4 and T5 by Warrant (Table 14E).

3.7.3 Civic Buildings (CB) Specific to T3-T5 Zones

- a. Developers shall covenant to construct a Meeting Hall or a Third Place in proximity to the Main Civic Space of each Pedestrian Shed.
- b. [RESERVED]
- c. One Civic Building lot suitable for a childcare building shall be reserved within each Pedestrian Shed. The Developer or a Homeowners' Association or other Community Council may organize, fund and construct an appropriate building as the need arises.
- d. Civic Building sites shall not occupy more than 20% of the area of each Pedestrian Shed.
- e. Civic Building sites should be located within or adjacent to Civic Spaces, or at the axial termination of significant Thoroughfares.
- f. Civic Buildings shall not be subject to the standards of Article 5. The particulars of their design shall be determined by Variance.
- g. Parking for Civic Buildings shall be adjusted by Warrant. Civic parking lots may remain unpaved if graded, compacted and landscaped.

3.7.4 Civic Functions Specific to T1 & T2 Zones

- a. Civic Buildings and Civic Spaces related to education, recreation and culture may be erected within T1 Natural and T2 Rural Zones by Variance.

- b. Those portions of the T1 Natural and T2 Rural Zones that occur within a development parcel are an integral part of the Civic Function allocation and should conform to one or more of the Types specified in Table 13.

3.8 SPECIAL REQUIREMENTS

3.8.1 A New Community Plan may designate the following special requirements:

- a. A differentiation of the Thoroughfares as a Primary-Grid (P-Grid) and a Secondary-Grid (S-Grid). Buildings along the P-Grid shall be held to the highest standard of this Code in support of pedestrian activity. Buildings along the S-Grid may be more readily considered for Warrants and Variances allowing automobile-oriented standards. The Frontages assigned to the S-Grid shall not exceed 30% of the total length within a Pedestrian Shed.
- b. A designation for mandatory/required or recommended Retail Frontage requiring that a building provide a Shopfront at sidewalk level along the entire length of the Frontage. The Shopfront shall be no less than 70% glazed in clear glass and provided with an awning overlapping the sidewalk as generally illustrated in Table 7. The first floor shall be confined to Retail use through the depth of the Second Layer. (See Table 16D)
- c. A designation for mandatory or recommended Gallery Frontage, requiring that a building provide a permanent cover over the sidewalk, either cantilevered or supported by columns. The Gallery Frontage may be combined with a Retail Frontage as shown in Table 7.
- d. A designation of Coordinated Streetscape Frontage, requiring that the Public and Private Frontages be coordinated as a single, coherent landscape and paving design.
- e. A designation of Terminated Vista location, requiring that the building be provided with architectural articulation of a Type and character that responds to the location as approved by the CRC.
- f. A designation for Cross Block Passages, requiring a minimum 8-foot-wide pedestrian access be reserved between buildings.
- g. A designation of Buildings of Value, requiring that such buildings and structures may be altered or demolished only in accordance with Municipal Standards and Protocols.

4.1 INSTRUCTIONS

- 4.1.1 Within the Transect Map the City's Planning Office has prepared on its behalf, Infill Community Plans to guide further development of the Bayard Community.
- 4.1.2 Infill Community Plans shall be prepared in a process of public consultation, as determined and organized by the City of Jacksonville Planning Office and shall require approval by the Jacksonville City Council. The requirements of such plans are mandatory.
- 4.1.3 For any site greater than 40 acres, the landowner or developer may initiate the preparation of a New Community Plan subject to the provisions of Article 3 (other than the minimum acreage requirements) as approved by the City Council.
- 4.1.4 For smaller sites, developers and landowners shall use only Articles 1, General To All Plans, 5, Building Scale Plans, 6, Standards and Tables, and 7, Definitions of Terms, in accordance with the Transect Zones mapped by the Jacksonville Planning Office under the Infill Community Scale Plan provisions of Article 4.
- 4.1.5 Infill Community Plans shall identify, assign and follow the requirements of the Community Types described in Section 4.3.
- 4.1.6 Infill Community Plans shall assign Transect Zones and Civic Functions within each Community Type as described in Sections 4.3 and 4.4 with detailed provisions for site and building development as described in Article 5.
- 4.1.7 Infill Community Plans should consist of two maps: The first showing the Town Centers and the Neighborhoods with their various Transect Zones; and the second assigning the Special Requirements as provided in Section 4.5.
- 4.1.8 Twenty years after the approval is granted, each Transect Zone shall be considered for the next successional (next higher-numbered) Transect Zone through public hearing by the Jacksonville City Council.

4.2 TRANSECT ZONES

- 4.2.1 Infill Community Plans shall consist of TNDs and/or TODs/RCDs composed of Transect Zones as described in Tables 1 and 14.

4.3 COMMUNITY TYPES

Infill Community Plans, including smaller infill sites, shall be planned according to the following Community Types as determined by the City Planning Office and approved by the Jacksonville City Council. An Infill Community Plan shall include and assign, to the extent applicable, Neighborhoods, Town Centers and/or Downtowns. Infill Community Plans shall be based on conserving, completing or creating Transect-based urban structure.

4.3.1 Neighborhoods (TND or Traditional Neighborhood Development)

Neighborhoods shall be urbanized areas at least 40 acres that are primarily Residential. A Neighborhood shall be based upon a partial or entire Standard Pedestrian Shed. The physical center of the Neighborhood should be located at an important traffic intersection associated with a Civic or Commercial institution. The edges of the Neighborhood should blend into an adjacent Neighborhood, Town Center or Downtown without buffer. A Neighborhood Plan shall meet the requirements for a TND as set forth in Tables 2 and 14A.

4.3.2 **Downtowns (RCD, Regional Center Development, or TOD, Transit Oriented Development)**

- a. Downtowns shall be urbanized areas that are primarily mixed-use. A Downtown shall be defined by a Long Pedestrian Shed, oriented around an important Commercial corridor. Downtowns should be the location of large Commercial and Retail uses as well as government and other Civic institutions of regional importance. The edges of a Downtown should blend into adjacent Neighborhoods without buffer. A Downtown Plan shall meet the requirements for an RCD as set forth in Tables 2 and 14.

4.4 **CIVIC FUNCTIONS**

4.4.1 **General**

- a. Infill Plans shall designate, or allow by Warrant, Civic Space (CS) and Civic Buildings (CB).
- b. Civic Functions may be Warranted so long as they do not occupy more than a total of 20% of the area of each Pedestrian Shed. A Civic Function requiring more than 20% of the Pedestrian Shed shall be subject to the creation of a Special District with its own standards (To be coordinated per Table 15).
- c. Parking for Civic Functions shall be determined by Warrant.

4.4.2 **Civic Space (CS)**

- a. Civic Spaces may be approved by Warrant in any Transect Zone.
- b. Civic Spaces shall be generally designed as described in Table 13.

4.4.3 **Civic Building (CB)**

- a. Civic Buildings shall be approved by Warrant in any Transect Zone on sites reserved for Civic Buildings.
- b. Civic Buildings shall not be subject to the Requirements of Article 5. The particulars of their design shall be determined by Exception.

4.5 **SPECIAL REQUIREMENTS**

4.5.1 **An Infill Plan may designate the following special requirements:**

- a. A differentiation of the Thoroughfares as a Primary-Grid (P-Grid) and a Secondary-Grid (S-Grid). Buildings along the P-Grid shall be held to the highest standard of this Code in support of pedestrian activity. Buildings along the S-Grid may be more readily considered for Warrants and Variances allowing automobile-oriented standards. The Frontages assigned to the S-Grid shall not exceed 30% of the total length within a Pedestrian Shed.
- b. A designation for mandatory or recommended Retail Frontage requiring that a building provide a shopfront at sidewalk level along the entire length of the Frontage. The shopfront shall be no less than 70% glazed in clear glass and provided with an awning overlapping the sidewalk as generally illustrated in Table 7. The first floor shall be confined to retail use through the depth of the First Layer.
- c. A designation for mandatory or recommended Gallery Frontage, requiring that a building provide a permanent cover over the sidewalk, either cantilevered or supported by columns. The Gallery Frontage may be combined with a Retail Frontage as shown in Table 7.

- d. A designation of Coordinated Streetscape Frontage, requiring that the Public and Private Frontages be coordinated as a single, coherent landscape and paving design.
- e. A designation of Terminated Vista location, requiring that the building be provided with architectural articulation of a type and character that responds to the location as approved by the CRC.
- f. A designation for Cross Block Passages, requiring a minimum 8-foot-wide pedestrian access be reserved between certain buildings.
- g. A designation of Buildings of Value, requiring that such buildings and structures may be altered or demolished only when in accordance with preservation standards and protocols adopted by the City Council.

4.6 PRE-EXISTING CONDITIONS

- 4.6.1 Existing buildings that do not conform to the provisions of this Code, may continue in use as they are until a Substantial Modification is requested, at which time the Consolidated Review Committee (CRC) shall determine if they are a Grandfathered Property, as well as which provisions of this section that shall apply.
- 4.6.2 Renovation of existing buildings at any time shall require upgrade to the current Florida Building Code.
- 4.6.3 The modification of existing buildings is permitted by right if such changes result in greater conformance with the specifications of this section, and the modifications comply with the current Florida Building Code requirements.
- 4.6.4 [RESERVED]
- 4.6.5 Any addition to or modification of a building actually or potentially eligible for inclusion on a Local, State, or National historic register, including without limitation, the architectural harmony (similar materials, window proportions, color range, mass/void ratio, roof type and pitch) of such addition or modification, shall be subject to approval by the City of Jacksonville Planning Office.
- 4.6.6 The restoration or rehabilitation of an existing building shall not require the provision of (a) parking in addition to that existing or (b) on-site stormwater retention/detention in addition to that existing, except to the extent required by applicable Local, State or Federal law.

5.1 INSTRUCTIONS

- 5.1.1 Lots and buildings located within the Bayard Community Plan shall be subject to the requirements of this Article.
- 5.1.2 An owner or a developer may have site and building plans prepared on their behalf and submit the same for consideration for administrative approval by the CRC.
- 5.1.3 All site and building plans shall be subject to approval by the CRC, and to the extent required under applicable subdivision law, by the City Council of Jacksonville, Florida, prior to any work being commenced or other action being taken by the owner or developer with respect to such plans.
- 5.1.4 The requirements described in this Article shall control the Disposition, Configuration and Function of buildings, as well as their architectural, landscape, parking, signage, ambient and visitability standards. Special Requirements that appear on the Bayard Community Plan shall be adhered to.
- 5.1.5 Site and building plans submitted under this Article shall show the following, in compliance with the standards described in this Article:
- a. For preliminary site and building approval:
 - Building Disposition
 - Building Configuration
 - Building Function
 - parking standards
 - b. For final approval, in addition to the above:
 - architectural standards
 - landscape standards
 - signage standards
 - ambient standards
 - visitability standards
 - Special Requirements

5.2 SPECIFIC TO NATURAL AND RURAL TRANSECT ZONES (T1 & T2)

The following shall be applicable to Zones T1 and T2:

- 5.2.1 Buildings in the T1 Zone are permitted by Variance and in the T2 Zone by right. Permission to build in T1 and the standards for Disposition, Configuration, Function, parking, architectural, environmental, ambient and visitability shall be determined concurrently as Variances, in public hearing of the Jacksonville City Council.
- 5.2.2 **Environmental Standards**
The modification of the natural conditions shall be according to Local, State and Federal guidelines.

5.3 SPECIFIC TO SUB-URBAN TRANSECT ZONE (T3)**5.3.1 Building Disposition (T3)**

- a. Newly platted lots shall be dimensioned according to Section 5.3.11
- b. Buildings shall be disposed in relation to the boundaries of their lots according to Section 5.3.11
- c. One Principal Building at the Frontage, and one Outbuilding to the rear of the Principal Building, may be built on each lot as shown in Table 16C.
- d. Lot coverage by building shall not exceed that shown in Section 5.3.11.

- e. Facades shall be built parallel to a rectilinear Principal Frontage Line or parallel to the tangent of a curved Principal Frontage Line.
- f. Setbacks for Principal Buildings shall be as shown in Table 14G. Setbacks may otherwise be adjusted by Warrant.
- g. Rear Setbacks for Outbuildings shall be a minimum of 15 feet measured from the centerline of the Alley or Rear Lane easement. In the absence of Rear Alley or Lane, the rear Setback shall be as shown in Section 5.3.11.
- h. Building Types shall be as shown in Table 9.
- i. [RESERVED]

5.3.2 Building Configuration (T3)

- a. Private Frontage types shall conform to and be allocated in accordance with Table 7 and Section 5.3.11.
- b. [RESERVED]
- b. Stoops, bay windows, open porches and balconies may encroach into the Setbacks as shown in Section 5.3.11.
- c. [RESERVED]
- d. Building Heights shall conform to Table 8 and be as shown in Section 5.3.11.
- e. [RESERVED]
- f. All specified Building Heights may be increased by the base elevations required by applicable FEMA standards.

5.3.3 Building Function & Density (T3)

- a. Buildings in each Transect Zone shall conform to the Functions described in Table 10 or 12 and Section 5.3.11. Functions that do not conform to the requirements of Tables 10 and 12 shall require approval by Variance.
- b. The Actual Parking available to meet the Required Parking shown on Table 11 shall constitute the Base Density. Functions shall be limited by the Base Density, subject to upward adjustment in accordance with paragraph 5.3.3c.
- c. The Base Density may be adjusted upward by adding the Actual Parking available for each of two Functions within any pair of adjacent Blocks, and the resulting sum then multiplied by the corresponding Sharing Factor (Table 12). The result shall be the Effective Parking available for calculating an Adjusted Density. Conversely: The Effective Parking required is the sum of the Required Parking divided by the Sharing Factor.
- d. [RESERVED]

5.3.4 Parking Standards (T3)

- a. Vehicular parking shall be required and adjusted for mixed-use as shown in Tables 11 and 12.
- b. On-street parking available along the Frontage Lines that correspond to each lot shall be counted toward the parking requirement of the building on the lot.
- c. Maximum parking ratios may be established by the CRC.
- d. Parking shall be accessed by the Alley or Rear Lane, when such are available on the Community Plan.
- e. Parking lots shall be masked from the Frontage by a Liner Building or Streetscreen as specified in Section 5.3.5b.
- f. Open parking areas shall be located at the Second and Third Lot Layers, as shown in Table 16D, except that Driveway aprons and drop-offs may be located at the First Layer. Garages shall be located at the Third Layer.

- g. [RESERVED]
- h. [RESERVED]
- i. [RESERVED]
- j. [RESERVED]
- k. For buildings on Secondary Grids (S-Grids), parking lots may be allowed on the Frontage by Warrant (see Section 5.8.1a).

5.3.5 Architectural Standards (T3)

- a. Building wall materials may be combined on each Facade only horizontally, with the heavier below the lighter.
- b. Streetscreens should be between 3.5 and 8 feet in height and constructed of a material matching the adjacent building Facade. The Streetscreen may be replaced by a hedge or fence by Warrant. Streetscreens shall have openings no larger than necessary to allow automobile and pedestrian access.
- c. All openings, including porches, galleries, arcades and windows, with the exception of storefronts, shall be square or vertical in proportion.
- d. Openings above the first Story shall not exceed 50% of the total second Story building wall area, with each Facade being calculated independently.
- e. [RESERVED].
- f. Doors and windows that operate as sliders are prohibited along Frontages.
- g. Pitched roofs, if provided, shall be symmetrically sloped no less than 5:12, except that porches and attached sheds may be no less than 2:12.
- h. The exterior finish material on all Facades shall be limited to brick, wood siding, cementitious siding and/or stucco.
- i. Flat roofs shall be enclosed by parapets a minimum of 42 inches high, or as required to conceal mechanical equipment to the satisfaction of the CRC.
- j. Balconies and porches shall be made of painted wood.
- k. Fences, if provided at the First and Second Lot Layers, shall be wood board and painted. Fences at Lot Lines in the Third and Fourth Lot Layers may be chain link, except when located along Frontage Lines.
- l. Example plans and elevations that exemplify the Traditional Neighborhood Development architectural standards, for residential and non-residential buildings, that are to be permitted in the Bayard Community, are provided in the Traditional Neighborhood Design, three volume series, © 1997 HomeStyles Publishing and Marketing, Inc., and its licensors.

5.3.6 Environmental Standards (T3)

- a. Transect Zones manifest a range of responses to natural and urban conditions. In case of conflict, to the extent not inconsistent with applicable state or federal law, the natural infrastructure shall have priority in the more rural zones (T1-T3) and the urban infrastructure shall have priority in the more urban zones (T4-T5) as detailed in Sections 5.2 through 5.5.
- b. The landscape installed may consist primarily of native species requiring minimal irrigation, fertilization and maintenance.
- c. Impermeable surface by building shall be minimized and confined to the ratio of lot coverage by building shown in Table 14F.
- d. To the extent not inconsistent with applicable Local, State or Federal law, the management of storm water shall be primarily through retention and percolation

on the individual lot or through swales in the Public Frontage to master storm water ponds.

5.3.7 Landscape Standards (T3)

- a. A minimum of one tree to match the species of street trees on the Public Frontage shall be planted within the First Layer for each 30 feet of Frontage Line as illustrated in Table 16D.
- b. [RESERVED].
- c. [RESERVED].
- d. Trees shall be of various species, naturalistically clustered, with an understory installed below for maintenance. Lawn shall be permitted by Warrant.

5.3.8 Signage Standards (T3)

- a. One address number no more than 6 inches measured vertically shall be attached to the building in proximity to the principal entrance or at a mailbox.
- b. One blade sign for each business may be permanently installed perpendicular to the Facade. Such a sign shall not exceed a total of 4 square feet.
- c. [RESERVED]
- d. There shall be no signage permitted additional to that specified in this section.
- e. Signage shall not be lit.

5.3.9 Ambient Standards (T3)

- a. Sound levels measured at the building Frontage shall not exceed 65 decibels from sunrise to sunset and 55 decibels from sunset to sunrise.
- b. Average lighting levels measured at the building Frontage shall not exceed 1.0 fc (foot-candles).
- c. Streetlights shall be of a general type illustrated in Table 5.
- d. Outdoor storage shall be screened from view from any Frontage by a Streetscreen in conformance with Section 5.3.5b.

5.3.10 Visitability Standards (T3)

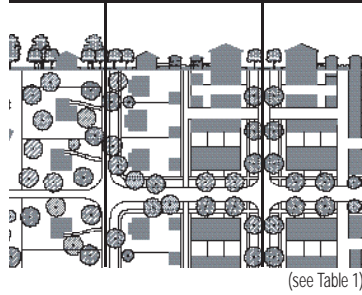
- a. There shall be provided one zero-step entrance to each building from an accessible path at the front, side, or rear of each building.
- b. All first floor interior doors (including bathrooms) shall provide 32 inches of clear passage.
- c. There shall be a half or full bath provided on the first Story of each building.

SMARTCODE

Community of Bayard, Jacksonville, Florida
June 1, 2007 Draft Code

SECTION 5.3.11

T3



BUILDING FUNCTION (see Tables 10 & 11)

a. Residential	permitted
b. Lodging	prohibited
c. Office	prohibited
d. Retail	prohibited

BUILDING TYPE (see Table 9)

a. Edgeyard	permitted
b. Sideyard	prohibited
c. Rearyard	prohibited
d. Courtyard	prohibited

PRIVATE FRONTAGES (see Table 7)

a. Common Lawn	permitted
b. Porch & Fence	permitted
c. Terrace or L.C.	prohibited
d. Forecourt	prohibited
e. Stoop	prohibited
f. Shopfront & Awning	prohibited
g. Gallery	prohibited
h. Arcade	prohibited

Refer to Summary Table 14

LOT OCCUPATION

a. Lot Width	50 ft. min.
b. Lot Coverage	60% max.

BUILDING HEIGHT

a. Principal Building	2 stories max.
b. Outbuilding	2 stories max.

BUILDING DISPOSITION

a. Front Setback	12 ft. min.; 24 ft. max.
b. Side Setback	3 ft. min.; 6 ft. min.*
c. Rear Setback	10 ft. min.; 10 ft. max.**
d. Frontage Buildout	50% min.

ELEMENT ENCROACHMENTS

a. At Bldg. Frontage	12 ft. max.
b. At Side Setback	3 ft. max.
c. At Bldg. Rear	0 ft.

OUTBUILDING DISPOSITION/PARKING

a. Front Setback	20 ft. min. + Front Bldg. Setback
b. Side Setback	3 ft. min.
c. Rear Setback	3 ft.; 3 ft. max.**

PRIVATE PROVISIONS

See Tables 11 & 12

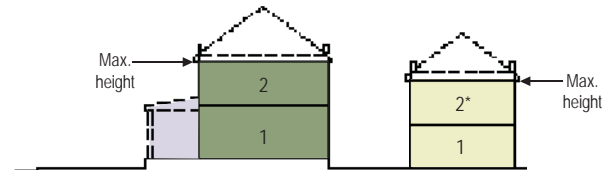
* Min. at Corner Lots

** Max. at Corner Lots

*** or 15 ft. from center line of alley

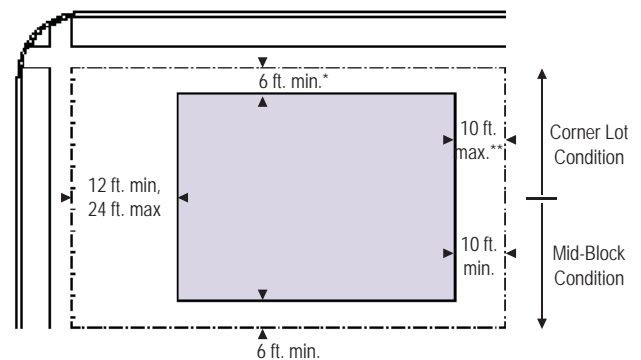
BUILDING HEIGHT

1. Building height shall be a max. height of 35 feet,
2. Maximum height shall be measured at base flood elevation of 9 feet above mean sea level to the peak of the roof.
3. Each story shall not exceed 14 ft. clear, floor to ceiling.



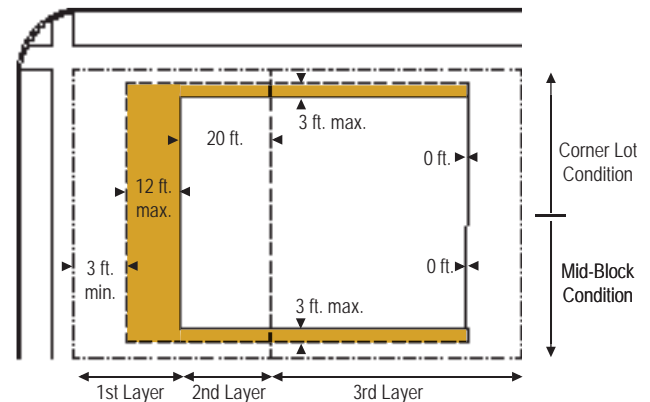
BUILDING DISPOSITION

1. The facades and elevations of principal buildings shall be distanced from the lot lines as shown (See Table 16.c, Definitions Illustrated)
2. Facades shall be built along the principal frontage to a minimum of 50% of its width of the principal frontage.



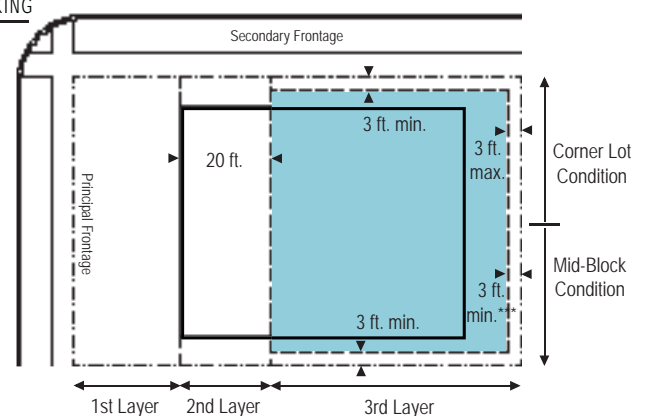
ELEMENT ENCROACHMENTS

1. Stoops, bay windows, open porches, balconies may be located within the setbacks as shown in the diagram.
2. Minimum 3 feet between frontage line and maximum encroachment line.
3. Utility connections, A/C units and direct-vent fireplaces shall only be located within the 3rd Layer along elevations (see Table 16.d and 16.e).



OUTBUILDING DISPOSITION/PARKING

1. The facades and elevations of the Outbuildings shall be distanced from the lot lines as shown.
2. Uncovered parking spaces may be provided within the 2nd and 3rd Layer as shown in the diagram (see Table 16.d).
3. Covered parking shall be provided within the 3rd Layer as shown in the diagram (see Table 16.d).
4. Trash containers shall be stored within the 3rd Layer.



SC20

5.4 SPECIFIC TO GENERAL URBAN TRANSECT ZONES (T4)**5.4.1 Building Disposition (T4)**

- a. Newly platted lots shall be dimensioned according to Section 5.4.11
- b. Buildings shall be disposed in relation to the boundaries of their lots according to Section 5.4.11
- c. One Principal Building at the Frontage, and one Outbuilding to the rear of the Principal Building, may be built on each lot as shown in Table 16C.
- d. Lot coverage by building shall not exceed that shown in Section 5.4.11.
- e. Facades shall be built parallel to a rectilinear Principal Frontage Line or parallel to the tangent of a curved Principal Frontage Line.
- f. Setbacks for Principal Buildings shall be as shown in Table 14G. Setbacks may otherwise be adjusted by Warrant.
- g. Rear Setbacks for Outbuildings shall be a minimum of 15 feet measured from the centerline of the Alley or Rear Lane easement. In the absence of Rear Alley or Lane, the rear Setback shall be as shown in Section 5.4.11.
- h. Building Types shall be as shown in Table 9.
- i. A minimum Residential housing mix of three Types (none less than 20%) shall be required in the General Urban Zone, selected from Table 9.

5.4.2 Building Configuration (T4)

- a. Private Frontage types shall conform to and be allocated in accordance with Table 7 and Section 5.4.11.
- b. Awnings may encroach the public sidewalk without limit only in designated areas permitting Live-Work units. Stoops, open porches, bay windows and balconies may encroach up to 50% of the depth of the minimum Setback.
- c. [RESERVED]
- d. Building Heights shall conform to Table 8 and be as shown in Section 5.4.11.
- e. All specified Building Heights may be increased by the base elevations required by applicable FEMA standards.

5.4.3 Building Function & Density (T4)

- a. Buildings in each Transect Zone shall conform to the Functions described in Tables 10 or 11 and Section 5.4.11. Functions that do not conform to the requirements of Tables 10 or 11 shall require approval by Variance.
- b. The Actual Parking available to meet the Required Parking shown on Table 12 shall constitute the Base Density. Functions shall be limited by the Base Density, subject to upward adjustment in accordance with paragraph 5.4.3c.
- c. The Base Density may be adjusted upward by adding the Actual Parking available for each of two Functions within any pair of adjacent Blocks, and the resulting sum then multiplied by the corresponding Sharing Factor (Table 12). The result shall be the Effective Parking available for calculating an Adjusted Density. Conversely: The Effective Parking required is the sum of the Required Parking divided by the Sharing Factor.
- d. [RESERVED]
- e. Accessory uses of Limited Lodging or Limited Office shall be permitted within an Outbuilding.
- f. Live-Work units shall be permitted only within designated areas identified on the Community Plan.

5.4.4 Parking Standards (T4)

- a. Vehicular parking shall be required as shown in Tables 11 and 12.
- b. On-street parking available along the Frontage Lines that correspond to each lot shall be counted toward the parking requirement of the building on the lot.
- c. Maximum Parking ratios may be established by the CRC.
- d. Parking shall be accessed by the Alley or Rear Lane, when such are available on the Community Plan.
- e. Parking lots shall be masked from the Frontage by a Liner Building or Streetscreen as specified in Section 5.4.5b.
- f. All parking areas except for Driveways shall be located at the Third Layer as illustrated in Table 16D. Garages shall be at the Third Layer.
- g. Twenty-five percent (25%) of the required parking may be provided within one-quarter mile of the site that it serves, subject to the provision of pedestrian enhancements along thoroughfares providing shortest route to and from the site, and subject to approval by Variance.
- h. [RESERVED]
- i. [RESERVED]
- j. A minimum of one bicycle rack place shall be provided within the Public or Private Frontage for every ten vehicular parking spaces.
- k. [RESERVED]

5.4.5 Architectural Standards (T4)

- a. Building wall materials may be combined on each Facade only horizontally, with the heavier below the lighter.
- b. Streetscreens should be between 3.5 and 8 feet in height and constructed of a material matching the adjacent building Facade. The Streetscreen may be replaced by a hedge or fence by Warrant. Streetscreens shall have openings no larger than necessary to allow automobile and pedestrian access.
- c. All openings, including porches, galleries, arcades and windows, with the exception of storefronts, shall be square or vertical in proportion.
- d. Openings above the first Story shall not exceed 50% of the total second Story building wall area, with each Facade being calculated independently.
- e. [RESERVED].
- f. Doors and windows that operate as sliders are prohibited along Frontages.
- g. Pitched roofs, if provided, shall be symmetrically sloped no less than 5:12, except that porches and attached sheds may be no less than 2:12.
- h. Flat roofs shall be enclosed by parapets a minimum of 42 inches high, or as required to conceal mechanical equipment to the satisfaction of the CRC.
- i. The exterior finish material on all Facades shall be limited to brick, wood siding, fiber-cement siding and/or stucco.
- j. Balconies and porches shall be made of painted wood or metal.
- k. Fences, if provided at the First and Second Lot Layers, shall be wood board and painted. Fences at Lot Lines in the Third and Fourth Lot Layers may be chain link, except when located along Frontage Lines.
- l. Example plans and elevations that exemplify the Traditional Neighborhood Development architectural standards, for residential and non-residential buildings,

that are to be permitted in the Bayard Community, are provided in the Traditional Neighborhood Design, three volume series, © 1997 HomeStyles Publishing and Marketing, Inc., and its licensors.

5.4.6 Environmental Standards (T4)

- a. Transect Zones manifest a range of responses to natural and urban conditions. In case of conflict, to the extent not inconsistent with applicable Local, State or Federal law, the natural infrastructure shall have priority in the more rural zones (T1-T3) and the urban infrastructure shall have priority in the more urban zones (T4-T5) as detailed in Sections 5.2 through 5.5.
- b. The species of landscape installed shall consist primarily of durable species tolerant of soil compaction.
- c. Impermeable surface shall be confined to the ratio of lot coverage by building, as shown in Table 14F.
- d. To the extent not inconsistent with applicable Local, State or Federal law, management of storm water shall be primarily off-site through underground storm drainage to master storm water ponds, thereby no retention or detention shall be required on the individual lot.

5.4.7 Landscape Standards (T4)

- a. A minimum of one tree to match the species of street trees on the Public Frontage shall be planted within the First Layer for each 30 feet of Frontage Line as illustrated in Table 16D.
- b. [RESERVED].
- c. [RESERVED].
- d. Trees of species matching the planting on the Public Frontage as shown in Table 4. Lawn shall be permitted.

5.4.8 Signage Standards (T4)

- a. One address number no more than 6 inches measured vertically shall be attached to the building in proximity to the principal entrance or at a mailbox.
- b. One blade sign for each business may be permanently installed perpendicular to the Facade. Such a sign shall not exceed a total of 4 square feet .
- c. [RESERVED].
- d. There shall be no signage permitted additional to that specified in this section.

5.4.9 Ambient Standards (T4)

- a. Sound levels measured at the building Frontage shall not exceed 65 decibels from sunrise to sunset and 55 decibels from sunset to sunrise.
- b. Average lighting levels measured at the building Frontage shall not exceed 2.0 fc (foot-candles) .
- c. Streetlights shall be of a general type illustrated in Table 5.
- d. Outdoor storage shall be screened from view from any Frontage by a Streetscreen in conformance with Section 5.4.5b.

5.4.10 Visitability Standards (T4)

- a. There shall be provided one zero-step entrance to each building from an accessible path at the front, side, or rear of each building.
- b. All first floor interior doors (including bathrooms) shall provide 32 inches of clear passage.
- c. There shall be a half or full bath provided on the first Story of each building.

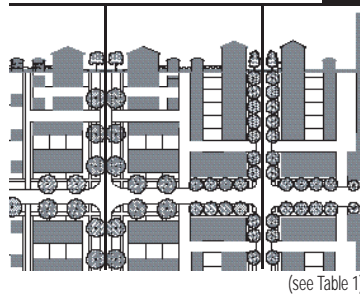
SMARTCODE

Community of Bayard, Jacksonville, Florida
June 1, 2007 Draft Code

ARTICLE 5. BUILDING SCALE PLANS

SECTION 5.4.11

T4



BUILDING FUNCTION (see Tables 10 & 11)

a. Residential	open use
b. Lodging	limited
c. Office	restricted
d. Retail	restricted to live work area

BUILDING TYPE (see Table 9)

a. Edgeyard	permitted
b. Sideyard	permitted
c. Rearyard	permitted
d. Courtyard	permitted

PRIVATE FRONTAGES (see Table 7)

a. Common Lawn	permitted
b. Porch & Fence	permitted
c. Terrace or L.C.	permitted
d. Forecourt	permitted
e. Stoop	permitted
f. Shopfront & Awning	permitted (live work area only)
g. Gallery	prohibited
h. Arcade	prohibited

LOT OCCUPATION

Refer to Summary Table 14

a. Lot Width	24 ft. min.; 168 ft. max.
b. Lot Coverage	70% max

BUILDING HEIGHT

a. Principal Building	3 stories max. (see Item #1)
b. Outbuilding	2 stories max.

BUILDING DISPOSITION

a. Front Setback	6 ft. min.; 18 ft. max.
b. Side Setback	0 ft. min.; 6 ft. min.*
c. Rear Setback	3 ft. min.***; 3 ft. max.**
d. Frontage Buildout	60% min.

ELEMENT ENCROACHMENTS

a. At Bldg. Frontage	3 ft. max.
b. At Side Setback	0 ft. min.; 3 ft. max.**
c. At Bldg. Rear	0 ft.

OUTBUILDING DISPOSITION/PARKING

a. Front Setback	40 ft. max. from rear prop. line
b. Side Setback	0 ft.; 3 ft. max.**
c. Rear Setback	3 ft. min.***; 3 ft. max**

PRIVATE PROVISIONS

See Tables 11 & 12

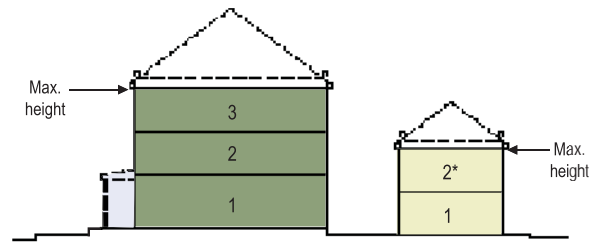
* minimum corner lots

** maximum corner lots

*** or 15 ft. from center line of alley

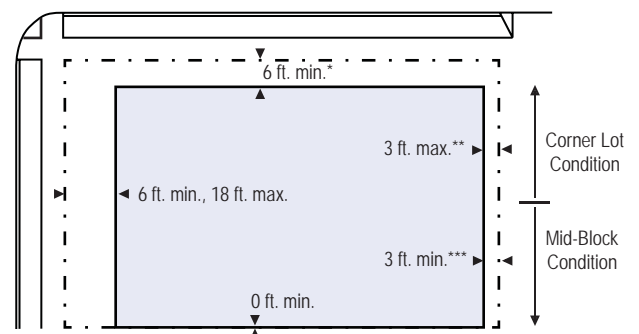
BUILDING HEIGHT

1. Maximum three-story height for buildings adjacent to a T5 zone and two-story maximum height for buildings adjacent to a T3 zone.
2. Building height shall be measured in number of stories, excluding a raised basement, or inhabited attic.
3. Each story shall not exceed 14 ft. clear, floor to ceiling.
4. Maximum height shall be measured to the eave or roof deck.



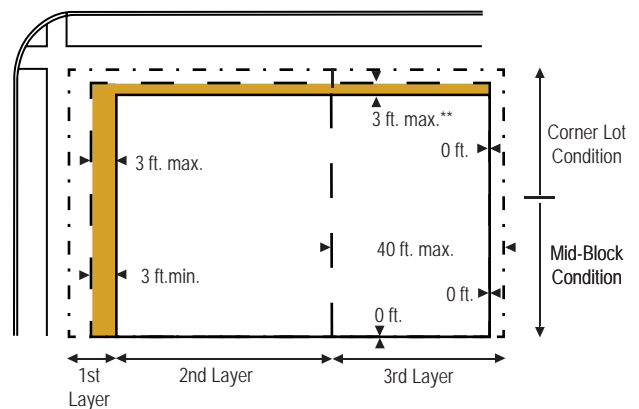
BUILDING DISPOSITION

1. The facades and elevations of principal buildings shall be distanced from the lot lines as shown.
2. Buildings shall have facades along principal frontage lines and elevations along lot lines. (see Table 16.e).



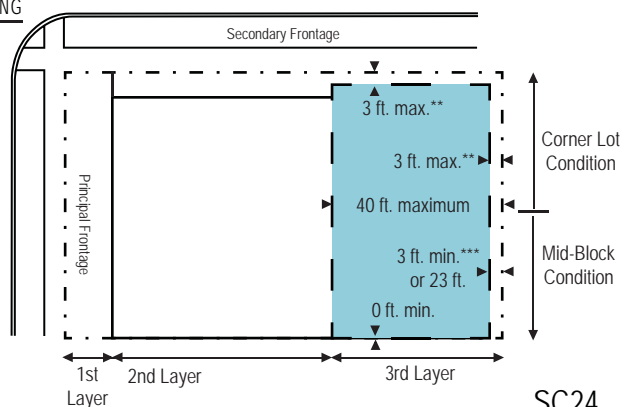
ELEMENT ENCROACHMENTS

1. Awnings, stoops, bay windows, open porches and balconies may be located within the set-backs as stipulated in Section 5.4.2b.
2. Minimum 3 feet between frontage line and maximum encroachment line.
3. Utility connections, A/C units and direct-vent fireplaces shall only be located within the 3rd Layer along elevations (see Table 16.d and 16.e).



OUTBUILDING DISPOSITION/PARKING

1. The elevations and facades of the Outbuilding shall be distanced from the lot as shown.
2. Uncovered parking spaces may be provided within the 3rd Layer as shown in the diagram (see Table 16.d).
3. Covered parking shall be provided within the 3rd Layer as shown in the diagram (see Table 16.d).
4. Trash containers shall be stored within the 3rd Layer.



5.5 SPECIFIC TO URBAN CENTER TRANSECT ZONES (T5)**5.5.1 Building Disposition (T5)**

- a. Newly platted lots shall be dimensioned according to Section 5.5.11
- b. Buildings shall be disposed in relation to the boundaries of their lots according to Section 5.5.11
- c. One Principal Building at the Frontage, and one Outbuilding to the rear of the Principal Building, may be built on each lot as shown in Table 16C.
- d. Lot coverage by building shall not exceed that shown in Section 5.5.11.
- e. Facades shall be built parallel to the Principal Frontage Line along a minimum of 70% of its length on the Setback shown in Section 5.5.11. In the absence of a building along the remainder of the Frontage Line, a Streetscreen shall be built co-planar with the Facade.
- f. Setbacks for Principal Buildings shall be as shown in Table 14G. Setbacks may otherwise be adjusted by Warrant.
- g. Rear Setbacks for Outbuildings shall be a minimum of 15 feet measured from the centerline of the Alley or Rear Lane easement. In the absence of Rear Alley or Lane, the rear Setback shall be as shown in Section 5.5.11.
- h. Building Types shall be as shown in Table 9.
- i. [RESERVED]
- j. Buildings shall have their principal pedestrian entrances on a Frontage Line.

5.5.2 Building Configuration (T5)

- a. Private Frontage types shall conform to and be allocated in accordance with Table 7 and Section 5.5.11.
- b. Awnings may encroach the public sidewalk without limit. Stoops and balconies may encroach 100% of the depth of a Setback. Open porches and bay windows may encroach up to 50% of the depth of the Setback. All arcades shall be a minimum of 12 feet wide.
- c. Loading docks and service areas shall be permitted on Frontages only by Variance.
- d. Building Heights shall conform to Table 8 and be as shown in Section 5.5.11. All buildings are required to have a minimum two-story building height, with exception of those buildings fronting Highway U.S.-1. Upon approval of the U.S.-1 boulevard enhancements, it is recommended that the minimum two-story building height be required for all buildings in this Transect.
- e. A first level Residential or Lodging Function shall be raised a minimum of 2 feet from average sidewalk grade.
- f. All specified Building Heights may be increased by the base elevations required by applicable FEMA standards.

5.5.3 Building Function & Density (T5)

- a. Buildings in each Transect Zone shall conform to the Functions described in Tables 10 or 11 and Section 5.5.11. Functions that do not conform to the requirements of Tables 10 or 11 shall require approval by Variance.
- b. The Actual Parking available to meet the Required Parking shown on Table 12 shall constitute the Base Density. Functions shall be limited by the Base Density, subject to upward adjustment in accordance with paragraph 5.5.3c.
- c. The Base Density may be adjusted upward by adding the Actual Parking available for each of two Functions within any pair of adjacent Blocks, and the resulting sum then multiplied by the corresponding Sharing Factor (Table 12). The result

shall be the Effective Parking available for calculating an Adjusted Density. Conversely the Effective Parking required is the sum of the Required Parking divided by the Sharing Factor.

d. [RESERVED]

e. Accessory uses of Limited Lodging or Limited Office shall be permitted within an Outbuilding.

f. First story Commercial shall be permitted throughout and shall be required at Mandatory Shopfront Frontages.

g. Manufacturing within the first Story may be permitted by Variance.

5.5.4 **Parking Standards (T5)**

a. Vehicular parking shall be required as shown in Tables 11 and 12.

b. On-street parking available along the Frontage Lines that correspond to each lot shall be counted toward the parking requirement of the building on the lot.

c. Maximum Parking ratios may be established by the CRC.

d. Parking shall be accessed by the Alley or Rear Lane when such is available in the Community Plan.

e. Parking lots shall be masked from the Frontage by a Liner Building or Streetscreen as specified in Section 5.5.5b.

f. All parking areas shall be located at the Third Lot Layer .

g. The required parking may be provided within one-quarter mile of the site that it serves, subject to approval by Variance.

h. The vehicular entrance of a parking lot on a Frontage shall be no wider than 20 feet. The vehicular entrance of a parking garage on a Frontage shall be no wider than 30 feet.

i. Pedestrian entrances to all parking lots and parking structures shall be directly from a Frontage Line. Only underground parking structures may be entered by pedestrians directly from a Principal Building.

j. A minimum of one bicycle rack place shall be provided within the Public or Private Frontage for every ten vehicular parking spaces.

k. For buildings on Secondary Grids (S-Grids), parking lots may be allowed on the Frontage by Warrant (see Section 5.8.1a).

5.5.5 **Architectural Standards (T5)**

a. Building wall materials may be combined on each Facade only horizontally, with the heavier below the lighter.

b. Streetscreens should be between 3.5 and 8 feet in height and constructed of a material matching the adjacent building Facade. The Streetscreen may be replaced by a hedge or fence by Warrant. Streetscreens shall have openings no larger than necessary to allow automobile and pedestrian access.

c. All openings, including porches, galleries, arcades and windows, with the exception of storefronts, shall be square or vertical in proportion.

d. Openings above the first Story shall not exceed 50% of the total second Story building wall area, with each Facade being calculated independently.

e. The Facades on Retail Frontages shall be detailed as storefronts and glazed with clear glass no less than 70% of the sidewalk-level story.

f. Doors and windows that operate as sliders are prohibited along Frontages.

g. Buildings may have flat roofs enclosed by parapets or sloped roofs. Pitched

roofs shall be symmetrically sloped no less than 5:12, except that porches and attached sheds may be no less than 2:12.

- h. Flat roofs shall be enclosed by parapets a minimum of 42 inches high, or as required to conceal mechanical equipment to the satisfaction of the CRC.
- i. The exterior finish materials on all Facades shall be limited to stone, brick and/or stucco.
- j. Balconies, galleries and arcades shall be made of concrete, painted wood or metal.
- k. Streetscreens shall be located coplanar with the building Facade line as shown in Table 16D.
- l. Example plans and elevations that exemplify the Traditional Neighborhood Development architectural standards, for residential and non-residential buildings, that are to be permitted in the Bayard Community, are provided in the Traditional Neighborhood Design, three volume series, © 1997 HomeStyles Publishing and Marketing, Inc., and its licensors.

5.5.6 Environmental Standards (T5)

- a. Transect Zones manifest a range of responses to natural and urban conditions. In case of conflict, to the extent not inconsistent with applicable Local, State or Federal law, the natural infrastructure shall have priority in the more rural zones (T1-T3) and the urban infrastructure shall have priority in the more urban zones (T4-T5) as detailed in Sections 5.2 through 5.5.
- b. The landscape installed shall consist primarily of durable species tolerant of soil compaction.
- c. Impermeable surface by building shall be confined to the ratio of lot coverage as shown in Table 14F.
- d. To the extent not inconsistent with applicable Local, State or Federal law management of storm water shall be primarily off-site through underground storm drainage to master storm water ponds, thereby no retention or detention shall be required on the individual lot.

5.5.7 Landscape Standards (T5)

- a. A minimum of one tree to match the species of street trees on the Public Frontage shall be planted within the First Layer for each 30 feet of Frontage Line as illustrated in Table 16D.
- b. [RESERVED].
- c. The First Layer as shown in Table 16D shall be landscaped or paved to match the enfronting Public Frontage as shown in Table 4.
- d. Trees shall be a species with shade canopies that, at maturity, begin higher than the top of the second Story of buildings.

5.5.8 Signage Standards (T5)

- a. One address number no more than 6 inches measured vertically shall be attached to the building in proximity to the principal entrance or at a mailbox.
- b. Blade signs, not to exceed 6 square ft. for each separate business entrance, may be attached perpendicular to the Facade.
- c. [RESERVED].
- d. A single external sign band may be applied to the Facade of each building, providing that such sign not exceed 3 feet in height by any length.
- e. Signage shall be externally lit, except that signage within the shopfront glazing may be neon lit.

f. There shall be no signage permitted additional to that specified in this section.

5.5.9 **Ambient Standards (T5)**

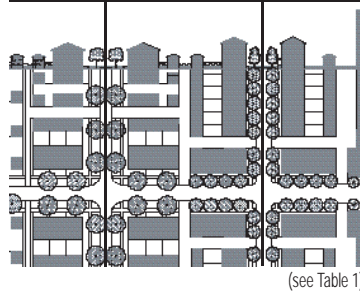
- a. Sound levels measured at the building Frontage shall not exceed 70 decibels from sunrise to midnight and 60 decibels from midnight to sunrise.
- b. Average lighting levels measured at the building Frontage shall not exceed 5.0 fc (foot-candles).
- c. Streetlights shall be of a general type illustrated in Table 5.
- d. Outdoor storage shall be screened from view from any Frontage by a Streetscreen in conformance with Section 5.5.5b.

5.5.10 **Visitability Standards (T5)**

- a. There shall be provided one zero-step entrance to each building from an accessible path at the front, side, or rear of each building.
- b. All first floor interior doors (including bathrooms) shall provide 32 inches of clear passage.
- c. There shall be a half or full bath provided on the first Story of each building.

SECTION 5.5.11

T5



BUILDING FUNCTION (see Tables 10 & 11)

a. Residential	restricted use
b. Lodging	open use
c. Office	open use
d. Retail	open use

BUILDING TYPE (see Table 9)

a. Edgeyard	prohibited
b. Sideyard	permitted
c. Rearyard	permitted
d. Courtyard	permitted

PRIVATE FRONTAGES (see Table 7)

a. Common Lawn	prohibited
b. Porch & Fence	prohibited
c. Terrace or L.C.	permitted
d. Forecourt	permitted
e. Stoop	permitted
f. Shopfront & Awning	permitted
g. Gallery	permitted
h. Arcade	permitted

Refer to Summary Table 14

LOT OCCUPATION

a. Lot Width	18 ft. min 180 ft. max
b. Lot Coverage	80% max

BUILDING HEIGHT

a. Principal Building	4 stories max.
b. Outbuilding	2 stories max.

BUILDING DISPOSITION

a. Front Setback	6 ft. min.; 12 ft. max.
b. Side Setback	0 ft. min.; 24 ft. max.
c. Rear Setback	3 ft. min.; 3 ft. max.**
d. Frontage Buildout	70% min.

ELEMENT ENCROACHMENTS

a. At Bldg. Frontage	6 ft. max. (+ arcades)
b. Side Setback	0 ft. min.; 3 ft. max.**
c. At Bldg. Rear	0 ft.

OUTBUILDING DISPOSITION/PARKING

a. Front Setback	40 ft. max from rear prop.
b. Side Setback	0 ft.
c. Rear Setback	3 ft. min.***, 3 ft. max.**

PRIVATE PROVISIONS

See Tables 11 & 12

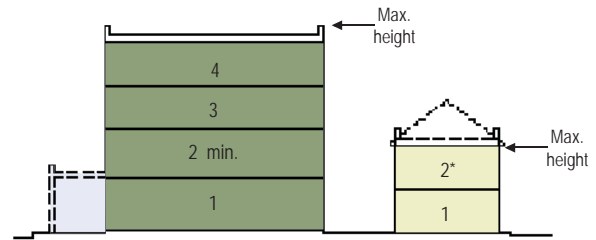
* Minimum corner lots

** Maximum corner lots

*** or 15 ft. from center line of alley

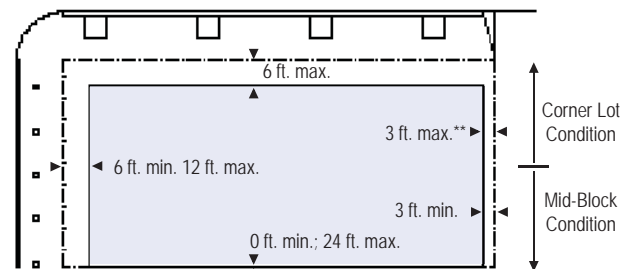
BUILDING HEIGHT

- Maximum four-story building height permitted only in designated areas identified on the Transect Map. Min. 2-story height requirement shall be as stipulated in Section 5.5.2d.
- Building height shall be measured in number of stories, excluding a raised basement, or inhabited attic.
- Each story shall not exceed 14 ft. clear, floor to ceiling.
- Maximum height shall be measured to the eave or roof deck.



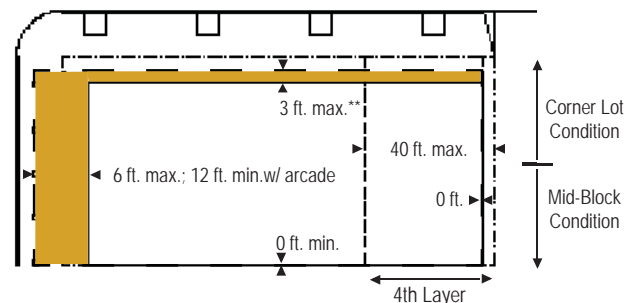
BUILDING DISPOSITION

- The facades and elevations of a building shall be distanced from the frontage and lot lines as shown.
- Buildings shall have facades along the principal frontage lines and elevations along lot lines (see Table 16e).



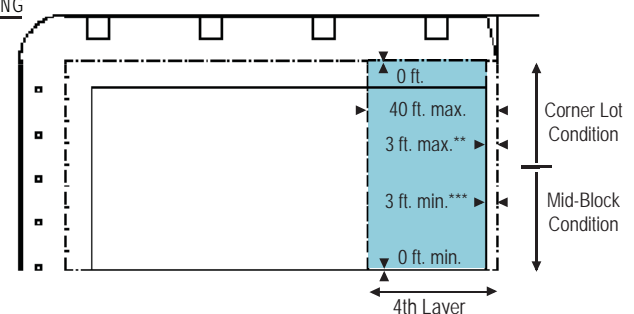
ELEMENT ENCROACHMENTS

- Awnings, stoops, bay windows, open porches and balconies may be located within the set-backs as stipulated in Section 5.5.2b.
- Minimum 3 feet between frontage line and maximum encroachment line.
- Utility connections. A/C units and direct-vent fireplaces shall only be located within the 3rd Layer along elevations (see Table 16.d and 16.e).



OUTBUILDING DISPOSITION/PARKING

- The elevations and facades of the Outbuilding shall be distanced from the lot as shown.
- Uncovered parking spaces may be provided within the 4th Layer as shown in the diagram (see Table 16.d).
- Covered parking shall be provided within the 4th Layer as shown in the diagram (see Table 16.d).
- Trash containers shall be stored within the 4th Layer.



5.6 **SPECIFIC TO URBAN CORE TRANSECT ZONES (T6)**
[RESERVED]

5.7 **CIVIC FUNCTIONS**

5.7.1 **General**

- a. Community Plans shall designate, or allow by Variance, Civic Space (CS) and Civic Building (CB).
- b. Civic Functions may be Warranted so long as they do not occupy more than a total of 20% of the area of each Pedestrian Shed. A Civic Function requiring more than 20% of the Pedestrian Shed shall be subject to the creation of a Special District. (See Section 5.8)
- c. Parking for Civic Functions shall be determined by Warrant.

5.7.2 **Civic Space (CS)**

- a. Civic Spaces may be approved by Warrant in any Transect Zone.
- b. Civic Spaces shall be generally designed as described in Table 13.

5.7.3 **Civic Building (CB)**

- a. Civic Buildings shall be approved by Warrant or Variance in any Transect Zone, on sites reserved for them.
- b. Civic Buildings shall not be subject to the Requirements of the Building Scale Code. The particulars of their design shall be determined by Variance.

5.8 **SPECIAL REQUIREMENTS**

5.8.1 A Community Plan may designate the following special requirements:

- a. A differentiation of the Thoroughfares as a Primary-Grid (P-Grid) and a Secondary-Grid (S-Grid). Buildings along the P-Grid shall be held to the highest standard of this Code in support of pedestrian activity. Buildings along the S-Grid may be more readily considered for Warrants and Variances allowing automobile-oriented standards. The Frontages assigned to the S-Grid shall not exceed 30% of the total length within a Pedestrian Shed.
- b. A designation for Mandatory or Recommended Retail Frontage requiring that a building provide a Shopfront at sidewalk level along the entire length of the Frontage. The Shopfront shall be no less than 70% glazed in clear glass and provided with an awning overlapping the sidewalk as generally illustrated in Table 7. The first floor shall be confined to Retail use through the depth of the First Layer.
- c. A designation for mandatory or recommended Gallery Frontage, requiring that a building provide a permanent cover over the sidewalk, either cantilevered or supported by columns. The Gallery Frontage may be combined with a Retail Frontage as shown in Table 7.
- d. A designation of Coordinated Streetscape Frontage, requiring that the Public and Private Frontages be coordinated as a single, coherent landscape and paving design.
- e. A designation of Terminated Vista location, requiring that the building be provided with architectural articulation of a type and character that responds to the location as approved by the CRC.
- f. A designation for Cross Block Passages, requiring a minimum 8-foot-wide pedestrian access be reserved between buildings.

- g. A designation of Buildings of Value, requiring that such buildings and structures may be altered or demolished only when in accordance with preservation standards and protocols adopted by the Planning Commission.

5.9 PRE-EXISTING CONDITIONS

- 5.9.1 Existing buildings that do not conform to the provisions of this Code may continue in use as they are until a Substantial Modification is requested, at which time the Consolidated Review Committee (CRC) shall determine if they are a Grandfathered Property, as well as which provisions of this section that shall apply.
- 5.9.2 Renovation of existing buildings at any time shall require upgrade to the current Florida Building Code.
- 5.9.3 The modification of existing buildings is permitted by right if such changes result in greater conformance with the specifications of this section or Article 4 and comply with the Florida Building Code.
- 5.9.4 [RESERVED]
- 5.9.5 Any addition to or modification of a Building of Value that has been designated as such by the Planning Commission or is actually or potentially eligible for inclusion on a state, local or national historic register, including without limitation, the architectural harmony (similar materials, window proportions, color range, mass/void ratio, roof type and pitch) of such addition or modification, shall be subject to approval by the Planning Office.
- 5.9.6 The restoration or rehabilitation of an existing building shall not require the provision of (a) parking in addition to that existing or (b) on-site stormwater retention/detention in addition to that existing, except to the extent required by applicable Local, State or Federal law.

The Tables and their associated metrics are an integral part of the SmartCode. Like the preceding text pages, they are meant to be law after calibration and adoption. During calibration, individual Tables may be removed if not needed, and individual metrics may be adjusted for local character and custom. Table 14 is a summary of the metrics of the other Tables. Article 5 includes building scale codes, one page for each of the four urban Transect Zones. If the metrics are changed on Table 14 during calibration, be sure to adjust the Article 5 tables accordingly, and vice versa.

TABLE 1: Transect Zone Descriptions. This table provides description of the character of each Transect Zone.

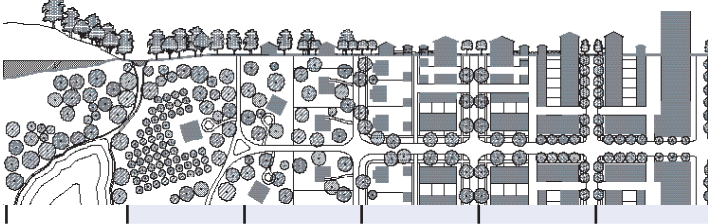
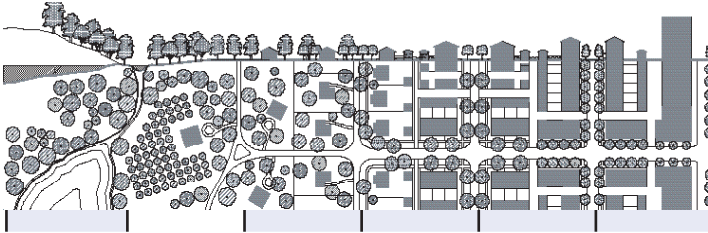
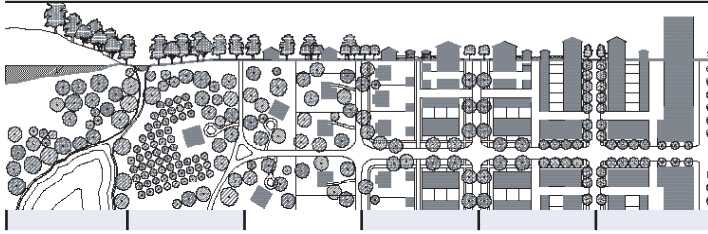
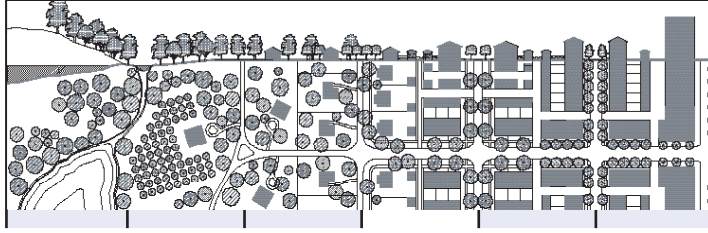
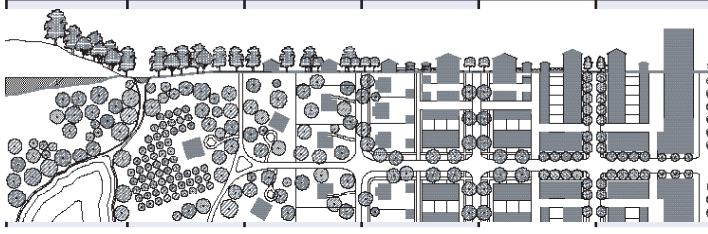
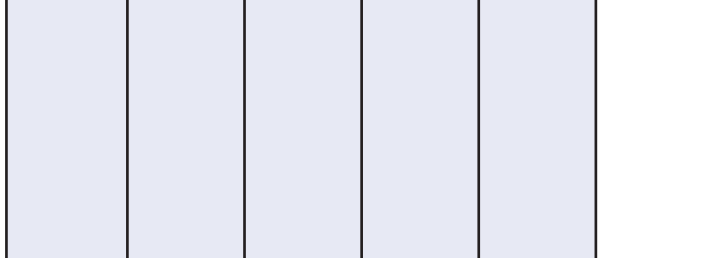
<p>T1 THE NATURAL ZONE consists of lands approximating or reverting to a wilderness condition, including lands unsuitable for settlement due to topography, hydrology or vegetation.</p>	
<p>T2 THE RURAL ZONE consists of lands in open or cultivated state or sparsely settled. These include woodland, agricultural lands, grasslands and irrigable deserts.</p>	
<p>T3 THE SUB-URBAN ZONE, consists of low density suburban residential areas, differing by allowing home occupations. Planting is naturalistic with setbacks relatively deep. Blocks may be large and the roads irregular to accommodate natural conditions.</p>	
<p>T4 THE GENERAL URBAN ZONE consists of a mixed-use but primarily residential urban fabric. It has a wide range of building types: single, sideyard, and rowhouses. Setbacks and landscaping are variable. Streets typically define medium-sized blocks.</p>	
<p>T5 THE URBAN CENTER ZONE consists of higher density mixed-use building types that accommodate retail, offices, rowhouses and apartments. It has a tight network of streets, with wide sidewalks, steady street tree planting and buildings set close to the frontages.</p>	
<p>T6 TRANSECT 6 ZONES ARE NOT INCLUDED IN THE BAYARD COMMUNITY PLAN AT THIS TIME</p> <p>THE URBAN CORE ZONE consists of the highest density, with the greatest variety of uses, and civic buildings of regional importance. It may have larger blocks; streets have steady street tree planting and buildings set close to the frontages.</p>	

TABLE 2: Sector/Community Allocation. Table 2 defines the geography, including both natural and infrastructure elements, determining areas that are or are not suitable for development. Specific Community Types of various intensities are allowable in specific Sectors. This table also allocates the proportions of Transect Zones within each Community Type. Within the larger scale Sector study the Bayard Community Plan is comprised of the Successional Communities G3, Intended, and G4, Infill, growth patterns.

TABLE 2A

DEVELOPED AREAS
DRY FLAT & ROLLING LAND
MEDIUM SLOPES WOODLANDS
FLOOD PLAIN OPEN SPACE TO BE ACQUIRED CORRIDORS TO BE ACQUIRED BUFFERS TO BE ACQUIRED LEGACY WOODLAND LEGACY FARMLAND LEGACY VIEWSHEDS CLD RESIDUAL OPEN SPACE
SURFACE WATERBODIES PROTECTED WETLANDS PROTECTED HABITAT RIPARIAN CORRIDORS PURCHASED OPEN SPACE CONSERV. EASEMENTS LAND TRUST TRANSPORT. CORRIDORS CLD RESIDUAL OPEN SPACE
RURAL GROWTH BOUNDARY
URBAN GROWTH BOUNDARY

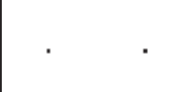




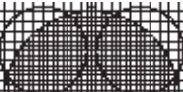
PRIMARILY OPEN SPACE		PRIMARILY NEW COMMUNITIES			SUCCESSIONAL COMMUNITIES			
01 OPEN SECTOR 1, PRESERVED	02 OPEN SECTOR 2, RESERVED	G1 GROWTH SECTOR 1, RESTRICTED	G2 GROWTH SECTOR 2, CONTROLLED	G3 GROWTH SECTOR 3, INTENDED	G4 GROWTH SECTOR 4, INFILL			
								
		CLD	CLD	TND	TND	RCD	TND	RCD

TABLE 2B

T1	NO MINIMUM	NO MINIMUM						
T2	NO MINIMUM	NO MINIMUM	50% MIN	50% MIN	NO MIN	NO MIN		
T3			10 - 30%	10 - 30%	10 - 30%	10 - 30%		
T4			20 - 40%	20 - 40%	30 - 50%	30 - 50%	10 - 30%	VARIABLE
T5					10 - 30%	10 - 30%	10 - 30%	VARIABLE
T6						40 - 80%	VARIABLE	VARIABLE

TABLE 3A: Vehicular Lane Dimensions. This table assigns lane widths to Transect Zones. The Design ADT (Average Daily Traffic) is the determinant for each of these sections. The most typical assemblies are shown in Table 3B. Specific requirements for truck and transit bus routes and truck loading shall be decided by Warrant.

DESIGN SPEED	TRAVEL LANE WIDTH	T1	T2	T3	T4	T5
Below 20 mph	12 feet			■	■	■
20-30 mph	10 feet	■	■	■	■	■
25-35 mph	11 feet		■	■	■	■
Above 35 mph (US-1)	12 feet					■

- BY RIGHT
- BY VARIANCE

DESIGN SPEED	PARKING LANE WIDTH					
20-25 mph	(Angle) 20 feet					■
20-25 mph	(Parallel) 7 feet			■	■	■
Above 35 mph	(Parallel) 9 feet					

(See Table 16b)

DESIGN SPEED	EFFECTIVE TURNING RADIUS					
Below 20 mph	5-10 feet			■	■	■
20-25 mph	10-15 feet		■	■	■	■
25-30 mph	15-20 feet	■	■	■		
Above 35 mph	20-30 feet					

TABLE 3B: Vehicular Lane/Parking Assemblies. The projected design speeds determine the dimensions of the vehicular lanes and turning radii assembled to create thoroughfares.









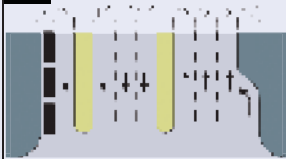




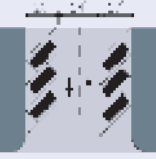

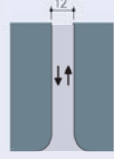
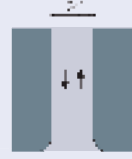
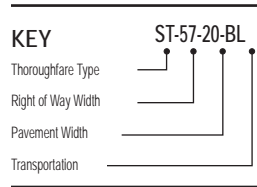
	ONE WAY MOVEMENT			TWO WAY MOVEMENT		
a. NO PARKING	T1 T2 T3	T1 T2 T3	T1 T2			
						
Design AADT	300 VPD		2,500 VPD			
Pedestrian Crossing	3 Seconds		5.5 Seconds			
Design Speed	20-30 MPH		20-25 MPH			
b. YIELD PARKING	T3 T4		T3 T4			
						
Design ADT	1,000 VPD		1,000 VPD			
Pedestrian Crossing	5 Seconds		7 Seconds			
c. PARKING ONE SIDE PARALLEL	T3 T4	T3 T4 T5	T4 T5	T5	T5	
						
Design ADT	5,000 VPD	18,000 VPD	16,000 VPD	15,000 VPD		
Pedestrian Crossing	5 Seconds	8 Seconds	8 Seconds	10 Seconds	5 Seconds 13 Seconds 14 Seconds	
Design Speed	20-30 MPH		25-30 MPH	25-30 MPH		
d. PARKING BOTH SIDES PARALLEL	T4		T4 T5	T5		
						
Design ADT	8,000 VPD		15,000 VPD	22,000 VPD		
Pedestrian Crossing	7 Seconds		10 Seconds	16 Seconds		
Design Speed	Below 20 MPH		25-30 MPH	25-30 MPH		
e. PARKING BOTH SIDES DIAGONAL	T5		T5	T5		
						
Design ADT	18,000 VPD		15,000 VPD	22,000 VPD		
Pedestrian Crossing	15 Seconds		17 Seconds	20 Seconds		
Design Speed	Below 20 MPH		20-25 MPH	25-30 MPH		
f. PARKING ACCESS			T3 T4	T5		
						
Design ADT						
Pedestrian Crossing			4 Seconds	7 Seconds		
Design Speed						

TABLE 3C: Thoroughfare Assemblies. These thoroughfares are assembled from the elements that appear in Tables 3A and 3B and incorporate the Public Frontages of Table 4. The key gives the thoroughfare type followed by the right-of-way width, followed by the pavement width, and in some instances followed by specialized transportation capability.

**THOROUGHFARE TYPES**

Boulevard:	BV
Commercial Avenue:	CAV
Commercial Street:	CS
Avenue	AV
Street:	ST
Road:	RD
Rear Alley:	RA
Rear Lane:	RL
Bicycle Trail:	BT
Bicycle Route:	BR
Bicycle Lane:	BL
Path:	PT
Transit Route:	TR

Thoroughfare Type
Transect Zone Assignment
Right-of-Way Width
Pavement Width
Movement
Design Speed
Pedestrian Crossing Time
Traffic Lanes
Parking Lanes
Curb Radius
Public Frontage Type
Walkway Type
Planter Type
Curb Type
Landscape Type
Transportation Provision

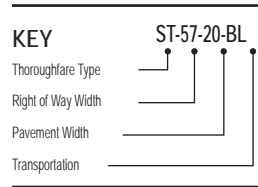
RL-24-12
Rear Lane
T4, T3
24 feet
12 feet
Yield Movement
10 MPH
3.5 seconds
2 lanes
None
10 to 15 feet
None
None
None
Inverted Crown
Opportunistic
None

RA-24-24
Rear Alley
T5, T4
24 feet
24 feet
Slow Movement
10 MPH
6.75 seconds
2 lanes
None
10 to 15 feet
None
None
None
Inverted Crown
Opportunistic
None

SMARTCODE

Community of Bayard, Jacksonville, Florida
June 1, 2007 Draft Code

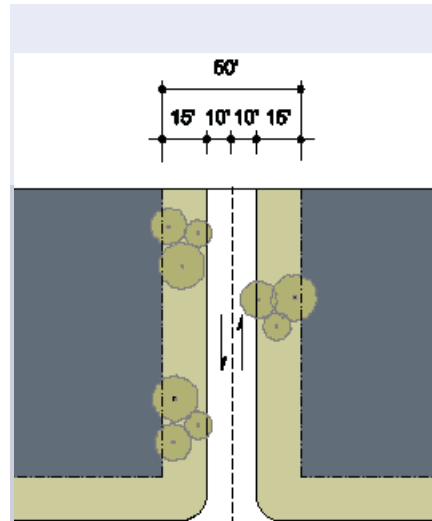
TABLE 3C THOROUGHFARE ASSEMBLIES (continued)



THOROUGHFARE TYPES

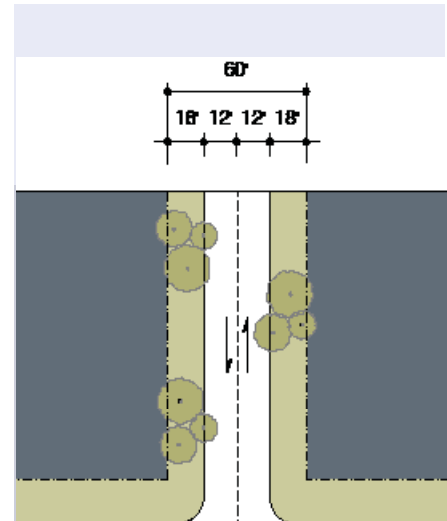
Boulevard:	BV
Commercial Avenue:	CAV
Commercial Street:	CS
Avenue	AV
Street:	ST
Road:	RD
Rear Alley:	RA
Rear Lane:	RL
Bicycle Trail:	BT
Bicycle Route:	BR
Bicycle Lane:	BL
Path:	PT
Transit Route:	TR

Thoroughfare Type
Transect Zone Assignment
Right-of-Way Width
Pavement Width
Movement
Design Speed
Pedestrian Crossing Time
Traffic Lanes
Parking Lanes
Curb Radius
Public Frontage Type
Walkway Type
Planter Type
Curb Type
Landscape Type
Transportation Provision



RD-50-20

Road
T3, T2, T1
50 feet
20 feet
Slow Movement
20 MPH
5.75 seconds
2 lanes
None
25 feet
Common Yard, Porch and Fence
Path optional (4 ft. min. sidewalk when provided)
Continuous Swale
Swale
Trees clustered at 30' o.c. Avg.
BT



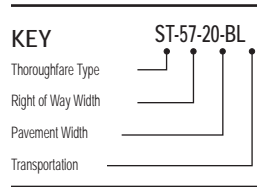
RD-60-24

Road
T3
60 feet
24 feet
Slow Movement
20 MPH
7 seconds
2 lanes
None
25 feet
Common Yard, Porch and Fence
Path optional (4 ft. min. sidewalk when provided)
Continuous Swale
Swale
Trees clustered at 30' o.c. Avg.
BT

SMARTCODE

Community of Bayard, Jacksonville, Florida
June 1, 2007 Draft Code

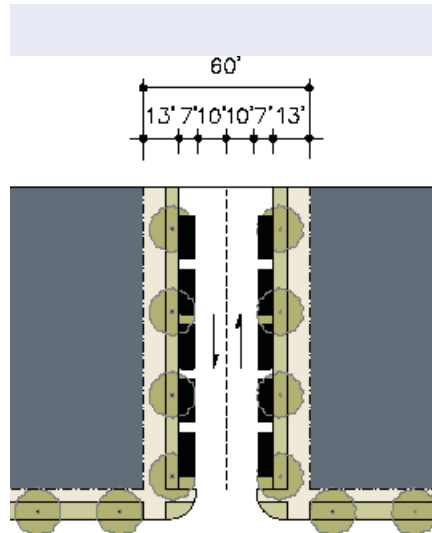
TABLE 3C THOROUGHFARE ASSEMBLIES (continued)



THOROUGHFARE TYPES

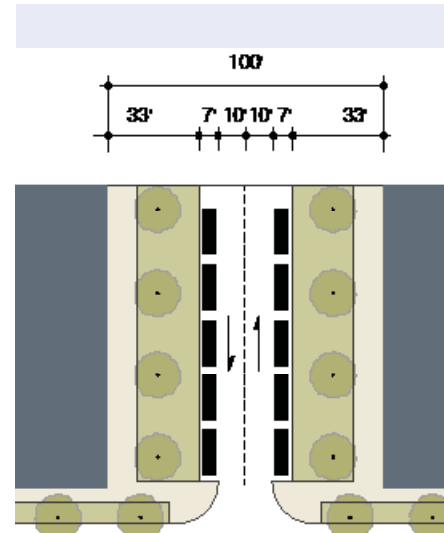
Boulevard:	BV
Commercial Avenue:	CAV
Commercial Street:	CS
Avenue	AV
Street:	ST
Road:	RD
Rear Alley:	RA
Rear Lane:	RL
Bicycle Trail:	BT
Bicycle Route:	BR
Bicycle Lane:	BL
Path:	PT
Transit Route:	TR

Thoroughfare Type
Transect Zone Assignment
Right-of-Way Width
Pavement Width
Movement
Design Speed
Pedestrian Crossing Time
Traffic Lanes
Parking Lanes
Curb Radius
Public Frontage Type
Walkway Type
Planter Type
Curb Type
Landscape Type
Transportation Provision



ST-60-34

Street
T5, T4, T3
60 feet
34 feet
Slow Movement
20 MPH
5.75 seconds
2 lanes
Both Sides @ 7 feet marked
15 feet
Common Yard, Porch & Fence, Stoop, Forecourt, Terr./Lt. Ct.
6 foot Sidewalk
7 foot Continuous Planter
Curb or Swale
Trees at 30' o.c. Avg.
BR



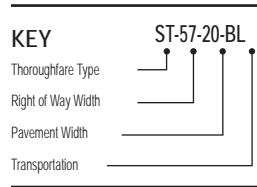
ST-100-34

Street
T5, T4, T3
100 feet
34 feet
Slow Movement
20 MPH
5.75 seconds
2 lanes
Both Sides @ 7 feet marked
15 feet
Com. Yd., Porch & Fence, Stoop, Forecourt, Terr./Lt. Ct., Shplnt.
10 foot Sidewalk
23 foot Continuous Planter
Curb
Trees at 30' o.c. Avg.
BR

SMARTCODE

Community of Bayard, Jacksonville, Florida
June 1, 2007 Draft Code

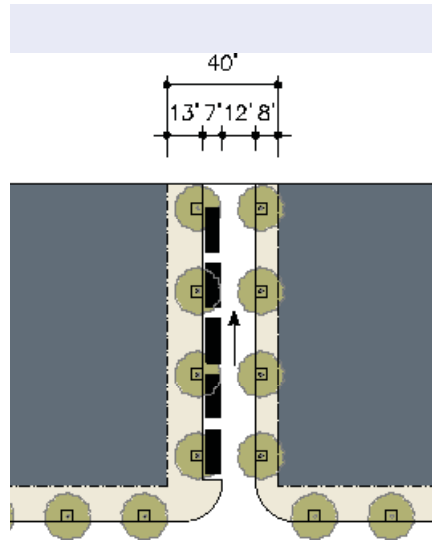
TABLE 3C THOROUGHFARE ASSEMBLIES (continued)



THOROUGHFARE TYPES

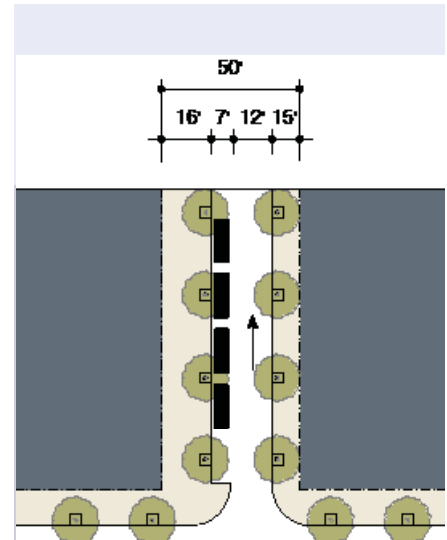
Boulevard:	BV
Commercial Avenue:	CAV
Commercial Street:	CS
Avenue	AV
Street:	ST
Road:	RD
Rear Alley:	RA
Rear Lane:	RL
Bicycle Trail:	BT
Bicycle Route:	BR
Bicycle Lane:	BL
Path:	PT
Transit Route:	TR

Thoroughfare Type
Transect Zone Assignment
Right-of-Way Width
Pavement Width
Movement
Design Speed
Pedestrian Crossing Time
Traffic Lanes
Parking Lanes
Curb Radius
Public Frontage Type
Walkway Type
Planter Type
Curb Type
Landscape Type
Transportation Provision



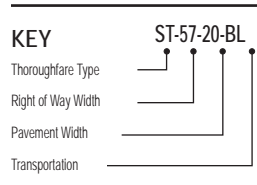
CS-40-19

Commercial Street
T5, T4
40 feet
19 feet
Yield Movement
20 MPH
3.5 seconds
1 lane
One Side @ 7 feet marked
15 feet
Shop Front, Gallery/Awning, Forecourt, Terr./Lt. Cl.
8 ft. and 13 ft. Sidewalks
4'x4' Tree Well
Curb
Trees at 30' o.c. Avg.
BR

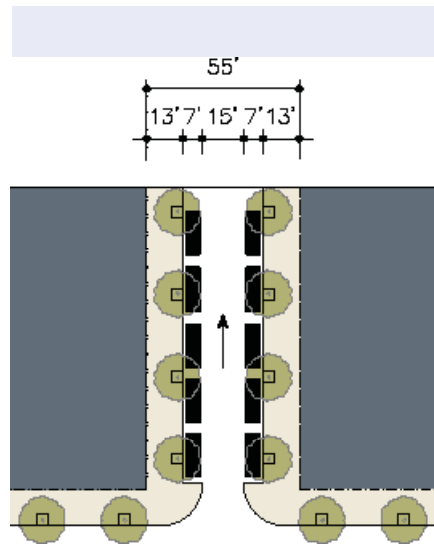


CS-50-19

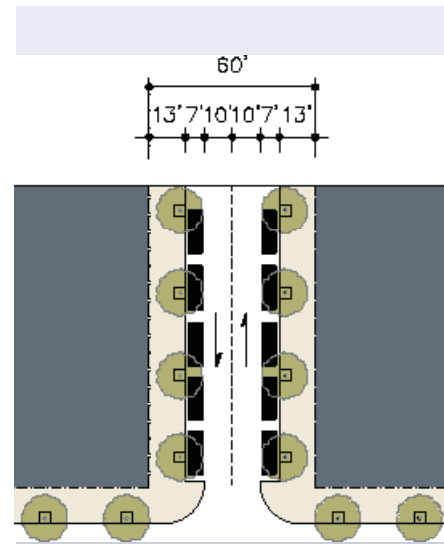
Commercial Street
T5, T4
50 feet
19 feet
Slow Movement
20 MPH
3.5 seconds
1 lane
One Side @ 7 feet marked
15 feet
Gallery/Arcade, Shopfront/Awning, Forecourt, Terr./Lt. Cl.
15 ft. and 16 ft. Sidewalks
4'x4' Tree well
Curb
Trees at 30' o.c. Avg.
BR

**THOROUGHFARE TYPES**

Boulevard:	BV
Commercial Avenue:	CAV
Commercial Street:	CS
Avenue	AV
Street:	ST
Road:	RD
Rear Alley:	RA
Rear Lane:	RL
Bicycle Trail:	BT
Bicycle Route:	BR
Bicycle Lane:	BL
Path:	PT
Transit Route:	TR

**CS-55-29**

Thoroughfare Type	Commercial Street
Transect Zone Assignment	T5, T4
Right-of-Way Width	55 feet
Pavement Width	29 feet
Movement	Slow Movement
Design Speed	20 MPH
Pedestrian Crossing Time	4.25 seconds
Traffic Lanes	1 lane
Parking Lanes	Both Sides @ 7 feet marked
Curb Radius	15 feet
Public Frontage Type	Gallery/Arcade, Shopfront/Awning, Forecourt, Terr./Lt. Ct.
Walkway Type	13 foot Sidewalk
Planter Type	4'x4' Tree Well
Curb Type	Curb
Landscape Type	Trees at 30' o.c. Avg.
Transportation Provision	BR

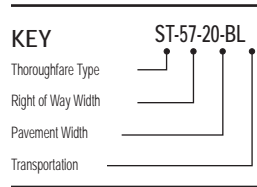
**CS-60-34**

Thoroughfare Type	Commercial Street
Transect Zone Assignment	T5, T4
Right-of-Way Width	60 feet
Pavement Width	34 feet
Movement	Slow Movement
Design Speed	20 MPH
Pedestrian Crossing Time	5.75 seconds
Traffic Lanes	2 lanes
Parking Lanes	Both Sides @ 7 feet marked
Curb Radius	15 feet
Public Frontage Type	Gallery/Arcade, Shopfront/Awning, Forecourt, Terr./Lt. Ct.
Walkway Type	13 foot Sidewalk
Planter Type	4'x4' Tree Well
Curb Type	Curb
Landscape Type	Trees at 30' o.c. Avg.
Transportation Provision	BR

SMARTCODE

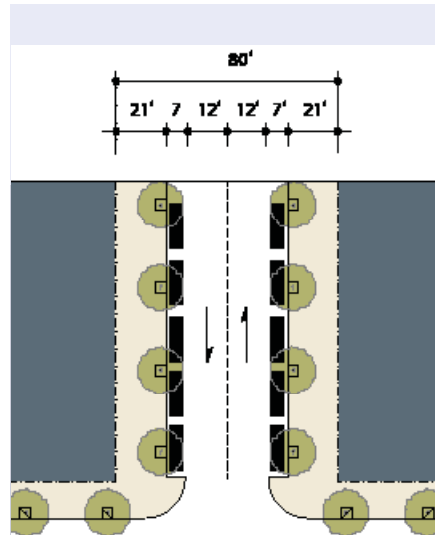
TABLE 3C THOROUGHFARE ASSEMBLIES (continued)

Community of Bayard, Jacksonville, Florida
June 1, 2007 Draft Code

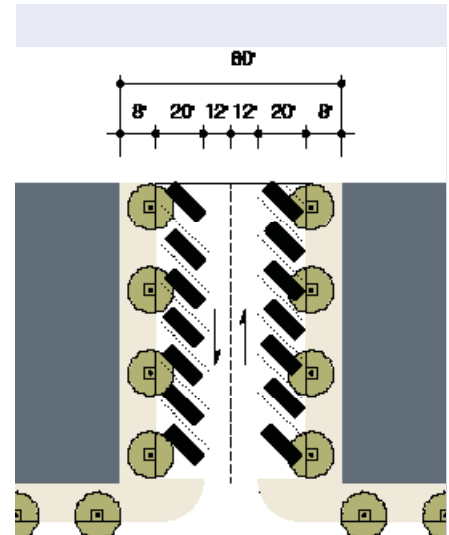


THOROUGHFARE TYPES

Boulevard:	BV
Commercial Avenue:	CAV
Commercial Street:	CS
Avenue	AV
Street:	ST
Road:	RD
Rear Alley:	RA
Rear Lane:	RL
Bicycle Trail:	BT
Bicycle Route:	BR
Bicycle Lane:	BL
Path:	PT
Transit Route:	TR



CS-80-38

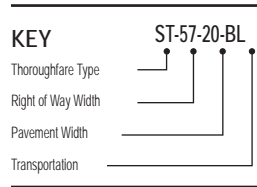


CS-80-64

Thoroughfare Type
Transect Zone Assignment
Right-of-Way Width
Pavement Width
Movement
Design Speed
Pedestrian Crossing Time
Traffic Lanes
Parking Lanes
Curb Radius
Public Frontage Type
Walkway Type
Planter Type
Curb Type
Landscape Type
Transportation Provision

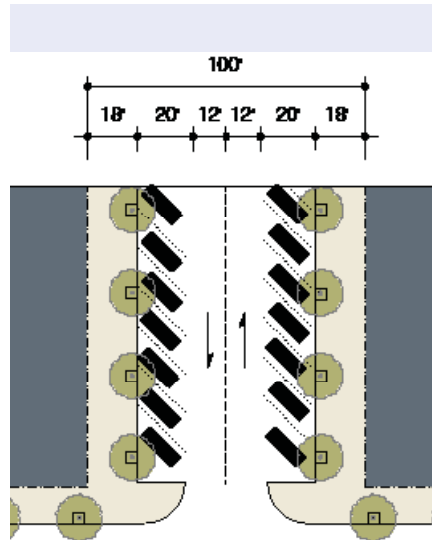
Commercial Street
T5
80 feet
38 feet
Slow Movement
20 MPH
7 seconds
2 lanes
Both Sides @ 7 feet marked
15 feet
Gallery/Arcade, Shopfront/Awning, Forecourt, Terr./Li. Cl.
21 foot Sidewalk
4'x4' Tree Well
Curb
Trees at 30' o.c. Avg.
BR

Commercial Street
T5
80 feet
64 feet
Slow Movement
25 MPH
7 seconds
2 lanes
Both sides Back In Angled @ 20 feet marked
15 feet
Gallery/Arcade, Shopfront/Awning, Forecourt, Terr./Li. Cl.
8 foot Sidewalk
4'x4' Tree Well
Curb
Trees at 30' o.c. Avg.
BR

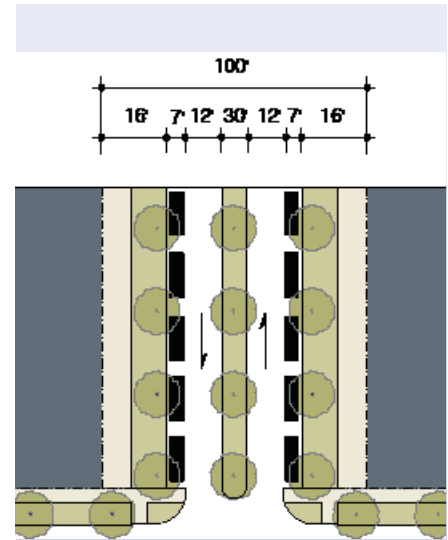


THOROUGHFARE TYPES

Boulevard:	BV
Commercial Avenue:	CAV
Commercial Street:	CS
Avenue:	AV
Street:	ST
Road:	RD
Rear Alley:	RA
Rear Lane:	RL
Bicycle Trail:	BT
Bicycle Route:	BR
Bicycle Lane:	BL
Path:	PT
Transit Route:	TR



CS-100-64



AV-100-38

Thoroughfare Type	Commercial Street
Transect Zone Assignment	T5
Right-of-Way Width	100 feet
Pavement Width	64 feet
Movement	Slow Movement
Design Speed	20 MPH
Pedestrian Crossing Time	7 seconds
Traffic Lanes	2 lanes
Parking Lanes	Both sides Back In Angled @ 20 feet marked
Curb Radius	15 feet
Public Frontage Type	Gallery/Arcade, Shopfront/Awning, Forecourt, Terr./LT. Ct.
Walkway Type	18 foot Sidewalk
Planter Type	4'x4' Tree Well
Curb Type	Curb
Landscape Type	Trees at 30' o.c. Avg.
Transportation Provision	BR

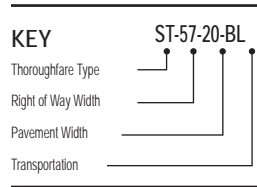
Thoroughfare Type	Avenue
Transect Zone Assignment	T4, T3
Right-of-Way Width	100 feet
Pavement Width	38 feet
Movement	Slow Movement
Design Speed	25 MPH
Pedestrian Crossing Time	19.5 seconds
Traffic Lanes	2 lanes
Parking Lanes	Both Sides @ 7 feet marked
Curb Radius	10 feet
Public Frontage Type	Common Yard, Porch & Fence, Forecourt, Stoop
Walkway Type	8 foot Sidewalk
Planter Type	8 foot Continuous Planter
Curb Type	Curb
Landscape Type	Trees at 30' o.c. Avg.
Transportation Provision	BR

Thoroughfare Type	Avenue
Transect Zone Assignment	T4, T3
Right-of-Way Width	100 feet
Pavement Width	38 feet
Movement	Slow Movement
Design Speed	25 MPH
Pedestrian Crossing Time	19.5 seconds
Traffic Lanes	2 lanes
Parking Lanes	Both Sides @ 7 feet marked
Curb Radius	10 feet
Public Frontage Type	Common Yard, Porch & Fence, Forecourt, Stoop
Walkway Type	8 foot Sidewalk
Planter Type	8 foot Continuous Planter
Curb Type	Curb
Landscape Type	Trees at 30' o.c. Avg.
Transportation Provision	BR

SMARTCODE

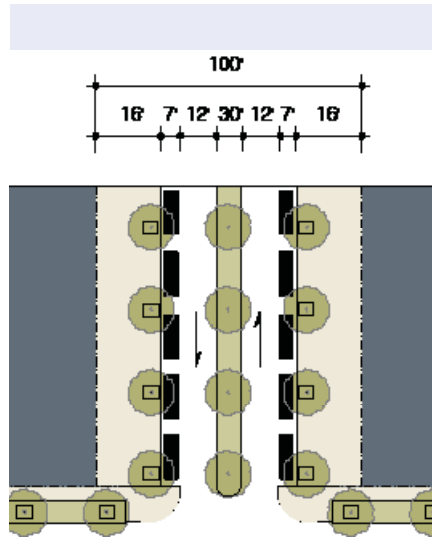
TABLE 3C THOROUGHFARE ASSEMBLIES (continued)

Community of Bayard, Jacksonville, Florida
June 1, 2007 Draft Code



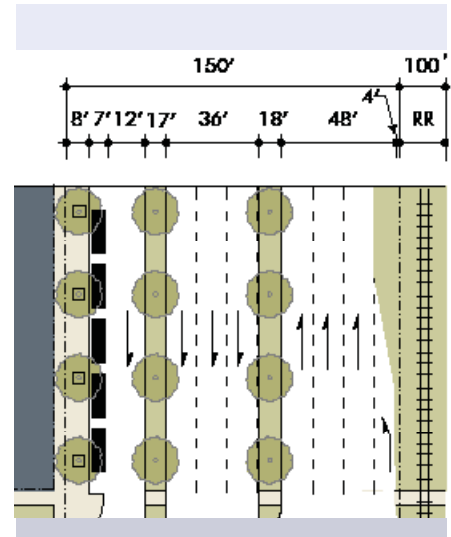
THOROUGHFARE TYPES

Boulevard:	BV
Commercial Avenue:	CAV
Commercial Street:	CS
Avenue	AV
Street:	ST
Road:	RD
Rear Alley:	RA
Rear Lane:	RL
Bicycle Trail:	BT
Bicycle Route:	BR
Bicycle Lane:	BL
Path:	PT
Transit Route:	TR



CAV-100-38

Thoroughfare Type	Commercial Avenue
Transect Zone Assignment	T5
Right-of-Way Width	100 feet
Pavement Width	38 feet
Movement	Slow Movement
Design Speed	20 MPH
Pedestrian Crossing Time	19.5 seconds
Traffic Lanes	2 lanes
Parking Lanes	Both Sides @ 7 feet marked
Curb Radius	10 feet
Public Frontage Type	Gallery/Arcade, Shopfront/Awning, Forecourt, Terr./Lt. Ct.
Walkway Type	16 foot Sidewalk
Planter Type	4' x 4' Tree Well
Curb Type	Curb
Landscape Type	Trees at 30' o.c. Avg.
Transportation Provision	BR



BV-250-103

Thoroughfare Type	Boulevard
Transect Zone Assignment	Highway US - 1
Right-of-Way Width	250 feet (150 ft. Vehicular + 100 ft. Railroad)
Pavement Width	19 feet - 36 feet - 48 feet
Movement	Free Movement
Design Speed	20 MPH for Slip Lane; 45 mph for Through Lanes
Pedestrian Crossing Time	5.5 seconds - 10.25 seconds - 13.75 seconds
Traffic Lanes	6 lanes & one one-way Slip Lane
Parking Lanes	7 feet
Curb Radius	10 feet
Public Frontage Type	Gallery/Arcade, Shopfront/Awning, Forecourt, Terr./Lt. Ct.
Walkway Type	8 ft. Sidewalk (Min. addit. 6 ft. sidewalk constr. by retail use)
Planter Type	4' x 4' Tree Well
Curb Type	Curb
Landscape Type	Trees at 30' o.c. Avg.
Transportation Provision	BR

TABLE 4A: Public Frontages - General The Public Frontage is the area between the private lot line and the edge of the vehicular lanes. Dimensions are given in Table 4B (Public Frontages - Specific)

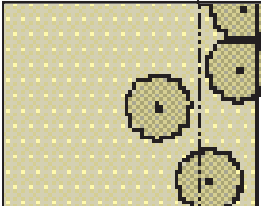
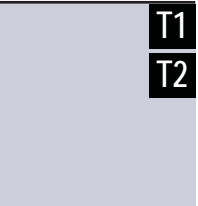
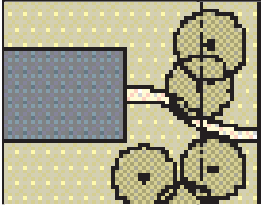
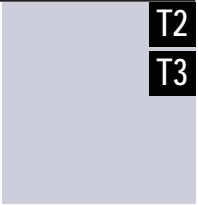
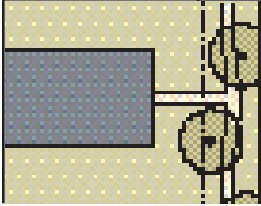
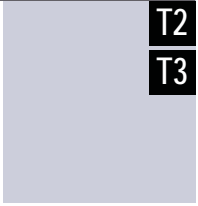
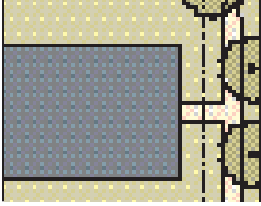
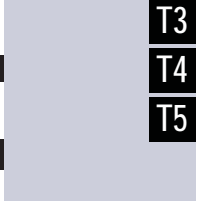
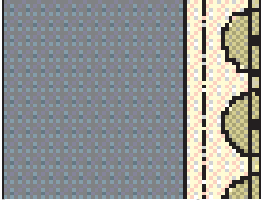
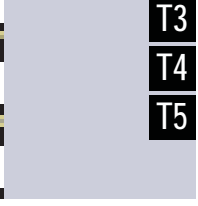

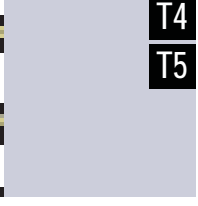


	PLAN		
	PRIVATE FRONTAGE	LOT R.O.W. PUBLIC FRONTAGE	
a. (HW) For Highways: This frontage has open swales drained by percolation, bicycle trails and no parking. The landscaping consists of the natural condition or multiple species arrayed in naturalistic clusters. Buildings are buffered by distance or berms.			T1 T2
b. (RR) For Rural Roads: This frontage has open swales drained by percolation, without parking. The landscaping consists of multiple tree and shrub species arrayed in naturalistic clusters			T2 T3
c. (SR) For Standard Roads: This frontage has open swales drained by percolation and a walking path or bicycle trail along one or both sides and yield parking. The landscaping consists of multiple species arrayed in naturalistic clusters.			T2 T3
d. (RS) For Residential Street: This frontage has raised curbs drained by inlets and narrow sidewalks separated from the vehicular lanes by a wide continuous planter, with parking on one or both sides. The landscaping consists of street trees of a single or alternating species aligned in a regularly spaced alley.			T3 T4 T5
e. (SS) (AV) For Standard Streets or Avenues: This frontage has raised curbs drained by inlets and wide sidewalks separated from the vehicular lanes by a narrow continuous planter with parking on both sides. The landscaping consists of a single tree species aligned in a regularly spaced alley.			T3 T4 T5
f. (CS) (AV) For Commercial Streets or Avenues: This frontage has raised curbs drained by inlets and very wide sidewalks along both sides separated from the vehicular lanes by separate tree wells with grates and parking on both sides. The landscaping consists of a single tree species aligned with regular spacing where possible but clears the storefront entrances.			T4 T5
g. (BV) For Boulevards: This frontage has slip roads on one side. It consists of raised curbs drained by inlets and sidewalks on one side, separated from the vehicular lanes by planters. The landscaping consists of double rows of a single tree species aligned in a regularly spaced alley.			T5

Table 4B: Public Frontages - Specific. This table assembles prescriptions and dimensions for the public frontage elements - curbs, walkways and planters – relative to specific thoroughfare types within Transect Zones. Table 4B-a assembles all of the elements for the various street types. Locally appropriate planting species should be filled in to the calibrated Code.

TRANSECT ZONE Public Frontage Type	R U R A L U R B A N															
	T1	T2	T2	T3	T3	T4	T5	T4	T5							
	RD		RD & ST		ST-AV			CS-CAV-BV								
a. Assembly: The principal variables are the type and dimension of curbs, walkways, planters and landscape.																
Total Width	15 feet		13 feet -33 feet		13 feet -33 feet			13 feet -33 feet								
b. Curb: The detailing of the edge of the vehicular pavement, incorporating drainage.																
Type	Open Swale		Open Swale		Raised Curb			Raised Curb								
Radius	25 feet		15-25 feet		10 feet -15 feet			10-15 feet								
c. Walkway: The pavement dedicated exclusively to pedestrian activity.																
Type	Path Optional		Path Optional		Sidewalk			Sidewalk								
Width	n/a		4-6 feet		6 feet - 10 feet			13-31 feet								
d. Planter: The layer which accommodates street trees and other landscape.																
Arrangement	Clustered		Clustered		Regular			Opportunistic								
Species	Multiple		Multiple		Single			Single								
Planter Type	Continuous Swale		Continuous Swale		Continuous Planter			Tree Well								
Planter Width	15 feet		7 feet-23 feet		7 feet - 23 feet			4 feet - 6 feet								

TABLE 5: Public Lighting. Lighting varies in brightness and also in the character of the fixture according to the Transect. The table shows five common types. A listed set of streetlights corresponding to these types would be approved by the utility company and listed on the page.












	T1	T2	T3	T4	T5	Specifications
Cobra Head 	■	■				JEA approved light standards. <hr/> <hr/> <hr/> <hr/> <hr/> <hr/> <hr/> <hr/>
Pipe 		■	■			JEA approved light standards. <hr/> <hr/> <hr/> <hr/> <hr/> <hr/> <hr/> <hr/>
Post 			■	■		JEA approved light standards. <hr/> <hr/> <hr/> <hr/> <hr/> <hr/> <hr/> <hr/>
Column 			■	■	■	GranVille luminaire with black decorative cover, Leaf housing on a 12-foot Wadsworth black decorative aluminium post. <hr/> <hr/> <hr/> <hr/> <hr/> <hr/> <hr/> <hr/>
Double Column 					■	Twin GranVille luminaires with black decorative cover, Leaf housing on a 12-foot Wadsworth black decorative aluminium post. <hr/> <hr/> <hr/> <hr/> <hr/> <hr/> <hr/> <hr/>

TABLE 6: Public Planting. This table shows six common types of street tree shapes and their appropriateness within the Transect Zones. The local planning office selects species appropriate for the bioregion.

	T1	T2	T3	T4	T5	Specific Planting
Pole 				■	■	Phoenix dactylifera 'Medjool' - Medjool Date Palm <hr/> Sabal Palmetto - Sabal Palm <hr/> Washingtonia Robusta - Washington Palm <hr/>
Oval 			■	■	■	Ilex attenuata 'East Palatka' - East Palatka Holly <hr/> Quercus phellos 'Hightower' - Hightower Willow Oak <hr/> <hr/> <hr/>
Ball 		■	■	■		Lagerstroemia indica 'Tuscarora' - Lavender Crape Myrtle <hr/> Ulmus parvifolia 'Emer II' - Allee Elm <hr/> <hr/> <hr/>
Pyramid 	■	■	■	■		Acer rubrum 'October Glory' - Red Maple <hr/> Magnolia grandiflora 'Migtig' - Migtig Magnolia <hr/> Magnolia grandiflora - Southern Magnolia <hr/> <hr/>
Umbrella 		■	■	■	■	Ulmus parvifolia 'Drake' - Drake Elm <hr/> <hr/> <hr/> <hr/> <hr/>
Vase 	■	■	■	■		Quercus alba - White Oak <hr/> Quercus michauxii - Swamp Chesnut Oak <hr/> Quercus virginiana - Live Oak <hr/> Ulmus alata - Winged Elm <hr/>

* All street trees shall have a minimum 4 inch caliper when installed.

** Other tree species shall be considered by the Consolidated Review Committee during Site Plan Review.

TABLE 7: Private Frontages. The Private Frontage is the area between the building and the lot lines.

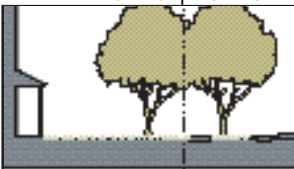
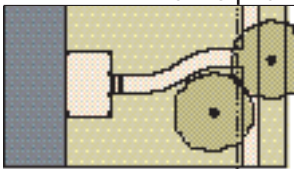

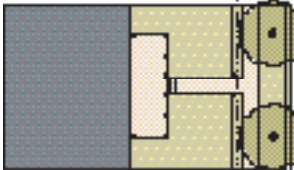



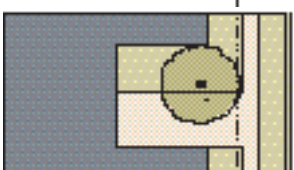


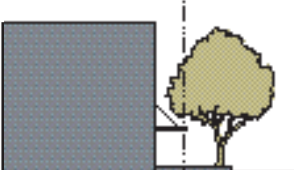

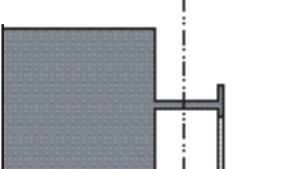

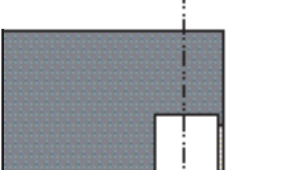

	SECTION		PLAN	
	LOT PRIVATE FRONTAGE	R.O.W. PUBLIC FRONTAGE	LOT PRIVATE FRONTAGE	R.O.W. PUBLIC FRONTAGE
a. Common Yard: a frontage wherein the facade is set back substantially from the frontage line. The front yard created remains unfenced and is visually continuous with adjacent yards, supporting a common landscape. The deep setback provides a buffer from the higher speed thoroughfares.				T2 T3
b. Porch & Fence: a frontage wherein the facade is set back from the frontage line with an attached porch permitted to encroaching. A fence at the frontage line maintains the demarcation of the yard. The porches shall be no less than 8 feet deep.				T3 T4
c. Terrace or Light Court: a frontage wherein the facade is set back from the frontage line by an elevated terrace or a sunken light court. This type buffers residential use from urban sidewalks and removes the private yard from public encroachment. The terrace is suitable for conversion to outdoor cafes.				T4 T5
d. Forecourt: a frontage wherein a portion of the facade is close to the frontage line and the central portion is set back. The forecourt created is suitable for vehicular drop-offs. This type should be allocated in conjunction with other frontage types. Large trees within the forecourts may overhang the sidewalks.				T4 T5
e. Stoop: a frontage wherein the facade is aligned close to the frontage line with the first story elevated from the sidewalk sufficiently to secure privacy for the windows. The entrance is usually an exterior stair and landing. This type is recommended for ground-floor residential use.				T4 T5
f. Shopfront and Awning: a frontage wherein the facade is aligned close to the frontage line with the building entrance at sidewalk grade. This type is conventional for retail use. It has a substantial glazing on the sidewalk level and an awning that may overlap the sidewalk to the maximum extent possible.				T4 T5
g. Gallery: a frontage wherein the facade is aligned close to the frontage line with an attached cantilevered shed or a lightweight colonnade overlapping the sidewalk. This type is conventional for retail use. The gallery shall be no less than 10 feet wide and may overlap the whole width of the sidewalk to within 2 feet of the curb.				T4 T5
h. Arcade: a frontage wherein the facade is a colonnade that overlaps the sidewalk, while the facade at sidewalk level remains at the frontage line. This type is conventional for retail use. The arcade shall be no less than 12 feet wide and may overlap the whole width of the sidewalk to within 2 feet of the curb.				T5

TABLE 8: Building Configuration. This table shows prescribed building heights for each Transect Zone. The vertical extent of a building is measured by number of stories, not including a raised basement or an inhabited attic. Heights are measured from the average grade of the frontage line to the eave of a pitched roof or to the surface of a flat roof. See Section 5.4.11 for height restrictions within Transect 4.

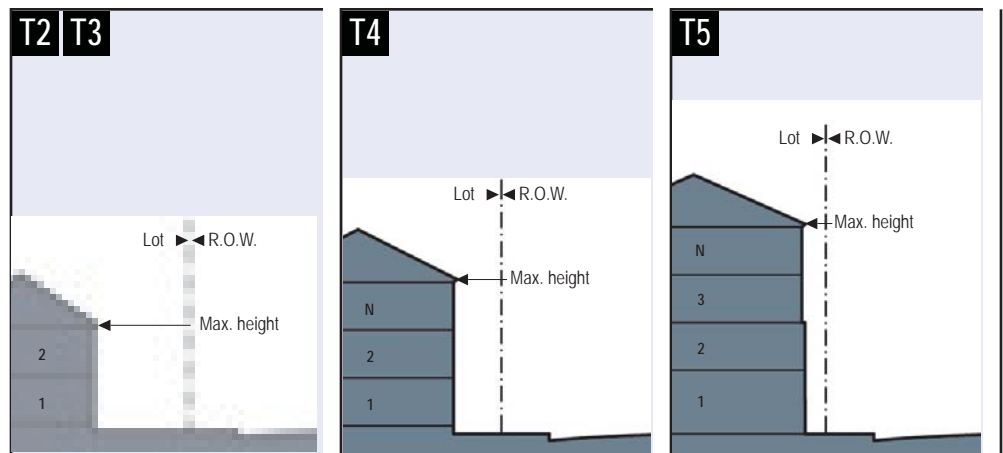


TABLE 9: Building Type. This table approximates the location of the structure relative to the boundaries of each individual lot, establishing suitable basic building types for each Transect Zone.

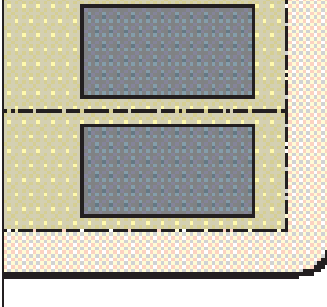
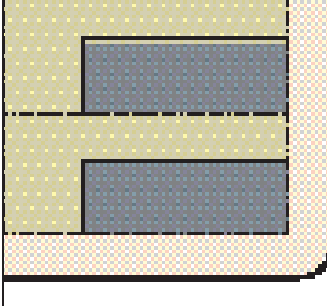
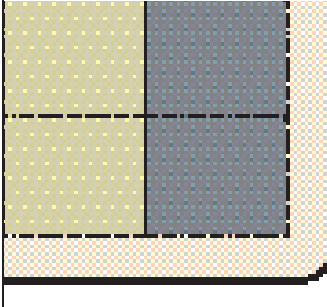
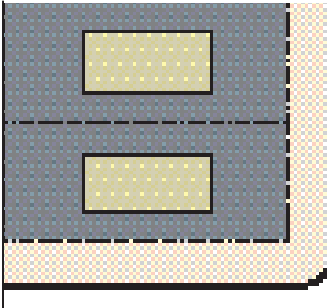
<p>a. Edgeyard: Specific Types - Single family House, Cottage, Villa, Estate House, Urban Villa. A building that occupies the center of its lot with Setbacks on all sides. This is the least urban of types as the front yard sets it back from the frontage, while the side yards weaken the spatial definition of the public Thoroughfare space. The front yard is intended to be visually continuous with the yards of adjacent buildings. The rear yard can be secured for privacy by fences and a well-placed Backbuilding and/or Outbuilding.</p>	 <div data-bbox="1404 331 1451 468"> <p>T2</p> <p>T3</p> <p>T4</p> </div>
<p>b. Sideyard: Specific Types - 'Charleston' Single Sideyard House, zero-lot-line house. A building that occupies one side of the lot with the Setback to the other side. The visual opening of the side yard on the street frontage causes this building type to appear freestanding. A shallow frontage Setback defines a more urban condition. If the adjacent building is similar with a blank party wall, the yard can be quite private. This type permits systematic climatic orientation in response to the sun or the breeze.</p>	 <div data-bbox="1404 657 1451 751"> <p>T4</p> <p>T5</p> </div>
<p>c. Rearyard: Specific Types - Townhouse, Rowhouse, Live-Work unit, perimeter block. A building that occupies the full frontage, leaving the rear of the lot as the sole yard. This is a very urban type as the continuous Facade steadily defines the public Thoroughfare. The rear Elevations may be articulated for functional purposes. In its Residential form, this type is the Rowhouse. For its Commercial form, the rear yard can accommodate substantial parking.</p>	 <div data-bbox="1404 982 1451 1077"> <p>T4</p> <p>T5</p> </div>
<p>d. Courtyard: Specific Types - Patio House. A building that occupies the boundaries of its lot while internally defining one or more private patios. This is the most urban of types, as it is able to shield the private realm from all sides while strongly defining the public Thoroughfare. Because of its ability to accommodate incompatible activities, masking them from all sides, it is recommended for workshops, Lodging and schools. The high security provided by the continuous enclosure is useful for crime-prone areas.</p>	 <div data-bbox="1404 1308 1451 1360"> <p>T5</p> </div>

TABLE 10: Specific Function. This table expands the Building Function categories of Table 11 to delegate specific functions within Transect Zones. Table 10 should be customized for local character and requirements.

	T1	T2	T3	T4	T5
a. RESIDENTIAL					
Apartment Building				■	■
Row House				■	■
Duplex House				■	■
Sideyard House				■	■
Cottage			■	■	
House		■	■	■	
Estate House		■	■		
Accessory Unit		■	■	■	■
Manufactured House					
Live-Work Unit			■	■	■
b. LODGING					
Hotel (no room limit)					■
Inn (up to 12 rooms)			□	■	■
Inn (up to 5 rooms)			■	■	■
c. OFFICE					
Office Building					■
Live-Work Unit				■	■
d. RETAIL					
Open-Market Building					■
Retail Building					■
Display Gallery					■
Restaurant					■
Kiosk					■
Push Cart					□
Liquor Selling Establishment					□
e. CIVIC					
Bus Shelter			■	■	■
Fountain or Public Art		■	■	■	■
Library				■	■
Live Theater					■
Museum					□
Outdoor Auditorium					■
Parking Structure					■
Passenger Terminal					□
Playground		■	■	■	■
Surface Parking Lot				□	□
Religious Assembly			■	■	■

	T1	T2	T3	T4	T5
f. OTHER: AGRICULTURE					
Greenhouse	■	■			
Stable	■	■			
f. OTHER: AUTOMOTIVE					
Gasoline					□*
Drive -Through Facility					□*
f. OTHER: CIVIL SUPPORT					
Fire Station			■	■	■
Police Station				■	■
Funeral Home					□
Medical Clinic					■
f. OTHER: EDUCATION					
College					□
Other- Childcare Center		□	□	□	■
f. OTHER: INDUSTRIAL					
Laboratory Facility					□
Electric Substation	□	□	□	□	□
Wireless Transmitter	□	□			
Cremation Facility					□

- BY RIGHT
- BY VARIANCE
- * New Gasoline and Drive-Through Facilities shall be permitted only in Transect 5 Zone south of Eli Avenue and west of Targonski Street alignments.

TABLE 11: Building Function - General. This table categorizes building functions within Transect Zones. For greater precision describing the functions, see Table 10.

	T3	T4	T5
a. RESIDENTIAL	Open Residential: The number of dwellings on each lot is restricted to one within a principal building and one within an ancillary building, with 2.0 parking places for each. Both dwellings shall be under single ownership. The habitable area of the ancillary dwelling shall not exceed 625 square feet.	Open Residential: The number of dwellings on each lot is limited by the requirement of 1.5 parking places for each dwelling, a ratio which may be reduced for Live-Work type buildings according to the shared parking standards (See Table 10).	Open Residential: The number of dwellings on each lot is limited by the requirement of 1.0 parking place for each dwelling, a ratio which may be reduced according to the shared parking standards (See Table 10).
b. LODGING	Limited Lodging: The number of bedrooms available on each lot for lodging is limited by the requirement of 1.0 assigned parking place for each bedroom, up to five, in addition to the parking requirement for the dwelling. Food service may be provided in the a.m.	Limited Lodging: The number of bedrooms available on each lot for lodging is limited by the requirement of 1.0 assigned parking place for each bedroom, up to twelve, in addition to the parking requirement for the dwelling. Food service may be provided in the a.m. The maximum length of stay shall not exceed fourteen days.	Open Lodging: The number of bedrooms available on each lot for lodging is limited by the requirement of 1.0 assigned parking place for each bedroom. Food service may be provided at all times.
c. OFFICE	Restricted Office: Home occupation use only.	Restricted Office: Home occupation use only.	Open Office: The building area available for office use on each lot is limited by the requirement of 2.5 assigned parking places per 1000 square feet of net office space.
d. RETAIL	Prohibited Retail: Retail use is prohibited in this Transect	Prohibited Retail: Retail use is prohibited in this Transect except in Live-Work type buildings identified on the Transect Map. Live-Work buildings shall be limited by the requirement of 3.0 assigned parking places per 1000 square feet of net retail space in addition to the parking requirement for each dwelling.	Open Retail: The building area available for retail use is limited by the requirement of 3.0 assigned parking places per 1000 square feet of net retail space.
e. CIVIC	See Table 10	See Table 10	See Table 10
f. OTHER	See Table 10	See Table 10	See Table 10

TABLE 12: Parking Calculation. The Required Parking table summarizes the parking requirements of Table 11 for each site or, conversely, the amount of building allowed on each site given the parking available.

REQUIRED PARKING (See table 11)			
	T2 T3	T4	T5
RESIDENTIAL	2.0 / dwelling	1.5 / dwelling	1.0 / dwelling
LODGING	1.0 / bedroom	1.0 / bedroom	1.0 / bedroom
OFFICE			2.5 / 1000 sq. ft.
RETAIL			3.0 / 1000 sq. ft.
CIVIC	To be determined by warrant		
OTHER	To be determined by warrant		

SHARING FACTOR		
Function	with	Function
RESIDENTIAL		RESIDENTIAL
LODGING		LODGING
OFFICE		OFFICE
RETAIL		RETAIL

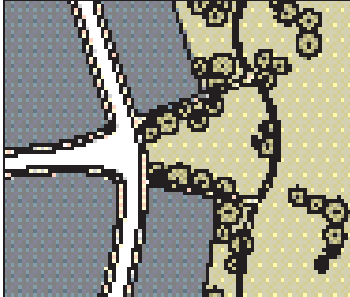

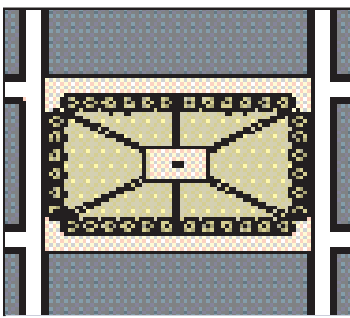
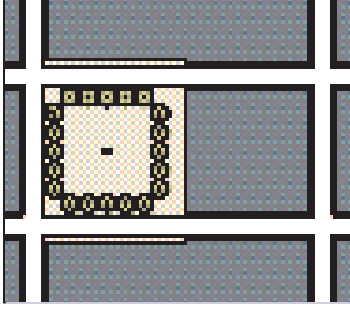
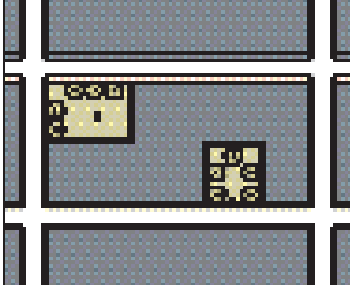
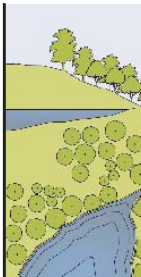





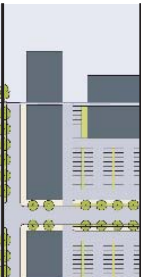
<p>a. Park: A natural preserve available for unstructured recreation. A park may be independent of surrounding building frontages. Its landscape shall consist of paths and trails, meadows, woodland and open shelters, all naturalistically disposed. Parks may be lineal, following the trajectories of natural corridors. The minimum size shall be 15 acres. Larger parks may be approved by warrant as districts in all zones.</p>	 <div>T1 T2 T3</div>
<p>b. Green: An open space, available for unstructured recreation. A green may be spatially defined by landscaping rather than building frontages. Its landscape shall consist of lawn and trees, naturalistically disposed. The minimum size shall be 2 acres and the maximum shall be 15 acres.</p>	 <div>T3 T4 T5</div>
<p>c. Square: An open space available for unstructured recreation and civic purposes. A square is spatially defined by building frontages. Its landscape shall consist of paths, lawns and trees, formally disposed. Squares shall be located at the intersection of important thoroughfares. The minimum size shall be .5 acre and the maximum shall be 5 acres.</p>	 <div>T4 T5</div>
<p>d. Plaza: An open space, available for civic purposes and commercial activities. A plaza shall be spatially defined by building frontages. Its landscape shall consist primarily of pavement. Trees are optional. Plazas shall be located at the intersection of important streets. The minimum size shall be 0.5 acre and the maximum shall be 2 acres.</p>	 <div>T5</div>
<p>e. Playground: An open space designed and equipped for the recreation of children. A playground shall be fenced and may include an open shelter. Playgrounds shall be interspersed within residential areas and may be placed within a block. Playgrounds may be included within parks and greens. There shall be no minimum or maximum size.</p>	 <div>T1 T2 T3 T4 T5</div>

TABLE 14 SMARTCODE SUMMARY

R U R A L							U R B A N		DISTRICTS
									
T1	T2	T3	T4	T5	T6	SD			
NATURAL ZONE	RURAL ZONE	SUB-URBAN ZONE	GENERAL URBAN ZONE	URBAN CENTER ZONE	URBAN CORE ZONE	SPECIAL DISTRICTS			
A. ALLOCATION OF ZONES (see Section 3.1 and Table 2)									
TND	no minimum	no minimum	40 - 60% (52.7%)	10 - 30 % (15.2%)	20 - 40% (32.1%)				
TOD	no minimum	no minimum	10 -25%	30 - 50%	30 - 50%				
B. BASE RESIDENTIAL DENSITY (see Section 3.4)									
By Right	1 unit /100 ac.	Recommend: 1 unit /10 ac. Permitted: 7 units/ac. gross	7 units / ac. gross	12 units / ac. gross	20 units / ac. gross				
C. BLOCK SIZE									
Block Perimeter	no maximum	no maximum	2,400 ft. max	2,200 ft. max	1,800 ft. max				
D. PUBLIC FRONTAGES (see Tables 3 and 4)									
HW & RR	permitted	permitted	prohibited	prohibited	prohibited				
BV	prohibited	prohibited	prohibited	prohibited	permitted				
SR	prohibited	permitted	permitted	prohibited	prohibited				
RS	prohibited	prohibited	permitted	permitted	permitted				
SS & AV	prohibited	prohibited	permitted	permitted	permitted				
CS & AV	prohibited	prohibited	permitted	permitted	permitted				
Rear Lane	permitted	permitted	permitted	permitted	prohibited				
Rear Alley	prohibited	prohibited	permitted	required	required				
Path	permitted	permitted	permitted	permitted	prohibited				
Passage	prohibited	prohibited	permitted	permitted	permitted				
Bicycle Trail	permitted	permitted	permitted	prohibited *	prohibited				
Bicycle Route	permitted	permitted	permitted	permitted	permitted				
Bicycle Lane	permitted	permitted	permitted	permitted	permitted		[* permitted within Open Spaces]		
E. CIVIC SPACES (see Table 13)									
Park	permitted	permitted	permitted	prohibited	prohibited				
Green	prohibited	prohibited	permitted	permitted	permitted				
Square	prohibited	prohibited	prohibited	permitted	permitted				
Plaza	prohibited	prohibited	prohibited	permitted	permitted				
Playground	permitted	permitted	permitted	permitted	permitted				
F. LOT OCCUPATION									
Lot Width	by Variance	by Warrant	50 ft. min; 200 ft. max.	24 ft. min; 96 ft. max.	18 ft. min; 180 ft. max.				
Lot Coverage	by Variance	by Warrant	60% max.	70% max.	80% max.				
G. BUILDING DISPOSITION									
Front Setback	by Variance	48 ft. min	12 ft. min; 24 ft. max.	6 ft. min; 18 ft. max.	6 ft. min; 12 ft. max.	[* min. at corner lots]			
Side Setback	by Variance	48 ft. min	3 ft. min; 6 ft. min.*	0 ft. min; 6 ft. min.*	0 ft. min; 24 ft. max.	[**max. at corner lots]			
Rear Setback	by Variance	48 ft. min	10 ft. min.; 10 ft. max.**	3 ft. min.***; 3 ft. max.**	3 ft. min.; 3 ft. max.**	[*** 15 ft. from center line of alley]			
H. BUILDING TYPE (see Table 9)									
Edgeyard	permitted	permitted	permitted	permitted	prohibited				
Sideyard	prohibited	prohibited	permitted	permitted	permitted				
Rearyard	prohibited	prohibited	permitted	permitted	permitted				
I. PRIVATE FRONTAGES (see Table 7)									
Common Yard	not applicable	permitted	permitted	prohibited	prohibited				
Porch & Fence	not applicable	prohibited	permitted	permitted	prohibited				
Terrace or L.C.	not applicable	prohibited	prohibited	permitted	permitted				
Forecourt	not applicable	prohibited	prohibited	permitted	permitted				
Stoop	not applicable	prohibited	prohibited	permitted	permitted				
Shopfront & Awning	not applicable	prohibited	prohibited	permitted	permitted				
Gallery	not applicable	prohibited	prohibited	permitted	permitted				
Arcade	not applicable	prohibited	prohibited	prohibited	permitted				
J. BUILDING HEIGHT (see Table 8)									
Principal Building	not applicable	2 stories max	2 stories max	3 stories max	4 stories max; 2 min				
Outbuilding	not applicable	2 stories max	2 stories max	2 stories max	2 stories max				
K. BUILDING FUNCTION (see Table 10 &11)									
Residential	prohibited	restricted use	open use	open use	open use				
Lodging	prohibited	restricted use	limited use	limited use	open use				
Office	prohibited	restricted use	restricted use	restricted use	open use				
Retail	prohibited	prohibited	prohibited	prohibited	open use				

DISPOSITION

CONFIGURATION

FUNCTION

ARTICLE 5

ARTICLE 2.3.4

Table 15: Special District Summary. Special Districts (SD) are areas that cannot comply with this Code. The metrics for each column of this table (SD1, SD2, etc.) are to be filled out with the details of each Special District as they currently exist, or as they are permitted.

More pages can be added.

Note: This table to be completed as each Special District is approved.

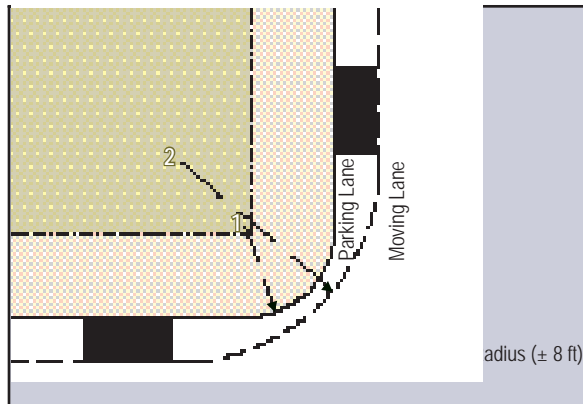
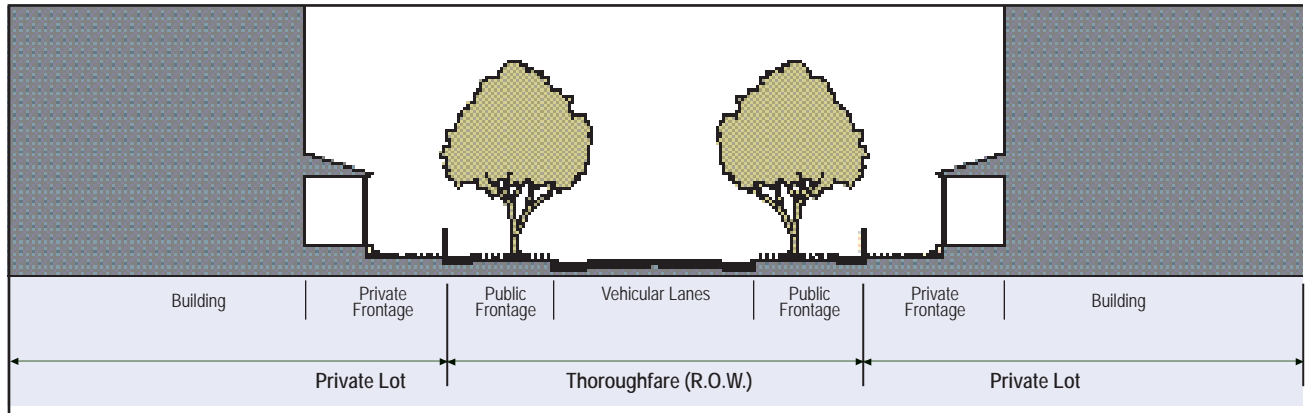
	DISTRICT SD1	DISTRICT SD2	DISTRICT SD3	DISTRICT SD4	DISTRICT SD5	DISTRICT SD6	DISTRICT SD7	
A. ALLOCATION OF ZONES								
a. CLD / Cluster								
b. TND / Village								
c. TOD / Town								
B. BASE DENSITY ALLOCATION								
a. HousingBy Right								
b. By TDR								
c. Other Functions								
C. BLOCK SIZE								
a. Block Perimeter								
D. PUBLIC FRONTAGES								
a. Rural Road								
b. Standard Road								
c. Residential Road								
d. Residential Street								
e. Standard Street								
f. Commercial Street								
g. Avenue								
h. Boulevard								
i. Rear Lane								
j. Rear Alley								
k. Path								
l. Passage								
m. Bicycle Trail								
n. Bicycle Lane								
o. Bicycle Route								
E. CIVIC SPACES								
a. Park								
b. Green								
c. Square								
d. Plaza								
e. Playground								
F. LOT OCCUPATION								
a. Lot Width								
b. Lot Coverage								
G. BUILDING SETBACK								
a. Front Setback								
b. Side Setback								
c. Rear Setback								
H. BUILDING TYPE								
a. Edgeyard								
b. Sideyard								
c. Rearyard								
I. PRIVATE FRONTAGES TYPE								
a. Common Yard								
b. Porch & Fence								
c. Terrace or L.C.								
d. Forecourt								
e. Stoop								
f. Shopfront								
g. Gallery								
h. Arcade								
i. Parking Lot								
J. BUILDING HEIGHT								
a. Principal Building								
b. Outbuilding								
K. BUILDING FUNCTION								
a. Residential								
b. Lodging								
c. Office								
d. Retail								

DISPOSITION

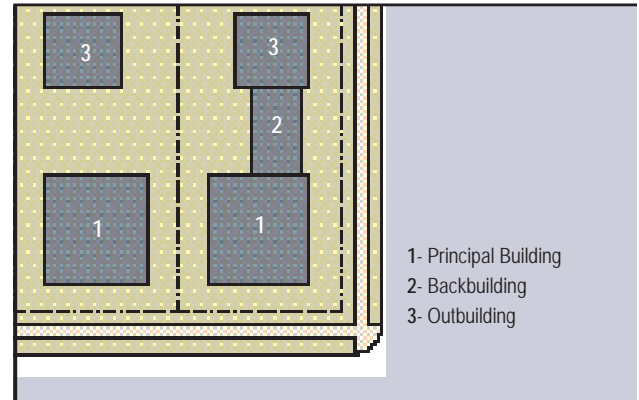
CONFIGURATION

FUNCTION

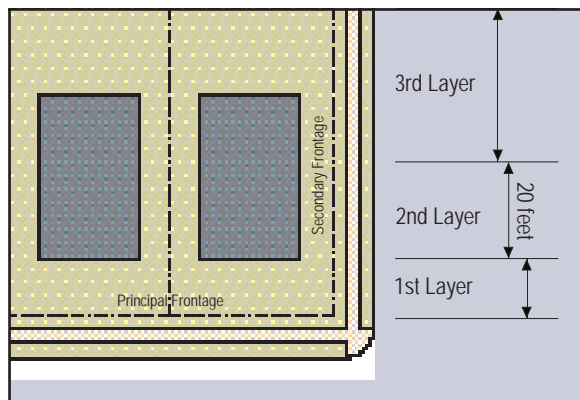
a. THOROUGHFARE & FRONTAGES



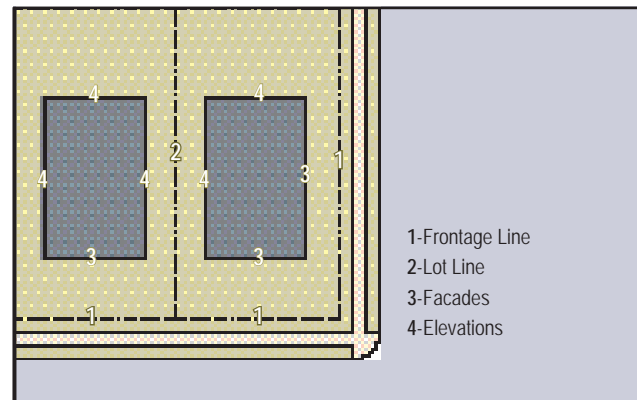
c. BUILDING DISPOSITION



d. LOT LAYERS



e. FRONTAGE & LOT LINES



This Article provides definitions for terms in this Code that are technical in nature or that otherwise may not reflect a common usage of the term. If a term is not defined in this Article, then the Planning Office shall determine the correct definition of the term.

DEFINITIONS

Accessory Building: an Outbuilding with an Accessory Unit.

Accessory Unit: an apartment not greater than 600 square feet sharing ownership and utility connections with a Principal Building. An Accessory Unit may or may not be within an Outbuilding. Accessory Units do count toward maximum density calculations, see Tables 10 and 16 (Syn: Ancillary Unit).

Administrative Decision: Any decision made by the City's Director of Community Development, or his or her designee.

Affordable Housing: dwellings consisting of rental units or for-sale units. Both shall be economically within the means of the equivalent of the starting salary of a local elementary school teacher.

Allee: a regularly spaced and aligned row of trees usually planted along a Thoroughfare or Pedestrian Path.

Apartment: a dwelling unit sharing a building and a lot with other dwellings and/or uses. Apartments may be for rent or for sale as condominiums.

Avenue (AV): a thoroughfare of high vehicular capacity and low speed. Avenues are short distance connectors between urban centers. Avenues may be equipped with a landscaped median. Avenues become collectors upon exiting urban areas.

Backbuilding: a single-story structure connecting a principal building to an outbuilding (see Table 16).

Bicycle Lane (BL): a dedicated bicycle lane running within a moderate-speed vehicular thoroughfare, demarcated by striping.

Bicycle Route (BR): a thoroughfare suitable for the shared use of bicycles and automobiles moving at low speeds.

Bicycle Trail (BT): a bicycle way running independently of a high-speed vehicular thoroughfare.

Block: the aggregate of private lots, passages, rear lanes and alleys, circumscribed by thoroughfares.

Block Face: the aggregate of all the building facades on one side of a block. The Block Face provides the context for establishing architectural harmony.

Boulevard (BV): a thoroughfare designed for high vehicular capacity and moderate speed. Boulevards are long-distance thoroughfares traversing urbanized areas. Boulevards are usually equipped with slip roads buffering sidewalks and buildings. Boulevards become arterials upon exiting urban areas.

Brownfield: an area previously used primarily as an industrial site.

Building Configuration: the form of a building, based on its massing, private frontage, and height.

Building Disposition: the placement of a building on its lot (see Tables 9 & 16).

Building Function: the uses accommodated by a building and its lot. Functions are categorized as Restricted, Limited, or Open, according to the intensity of the use (see Tables 10 & 11).

Building Height: the vertical extent of a building measured in stories, not including a raised basement or a habitable attic. Height limits do not apply to masts, belfries, clock towers, chimney flues, water tanks, elevator bulkheads and similar structures. Building Height shall be measured from the average grade of the enfronting thoroughfare (see Table 8).

Building Type: a structure category determined by function, disposition on the lot, and configuration, including frontage and height.

By Right Permit: a proposal for a building or community plan that complies with this code and may thereby be processed administratively, without public hearing (see **Deviations**).

CLD: Conservation Land Development, Clustered Land Development. An incomplete neighborhood, standing free in the countryside. Because of a location away from transportation, CLD has a weak commercial center. This is the only Community type permitted by Right in the Restricted Growth Sector. (Syn: Hamlet, Cluster)

Civic: the term defining not-for-profit organizations dedicated to arts, culture, education, recreation, government, transit, and municipal parking.

Civic Building: a building designed specifically for a civic function. Civic Buildings shall not be subject to the requirements of Article 5. The particulars of their design shall be determined by Variance.

Civic Parking Reserve: parking structure or lot within a quarter-mile of the site that it serves. Space may be leased or bought from this Reserve to satisfy parking requirements.

Civic Space: an outdoor area dedicated for public use. Civic Space types are defined by the combination of certain physical constants including the relationship between their intended use, their size, their landscaping and their enfronting buildings (see Table 13).

Commercial: the term collectively defining workplace, office and retail functions.

Common Destination: An area of focused community activity defining the approximate center of a Pedestrian Shed. It may include without limitation one or more of the following: a Civic Space, a Civic Building, a Commercial center, a bus stop. A Common Destination may act as the social center of a Neighborhood. (See Section 3.7 Civic Function).

Community Type: a category defining the physical form of a settlement. The three basic Community Types addressed in this Code are CLD, TND, and RCD/TOD. The choice of Community Type will depend upon the regional Sector, level of urban intensity desired, particulars of the site, transportation, and implementation.

Consolidated Review Committee (CRC): Usually part of the Planning Office, a CRC is comprised of a representative from each of the various regulatory agencies that have jurisdiction over the permitting of a project, and if possible a representative of the Bayard Community, as well as a representative of the UDC, or DDC, and the town architect. See Section 1.4.3.

Context: surroundings made up of the particular combination of elements that create specific habitat.

Corridor: a lineal geographic system incorporating transportation and/or greenway trajectories. A transportation corridor may be a lineal urban Transect Zone.

Cottage: an edgeyard building type. A single-family dwelling, on a regular lot, often shared with an ancillary building in the rearyard.

Courtyard Building: a building that occupies the boundaries of its lot while internally defining one or more private patios.

Curb: the edge of the vehicular pavement detailed as a raised curb or flush to a swale. The Curb usually incorporates the drainage system (see Table 4).

DDC: Design and Development Center. See **UDC**.

Density: the number of dwelling units within a standard measure of land area, usually given as units per acre (see Section 3.4).

Design Speed: is the velocity at which a thoroughfare tends to be driven without the constraints of signage or enforcement. There are three ranges of speed: Very Low: (below 20 MPH); Low: (20-25 MPH); Moderate: (25-35 MPH); High: (above 35 MPH). Lane width is determined by desired design speed.

Developable areas: residual to the Preserved Open Sector.

District: see **Special District**.

Driveway: a vehicular lane within a lot, usually leading to a garage. A Driveway in the First Layer may be used for parking if it is no more than 18 feet wide, thereby becoming subject to the constraints of a parking lot.

Edgeyard Building: a building that occupies the center of its lot with setbacks on all sides.

Elevation: an exterior wall of a building not along a Frontage Line. See: **Facade** (Table 16)

Enfront: to place an element along a frontage line, as in "porches enfront the street."

Entrance, Principal: the main point of access of pedestrians into a building.

Estate House (Syn.: Country house, Villa): an edgeward building type. A single-family dwelling on a very large lot of rural character, often shared by one or more ancillary buildings.

Existing Local Codes: The City of Jacksonville's Zoning Code, Chapter 656, Land Development Procedures Manual, and Subdivision Ordinances.

Facade: the exterior wall of a building that is set along a Frontage Line (see **Elevation**; **Frontage Line**).

Frontage Line: those lot lines that coincide with a public frontage. Facades along Frontage Lines define the public realm and are therefore more regulated than the elevations that coincide with other Lot Lines (see Table 16).

GIS (Geographic Information System): a computerized program in widespread municipal use that organizes data on maps. Various municipal departments can input information including the location of wetlands, thoroughfares, water/sewer lines, boundaries, building footprints, schools, zoning, land-use, etc. GIS makes information available as layered databases. The protocol for preparing a Sector Plan should be based on GIS information (Section 2.1).

Grandfathered Property: Those properties and their uses owned prior to the adoption of the Code, or that were inherited from immediate family members after the adoption of the Code. Owners of these properties are permitted to maintain the existing uses and activities as per the Existing Local Codes until ownership is transferred to a non-immediate family member. Uses of a structure, land or water, or of a structure and/or land and water in combination that are not in conformance with the City of Jacksonville's adopted 2010 Comprehensive Plan shall not be extended or enlarged.

Greenfield: a project planned for an undeveloped area outside the existing urban fabric. See **Infill**.

Greenway: an open space corridor in largely natural conditions which may include Trails for bicycles and pedestrians.

Greyfield: an area previously used primarily as a parking lot. Shopping centers and shopping malls are typical Greyfield sites.

Growth Sector: one of the three Sectors for New Communities or the Infill Sector, where development is permitted by right.

Hamlet: See **CLD**.

Home Occupation: non-retail commercial enterprises permitted in Zones T3-6. The work quarters should be invisible from the frontage, located either within the house or in an outbuilding. Permitted activities are defined by the Restricted Office category (Table 11).

House (Syn.: **Single**): an edgeyard building type. A single-family dwelling on a large lot, often shared with an ancillary building in the rearyard.

Independent Building: a building designed by a different architect from the adjacent buildings.

Infill: a project within existing urban fabric.

Inside Turning Radius: the curved edge of a thoroughfare at an intersection, measured at the inside edge of the vehicular tracking. The smaller the Turning Radius, the smaller the pedestrian crossing distance and the more slowly the vehicle is forced to make the turn. (See Tables 3 and 16)

Long Pedestrian Shed: A Pedestrian Shed of 1/2 mile radius used for mapping community types when a transit stop (bus or rail) is present or proposed as the Common Destination. People have been shown to walk ten minutes to transit. See **Pedestrian Shed**.

Layer: a range of depth of a lot within which certain elements are permitted (see Table 16).

Linear Pedestrian Shed: A Pedestrian Shed that is elongated along an important Commercial corridor such as a main street. The resulting shed is shaped like a lozenge. (Sometimes called an Elongated Pedestrian Shed.)

Liner Building: a building specifically designed to mask a parking lot or a parking garage from a frontage. A Liner Building, if less than 30 feet deep and two stories, shall be exempt from parking requirements.

Live-Work: a fee-simple dwelling unit that contains a Commercial component anywhere in the unit. (Syn.: **Flexhouse**.) (See **Work-Live**.)

Lodging: premises available for daily and weekly renting of bedrooms. The area allocated for food service shall be calculated and provided with parking according to retail use.

Lot Line: the boundary that legally and geometrically demarcates a lot (see **Frontage Line**). Such lines appear graphically on Community and Site Plans. Codes reference lot lines as the baseline for measuring setbacks (see Tables 16 and 14G).

Lot Width: the length of the principal Frontage Line of a lot.

Manufacturing: premises available for the creation, assemblage and/or repair of artifacts, using table-mounted electrical machinery and including their retail sale.

Meeting Hall: a building available for gatherings, including conferences. It should accommodate at least one room equivalent to a minimum of 10 square feet per projected dwelling unit within the pedestrian shed in which the meeting hall is located. A Meeting Hall shall be completed upon the sale of 75% of the dwelling units. The Meeting Hall may be used for the marketing purposes of the development until the sale of 75% of the dwelling units, at which time control of its use shall be given to the [Community Council].

Mixed Use: multiple functions within the same building through superimposition or adjacency, or in multiple buildings within the same area by adjacency. Mixed use is one of the principles of TND development from which many of its benefits are derived, including compactness, pedestrian activity, and parking space reduction.

Neighborhood: an urbanized area at least 40 acres that is primarily Residential. A Neighborhood shall be based upon a partial or entire Standard Pedestrian Shed. The physical center of the Neighborhood should be located at an important traffic intersection associated with a Civic or Commercial institution.

Net Developable Area, Net Site Area: the developable areas of a site. The Net Site Area shall be allocated to the various Transect Zones according to the parameters in Table 14A.

Office: premises available for the transaction of general business but excluding retail, artisanal and manufacturing uses.

Open Sector: One of the two Sectors where development is not permitted.

Outbuilding: an accessory building, usually located towards the rear of the same lot as a Principal Building. It is sometimes connected to the principal building by a Backbuilding. Outbuildings shall not exceed 600 square feet of habitable space, excluding parking areas (see Table 16).

Parking Structure: a building containing two or more stories of parking. Parking Structures shall have Liner Buildings at the first story or higher.

Passage (PS): a pedestrian connector passing between buildings, providing short-cuts through long blocks and connecting rear parking areas to frontages. Passages may be roofed over.

Path (PT): a pedestrian way traversing a park or rural area, with landscape matching the contiguous open space. Paths should connect directly with the urban sidewalk network.

Pedestrian Shed: An area, approximately circular, that is centered on a Common Destination. A Pedestrian Shed is applied to determine the approximate size of a Neighborhood. A Standard Pedestrian Shed is 1/4 mile radius or 1320 feet, about the distance of a five-minute walk at a leisurely pace. It has been shown that provided with a pedestrian environment, most people will walk this distance rather than drive. The outline of the shed must be refined according to actual site conditions, particularly along Thoroughfares. The Common Destination should have the present or future capacity to accommodate a T5 Transect Zone for TND and a T6 Zone for RCD. A Long Pedestrian Shed is 1/2 mile radius or 2640 feet, and may be used for mapping when transit is present or proposed. (Sometimes called a "walkshed" or "walkable catchment.") A Linear Pedestrian Shed is elongated to follow a Commercial corridor. See **Standard, Long, or Linear Pedestrian Shed**.

Planter: the element of the public streetscape which accommodates street trees. Planters may be continuous or individual.

Primary-Secondary Grid: thoroughfare designations appearing on the Regulating Plan. Buildings on the P-Grid are subject to all of the provisions of this Code. Buildings on the S-Grid are exempt from certain provisions, allowing for Warranted open parking lots, unlined parking decks, drive-throughs and hermetic building fronts.

Principal Building: the main building on a lot, usually located toward the frontage (see Table 16).

Private Frontage: the privately held layer between the frontage line and the principal building facade. The structures and landscaping within the Private Frontage may be held to specific standards. The variables of Private Frontage are the depth of the setback and the combination of architectural elements such as fences, stoops, porches and galleries (see Table 7).

Public Frontage: the area between the curb of the vehicular lanes and the Frontage Line. Elements of the Public Frontage include the type of curb, walk, planter, street tree and streetlight (see Table 4).

Public Realm: Those parts of the urban fabric that are held in common such as plazas, squares, parks, Thoroughfares and Civic Buildings. There is an ethical and Civic connotation to the term that is beyond the physical and the utilitarian.

Quality of Life/Standard of Living: Two conventional measures of human well-being. Standard of Living is a quantitative measure, while Quality of Life is qualitative. Standard of Living measures such benchmarks as family income, cars owned, miles of highway travel, dwelling size, and number of bathrooms, and appliances. Quality of Life measures availability of leisure time and discretionary income, both requisites of personal choice. A commitment to conventional suburban development necessitates a high standard of living but virtually precludes a high quality of life, as discretionary time is consumed by the inevitable driving about and discretionary income is committed to automobile ownership costs.

Rear Alley (AL): a vehicular driveway located to the rear of lots providing access to service areas and parking, and containing utility easements. Alleys should be paved from building face to building face, with drainage by inverted crown at the center or with roll curbs at the edges.

Rear Lane (LA): a vehicular driveway located to the rear of lots providing access to parking and outbuildings and containing utility easements. Rear lanes may be paved lightly to driveway standards. Its streetscape consists of gravel or landscaped edges, no raised curb and is drained by percolation.

Rearyard Building: a building that occupies the full frontage line, leaving the rear of the lot as the sole yard. This is a more urban type, as the continuous facade spatially defines the public thoroughfare. For its residential function, this type yields a rowhouse. For its commercial function, the rear yard can accommodate substantial parking.

RCD or Regional Center Development : a Community Type based upon a partial or entire Long Pedestrian Shed, oriented toward a strong Town Center. With transit existing or proposed, it is called TOD (Transit-Oriented Development). The minimum developable area of an RCD/TOD is 160 acres. This Community Type is permitted by right within the G-3 Intended Growth Sector (see Section 3.3.3) and the G-4 Infill

Growth Sector (see Section 4.3.2) RCD/TOD may be adjoined without buffers by one or several Standard Pedestrian Sheds that meet the individual Transect Zone requirements of an RCD as specified in Tables 2 and 14A.

Residential: premises available for long-term human dwelling.

Retail: premises available for the sale of merchandise and food service.

Retail Frontage Line: Frontage Lines designated on a Community Plan that require the provision of a Shopfront, causing the ground level to be available for retail use.

Road (RD): a local, rural and suburban thoroughfare of low vehicular speed and capacity. Its public frontage consists of swales drained by percolation and a walking path or bicycle trail along one or both sides. The landscaping consists of multiple species composed in naturalistic clusters. This type is allocated to the more rural Transect Zones (T1-T3).

Rowhouse: a single-family dwelling that shares a party wall with another of the same type and occupies the full frontage line (Syn: Townhouse; see **Rearyard Building**).

Rural Boundary Line: the extent of potential urban growth as determined by existing geographical determinants. The rural boundary is permanent.

Secondary Grid: see **Primary-Secondary Grid**.

Sector: a neutral term for a geographic area. In the SmartCode there are six specific Sectors that establish the legal boundaries for several kinds of development. Two Sectors represent unbuildable open space (Preserve and Reserve) and the other four are Urban Growth Sectors of varying intensity (Restricted, Controlled, Intended and Infill Growth Sectors). Sectors address the legal status of place at the regional scale while Transect Zones address the physical character of communities. Sectors contain Community Types (CLD, TND, RCD, TOD), which contain Transect Zones, which contain design standards appropriate to those T-Zones.

Service Boundary Line: the extent of potential or feasible urban growth as determined by the extension of infrastructure, principally sewer.

Setback: the area of a lot measured from the lot line to a building facade or elevation. This area must be maintained clear of permanent structures with the exception of: galleries, fences, garden walls, arcades, porches, stoops, balconies, bay windows, terraces and decks (that align with the first story level) which are permitted to encroach into the Setback (see Section 5.2.1 and Table 14G).

Shared Parking Policy: an accounting for parking spaces that are available to more than one function. The requirement is reduced by a factor, shown as a calculation. The Shared Parking ratio varies according to multiple functions in close proximity which are unlikely to require the spaces at the same time (see Tables 11 and 12).

Sideyard Building: a building that occupies one side of the lot with a setback to the other side.

Sidewalk: the paved layer of the public frontage dedicated exclusively to pedestrian activity.

Specialized Building: a building that is not subject to Residential, Commercial, or Lodging classification. Most specialized buildings are dedicated to manufacturing and transportation, and are distorted by the trajectories of machinery.

Special District (SD): Special District designations shall be assigned to areas that, by their intrinsic function, disposition, or configuration, cannot conform to one of the six normative Transect Zones or four Community Types specified by this Code. Typical Districts may include large parks, institutional campuses, refinery sites, airports, etc.

Standard Pedestrian Shed: An area, approximately circular, that is centered on a Common Destination. A Pedestrian Shed is applied to determine the approximate size of a Neighborhood. A Standard Pedestrian Shed is 1/4 mile radius or 1320 feet, about the distance of a five-minute walk at a leisurely pace. It has been shown that provided with a pedestrian environment, most people will walk this distance rather than drive. The outline of the shed must be refined according to actual site conditions, particularly along thoroughfares. (Sometimes called a “walkshed” or “walkable catchment.”) See **Pedestrian Shed**.

Story: a habitable level within a building of no more than 14 feet in height from finished floor to finished ceiling. Attics and raised basements are not considered stories for the purposes of determining building height.

Streamside Corridor: the zone within which a waterway flows, its width to be variably interpreted according to the Transect Zone.

Street (ST): a local urban thoroughfare of low speed and capacity. Its public frontage consists of raised curbs drained by inlets and sidewalks separated from the vehicular lanes by a planter and parking on both sides. The landscaping consists of regularly placed street trees. This type is permitted within the more urban Transect Zones (T4-T6).

Streetscape: the urban element that establishes the major part of the public realm. The streetscape is composed of thoroughfares (travel lanes for vehicles and bicycles, parking lanes for cars, and sidewalks or paths for pedestrians) as well as the visible private frontages (building facades and elevations, porches, yards, fences, awnings, etc.), and the amenities of the public frontages (street trees and plantings, benches, streetlights, etc.).

Streetscreen: sometimes called Streetwall. A freestanding wall built along the frontage line, or coplanar with the facade, often for the purpose of masking a parking lot from the thoroughfare. Streetscreens [should] be between 3.5 and 8 feet in height and constructed of a material matching the adjacent building facade. The streetscreen may be a hedge or fence by Warrant. Streetscreens shall have openings no larger than is necessary to allow automobile and pedestrian access. In addition, all streetscreens over [4 feet] high should be [30%] permeable or articulated to avoid blank walls.

Substantial Modification: alterations to a building that are valued at more than 50% of the replacement cost of the entire building, if new.

TDR - Transfer of Development Rights: a method of relocating existing zoning rights from areas to be preserved as open space to areas to be more densely urbanized.

TDR Receiving Area: an area intended for development that may be made more dense by the purchase of development rights from TDR Sending Areas.

TDR Sending Area: an area previously zoned for development within the designated Reserved Open Sector (O2). The development rights assigned to this land may be purchased for TDR Receiving Areas. The sending areas, voided of their development rights, are re-allocated to the Preserved Open Sector (O1).

Terminated Vista: a location at the axial conclusion of a thoroughfare. A building located at a Terminated Vista designated on a Community Plan is required to be designed in response to the axis.

Third Place: a private building that includes a space conducive to unstructured social gathering. Third Places are usually bars, cafés, and corner stores.

Thoroughfare: a vehicular way incorporating moving lanes and parking lanes within a right-of-way (see Tables 3 and 16).

Tier: synonym for Sector.

TND or Traditional Neighborhood Development: a Community Type based upon a Standard Pedestrian Shed oriented toward a Common Destination consisting of a mixed-use center or corridor, and having a minimum developable area of 80 acres. This Community Type is permitted by right within the G-2 Controlled Growth Sector, the G-3 Intended Growth Sector (see Section 3.3.2) and the G-4 Infill Growth Sector (see Section 4.3.2). A TND may be comprised of a partial or entire Standard Pedestrian Shed or more than one Standard Pedestrian Shed. (Syn.: Village, Urban Village).

TOD: Transit-Oriented Development. TOD is Regional Center Development (RCD) with transit available or proposed. This Community Type is permitted by right within the G-3 Intended Growth Sector and G-4 Infill Growth Sector.

Town: RCD. A Community Type consisting of at least one Long Pedestrian Shed with a strong mixed-use center, or more than one TND sharing a center.

Town Center: the mixed-use center or main Commercial corridor of a community. A Town Center in a hamlet or small TND may consist of little more than a meeting hall, corner store, and main civic space. A Town Center for RCD or TOD communities may be a substantial downtown Commercial area, often connected to other Town Centers by transit.

Townhouse: Syn. **Rowhouse.** (See **Rearyard Building.**)

Transect: a system of ordering human habitats in a range from the most natural to the most urban. The SmartCode is based upon six Transect Zones which describe the physical character of place at any scale, according to the density and intensity of land use and urbanism.

Transect Zone (T-Zone): Transect Zones are administratively similar to the land-use zones in conventional codes, except that in addition to the usual building use, density, height, and setback requirements, other elements of the intended habitat are integrated, including those of the private lot and building and the enfronting public streetscape. The elements are determined by their location on the Transect scale. The T-Zones are: T1 Natural, T2 Rural, T3 Sub-Urban, T4 General Urban, T5 Urban Center, and T6 Urban Core (see Table 1).

Transition Line: a horizontal line spanning the full width of a facade, expressed by a material change or by a continuous horizontal articulation such as a cornice or a balcony.

Type: a category determined by function, disposition, and configuration, including size or extent. There are community types, street types, civic space types, etc. (See also: **Building Type**.)

UDC (Urban Design Center): A component of the Planning Office assigned to advise on the use of this Code and to aid in the design of the communities and buildings based on it.

Urban Village: A TND Community Type within an urbanized area. See: **TND**.

Variance: a ruling that would permit a practice that is not consistent with either a provision or the Intent of this Code (Section 1.2). Variances are usually granted by the City of Jacksonville City Council and Planning Commission in a public hearing. See Section 1.5.

Village: A Village is usually a TND Community Type standing isolated in the countryside, but with a stronger center than a hamlet due to its proximity to a transportation corridor. See: **TND**.

Warrant: a ruling that would permit a practice that is not consistent with a specific provision of this Code, but is justified by its Intent (Section 1.2). Warrants are usually granted administratively by the CRC. See Section 1.5.

Work-Live: a fee-simple mixed-use unit with a substantial Commercial component that may accommodate employees and walk-in trade. Therefore the unit shall require ADA compliance for accessibility. (Syn.: Live-With.) (See **Live-Work**.)

SMARTCODE

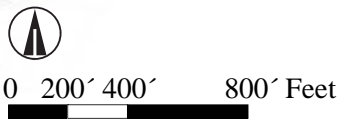
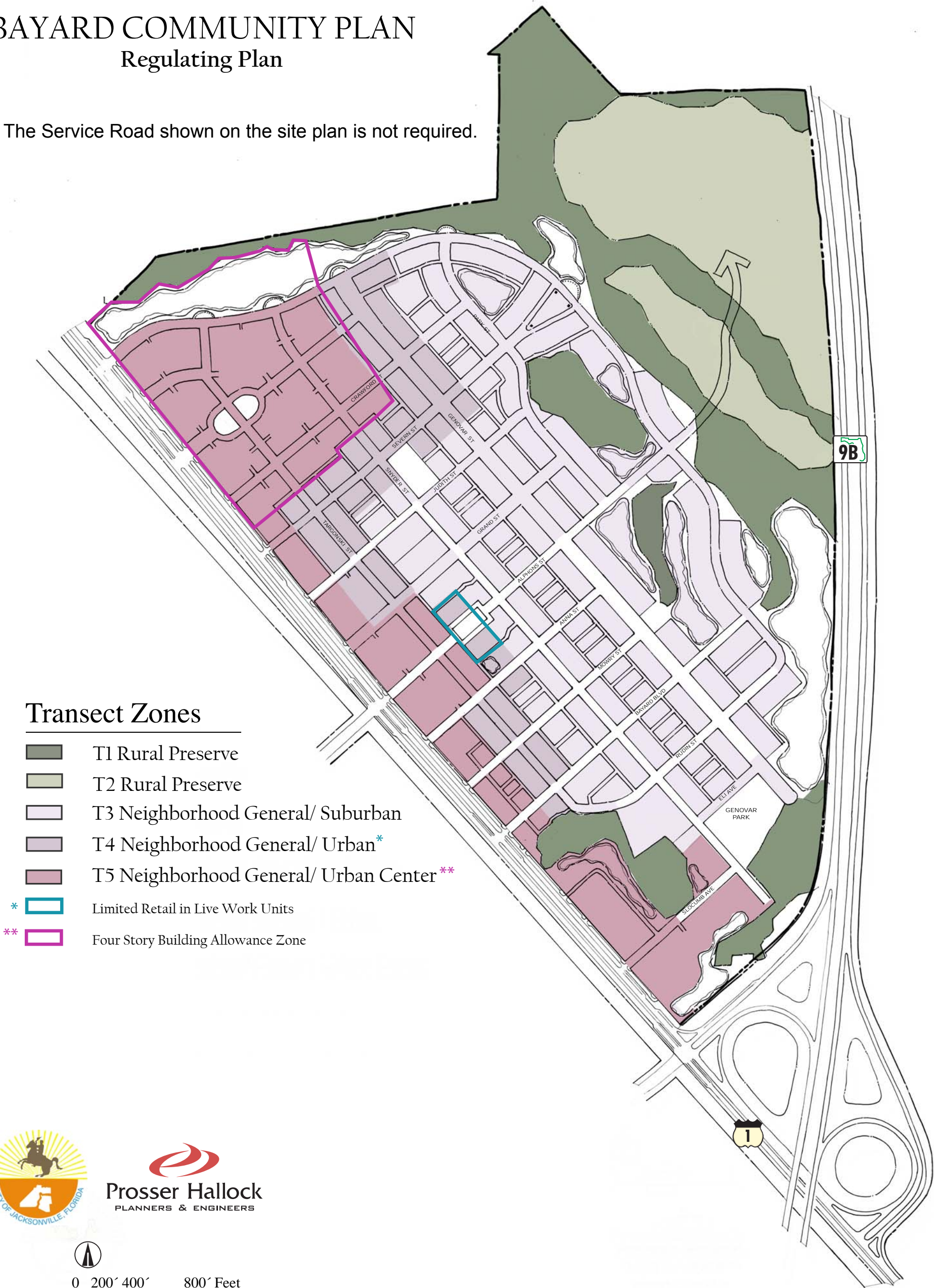
Community of Bayard, Jacksonville, Florida
June 1, 2007 Draft Code

MAPS

BAYARD COMMUNITY PLAN

Regulating Plan

The Service Road shown on the site plan is not required.

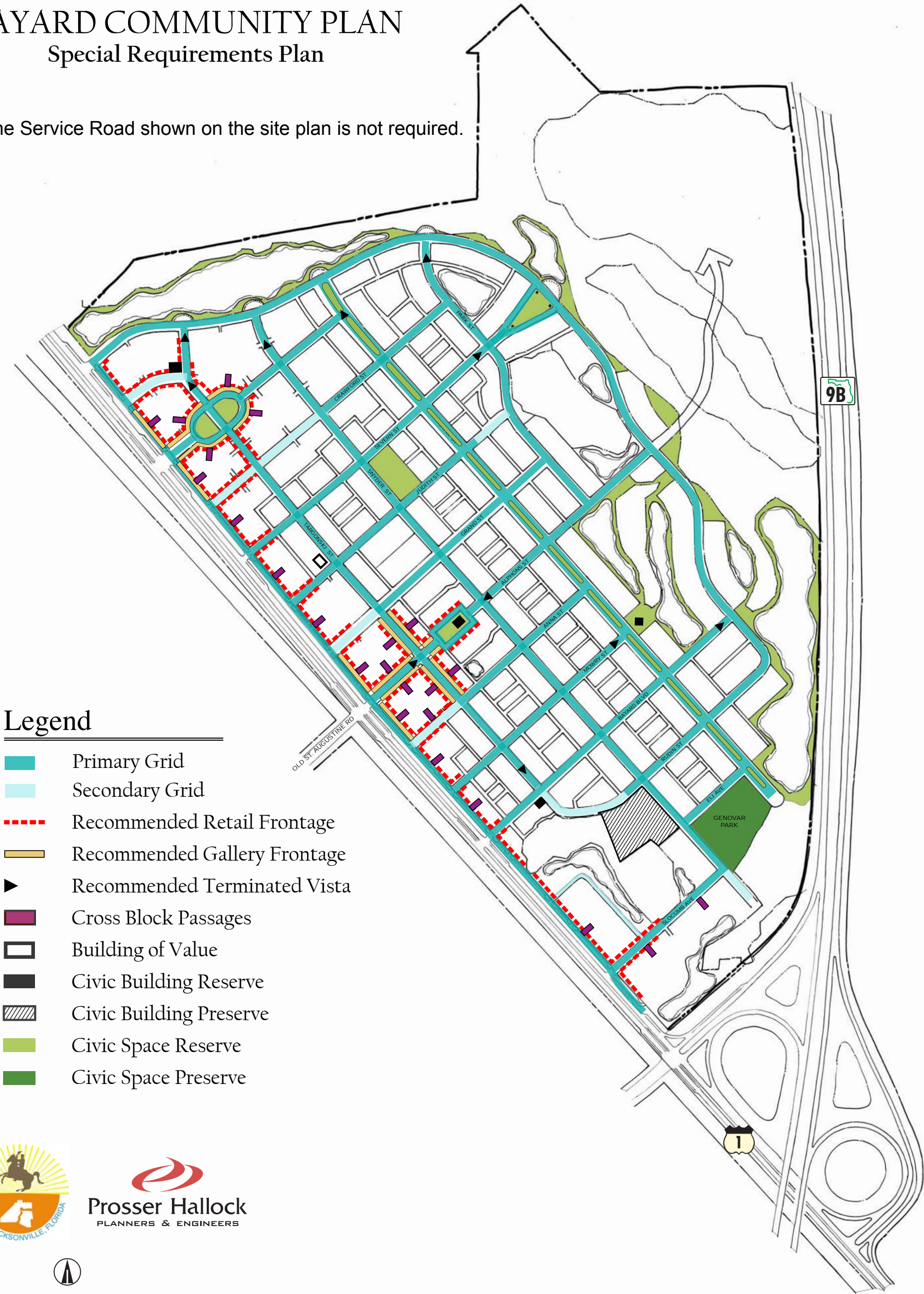


April 23, 2007
Project No. 104068.01

BAYARD COMMUNITY PLAN

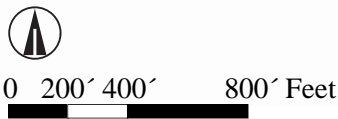
Special Requirements Plan

The Service Road shown on the site plan is not required.



Legend

- Primary Grid
- Secondary Grid
- Recommended Retail Frontage
- Recommended Gallery Frontage
- Recommended Terminated Vista
- Cross Block Passages
- Building of Value
- Civic Building Reserve
- Civic Building Preserve
- Civic Space Reserve
- Civic Space Preserve

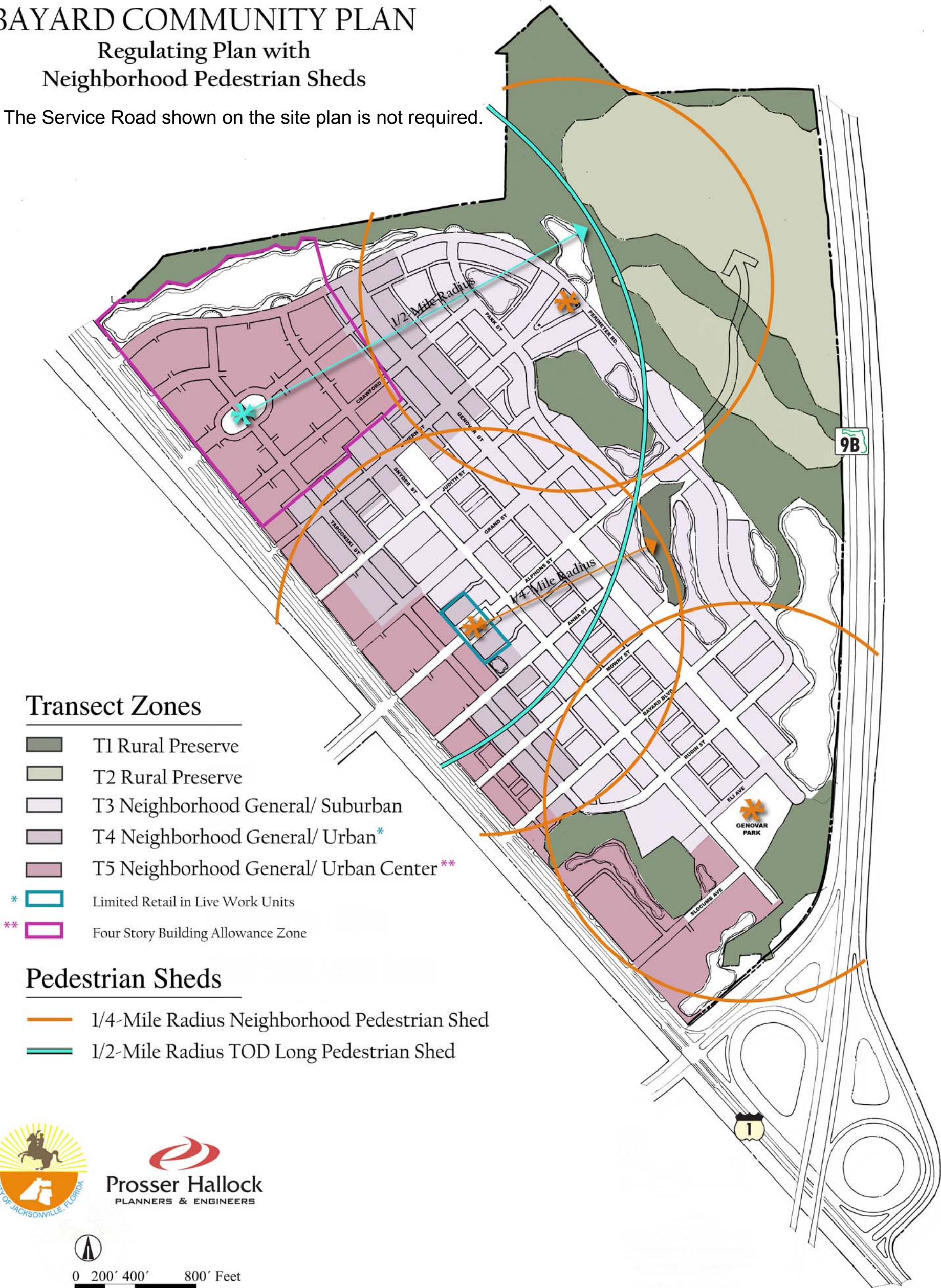


April 23 , 2007
Project No. 104068.01

BAYARD COMMUNITY PLAN

Regulating Plan with Neighborhood Pedestrian Sheds

The Service Road shown on the site plan is not required.



Prosser Hallock
PLANNERS & ENGINEERS



0 200' 400' 800' Feet

April 23, 2007
Project No. 104068.01

BAYARD COMMUNITY PLAN

Illustrative Plan

The Service Road shown on the site plan is not required.



Prosser Hallock
PLANNERS & ENGINEERS



0 200' 400' 800' Feet

April 23, 2007
Project No. 104068.01

BAYARD COMMUNITY PLAN

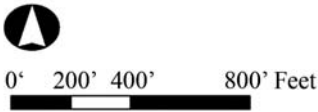
Thoroughfare Types

The Service Road shown on the site plan is not required.

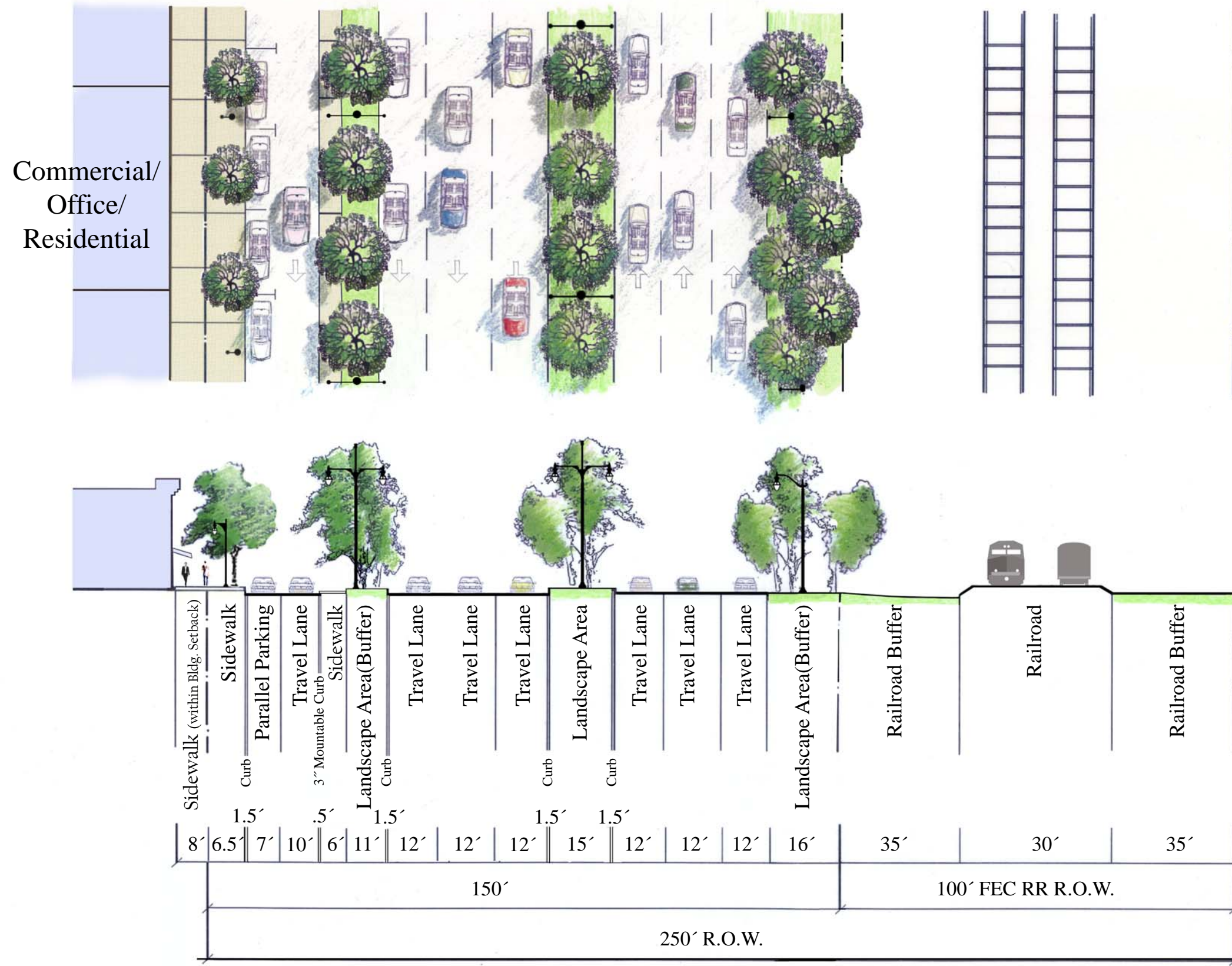


Thoroughfare Legend

Symbol	Type	ROW	Pavement Width
BV-250-111	Boulevard	250 Ft.	103 Ft.
CAV-100-38	Commercial Avenue	100 Ft.	38 Ft.
AV-100-38	Avenue	100 Ft.	38 Ft.
CS-80-64	Commercial Street	80 Ft.	64 Ft.
CS-80-38	Commercial Street	80 Ft.	38 Ft.
CS-60-34	Commercial Street	60 Ft.	34 Ft.
CS-55-29	Commercial Street	55 Ft.	29 Ft.
CS-50-19	Commercial Street	50 Ft.	19 Ft.
CS-40-19	Commercial Street	40 Ft.	19 Ft.
ST-100-34	Street	100 Ft.	34 Ft.
ST-60-34	Street	60 Ft.	34 Ft.
RD-60-24	Road	60 Ft.	24 Ft.
RD-50-20	Road	50 Ft.	20 Ft.
RA-24-24	Rear Alley	24 Ft.	24 Ft.
RL-24-12	Rear Lane	24 Ft.	12 Ft.



APPENDIX



BAYARD COMMUNITY PLAN

US 1 250' R.O.W. Section and Plan View

April 23, 2007



BAYARD COMMUNITY PLAN

US-1 Boulevard Design

Aerial Perspective at US-1/Alphons St. (South View)

Illustration 1

December 22, 2006



Project No. 104068.01



BAYARD COMMUNITY PLAN

US-1 Boulevard Design

Aerial Perspective at US-1/Alphons St (North View)

Illustration 2

December 22, 2006



Project No. 104068.01



BAYARD COMMUNITY PLAN

US-1 Boulevard Design

Ground Perspective at US-1/Alphons St.

Illustration 3

December 22, 2006



Project No. 104068.01



BAYARD COMMUNITY PLAN

US-1 Boulevard Design

Ground Perspective at US-1/Alphons St (South View).

Illustration 4

December 22, 2006



Project No. 104068.01